DEPARTMENT OF CITY PLANNING

RECOMMENDATION REPORT

City Planning Commission

Date: Thursday, June 27, 2023

Time: After 8:30 a.m.*

Place: Van Nuys City Hall, Council Chamber, 2nd floor.

This meeting may be available virtually, in hybrid format. Please check the meeting agenda (available at the link below) approximately 72 hours before the meeting for additional information or contact cpc@lacity.org.

https://planning.lacity.org/about/commissions-boards-hearings

Public Hearing: April 18, 2023

Appeal Status: Density Bonus Off-menu incentives and waivers are not further appealable. Density Bonus On-menu incentive is appealable to City Council by the applicant or abutting owners/occupants per LAMC Section 12.22-A,25(g)(2)(i)(f). Conditional Use is appealable to City Council.

Expiration Date: June 27, 2023

Multiple Approval: Yes

Case No.:	CPC-2023-0582-CU-DB-HCA-PHP
CEQA No.:	ENV-2023-0583-CE
Incidental Cases:	N/A
Related Cases:	N/A
Council No.:	5 - Katy Yaroslavsky
Plan Area:	Palms-Mar Vista- Del Rey
Specific Plan:	N/A
Certified NC:	Palms
Zone:	R3-1, RD2-1
Applicant:	Kelton Avenue Investments, LLC
Representative:	Jesi Harris and Brian Silveira, Brian Silveira & Associates

PROJECT LOCATION: **3676-3704 South Kelton Avenue; 10845 West Regent Street**

PROPOSED PROJECT: The proposed project includes the demolition of two existing duplexes for the construction, use, and maintenance of a new Density Bonus project with 43 dwelling units with 11 of the total units reserved for Very Low Income households. The proposed building is 62 feet in height and has 42,222 square feet of floor area. A total of 2,950 square feet of open space is proposed. The building will observe a 15-foot front yard, a 15-foot rear yard, and side yards of 7-feet and 2-inches. A total of 24 parking spaces are proposed (22 in a subterranean garage and two (2) within a ground floor garage).

REQUESTED ACTIONS:

1) Pursuant to CEQA Guidelines, Section 15332, Class 32 an Exemption from CEQA and that there is no substantial evidence demonstrating that an exception to a categorical exemption pursuant to CEQA Guidelines, Section 15300.2 applies;

2) Pursuant to LAMC Section 12.24-U,26, a Conditional Use Permit to allow a Density Bonus for a housing development project in which the density increase is greater than otherwise permitted by LAMC Section 12.22-A,25.

3) Pursuant to LAMC Section 12.22-A,25(g) a Density Bonus for a housing development project with 43 dwelling units, including 11 set aside for Very Low Income Households, with no required vehicular parking pursuant to AB 2097, and requesting the following:

   a. An On-Menu Incentive to permit a 23% increase in allowable floor area to allow 39,011 square feet of floor area (FAR 3.7:1) in lieu of the 31,590 square feet (FAR 3:1) permitted pursuant to LAMC 12.21.1-A,1;

   b. An On-Menu Incentive to permit averaging of density over the project site located on three continuous parcels zoned RD2-1 and R3-1;

   c. An Off-Menu Incentive to permit 17-foot increase in height to a maximum of 62 feet with no step-back after 45 feet of height in lieu of the maximum 45 feet allowed in the RD2-1 and R3-1 zones pursuant to LAMC 12.21.1;

   d. A Waiver of Development Standards to permit a 10% decrease in required northerly side yard setback to allow a 7-foot 2-inch side yard in lieu of the 8 feet required by the RD2 Zone pursuant to LAMC Section 12.09.1-B,2(a); and

   e. A Waiver of Development Standards to permit a 10% decrease in required southerly side yard setback to allow a 7-foot 2-inch side yard in lieu of the 8 feet required by the R3 Zone pursuant to LAMC Section LAMC 12.10-C,3.

RECOMMENDED ACTIONS:

1) **Find,** Pursuant to CEQA Guidelines, Section 15332, Class 32 an Exemption from CEQA and that there is no substantial evidence demonstrating that an exception to a categorical exemption pursuant to CEQA Guidelines, Section 15300.2 applies;

2) **Approve** a Conditional Use Permit to allow a Density Bonus for a housing development project in which the density increase is greater than otherwise permitted by LAMC Section 12.22-A,25;

3) **Approve** a Density Bonus Compliance Review to permit a housing development project consisting of 43 dwelling units, of which 11 will be set aside for Very Low Income households and with the following Incentives and Waivers of Development Standards:

   a. An On-Menu Incentive to permit a 23% increase in allowable floor area to allow 39,011 square feet of floor area (FAR 3.7:1) in lieu of the 31,590 square feet (FAR 3:1) permitted pursuant to LAMC 12.21.1-A,1;

   b. An On-Menu Incentive to permit averaging of density over the project site located on three continuous parcels zoned RD2-1 and R3-1;

c. An Off-Menu Incentive to permit 17-foot increase in height to a maximum of 62 feet with no step-back after 45 feet of height in lieu of the maximum 45 feet allowed in the RD2-1 and R3-1 zones pursuant to LAMC 12.21.1;

d. A Waiver of Development Standards to permit a 10% decrease in required northerly side yard setback to allow a 7-foot 2-inch side yard in lieu of the 8 feet required by the RD2 Zone pursuant to LAMC Section 12.09.1-B,2(a);

e. A Waiver of Development Standards to permit a 10% decrease in required southerly side yard setback to allow a 7-foot 2-inch side yard in lieu of the 8 feet required by the R3 Zone pursuant to LAMC Section 12.10-C,3; and

4) **Adopt** the attached Conditions of Approval;

5) **Adopt** the attached Findings;

VINCENT P. BERTONI, AICP
Director of Planning

Heather Bleemers
Senior City Planner

Renata Ooms
City Planner

ADVICE TO PUBLIC: *The exact time this report will be considered during the meeting is uncertain since there may be several other items on the agenda. Written communications may be mailed to the *Commission Secretariat, Room 272, City Hall, 200 North Spring Street, Los Angeles, CA 90012* (Phone No. 213-978-1300). While all written communications are given to the Commission for consideration, the initial packets are sent to the week prior to the Commission's meeting date. If you challenge these agenda items in court, you may be limited to raising only those issues you or someone else raised at the public hearing agendized herein, or in written correspondence on these matters delivered to this agency at or prior to the public hearing. As a covered entity under Title II of the Americans with Disabilities Act, the City of Los Angeles does not discriminate on the basis of disability, and upon request, will provide reasonable accommodation to ensure equal access to these programs, services and activities. Sign language interpreters, assistive listening devices, or other auxiliary aids and/or other services may be provided upon request. To ensure availability of services, please make your request not later than three working days (72 hours) prior to the meeting by calling the Commission Secretariat at (213) 978-1299.

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PROJECT ANALYSIS

PROJECT SUMMARY

The project involves the demolition of two existing duplexes (four units total) for the construction, use and maintenance of a new five-story, 43-unit apartment building with 11 of the units reserved for Very Low Income households (78 percent of the base density). The project will provide 33 automobile parking spaces within one subterranean parking level and one at-grade parking level.

The proposed building (see **Figure 1** and **Figure 2**) totals 39,011 square feet of floor area, for a maximum floor area ratio (FAR) of 3.7:1 and reaches a maximum height of 62 feet. The ground floor includes four apartment units with walk-up entrances from the sidewalk and landscaped patios within the front yard area (see **Figure 3**). The ground floor also includes a lobby entrance and a walkway to access the rear yard open space area. The ground floor parking area is located at the rear of the building. The unit-mix includes 14 two-bedroom units, 11 one-bedroom units and 18 studios.

Figure 1. Rendering of proposed project.

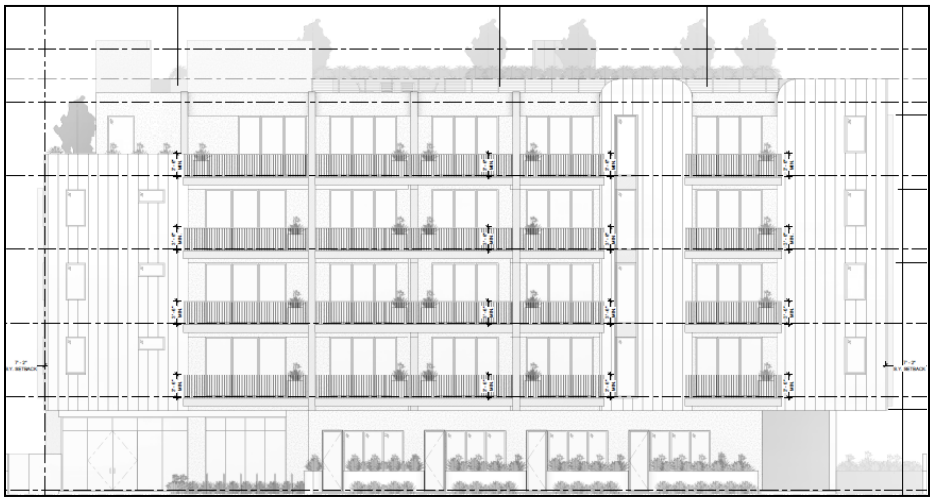


Figure 2. West elevation (Kelton Avenue).

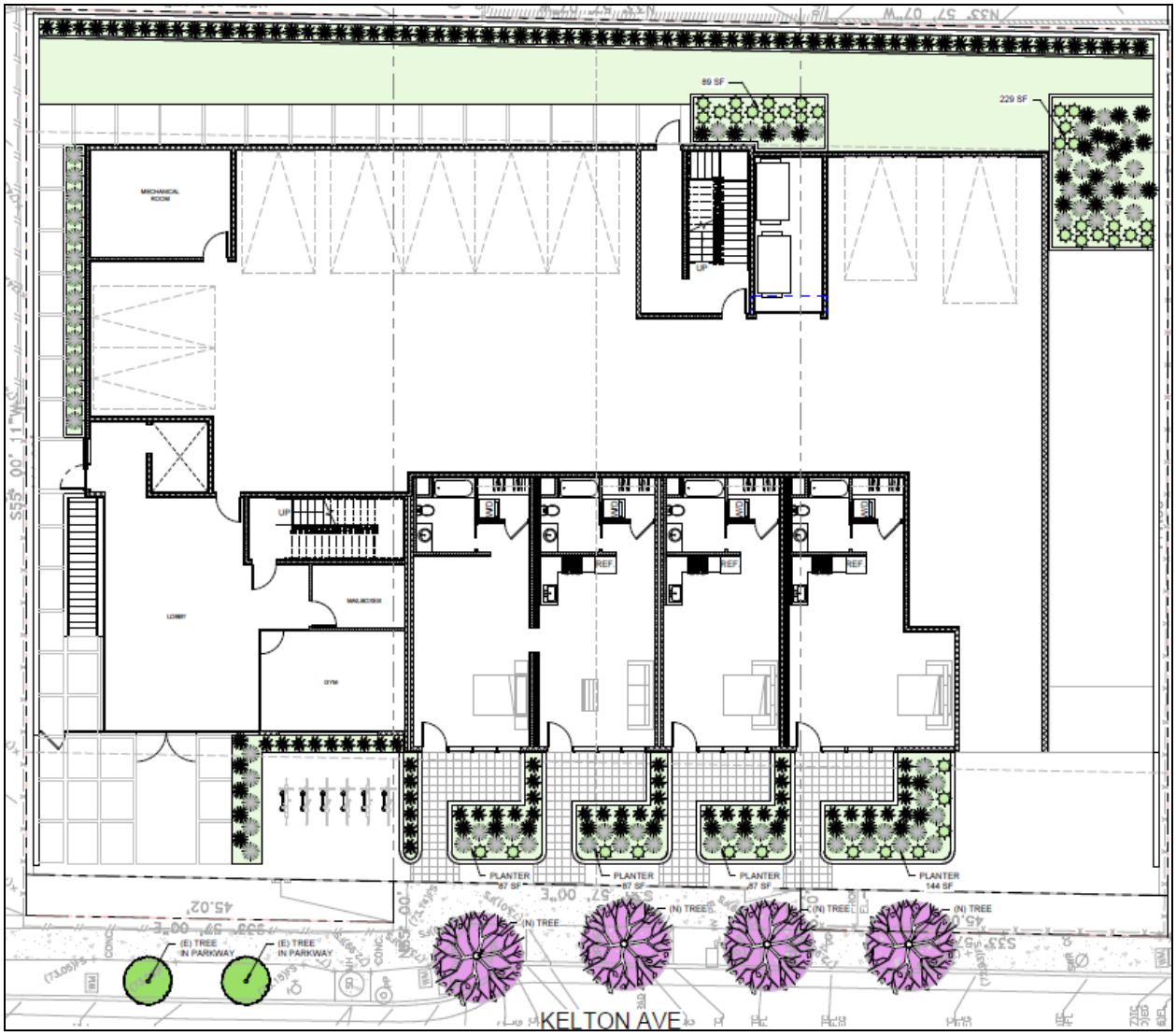


Figure 3. Ground Floor Plan.

The project will also provide approximately 2,950 square feet of open space, including a common roof deck, open space in the rear yard, and private balconies. The outdoor open space areas will be landscaped with planters, shrubs, and a minimum of 11 trees total (including street trees in the parkway). The building will observe a 15-foot front yard, a 15-foot rear yard, and side yards of 7-feet and 2-inches.

Vehicular access for the 33 vehicle parking spaces is provided via two driveways adjacent to each other, one providing access to the at-grade parking level and the other providing access to the below grade parking level. The project will also provide five short-term bicycle parking in the front yard area near the lobby entrance and 37 long-term bicycle parking spaces in the basement level parking garage.

PROJECT BACKGROUND

Project Site

The subject property (**Figure 4**) comprises three lots for a total of 15,334.56 square feet of lot area (0.352 acres) and is located in the Palms neighborhood on the east side of Kelton Avenue,

at the intersection of Regent Street. The project site is currently developed with two residential duplexes (total of four units).

The site is one block north of Venice Boulevard (Route 187) and is a 1.2 mile walk from the Metro E Line Palms Station. As shown in **Figure 5** below, the site is in close proximity to Downtown Culver City, Sony Pictures Studios, and commercial and retail amenities such as grocery stores (for example Trader Joes).

Figure 4. Aerial view of site (outlines in red) and surrounding area. Google Maps.

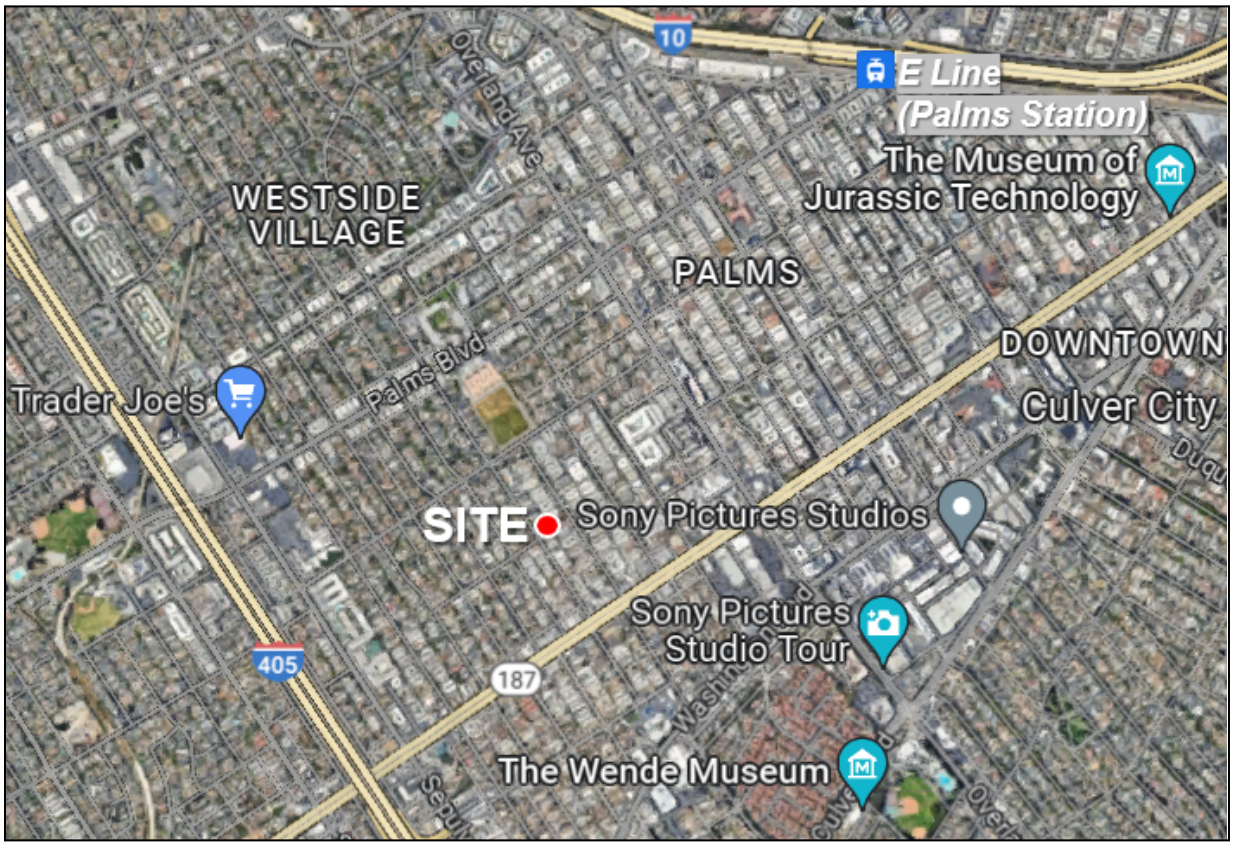


Figure 5. Vicinity Map showing key transportation facilities, employment centers, and commercial and entertainment hubs. Google Maps.

Figure 6. Zoning map (left) and land use map (right) with subject property outlined in blue. ZIMAS.

General Plan Land Use Designation and Zoning

The project site is located within the Palms-Mar Vista-Del Rey Community Plan, which is one of 35 Community Plans which together form the land use element of the General Plan. The Community Plan designates the site for Low Medium Residential land uses (corresponding to the R2, RD3, RD4, RZ3, RZ4, RU, RW1 Zones) and Medium Residential land uses (corresponding to the R3 Zone). The project site is zoned RD2-1 and R3-1 (see **Figure 6**) and is thus consistent with the existing land use designation. The subject property is located within a Tier 2 Transit Oriented Communities (TOC) area, a Transit Priority Area, and the West Los Angeles Transportation Improvement and Mitigation Specific Plan Area (West LA TIMP). Pursuant to the West LA TIMP, the project may be required to pay transportation impact fees which support the construction of multimodal transportation infrastructure on the Westside.

The site is within a Special Grading Area (BOE Basic Grid Map A-13372) and is located 2.7 kilometers from the nearest fault (Newport - Inglewood Fault Zone, Onshore). The site is not within an Alquist-Priolo Fault Zone, landslide area, liquefaction area, Tsunami Inundation Zone, Flood Zone, High Fire Severity Zone, Hazardous Waste / Border Zone, Methane Hazard Site, or High Wind Velocity Area.

Requested Entitlements

The applicant is requesting a Density Bonus to permit a housing development project consisting of 43 dwelling units, of which 11 will be set aside for Very Low Income households and with the following Incentives and Waivers of Development Standards:

a. An On-Menu Incentive to permit a 23% increase in allowable floor area to allow 39,011 square feet of floor area (FAR 3.7:1) in lieu of the 31,590 square feet (FAR 3:1);

b. An On-Menu Incentive to permit averaging of density over the project site located on three continuous parcels zoned RD2-1 and R3-1;

c. An Off-Menu Incentive to permit 17-foot increase in height to a maximum of 62 feet with no step-back after 45 feet of height in lieu of the maximum 45 feet allowed in the RD2-1 and R3-1 zones;

d. A Waiver of Development Standards to permit a 10% decrease in required northerly side yard setback to allow a 7-foot 2-inch side yard in lieu of the eight feet required by the RD2 zone;

e. A Waiver of Development Standards to permit a 10% decrease in required southerly side yard setback to allow a 7-foot 2-inch side yard in lieu of the eight feet required by the R3 zone; and

The applicant is also requesting a Conditional Use for a Density Bonus project to allow a Density Bonus for a housing development project in which the density increase is 201 percent which is greater than 35 percent which is otherwise permitted by LAMC Section 12.22-A,25.

Surrounding Properties

The surrounding area is zoned multifamily uses and is developed with multi-family residential buildings and single family homes. **Figure 7** shows immediately surrounding properties.

Figure 7. View of site (in red outlined) and surrounding properties. Google Maps.

Properties along Kelton Avenue, north of Regent Street, are zoned RD2-1 with a land use designation of Low Medium Residential and are developed with a mix of single family homes, duplexes, and two story apartment buildings. Properties on Kelton Avenue to the south of Regent Street are zoned R3-1 and are developed primarily with apartment buildings ranging in height from two to three stories with some single family homes and duplexes. Properties to the east of the site, fronting Midvale Avenue are zoned R3-1 with a Medium Residential Land Use designation and are developed with multifamily residential ranging in height from two to four stories. The project site is located one block north of Venice Boulevard where properties are zoned C2-1 for commercial uses one block south of Palms Middle School.

Streets

Kelton Avenue adjoining the subject property to the west, is a Local Street-Standard Street with a dedicated right-of-way width of 60 feet and improved with curb, gutter, and sidewalk in front of the subject property.

Relevant Cases

Subject Property: There are no previous relevant cases on the subject property.

Surrounding Properties: The following relevant cases were identified to be within a 1,000-foot radius of the project site and filed within the past 10 years:

Case No. CPC-2021-6888-CU-DB-HCA-PHP: On April 12, 2023, the City Planning Commission approved a new five-story, 56-foot-tall multifamily residential building with 27 dwelling units (including five units restricted to Very Low Income households). The following entitlements were approved at **3730 South Kelton Avenue**:

- A Conditional Use Permit for a 102.5 percent increase in density, in lieu of the otherwise allowed 35 percent increase in density.
- A Density Bonus/Affordable Housing Incentive Program Compliance Review to permit the construction of a Housing Development Project totaling 27 units, reserving five for Very Low Income households with the following On and Off-Menu Incentives and Waivers of Development Standards:
  - On-Menu Incentive for a building line setback of 18 feet in lieu 20-feet,
  - Off-Menu Incentive for an FAR of 4.28:1 in lieu of 3:1,
  - Off-Menu Incentive for a reduction in parking to allow 19 spaces in lieu of 42,
  - Waiver of Development Standards for an 11 foot increase in building height to allow 56 in lieu of 45 feet,
  - Waiver of Development Standards for a reduction in the rear setback to allow 12 feet in lieu of 15 feet, and
  - A Waiver of Development Standards to a reduction in open space to allow 2,724 square feet in lieu of 3,075 square feet.

Case No. DIR-2021-10077-TOC-HCA: On August 5, 2022, the Director of Planning approved a 60% increase in density consistent with the provisions of the TOC Affordable Housing Incentive Program for a Tier 2 project with a total of 20 dwelling units, including one unit reserved for Extremely Low Income Household and two units reserved for Very Low Income Household occupancy, with an FAR of 3.5:1, along with the following three Additional Incentives at **3748 - 3754 ½ South Veteran Avenue**:

- Side Yard. A 30% decrease in the required width of the northerly side yard;
- Open Space. A 20% decrease in required open space for 1,800 square feet, and
- Height. An 11-foot increase in height, and

Case No. DIR-2016-2670-DB-SPR: On December 5, 2017, the Director of Planning approved the following entitlements at **10801 West Venice Boulevard**:

- Density Bonus Compliance Review for a 5-story, 70-foot tall mixed-use building totaling 60,000 square feet with 63 dwelling units and 6,425 square feet of commercial space. The project will reserve 11 percent, or 6 dwelling units, of the 51 total base dwelling units permitted on the site for Very Low Income household occupancy. The following two Incentives are approved:
  - Yard/Setback. A 20 percent decrease in the required depth of the rear yard setback from 17 feet to 13 feet and seven-inches.
  - Floor Area Ratio. 3:1 FAR in lieu of the maximum permitted 1.5:1 FAR.
- Site Plan Review for a project with more than 50 units.

Case No. DIR-2014-4911-DB: On December 4, 2015, the Director of Planning Approve the following a Density Bonus Project totaling 15 dwelling units plus one guest room, reserving at two dwelling units (18 percent of the base) for Very Low Income household occupancy located at **3748-3754 South Veteran Avenue**. The approval including the following two incentives: (1) Height: An 11 foot increase in the allowable height to 56 feet in lieu of 45 feet;

(2) Density Calculation: Allow land required by dedication to be included as lot area for the purpose of calculating maximum density permitted.

The decision was appealed and on April 1, 2016, the City Planning Commission denied the appeal and upheld the approval.

Case No. DIR-2014-4339-DB: On October 15, 2015, the Director of Planning approved a Density Bonus project located at **3743 South Midvale Avenue** with the following entitlements:

- A project totaling 13 dwelling units, reserving at least 11 percent, or one (1) dwelling unit, of the nine (9) total "base" dwelling units permitted on the site, for Very Low Income household occupancy and the following two (2) incentives:
  - Side Yard. A 20 percent reduction of the north side yard to 5.5 feet in lieu of the required seven feet.
  - Height. An 11 foot increase in the allowable height to 56 feet in lieu of 45 feet.

Density Bonus / Affordable Housing Incentive Program

In accordance with California Government Code Section 65915 and LAMC Section 12.22-A,25, in exchange for setting aside a minimum percentage of the project's units for affordable housing, the project is eligible for a density bonus, reduction in parking, and incentives allowing for relief from development standards. The applicant has requested to utilize the provisions of City and State Density Bonus laws as follows:

Density

The subject property is zoned R3-1 which limits density to one (1) dwelling unit per 800 square foot of lot area and RD2-1 which limits density to one (1) dwelling unit per 2,000 square feet of lot area. The subject property has a total lot area of 15,334.6 square feet, with half of the property being zoned R3 and half being zoned RD2. As such, the density allowed by the underlying zones is 3.7 units on the RD2 site and 9.3 units on the R3 site. When Calculating base density for the purposes of Density Bonus calculations, 3.7 is rounded up to four (4) and 9.3 is rounded up to 10 for a base density on the subject property of 14 units units.[1] In exchange for reserving a portion of the units for affordable housing, the applicant is entitled to a maximum 35 percent density bonus by-right. The applicant is seeking an additional 166 percent density bonus (or a total of a 201 percent density bonus) through a Conditional Use to allow for the proposed 43 dwelling units to be built on the site.

Pursuant to the LAMC and California Government Code Section 65915, a Housing Development Project that sets aside a certain percentage of units as affordable, either in rental or for-sale units, shall be granted a corresponding density bonus, up to a maximum of 35 percent. While these provisions are limited to 35 percent, Government Code Section 65915(f) states that "the amount of density bonus to which an applicant is entitled shall vary according to the amount by which the percentage of affordable housing units exceeds the percentage established." As such, in instances where a project is seeking a density bonus increase that is more than 35 percent, the amount of required units that are set aside as affordable shall vary depending on the requested amount of density bonus. Therefore, it is appropriate that any project that requests a density bonus increase beyond 35 percent would extend the existing set-aside charts located in Section 12.22-A,25 of the LAMC. LAMC Section 12.24-U,26, which

[1] Assembly Bill 2501 clarifies that density calculations that result in a fractional number are to be rounded up to the next whole number. This applies to base density, number of bonus units, and number of affordable units required to be eligible for the density bonus.

implements this provision of State law, states, as a Conditional Use, a project may be granted additional density increases beyond the 35 percent maximum by providing additional affordable housing units. Consistent with this Section, Table 1 below illustrates how the maximum allowable Density Bonus increases for every unit set aside for Very Low Income Households (2.5 percent density increase for every additional one [1] percent of Very Low Income units provided), based on the base density and the chart prescribed in LAMC Section 12.22-A,25.

Table 1: Density Bonus Percentages

Very Low Income Units (Percentage of Base Density)	Maximum Density Bonus Permitted (Based on Base Density)
*5 %**	*20 %**
*6 %**	*22.5 %**
*7 %**	*25 %**
*8 %**	*27.5 %**
*9 %**	*30 %**
*10 %**	*32.5 %**
*11 %**	*35 %**
*76 %**	*197.5 %**
*77 %**	*200 %**
78 %*	***202.5 %****
*79 %**	*205 %**
*80 %**	*207.5 %**

**Existing set-aside chart as listed in Section 12.22-A,25 of the LAMC*

For the subject property, a 35 percent by-right density bonus would allow for 19 units (equal to an increase of five [5] units beyond the 14-unit base density [by-right density rounded up]) to be constructed on the project site. As illustrated in Table 2 above, in order to qualify for the 35 percent by-right density bonus, the project would be required to set aside 11 percent of the base density, or five units, for Very Low Income Households. The applicant is seeking an additional 166 percent density bonus (for a total of a 201 percent density bonus from the base density) through a Conditional Use to allow for a total of 43 dwelling units, representing an increase of 24 units beyond what would otherwise be permitted through the by-right 35 percent density bonus. In order to obtain the additional requested 166 percent density bonus, as shown in Table 2, the project must set aside at least 78 percent of the base density, equal to 10.92 units, for Very Low Income households. The project will provide 11 units, for Very Low Income households in exchange for the requested Density Bonus. As such, the Density Bonus request results in two (2) affordable units and the Conditional Use request results in an additional nine (9) units for a total of 11 affordable units.

Incentives

Pursuant to the LAMC and Government Code Section 65915, the applicant is entitled to three Incentives in exchange for reserving at least 15 percent of the base density for affordable households. The proposed project will set aside 11 units (78 percent of the base number of units) for affordable households. Accordingly, the applicant has requested three Incentives:

1. **Increased Floor Area Ratio (On-Menu).** The subject 15,334.6 square foot property is zoned RD2-1 and R3-1. The property's zoning and designation of Height District 1 permit a maximum FAR of 3:1, equal to a maximum of 31,590 square feet of total building area. Per

LAMC Section 12.22-A,25(f), a 35 percent increase in FAR is allowed as an on-menu Incentive. The project requests a 23 percent increase for a total of 39,011 square feet of building area, for an FAR of 3.7:1.

2. **Averaging (On-Menu).** The subject property is zoned R3-1 and RD2-1 which limit density to one unit per 800 square feet of lot area and 2,000 square feet of lot area, respectively. The applicant is requesting an on-menu incentive to permit averaging of allowable density across the site.

3. **Increased Height (Off-Menu).** The subject property is zoned R3-1 and RD2-1 which limit the building height to a maximum of 45 feet. Per LAMC Section 12.22-A,25(f) an additional 11 feet as an on-menu incentive. The applicant is requesting an off-menu incentive to permit a 17-foot increase in building height for a maximum height of 62 feet; and

Waiver of Development Standards

Per California Government Code Section 65915(e)(1) and Section 12.25-A,25(g) of the LAMC, a Housing Development Project may also request other "waiver(s) or reduction(s) of development standards that will have the effect of physically precluding the construction of a development meeting the [affordable set-aside percentage] criteria…at the densities or with the concessions or incentives permitted under [State Density Bonus Law]". In addition to the On- and Off-Menu Incentives, the project has requested two (2) Waivers of Development Standards, as follows:

1. **Reduced Side Yard (North).** A waiver of development standard to permit a 10% decrease in required northerly side yard setback to allow a 7-foot 2-inch side yard in lieu of the 8 feet required by the RD2 zone; This reduction is equivalent to what is permitted on the Density Bonus Menu of Incentives.

2. **Reduced Side Yard (South).** A waiver of development standard to permit a 10% decrease in required northerly side yard setback to allow a 7-foot 2-inch side yard in lieu of the 8 feet required by the R3 zone; This reduction is equivalent to what is permitted on the Density Bonus Menu of Incentives.

Housing Replacement

In addition to the requirements of California State Density Bonus Law, on October 9, 2019, the Governor signed into law the Housing Crisis Act of 2019 (SB 330). SB 330 creates new state laws regarding the production, preservation and planning for housing, and establishes a statewide housing emergency until January 1, 2025. During the duration of the statewide housing emergency, SB 330, among other things, creates new housing replacement requirements for Housing Development Projects by prohibiting the approval of any proposed housing development project on a site that will require the demolition of existing residential dwelling units or occupied vacant "Protected Units" unless the proposed housing development project replaces those units.

The Housing Crisis Act of 2019, as amended by SB 8 (California Government Code Section 66300 et seq.), prohibits the approval of any proposed housing development project on a site that will require demolition of existing dwelling units or occupied or vacant "Protected Units" unless the project replaces those units. The project shall provide at least as many residential dwelling units as the greatest number of residential dwelling units that existed on the property within the past five years. Additionally, the project must also replace all existing or demolished "Protected Units".

Pursuant to the Determination made by the LAHD dated July 6, 2022 the project will result in the demolition of two duplexes (four units total) and further determined the four units must be replaced with two Very Low Income units, one Low Income unit, and one market rate unit subject to the Rent Stabilization Ordinance (RSO). With the provision of the proposed 11 Very Low Income units, the housing project satisfies the SB 8 replacement requirements.

PUBLIC HEARING

A public hearing on this matter was held with the Hearing Officer virtually via zoom meeting on April 18, 2023, 2023. Comments from the public hearing are documented in Public Hearing and Communications, Page P-1.

PROJECT CONSIDERATIONS

Evolution of Project

The applicant has made project modifications since filing in response to comments from Planning Staff, community members, and comments from the Urban Design Studio's Professional Volunteer Program (PVP).

The project was originally filled with 24 vehicle parking spaces. However in response to community concerns of inadequate parking, the applicant increased the parking count to 33. This project has zero required parking per state law, all parking spaces provided have been volunteered by the applicant.

Staff recommended relocation of a basement stairwell located within the front yard and detracted from the landscaping. The applicant was able to relocate the stairwell to a more appropriate location, improving the project's interface with public right of way.

The PVP recommended that the ground floor units be redesigned as walk-up units in order to better activate the street. The applicant accommodated this change and the proposed project currently features three walk up units with patios in the front yard.

CONCLUSION

Based on the public hearing and information submitted to the record, staff recommends that the City Planning Commission find, based on its independent judgment, after consideration of the entire administrative record, that the project is categorically exempt from CEQA. Staff also recommends that the City Planning Commission approve the Density Bonus incentives and waivers of development standards and the Conditional Use for an additional 166 percent density bonus (for a total of 201 percent density bonus from the base density), thereby approving the project as proposed. The approval of the density bonus and conditional use will allow the addition of 11 Very Low Income Households and 32 market rate units neighborhood that is characterized as walkable and rich in job opportunities which aligns with the City's housing and economic development goals and objectives.

CONDITIONS OF APPROVAL

Pursuant to Sections 12.22-A,25 and 12.24-U,26 of the Los Angeles Municipal Code, the following conditions are hereby imposed upon the use of the subject property:

A. Development Conditions

Density Bonus

1. **Site Development.** Except as modified herein, the project shall be in substantial conformance with the plans dated April 13, 2023, submitted by the Applicant, stamped "Exhibit A," and /attached to the subject case file.

2. **Residential Density**. The project shall be limited to a maximum density of 43 dwelling units.

3. **Affordable Units.**

   a. A minimum of seven (11) units, that is at least 78 percent of the base dwelling units permitted in the underlying zone(s), shall be reserved as Very Low Income Households, as defined by the State Density Bonus Law per Government Code Section 65915(c)(2).

   b. **Changes in Restricted Units.** Deviations that increase the number of restricted affordable units or that change the composition of units or change parking numbers shall be consistent with LAMC Section 12.22-A,25.

4. **Housing Requirements.** Prior to issuance of a building permit, the owner shall execute a covenant to the satisfaction of the Los Angeles Housing Department (LAHD) to make at least 78 percent of the site's base density units (11 units) available to Very Low Income Households, for sale or rental as determined to be affordable to such Households by LAHD for a period of 55 years. In the event the applicant reduces the proposed density of the project, the number of required reserved on-site Restricted Units may be adjusted, consistent with LAMC Section 12.22-A,25, to the satisfaction of LAHD, and in consideration of the project's SB 8 Determination, dated July 6, 2022 (or any subsequent update to that letter as deemed necessary by LAHD). Enforcement of the terms of said covenant shall be the responsibility of LAHD. The applicant shall present a copy of the recorded covenant to the Department of City Planning for inclusion in this file. The project shall comply with the Guidelines for the Affordable Housing Incentives Program adopted by the City Planning Commission and with any monitoring requirements established by the LAHD. Refer to the Density Bonus Legislation Background section of this determination for more information.

5. **Housing Replacement.** Prior to issuance of a building permit, the owner shall execute a covenant to the satisfaction of the Los Angeles Housing Department (LAHD), and in compliance with LAHD's July 6, 2022 SB 330 Determination Letter (or any subsequent update to that letter as deemed necessary by LAHD), to make three (3) units, for sale or rental as determined to be affordable to such Households by LAHD for a period of 55 years and one additional market rate unit subject to the Rent Stabilization Ordinance (RSO). Enforcement of the terms of said covenant shall be the responsibility of LAHD. The applicant will present a copy of the recorded covenant to the Department of City Planning for inclusion in this file. The project shall comply with the Guidelines for the Affordable Housing Incentives Program adopted by the City Planning Commission and

with any monitoring requirements established by the LAHD. Refer to the Density Bonus Legislation Background section of this determination for more information.

On-site Restricted Affordable Units may be used to satisfy the Housing Replacement units required pursuant to SB 8 provided such units meet the income levels, to the satisfaction of LAHD.

6. **Incentives.**

   a. **FAR.** The project shall be permitted a 23% increase in allowable floor area to allow 39,011 square feet of floor area (FAR 3.7:1) in lieu of the 31,590 square feet (FAR 3:1) permitted pursuant to LAMC 12.21.1-A,1;

   b. **Height.** The project shall be permitted a 17-foot increase in height to a maximum of 62 feet with no step-back after 45 feet of height in lieu of the maximum 45 feet allowed in the RD2-1 and R3-1 zones pursuant to LAMC 12.21.1;

   c. **Averaging.** The project shall be permitted averaging of density over the project site located on three continuous parcels zoned RD2-1 and R3-1;

7. **Waivers of Development Standards.**

   a. **Side Yard (northerly).** The project shall be permitted 10% decrease in required northerly side yard setback to allow a 7-foot 2-inch side yard in lieu of the 8 feet required by the RD2 and R3 zones pursuant to LAMC 12.09.1-B,2(a) and LAMC 12.10-C,3, respectively.

   b. **Side Yard (southerly).** The project shall be permitted a 10% decrease in required southerly side yard setback to allow a 7-foot 2-inch side yard in lieu of the 8 feet required by the RD2 and R3 zones pursuant to LAMC 12.09.1-B,2(a) and LAMC 12.10-C,3, respectively.

8. **Parking**.

   a. **Parking.** The project shall not be required to provide vehicular parking, pursuant to California State Assembly Bill No. 2097 (AB 2097).

   b. **Unbundling.** Required parking may be sold or rented separately from the units, with the exception of all Restricted Affordable Units which shall include any required parking in the base rent or sales price, as verified by LAHD.

   c. **Bicycle Parking**. Bicycle parking shall be provided consistent with LAMC Section 12.21-A,16.

9. **Street Trees:**

   a. Street trees shall be provided to the satisfaction of the Urban Forestry Division. Street trees may be used to satisfy on-site tree requirements pursuant to LAMC Article Section 12.21.G.3 (Chapter 1, Open Space Requirement for Six or More Residential Units). Per Exhibit A and 12.21.G.3, six (6) Street trees shall be provided.

   b. Project shall preserve all healthy mature street trees whenever possible. All feasible alternatives in project design should be considered and implemented to

retain healthy mature street trees. A permit is required for the removal of any street tree and shall be replaced 2:1 as approved by the Board of Public Works and Urban Forestry Division.

c. Plant street trees at all feasible planting locations within dedicated streets as directed and required by the Bureau of Street Services, Urban Forestry Division. All tree plantings shall be installed to current tree planting standards when the City has previously been paid for tree plantings. The sub divider or contractor shall notify the Urban Forestry Division at: (213) 847- 3077 upon completion of construction for tree planting direction and instructions.

Note: Removal of street trees requires approval from the Board of Public Works. All projects must have environmental (CEQA) documents that appropriately address any removal and replacement of street trees. Contact Urban Forestry Division at: (213) 847-3077 for tree removal permit information

10. Landscaping:

a. All open areas not used for buildings, driveways, parking areas, or walkways shall be attractively landscaped and maintained in accordance with a landscape plan and an automatic irrigation plan, prepared by a licensed Landscape Architect and to the satisfaction of the Department of City Planning.

b. The landscaping shall be sufficient to qualify for the number of landscape points equivalent to 10% more than otherwise required by Section 12.40 of this Code and Landscape Ordinance Guidelines “O”.

c. Tree Wells.

i. The minimum depth of tree wells on the rooftop or any other location where planters are used shall be as follows:

(1) Minimum depth for trees shall be 42 inches.

(2) Minimum depth for shrubs shall be 30 inches.

(3) Minimum depth for herbaceous plantings and ground cover shall be 18 inches.

(4) Minimum depth for an extensive green roof shall be 3 inches.

ii. The minimum amount of soil volume for tree wells on the rooftop or any other location where planters are used shall be based on the size of the tree at maturity:

(1) 600 cubic feet for a small tree (less than 25 feet tall at maturity).

(2) 900 cubic feet for a medium tree (25-40 feet tall at maturity).

(3) 1,200 cubic feet for a large tree (more than 40 feet tall at maturity).

11. Street Lighting. No Street lighting improvements if no street widening per BOE improvement conditions. Otherwise relocate and upgrade street lunges: one (1) on Kelton Avenue.

12. **Solar.** The project shall provide for the installation of a photovoltaic system and comply with the Los Angeles Municipal Green Building Code, Section 99.05.211, to the satisfaction of the Department of Building and Safety.

13. **Electric Vehicle Parking.** All electric vehicle charging spaces (EV Spaces) and electric vehicle charging stations (EVCS) shall comply with the regulations outlined in Sections 99.04.106 and 99.05.106 of Article 9, Chapter IX of the LAMC.

14. **Materials**. A variety of high quality exterior building materials, consistent with Exhibit A, shall be used. Substitutes of an equal quality shall be permitted, to the satisfaction of the Department of City Planning.

15. **Circulation.** The applicant shall submit a parking and driveway plan to the Los Angeles Department of Transportation (LADOT) for approval. The project shall be limited to a maximum of one driveway, as shown in Exhibit A. The curb cut dimension shall be no more than 10 feet or as narrow as permitted by LADOT.

16. **Mechanical Equipment**. All mechanical equipment on the roof shall be screened from view by any abutting properties. The transformer, if located in the front yard, shall be screened with landscaping and/or materials consistent with the building façade on all exposed sides (those not adjacent to a building wall).

17. **Lighting.** Outdoor lighting shall be designed and installed with shielding, such that the light source does not illuminate adjacent residential properties or the public right-of-way, nor the above night skies.

18. **Graffiti.** All graffiti on the site shall be removed or painted over to match the color of the surface to which it is applied within 24 hours of its occurrence.

19. **Trash.** Trash receptacles shall be stored within a fully enclosed portion of the building at all times. Trash/recycling containers shall be locked when not in use and shall not be placed in or block access to required parking.

B. <u>Administrative Conditions</u>

20. **Final Plans.** Prior to the issuance of any building permits for the project by the Department of Building and Safety, the applicant shall submit all final construction plans that are awaiting issuance of a building permit by the Department of Building and Safety for final review and approval by the Department of City Planning. All plans that are awaiting issuance of a building permit by the Department of Building and Safety shall be stamped by Department of City Planning staff “Final Plans”. A copy of the Final Plans, supplied by the applicant, shall be retained in the subject case file.

21. **Notations on Plans.** Plans submitted to the Department of Building and Safety, for the purpose of processing a building permit application shall include all of the Conditions of Approval attached herein as a cover sheet, and shall include any modifications or notations required herein.

22. **Building Plans.** A copy of the first page of this grant and all Conditions and/or any subsequent appeal of this grant and its resultant Conditions and/or letters of clarification shall be printed on the building plans submitted to the Development Services Center and the Department of Building and Safety for purposes of having a building permit issued.

23. **Corrective Conditions.** The authorized use shall be conducted at all times with due regard for the character of the surrounding district, and the right is reserved to the City Planning Commission, or the Director pursuant to Section 12.27.1 of the Municipal Code, to impose additional corrective conditions, if, in the Commission's or Director's opinion, such conditions are proven necessary for the protection of persons in the neighborhood or occupants of adjacent property.

24. **Approvals, Verification and Submittals**. Copies of any approvals, guarantees or verification of consultations, reviews or approval, plans, etc., as may be required by the subject conditions, shall be provided to the Department of City Planning for placement in the subject file.

25. **Code Compliance.** All area, height and use regulations of the zone classification of the subject property shall be complied with, except wherein these conditions explicitly allow otherwise.

26. **Department of Building and Safety.** The granting of this determination by the Director of Planning does not in any way indicate full compliance with applicable provisions of the Los Angeles Municipal Code Chapter IX (Building Code). Any corrections and/or modifications to plans made subsequent to this determination by a Department of Building and Safety Plan Check Engineer that affect any part of the exterior design or appearance of the project as approved by the Director, and which are deemed necessary by the Department of Building and Safety for Building Code compliance, shall require a referral of the revised plans back to the Department of City Planning for additional review and sign-off prior to the issuance of any permit in connection with those plans.

27. **Department of Water and Power.** Satisfactory arrangements shall be made with the Los Angeles Department of Water and Power (LADWP) for compliance with LADWP's Rules Governing Water and Electric Service. Any corrections and/or modifications to plans made subsequent to this determination in order to accommodate changes to the project due to the under-grounding of utility lines, that are outside of substantial compliance or that affect any part of the exterior design or appearance of the project as approved by the Director, shall require a referral of the revised plans back to the Department of City Planning for additional review and sign-off prior to the issuance of any permit in connection with those plans.

28. **Covenant.** Prior to the issuance of any permits relative to this matter, an agreement concerning all the information contained in these conditions shall be recorded in the County Recorder's Office. The agreement shall run with the land and shall be binding on any subsequent property owners, heirs or assign. The agreement must be submitted to the Department of City Planning for approval before being recorded. After recordation, a copy bearing the Recorder's number and date shall be provided to the Department of City Planning for attachment to the file.

29. **Definition.** Any agencies, public officials or legislation referenced in these conditions shall mean those agencies, public offices, legislation or their successors, designees or amendment to any legislation.

30. **Enforcement.** Compliance with these conditions and the intent of these conditions shall be to the satisfaction of the Department of City Planning and any designated agency, or the agency's successor and in accordance with any stated laws or regulations, or any amendments thereto.

31. Expedited Processing Section. Prior to the clearance of any conditions, the applicant shall show proof that all fees have been paid to the Department of City Planning, Expedited Processing Section.

32. Indemnification and Reimbursement of Litigation Costs.

Applicant shall do all of the following:

a. Defend, indemnify and hold harmless the City from any and all actions against the City relating to or arising out of, in whole or in part, the City's processing and approval of this entitlement, including but not limited to, an action to attack, challenge, set aside, void, or otherwise modify or annul the approval of the entitlement, the environmental review of the entitlement, or the approval of subsequent permit decisions, or to claim personal property damage, including from inverse condemnation or any other constitutional claim.

b. Reimburse the City for any and all costs incurred in defense of an action related to or arising out of, in whole or in part, the City's processing and approval of the entitlement, including but not limited to payment of all court costs and attorney's fees, costs of any judgments or awards against the City (including an award of attorney's fees), damages, and/or settlement costs.

c. Submit an initial deposit for the City's litigation costs to the City within 10 days' notice of the City tendering defense to the applicant and requesting a deposit. The initial deposit shall be in an amount set by the City Attorney's Office, in its sole discretion, based on the nature and scope of action, but in no event shall the initial deposit be less than $50,000. The City's failure to notice or collect the deposit does not relieve the applicant from responsibility to reimburse the City pursuant to the requirement in paragraph (b).

d. Submit supplemental deposits upon notice by the City. Supplemental deposits may be required in an increased amount from the initial deposit if found necessary by the City to protect the City's interests. The City's failure to notice or collect the deposit does not relieve the applicant from responsibility to reimburse the City pursuant to the requirement in paragraph (b).

e. If the City determines it necessary to protect the City's interest, execute an indemnity and reimbursement agreement with the City under terms consistent with the requirements of this condition.

The City shall notify the applicant within a reasonable period of time of its receipt of any action and the City shall cooperate in the defense. If the City fails to notify the applicant of any claim, action, or proceeding in a reasonable time, or if the City fails to reasonably cooperate in the defense, the applicant shall not thereafter be responsible to defend, indemnify or hold harmless the City.

The City shall have the sole right to choose its counsel, including the City Attorney's office or outside counsel. At its sole discretion, the City may participate at its own expense in the defense of any action, but such participation shall not relieve the applicant of any obligation imposed by this condition. In the event the applicant fails to comply with this condition, in whole or in part, the City may withdraw its defense of the action, void its approval of the entitlement, or take any other action. The City retains the right to make all decisions with respect to its representations in any legal proceeding, including its inherent right to abandon or settle litigation.

For purposes of this condition, the following definitions apply:

"City" shall be defined to include the City, its agents, officers, boards, commissions, committees, employees, and volunteers.

"Action" shall be defined to include suits, proceedings (including those held under alternative dispute resolution procedures), claims, or lawsuits. Actions include actions, as defined herein, alleging failure to comply with any federal, state or local law.

Nothing in the definitions included in this paragraph are intended to limit the rights of the City or the obligations of the applicant otherwise created by this condition.

FINDINGS

Density Bonus/Affordable Housing Incentives Compliance Findings

1. Pursuant to Section 12.22 A.25 of the LAMC and Section 65915 of the California Government Code, the decision-maker shall approve a density bonus and requested incentive(s) and waiver(s) unless the Commission finds that:

a. *The Incentives do not result in identifiable and actual cost to provide for affordable housing costs as defined in California Health and Safety Code Section 50052.5 or Section 50053 for rents for the affordable units.*

The record does not contain substantial evidence that would allow the City Planning Commission to make a finding that the requested incentives do not result in identifiable and actual cost reductions that provide for affordable housing costs per State Law. The California Health and Safety Code Sections 50052.5 and 50053 define formulas for calculating affordable housing costs for Very Low, Low, and Moderate Income Households. Section 50052.5 addresses owner-occupied housing and Section 50053 addresses rental Households. Affordable housing costs are a calculation of residential rent or ownership pricing not to exceed 25 percent gross income based on area median income thresholds depending on affordability levels.

Based on the set-aside of 78 percent of the base density for Very Low Income Households, the applicant is entitled to three (3) incentives under both Government Code Section 65915 and the LAMC. The request for an off-menu height increase, an on-menu FAR increase, and the on-menu use of averaging qualify as requested Incentives. The remaining requests to allow for reduction in two side yard setbacks are waivers of development standards.

Floor Area Ratio

The subject property is zoned RD2-1 and R3-1. The property's zoning and designation of Height District 1 permit a maximum FAR of 3 to 1, equal to a maximum of 31,590 square feet of total building area. The applicant is requesting an on-menu incentive for a 23 percent increase in FAR for a maximum FAR of 3.7 to 1 to allow 39,011 square feet of floor area.

The requested increase in FAR will allow for the construction of a larger number of market rate and affordable units, larger-sized dwelling units that are competitive in the rental market, and retail space at the ground level. Granting of the incentive would result in a building design and construction efficiencies that provide for affordable housing costs. It enables the developer to expand the building envelope so that additional market rate and affordable units can be constructed, and the overall space dedicated to residential uses is increased. The additional market rate floor area will increase the availability of market rate rents that will be available to subsidize the operational costs of the affordable units. The increased building envelope also ensures that all dwelling units are of a habitable size, and competitive with the market, while providing a variety of unit types that will serve an array of households. This Incentive helps provide for the cost of the 11 dwelling units for Very Low Income Households.

Averaging

The subject property has a total lot area of 15,334.6 square feet with half of the site zoned R3-1 which limits density to one (1) dwelling unit per 800 square foot of lot area and half zoned RD2-1 which limits density to one (1) dwelling unit per 2,000 square feet of lot area. The density allowed by the underlying zones is 3.7 units on the RD2 zoned portion of the site and 9.3 units on the R3 zoned portion of the site. When Calculating base density for the purposes of Density Bonus calculations, 3.7 is rounded up to four (4) and 9.3 is rounded up to 10 for a base density on the subject property of 14 units units. The applicant has requested that the base density be averaged across the entire site such that the dwelling units may be provided on any portion of the site rather than allocating the units by zone. This incentive is offered on the menu of incentives for Density Bonus projects. Averaging allows the 43 units to be provided in the most economical way, by taking advantage of design efficiencies that allows the units to be distributed across the property and within fewer building levels, avoiding the construction of additional floors. Averaging also ensures that all dwelling units are of a habitable size, and competitive with the rental market, while providing a variety of unit types that will serve different types of households. This Incentive helps provide for the cost of the 11 dwelling units for Very Low Income Households.

Height

The subject property's RD2-1 and R3-1 Zone and Height District permits a maximum height of 45 feet. The project has requested an Off-Menu Incentive to allow a maximum height of 62 feet.

As proposed, the height increase will allow an additional 17 feet in building height and will accommodate the construction of an additional story to accommodate additional market rate and affordable units. Granting of the Off-Menu Incentive would result in a building design and construction efficiencies that provide for affordable housing costs given the limited size of the lot; it enables the developer to expand the building envelope so that additional units can be constructed and the overall space dedicated to residential uses and market rate floor area is increased. The additional floor area will increase rent revenue that will subsidize the operational costs of the affordable units in the project. The increased building envelope also ensures that all dwelling units are of a habitable size, and competitive with the rental market, while providing a variety of unit types that will serve different types of households. This Incentives supports the applicant's decision to set aside a minimum of 11 dwelling units for Very Low Income Households.

b. ***The waiver[s] or reduction[s] of development standards will not have the effect of physically precluding the construction of a development meeting the [affordable set-aside percentage] criteria of subdivision (b) at the densities or with the concessions or incentives permitted under [State Density Bonus Law]" (Government Code Section 65915(e)(1)***

A project that provides at least 5 percent of its base density for Very Low Income Households may request other "waiver[s] or reduction[s] of development standards that will have the effect of physically precluding the construction of a development meeting the [affordable set-aside percentage] criteria of subdivision (b) at the densities or with the concessions or incentives permitted under [State Density Bonus Law]" (Government Code Section 65915(e)(1)).

Side Yard Setback (northerly)

The RD2 Zone requires an 8 foot side yard. The project is proposed with a 10% decrease in required northerly side yard to allow a 7-foot 2-inch side yard.

Side Yard Setback (southerly)

The R3 Zone requires an 8 foot side yard. The project is proposed with a 10% decrease in required southerly side yard to allow a 7-foot 2-inch side yard.

The two waivers result in an additional combined 20 inches feet of building width which allows the project to accommodate the requested 39,011 square feet of floor area and thus the requested density of 43 units (11 Very Low Income). Adherence to the 8-foot side yard setbacks would physically preclude the construction of the project areas that encroach into the setback, and would physically preclude the floor area granted in the incentives that encroaches into the setback.

Conclusion

Application of these the Development Standards regarding side yards would have the effect of physically precluding construction of the proposed development providing 43 dwelling units with the requested incentives. These waivers are warranted.

c. ***The Incentives or waivers will have a Specific Adverse Impact upon public health and safety or the physical environment or any real property that is listed in the California Register of Historical Resources and for which there is no feasible method to satisfactorily mitigate or avoid the Specific Adverse Impact without rendering the development unaffordable to Very Low, Low and Moderate Income Households. Inconsistency with the zoning ordinance or general plan land use designation shall not constitute a specific, adverse impact upon the public health or safety.***

There is no evidence that the proposed density bonus incentives or waivers will have a specific adverse impact upon public health and safety or the physical environment, or any real property that is listed in the California Register of Historical Resources. A "specific adverse impact" is defined as "a significant, quantifiable, direct and unavoidable impact, based on objective, identified written public health or safety standards, policies, or conditions as they existed on the date the application was deemed complete" (LAMC Section 12.22-A,25(b)).

The project does not involve a contributing structure in a designated Historic Preservation Overlay Zone or on the City of Los Angeles list of Historical-Cultural Monuments. The project is not located on a substandard street in a Hillside area or a Very High Fire Hazard Severity Zone. There is no evidence in the record which identifies a written objective health and safety standard that has been exceeded or violated. Based on the above, there is no basis to deny the requested incentives or waivers. Therefore, there is no substantial evidence that the project’s proposed incentives or waivers will have a specific adverse impact on the physical environment, on public health and safety, or on property listed in the California Register of Historic Resources.

d. The incentives or waivers are contrary to state or federal law.

There is no substantial evidence in the record that the waivers or the incentives are contrary to state or federal law.

Conditional Use Findings

2. That the project will enhance the built environment in the surrounding neighborhood or will perform a function or provide a service that is essential or beneficial to the community, city or region.

The subject property comprises three lots for a total of 15,334.56 square feet of lot area and is located in the Palms neighborhood on the east side of Kelton Avenue, at the intersection of Regent Street. The project site is currently developed with two residential duplexes (total of four units). The site is one block north of Venice Boulevard (Route 187) and is a 1.2 mile walk from the Metro E Line Palms Station. The site is in close proximity to Downtown Culver City, Sony Pictures Studios, and commercial and retail amenities such as grocery stores (for example Trader Joes).

The project involves the demolition of two duplexes (four units) and the construction, use, and maintenance of a new five-story, 62-foot residential building consisting of 43 dwelling units, of which 11 will be set aside for Very Low Income households while the remaining 32 units will be rented at market rate. The dwelling units will comprise of a mix of studios, one-, and two- bedroom units. The project will provide 33 automobile parking spaces in one subterranean parking level and at grade parking level.

The existing duplexes do not utilize the site's full potential in providing much needed housing. The project will perform an essential function by providing 39 net-new dwelling units to Palms, including increasing the affordable housing stock by five net new units.
The additional 166 percent density bonus (beyond the 35 percent permitted through a by-right density bonus) approved herein results in an additional 24 units, for a total of 43 units. In exchange, the project will set aside at least 78 percent (11 units) of the base density for Very Low Income Households for a minimum of 55 years.

Therefore, the proposed 43-unit development, including the 11 units set aside for Very Low Income Households, will provide new market rate and affordable housing, thus performing a function that is essential and beneficial to the city and the region.

3. That the project's location, size, height, operations and other significant features will be compatible with and will not adversely affect or further degrade adjacent properties, the surrounding neighborhood or the public health, welfare, and safety.

The proposed project consists of the construction of a new five-story 43 dwelling unit residential development. The project site is currently developed with two duplexes (for a total of four units) which will be demolished as part of the proposed development.

The property is located within the Palms-Mar Vista-Del Rey Community Plan, a densely populated portion of the City of Los Angeles. The surrounding area is zoned multifamily uses and is developed with multi-family residential buildings and single family homes. The subject property is not adjacent to any single-family zoned properties. Properties along Kelton Avenue, north of Regent Street, are zoned RD2-1 with a land use designation of Low Medium Residential and are developed with a mix of single family homes, duplexes, and two story apartment buildings. Properties on Kelton Avenue to the south of Regent Street are zoned R3-1 and are developed primarily with apartment buildings ranging in height from two

to three stories with some single family homes and duplexes. Properties to the east of the site, fronting Midvale Avenue are zoned R3-1 with a Medium Residential Land Use designation and are developed with multifamily residential ranging in height from two to four stories. The project site is located one block north of Venice Boulevard where properties are zoned C2-1 for commercial uses one block south of Palms Middle School.

Construction of the 43-unit housing development will serve to benefit the neighborhood rather than degrade it. The façades are well-articulated and feature ground floor walk-up units which provide a transitional element to the smaller scale single family and duplexes to the north. The residential lobby and landscaping engage pedestrians along Kelton Avenue. Well-designed landscaping and front patio elements will create a pleasing transition from the pedestrian realm of the sidewalk to the façade of the building. Therefore, the project is compatible with the surrounding neighborhood and will not adversely affect nor degrade adjacent properties, surrounding neighborhood, or the public health, safety, or welfare.

With the exception of the requests herein, the proposed project is otherwise entirely consistent with the requirements of the underlying zone. The project's significant features, including the proposed building's use, density, height, and FAR, are permitted by the underlying zone and the provisions of Density Bonus law. The project has been thoughtfully designed to include landscaping and fully screened on-site parking.

Given the proposed project's location within the Palms-Mar Vista-Del Rey Community Plan area, along with the existing development in the immediate vicinity of the subject property and its proximity to commercial thoroughfares, the project's location, size, height, operations, and other significant features will be compatible with and will not adversely affect adjacent properties, the surrounding neighborhood, or the public health, welfare, and safety.

4. That the project substantially conforms with the purpose, intent and provisions of the General Plan, the applicable community plan, and any applicable specific plan.

The project site is located within the Palms-Mar Vista-Del Rey Community Plan, which is one of 35 Community Plans which together form the land use element of the General Plan. The Community Plan designates the site for Low Medium Residential land uses (corresponding to the R2, RD3, RD4, RZ3, RZ4, RU, RW1 Zones) and Medium Residential land uses (corresponding to the R3 Zone). The project site is zoned RD2-1 and R3-1 and is thus consistent with the existing land use designations. The subject property is located within the boundaries of the West LA TIMP Specific Plan and will be evaluated by LADOT for transportation impact fee assessment prior to receiving building permit clearances. The site is not subject to any other specific plan or community design overlay.

The proposed project conforms to the following goals, objectives and policies of the Community Plan:

Goal 1: A safe, secure, and high quality residential environment for all community residents.

Objective 1.1: To provide for the preservation of existing housing and for the development of new housing to meet the diverse economic and physical needs of the existing residents and projected population of the Plan area to the year 2010.

Policy 1.1.2: Protect the quality of residential environment and the appearance of communities with attention to site and building design.

Objective 1.4: To promote the adequacy and affordability of multiple-family housing and increase its accessibility to more segments of the population.

Policy 1.4.1: Promote greater individual choice in type, quality, price and location of housing.

The project is a residential development with 11 units restricted for families or persons of Very Low Income and maximizes the property's development potential. The project's Very Low Income and market rate units satisfy both the needs of affordable housing as well as the City's need for more housing overall. The project will result in the net addition of 11 covenanted affordable dwelling units in a community in-need of more affordable housing.

The project design incorporates four walk-up units at the ground floor, providing compatibility with the lower-scale, apartment buildings, duplexes, and single family homes located to the north along Kelton Avenue, many of which are characterized by walk-up entrances.

The uppermost story is set back from the northern building facade line by an additional XX feet in order to provide reduced building massing and greater compatibility with the RD2 zoned section of Kelton Avenue.

The project is further consistent with other elements of the General Plan, including the Housing Element and the Mobility Element.

The Housing Element of the General Plan will be implemented by the recommended action herein. The Housing Element is the City's blueprint for meeting housing and growth challenges. It identifies the City's housing conditions and needs, reiterates goals, objectives, and policies that are the foundation of the City's housing and growth strategy, and provides the array of programs the City has committed to implement to create sustainable, mixed-income neighborhoods across the City. The Housing Element includes the following objectives and policies relevant to the instant request:

Goal 1: Housing Production and Preservation.

Objective 1.1: Produce an adequate supply of rental and ownership housing in order to meet current and projected needs.

Policy 1.1.2: Expand affordable rental housing for all income groups that need assistance.

Goal 2: Safe, Livable, and Sustainable Neighborhoods.

Objective 2.2: Promote sustainable neighborhoods that have mixed-income housing, jobs, amenities, services and transit.

Objective 2.5: Promote a more equitable distribution of affordable housing opportunities throughout the City.

Policy 2.5.2: Foster the development of new affordable housing units citywide and within each community plan area.

The project provides 43 total units, including 11 units affordable for Very Low Income Households or individuals. In doing so, the project promotes the objectives of the Housing Element by adding to the City's housing stock and contributing to the need for mixed-income housing. The project site is currently developed with two duplex buildings. The project will

expand affordable rental housing while utilizing the property to its full potential, resulting in a net gain of 39 units to the City's housing stock. It is within close proximity to various major employment (including Sony Pictures Studios in Culver City) and retail centers (along Venice Boulevard and Downtown Culver City), along with several major transportation lines, thereby connecting residents to jobs, amenities, services, and transit.

The Mobility Element of the General Plan, also known as Mobility Plan 2035, provides policies with the ultimate goal of developing a balanced transportation network for all users. The project supports the following policies of the Mobility Element:

> Policy 3.3: Promote equitable land use decisions that result in fewer vehicle trips by providing greater proximity and access to jobs, destinations, and other neighborhood services.
>
> Policy 5.2: Support ways to reduce vehicle miles traveled (VMT) per capita.
>
> Policy 5.4: Continue to encourage the adoption of low and zero emission fuel sources, new mobility technologies, and supporting infrastructure.

The project proposes a residential development that will provide 32 market-rate units and 11 units reserved for Very Low Income Households. Accordingly, the project fulfills the Community Plan, Framework Element, and Housing Element goals and objectives of providing quality housing for all persons in the community, including those who otherwise might not be housed. The project utilizes development incentives to provide a higher number of residential units than would otherwise be permitted, thereby facilitating the creation of a higher number of affordable units and addressing the need for affordable housing in the City. Additionally, the project is a Density Bonus development located one block north of Venice Boulevard within a Tier 2 TOC area. Thus, the project will contribute towards the creation of sustainable neighborhoods and a reduction in vehicle trips and VMT.

In addition, the project has been conditioned to comply with the electric vehicle requirements of state law. The project has also been conditioned to provide solar infrastructure. Together, these conditions further support applicable policies in the Health and Wellness Element, Air Quality Element, and Mobility Element of the General Plan by reducing the level of pollution/greenhouse gas emissions, ensuring new development is compatible with alternative fuel vehicles, and encouraging the adoption of low emission fuel sources and supporting infrastructure. These conditions also support good planning practice by promoting overall sustainability and providing additional benefits and conveniences for residents, workers, and visitors.

The project contributes to and furthers several applicable goals, objectives, and policies of the plans that govern land use and development in the City. Therefore, the project substantially conforms with the purpose, intent, and provisions of the General Plan and the West Los Angeles Community Plan.

In addition to the above findings set forth in Section 12.24-E of the LAMC, the City Planning Commission shall find that:

5. The project is consistent with and implements the affordable housing provisions of the Housing Element of the General Plan.

The City's Housing Element for 2013-2021 was adopted by the City Council on December 3, 2013. The Housing Element of the General Plan will be implemented by the recommended action herein. The Housing Element is the City's blueprint for meeting housing and growth

challenges. It identifies the City's housing conditions and needs, reiterates goals, objectives, and policies that are the foundation of the City's housing and growth strategy, and provides the array of programs the City has committed to implement to create sustainable, mixed-income neighborhoods across the City.

As discussed in Finding No. 4, the project, including 32 market-rate units and 11 units reserved for Very Low Income Households, is consistent with many of the goals and objectives of the Housing Element of the General Plan.

6. The project contains the requisite number of Restricted Affordable Units, based on the number of units permitted by the maximum allowable density on the date of application, as follows:

a. 11% Very-Low Income Units for a 35% density increase; or

b. 20% Low Income Units for a 35% density increase; or

c. 40% Moderate Income Units for a 35% density increase in for-sale projects.

The project may then be granted additional density increases beyond 35% by providing additional affordable housing units in the following manner:

a. For every additional 1% set aside of Very-Low Income Units, the project is granted an additional 2.5% density increase; or

b. For every additional 1% set aside of Low Income Units, the project is granted an additional 1.5% density increase; or

c. For every additional 1% set aside of Moderate Income Units in for-sale projects, the project is granted an additional 1% density increase; or

d. In calculating the density increase and Restricted Affordable Units, each component of any density calculation, including base density and bonus density, resulting in fractional units shall be separately rounded up to the next whole number.

The subject 15,334.6 square foot property is zoned R3-1 and RD2-1, which limits density to one (1) dwelling unit per 800 square feet of lot area and one (1) dwelling unit per 2,000 square feet of lot area, respectively. The permitted base density on the subject property is 14 units (9.3 [rounded up to 10] plus 3.7 [rounded up to 4]).[2] In exchange for reserving a portion of the units for affordable housing, the applicant is entitled to a maximum 35 percent density bonus by-right. The applicant is seeking an additional 166 percent density bonus (or a total of a 201 percent density bonus) through a Conditional Use to allow for the proposed 43 dwelling units to be built on the site.

Pursuant to the LAMC and California Government Code Section 65915, a Housing Development Project that sets aside a certain percentage of units as affordable, either in rental or for-sale units, shall be granted a corresponding density bonus, up to a maximum of 35 percent. While these provisions are limited to 35 percent, Government Code Section 65915(f) states that "the amount of density bonus to which an applicant is entitled shall vary according to the amount by which the percentage of affordable housing units exceeds percentage established." As such, in instances where a project is seeking a density bonus increase that is more than 35 percent, the amount of required units that are set aside as

[2] Assembly Bill 2501 clarifies that density calculations that result in a fractional number are to be rounded up to the next whole number. This applies to base density, number of bonus units, and number of affordable units required to be eligible for the density bonus.

affordable shall vary depending on the requested amount of density bonus. Therefore, it is appropriate that any project that requests a density bonus increase beyond 35 percent would extend the existing set-aside charts located in Section 12.22-A,25 of the LAMC. LAMC Section 12.24-U,26, which implements this provision of State law, states, as a Conditional Use, a project may be granted additional density increases beyond the 35 percent maximum by providing additional affordable housing units. Consistent with this Section, the table below illustrates how the maximum allowable Density Bonus increases for every unit set aside for Very Low Income Households (2.5 percent density increase for every additional one (1) percent of Very Low Income units provided), based on the base density and the chart prescribed in Section 12.22-A,25 of the LAMC.

Density Bonus Percentages

Very Low Income Units (Percentage of Base Density)	Maximum Density Bonus Permitted (Based on Base Density)
*5 %**	*20 %**
*6 %**	*22.5 %**
*7 %**	*25 %**
*8 %**	*27.5 %**
*9 %**	*30 %**
*10 %**	*32.5 %**
*11 %**	*35 %**
*76 %**	*197.5 %**
*77 %**	*200 %**
78 %*	***202.5 %****
*79 %**	*205 %**
*80 %**	*207.5 %**

For the subject property, a 35 percent by-right density bonus would allow for 19 units (equal to an increase of five [5] units beyond the 14-unit base density) to be constructed on the project site. As illustrated in Table above, in order to qualify for the 35 percent by-right density bonus, the project would be required to set aside 11 percent of the base density, or two (2) units, for Very Low Income Households. The applicant is seeking an additional 166 percent density bonus (for a total of 201% density bonus from the base density) through a Conditional Use to allow for a total of 43 dwelling units, representing an increase of 24 units beyond what would otherwise be permitted through the by-right 35 percent density bonus. In order to obtain the additional requested 166 percent density bonus, as shown in the table above, the project must set aside at least 78 percent of the base density, equal to 10.92 units, for Very Low Income Households. The project proposes to set aside 11 units for Very Low Income Households in exchange for the requested Density Bonus.

7. The project meets any applicable dwelling unit replacement requirements of the California Government Code Section 65915(c)(3).

The project proposes the demolition of two existing duplexes (a total of four units). Per the Housing Crisis Act (SB 8) Replacement Unit Determination Letter dated June 6, 2022, three (3) of the existing units need to be replaced with equivalent type (two Very Low Income and one Low income or deeper levels of affordability). Additionally, the fourth existing unit must be replaced consistent with the City's Rent Stabilization ordinance. The project's 11 proposed Very Low Income units will fulfill the replacement housing requirements of this

determination. Therefore, the project will meet the applicable dwelling unit replacement requirements of the California Government Code Section 65915(c)(3).

8. **The project's Restricted Affordable Units are subject to a recorded affordability restriction of 55 years from the issuance of the Certificate of Occupancy, recorded in a covenant acceptable to the Housing Department, and subject to fees as set forth in Section 19.14 of the LAMC.**

   The proposed project has been conditioned to record a covenant for affordability restriction for the 11 Very Low Income units of a period of 55 years from the issuance of the Certificate of Occupancy, to the satisfaction of the Los Angeles Housing Department, and subject to fees as set forth in Section 19.14 of the LAMC.

9. **The project addresses the policies and standards contained in the City Planning Commission's Affordable Housing Incentives Guidelines.**

   The City Planning Commission approved the Affordable Housing Incentives Guidelines (under Case No. CPC-2005-1101-CA) on June 9, 2005. The Guidelines were subsequently approved by the City Council on February 20, 2008, as a component of the City of Los Angeles Density Bonus Ordinance. The Guidelines describe the density bonus provisions and qualifying criteria, incentives available, design standards, and the procedures through which projects may apply for a density bonus and incentives. LAHD utilizes these Guidelines in the preparation of Housing Covenants for Affordable Housing Projects. The Guidelines prescribe that the design and location of affordable units be comparable to the market rate units, the equal distribution of amenities, LAHD monitoring requirements, affordability levels, and procedures for obtaining LAHD sign-offs for building permits.

   The project will result in 43 new dwelling units, of which 11 will be reserved for Very Low Income Household occupancy and the remainder will be offered as market rate units. All residents of the proposed project will have access to all common and open space amenities within the building. The restricted units will comply with affordability requirements in the Guidelines set forth by LAHD in conformance with US Department of Housing and Urban Development (HUD). Additionally, as part of the building permit process, the applicant will execute a covenant to the satisfaction of LAHD who will ensure compliance with the Guidelines. Therefore, the project will address the policies and standards contained in the Guidelines.

CEQA Findings

10. **Class 32 Categorical Exemption.** The proposed project qualifies for a Class 32 Categorical Exemption because it conforms to the definition of "In-fill Projects". The project can be characterized as in-fill development within urban areas for the purpose of qualifying for Class 32 Categorical Exemption as a result of meeting five established conditions and if it is not subject to an Exception that would disqualify it. The Categorical Exception document dated July 2023 and attached to the subject case file provides the full analysis and justification for project conformance with the definition of a Class 32 Categorical Exemption.

PUBLIC HEARING AND COMMUNICATIONS

A public hearing for Case No. CPC-2023-0582-CU-DB-HCA-PHP was held by the Hearing Officer via teleconference on Tuesday, April 18, 2023, at approximately 2:00 p.m.

1. Attendees

The hearing was attended by representatives of the applicant (Brian Silveira and Jesi Harris), LA City Planning staff, and approximately 30 members of the public.

2. Testimony

a. Brian Silveira and Jesi Harris, the applicant's representatives, presented the project and highlighted features of the neighborhood including walkability and proximity to transit and job sites. Harris also highlighted the design and project features.

b. A total of 26 people provided verbal public comment with 22 members of the public opposed to the project and four (4) stating support. A summary of each comment is provided below.

Speaker #1 -- SUPPORT
- **Taylor,** local resident, 3rd generation Angelino.
- Support affordable units. Have seen people move due to raising housing costs. My dad is 70 and is threatened by rising housing costs. This will help people stay close to their families. This can change lives. Hope this gets done quickly. Need more projects like this.

Speaker #2 -- OPPOSE
- **Lee Wallach**, representative of Motor Avenue Improvement Association
- Insufficient outreach. It is not just about coming to a Neighbor Council meeting, we have not heard anything else from the developer or representatives. We are not a NIMBY community. Outreach discussions need to happen.
- Impacts will be considerable. Mitigations are needed under CEQA.
- Single units won't bring families
- Ask that all the requests be denied, we don't feel that this project as it stands is appropriate. We need real community outreach and input.

Speaker #3 -- OPPOSE
- **Katherine**, local resident, lives on same block two streets over
- Out of scale. Two stories taller than anything within a square mile. Remove two stories and provide more space (yards) on all sides.
- Palms is the highest density Neighborhood on Westside
- Spanish style architecture preferred.
- This project is plopping a five story prison in this neighborhood
- Wrong project for the area
- Very narrow street, one lane each direction. Construction equipment concerns.
- Too many concessions. Very residential area
- There was no consideration given to the neighborhood when this was developed

Speaker #4 -- OPPOSE
- **David Lefebvre**, local resident, three parcels north of the site.
- Objection to incentives and waivers
- Insufficient outreach. Not enough community input. Had not heard of this project until the hearing notice. I have been scrambling to educate myself and neighbors, to unite and come together. Proud of the community for uniting.
- I consider myself civically minded, I didn't even know the PLUM committee of the Neighborhood Council was a thing.
- Timing of the hearing is difficult for many. Attendance represents a fraction of the opposition.
- I started a petition. 170 signatures now. The petition details concerns.
- Out of scale. Too high. Too large. This building shocks the conscious. Take a look at the neighborhood and the building. I understand that many of the incentives are a matter of state and local law, and the building includes affordable units. If there is any discretion, reject the extra exemptions being requested.
- Considering legal action in the hope that we can reach an outcome that is in the best interest of the community.

Speaker #5 -- OPPOSE
- **Reanna Cruz,** local resident, award winning audio journalist.
- Need a quiet, low noise environment for work. I will be across the street from the development. Months or years of noise and construction will prevent me from doing my job and will cause undue stress.
- No outreach
- I come from a low income family, if there was more than just 11 units of affordable housing I might support this.
- Many exceptions are requested which gives me further pause that it will be handled with care, because already there is a lackadaisical attitude.
- Hearing is in the middle of a work day.People on this street are opposed. Massive disruption to livelihood.

Speaker #6 -- SUPPORT
- **Mica O'Brien,** local resident, Graduate student at UCLA
- Important to increase density across the city, especially in this dense neighborhood of Palms.
- Important to create a places where people of all incomes can live together
- This project helps us reach our climate goals more quickly
- Palms is a wonderful area, this project makes sure others can live her too
- Appreciate trees
- In the midst of an extreme housing crisis

Speaker #7 -- OPPOSE
- **A.J. R.,** local resident on Kelton Avenue, two parcels down from proposed project.
- Opposed to exemptions and concessions. The regulations are in place for a reason.
- Critique of referring to this project as mixed income because it is not majority low income, it is only ¼ low income.
- 3712 Kelton Ave is the large dark, three-story building -- it is three stories too high.

- Out of scale. Too tall. Opposed to the 17-foot height increase. Buildings on Overland Avenue are creating shadows, we are losing too much sun in Palms.
- Not what the neighborhood is about. We [the community] are trying to create a nice happy place. This is a vanity project.

Speaker #8 -- OPPOSE
- **Macreed,** local resident, 100 feet from site.
- Too many exemptions. Waivers are designed to increase the project's size and the revenue for owners. They are doing this to maximize return on investment.
- Out of scale. Nothing this size on Venice Boulevard or in Palms. At least two stories too tall. Prefer a two or three story building or a fourplex. Three stories would be acceptable.
- Too much density - It's great that people want to have housing, and increase density, but this is increasing the density of an already densely populated space.
- Bikes - I can't see anyone using the bike parking, have never seen a bike here.
- Shade/Shadow concerns - People across the street won't get sun until 10 AM.
- Narrow street - Kelton Avenue is a two lane street with parking on both sides.
- Parking issues

Speaker #9 -- OPPOSE
- **Sasha Urban,** local resident on Kelton Avenue, a few houses down
- Insufficient parking. 76 bike parking spaces is overkill when there is not even one parking spot per apartment. Biking is not popular in this neighborhood. We know each unit will have at least one car if not two.
- Object to the idea that this would be attractive to non car owners. As someone who drives but enjoys walking, Palms is not designed well for walking. North of Charnock has no sidewalks.
- Construction impacts to roadways. Construction site would block traffic at the three way intersection with Kelton Avenue and Regent Street.
- Aesthetics. Architecture does not match the neighborhood which has a lot of stylish homes with color, classic california. Proposed building would not be attractive. Is a fantasy that this is a good investment for the community.
- This is just a way for the developer to get government subsidies.

Speaker #10 -- OPPOSE
- **Kyle Hollar,** local resident, renter for 12 years on Kelton Avenue towards Venice
- This is not the way to approach the crisis of affordability and availability.
- Project is not near enough to an arterial road.
- The city has not shown they want to nurture development. This is not a safe place to bike or walk. Cars are speeding. Lack of sidewalks in the area, specifically Regent street directly across from this building.
- We don't need to allow these exemptions, they do not seem reasonable to me. Opposed to open space waiver, opposed to side yard waivers.
- Parking. We already lack parking. I have two spaces for my unit and guest parking is very difficult. Can't imagine how difficult it is for people who rely on street parking. Bike parking should not be something the building is hiding behind.

Speaker #11 -- OPPOSE
- **Albert,** local resident, across the street.
- In favor of low income housing and housing diversity. This is not the answer.
- No outreach was done.
- This feels like it was designed for Venice Boulevard or Sepulveda Boulevard and was copied and pasted here [on Kelton Avenue].
- Traffic. Regardless of public transit options, this will bring cars. Need speed bumps.
- Stormwater and sewage concerns. During rains, sewage was flowing into the streets, infrastructure cannot keep up, this project does not help. Regent Street [in front of the project site] is where the water tends to pool up.

Speaker #12 -- OPPOSE
- **Kelly Lally,** local resident, across the street.
- Lack of notice. Found out from notice in the mail. We are in a situation of trying to scramble, to figure out what is going on. People think the Hearing Notice is junk mail or a solicitation.
- Building is a shock of the conscious. This is a neighborhood with single family homes and small dingbat style apartments. Can we have smaller, shorter buildings?
- Big advocate for Affordable Housing. But this is not the solution, this is not the neighborhood for it. Neighborhood is too small. On Venice Boulevard or Sepulveda Boulevard this project would make sense. Not here.
- Traffic and Parking. Sometimes it takes 5 min to get down the road, there is nowhere to pull over, cars on both sides, forcing their way though. Assume each resident is going to have two cars. I understand it is very generous for the developer to provide 33 spaces.
- The place to start is to focus on building sidewalks in the neighborhood.
- Too many exemptions. Regulations are in place to protect residents, to prevent unchecked development.
- Construction Noise. I am the primary wage earner. I work from home. How loud is the equipment going to be? Apartment building two parcels down took two years to build.
- Trees. There are 20 mature trees on site. Can we keep green space intact?
- My landlord is going to capitalize on this.
- Property values will go down.

Speaker #13 -- OPPOSE
- **N.,** local resident, live next door. From New York City, lived in the Palms years.
- This will lower my quality of life.
- Outside of the 11 affordable units, who are the people who are being displaced?
- Over concentration population.
- I had my bike stolen.
- Need stop signs [at Regent and Kelton]
- One positive note is the neighborhood coming together to sign David's petition.
- Focus should be on people here, not people who are going to come in and ruin the neighborhood.

Speaker #14 -- OPPOSE
- **Tonie Pimienta**, local resident, Kelton Avenue
- Would have a significant direct and quantifiable impact. Significant effects related to traffic, noise, quality of life. The 43 unit tower would be excessively large. Nearest building remotely resembling it is on Overland (5 lanes). Stark opposition to our little Kelton. Offensive that [the developer states that] there would be no impacts.
- Strain inadequate parking situation. Increased traffic. Car was sideswiped by another car. No space [on the road]. I am impacted everyday by cars jutting into my driveway parking in spaces that are not parking spaces.
- Construction impacts: construction vehicles, noise, dust, workers looking for parking.
- Conflicts with fundamental purposes of zoning laws. Giving development carte blanche to ignore development stands. Sets a dangerous precedent.
- The Neighborhood Council PLUM committee meeting was set during Ramadan.
- We have all been working hard to play catchup.

Speaker #15 -- OPPOSE
- No name given. Local resident at 3648 Kelton. Four houses down.
- Cannot believe this project comes in front of the City and the City approves it. Street is so small. I don't know how they approve the projects on Overland, they have to come in and park on this side of Overland.
- Don't see any bicycles on Venice Boulevard. On weekends some bikes.
- Inadequate parking. Used to be two cars per apartment. Here there is not even one parking space per apartment. They are all going to have a car and fight for parking.
- Should build according to the City ordinance.
- I have concerns why this came to the hearing because you guys are approving all these waivers. This should not ever come to this stage. This is a waste of time for people living in nice areas like this.

Speaker #16 -- SUPPORT
- **S.K. Lee,** Resident of Plams.
- Project design looks fine.
- Severe housing shortage. This developer would provide much needed units. I hear people saying “it's only 11 affordable units” but there are currently only four units on site. It sounds like however many affordable units are proposed, it would never be enough.
- I live on a street in Palms that is the same size. Lots of similar projects to this on my narrow street.
- Parking is fine. Not everyone has a car. I share a car with my husband.
- Area is walkable.
- Project is following the rules it should, I don't think the variances are unreasonable.
- Support approving the project in accordance with housing laws and needs. Palms is a high resourced neighborhood and transit accessible. I use the bus all the time. Palms Expo Station. There are future bike lanes for the area.
- Not having sidewalks on Regent Street is not a reason to deny the project.
- People say they are in support of the project because of affordable housing, but say this is not a good place for the project. What is it if it’s not NIMBYism? If not here, then where?

Speaker # 17 -- OPPOSE
- **M.C.,** local resident, four houses down.
- Supporter of affordable housing. Opposed to this project.
- It is difficult on this street with parking and traffic congestion. Can be dangerous because there is cut-through traffic. Hit multiple times by people not paying attention.
- 43 units is 86 more people, probably 86 more cars, most people, many people each own cars. The city is so large. And the transit is not accessible enough.
- To address where the houses should be built: Venice Blvd would be fine. Would probably support that. Not realistic here [on Kelton Avenue].

Speaker #18 -- OPPOSE
- **Yumi,** homeowner, next door.
- Lack of outreach. Found out about this two weeks ago
- Out of scale/character: Neighborhood is characterized by a lot of nice single family homes. We would not like this 5-story building next to us.
- Parking on the street is crowded
- Community had to build a bump so people go slower.

Speaker #19 -- OPPOSE
- **Ryan Todd**
- Great to hear community organizing is happening.
- Parking is a problem. People park here for restaurants on Venice Boulevard.
- Traffic exacerbated by calming efforts on parallel streets which make Kelton a main thoroughfare.
- Not sure who is in support. I find it offensive that they bring up NIMBYism.

Speaker #20 -- OPPOSE
- **Chad Q.**
- Opposed to each variance
- Out of scale. Too tall. Shadow over neighborhood.
- Parking. Households will need more than one car. Households do not work in the immediate area.
- Developer has not visited the property
- If not here? With the rest of the monstrosities on Overland Avenue and Venice Boulevard.

Speaker #21 -- OPPOSE
- **Manzar Foroohar,** local resident, corner of Regent Street and Kelton Avenue.
- This project will put a shadow on my house. No morning sunshine.
- No privacy. No privacy in my living room which has a sliding door facing this project.
- I agree with every point my neighbors have made.
- Parking is nightmare
- Kelton Avenue is dangerous for driving and biking.

Speaker #22 -- SUPPORT
- **Gabby**
- Parking in the neighborhood is not a problem
- There needs to be more affordable housing and increased supply
- I am not sure what [other commenters] are talking about when they say it will change the character of the neighborhood, Palms has always been dense. I have lived here my whole life.
- It will elevate gentrification in other areas by building more housing in this neighborhood.

Speaker #23 -- OPPOSE
- **Chelsea**
- Development in the neighborhood is important, I understand the need to support housing density in LA. But I disagree with the exceptions.
- Parking issues. Patrons of bars park here.
- Dangerous for bikers and pedestrians. I am a runner, nearly hit by cars.
- Scale of project is unreasonable

Speaker #24 -- OPPOSE
- **Sky**
- Concerns about street parking for construction and safety. Project would need an 80-man crew. That amount of vehicles on the street is not safe. Is there a Traffic Control Plan or safety precautions in place?
- Transformer vault or larger transformers? Keep the beauty of the neighborhood.
- Streets ripped up for placement of utilities.
- Concerns with lack of transparency.

Speaker #25 -- OPPOSE
- No name provided.
- Project doesn't make sense. Why is this not on a bigger street -- Venice or Overland?
- Developer wants more families to come, but there is not enough parking.
- Feels like a money scheme project. Selfish.
- Shade/Shadow: Sunlight is necessary for mental health. This will block sunlight.
- 11 housing units for low income does not fit the housing crisis.

Speaker #26 -- OPPOSE
- **J. Hutchinson**
- Blindsided by the construction of this project.
- Out of scale. We don't mind an apartment building but not at this size.
- Shade/Shadow concerns.
- Parking. Need more parking than 24 parking spaces for 43 units.
- Construction: Going to take years to build. Where will the workers park?
- Infrastructure: Past sewage backup in homes.
- Outreach: Should have communicated with the community before getting this far.
- Daycare not far from there. Concern for children.

c. The applicant's team responded by Apologizing that people heard about this project from the hearing notice and said they plan to discuss with the public further. The applicant stated that the project is not near approval and that no decision is being made at this hearing, explaining that this project will be decided by the City Planning Commission.

d. Planning Staff and Hearing Officer, Renata Ooms, asked for clarification regarding the side yard dimensions, transformer location, and affordable units.

The applicant representative confirmed the side yard dimensions, noted the vaulted transformer location in the driveway, and provided context for Density Bonus projects, the housing crisis, and affordable housing development by for-profits and nonprofits. The applicant representative also noted that the number of parking spaces would be increasing from 24 to 33.

The Hearing Officer asked the applicant team to revisit the landscape plan to determine if additional trees could be accommodated, instructed the applicant to work with the community to communicate concussion best practices and develop solutions to construction-related concerns.

The Hearing Officer announced that the City Planning Commission date had not yet been scheduled and that a hearing notice would be mailed out to the 500-foot radius and to any interested parties. The Hearing Officer provided instructions to the public for submitting written comments and getting on the interested parties mailing list to receive notice for the City Planning Commission hearing.

Written Testimony

As of the preparation of this Staff Report, Planning staff received comment letters from over 40 individuals (approximately 60 letters were received with many individuals sending multiple letters). One comment letter stated support for the project while all others stated opposition. Two online petitions opposing the project were created as well. Comment letters and screen shots of the online petitions are attached in this report as Exhibit F.

Neighborhood Council

As of the preparation of this Staff Report, the project representatives had presented at the PLUM Committee meeting of the Palms Neighborhood Council. The PLUM Committee passed a motion to support the project with a 3-1 vote.

K E L T O N A V E

PROJECT INFORMATION

PROJECT SUMMARY:	NEW 5 STORY RESIDENTIAL BUILDING WITH 43 APARTMENT UNITS (32 MARKET RATE, 11 AFFORDABLE) + 1 FLOOR OF SUBTERRANEAN PARKING PROJECT UTILIZES INCENTIVES PER LAMC 12.22A.25
PROJECT ADDRESS:	3676 & 3704 KELTON AVE., LOS ANGELES, CA 90034
LOT SIZE:	5,053.9 SF, 5,286.4 SF, 4,994.3 SF
LOT AREA:	15,334.6 SF
ASSESSOR'S PARCEL #:	4252024020, 4252025042
TRACT:	TR 5848
MAP REFERENCE:	M B 67-57 (SHT 2), M B 61-59 (SHT 1)
BLOCK:	NONE
LOT:	77, VAC 20090809493, 19
CENSUS TRACT:	2718.04,
ZONING:	RD2-1, R3-1
HILLSIDE AREA:	NO
SPECIFIC PLAN AREA:	WEST LOS ANGELES TRANSPORTATION IMPROVEMENT & MITIGATION
METHANE HAZARD SITE:	NONE
SPECIAL GRADING AREA:	YES
USE:	RESIDENTIAL
HEIGHT LIMIT:	45'-0"
BUILDING FOOTPRINT:	
BUILDING SQUARE FOOTAGE:	42,490 SF
BUILDING HEIGHT:	62'-0"
TYPE OF CONSTRUCTION:	
OCCUPANCY GROUP:	R2 & S-2
NUMBER OF STORY:	5 + 1 FLOOR SUBTERRANEAN
PARKING REQUIREMENT:	45 PARKING SPACES
PARKING PROVIDED:	33 PARKING SPACES
SPRINKLER:	NFPA-13

F.A.R. PROVIDED VS. REQUIRED

PROVIDED		ALLOWED	
RESIDENTIAL FLOOR AREA:	39,011 SF	TOTAL LOT AREA:	15,334.6 SF
(F.A.R.) x 10,530 SF = 39,011 SF		BUILDABLE AREA:	10,530 SF
F.A.R. PROVIDED:	3.71:1	F.A.R. ALLOWED:	3:1
		(3) 10,530 SF =	31,590 SF
TOTAL PROVIDED:	**39,011 SF**	**TOTAL ALLOWED:**	**31,590 SF**

RESIDENTIAL DENSITY PROVIDED VS. REQUIRED

PROVIDED		ALLOWED	
DWELLING UNITS:	43 DWELLING UNITS	RD2 LOT AREA:	5,053.9 + 2,643.2 - 225 = 7,472 SF
MARKET RATE:	32	7,472 / 2,000 =	3.7
VERY LOW INCOME:	11	R3 LOT AREA:	4,994.3 + 2,643.2 - 225 = 7,412 SF
		7,412 / 800 =	9.3
		3.7 + 9.3 =	14
TOTAL PROVIDED:	**43 DWELLING UNITS**	**TOTAL ALLOWED:**	**14 DWELLING UNITS**

PARKING PROVIDED VS. REQUIRED

PROVIDED		REQUIRED	
TENANT PARKING:	33 SPACES	TENANT PARKING: 18 @ < 3 HABITABLE ROOMS (1 SPACE / UNIT) (18 UNITS) (1 SPACE) =	18 SPACES
		11 @ 3 HABITABLE ROOMS (1.5 SPACES / UNIT) (11 UNITS) (1.5 SPACES) =	17 SPACES
		14 @ > 3 HABITABLE ROOMS (2 SPACES) (14 UNITS) (2 SPACES) =	28 SPACES
		TOTAL =	**63 SPACES**
		BICYCLE ORD. 30% PARKING REDUCTION (18 SPACES REPLACED BY BICYCLE PARKING)	
TOTAL PROVIDED:	**33 SPACES**	**TOTAL:**	**45 SPACES**

BICYCLE PARKING PROVIDED VS. REQUIRED

PROVIDED		REQUIRED	
LONG TERM BICYCLE PARKING:		LONG TERM BICYCLE PARKING:	
RESIDENTIAL:	64 SPACES	RESIDENTIAL: 1 / 1.0 UNITS 25 / 1.0 =	25 SPACES
		1 / 1.5 UNITS 18 / 1.5 =	12 SPACES
SHORT TERM BICYCLE PARKING:			**37 SPACES**
RESIDENTIAL:	12 SPACES	SHORT TERM BICYCLE PARKING:	
		RESIDENTIAL: 1 / 10 UNITS 25 / 10 =	2.5 → 3 SPACES
		1 / 15 UNITS 18 / 15 =	1.2 → 2 SPACES
			5 SPACES
TOTAL LONG TERM ROVIDED:	64 SPACES	TOTAL LONG TERM REQUIRED:	37 SPACES
TOTAL SHORT TERM PROVIDED:	12 SPACES	TOTAL SHORT TERM REQUIRED:	5 SPACES

OPEN SPACE PROVIDED VS. REQUIRED

PROVIDED		REQUIRED	
COMMON OPEN SPACE:		29 UNITS @ < 3 HABITABLE ROOMS (100 SF) (29 UNITS) (100 SF) =	2,900 SF
COMMON GROUND FLOOR:	1,977 SF		
COMMON ROOF DECK:	2,150 SF	14 UNITS @ 3 HABITABLE ROOMS (125 SF) (14 UNITS) (125 SF) =	1,750 SF
PRIVATE OPEN SPACE TOWARDS CALCULATION BASED ON ZONING CODE 12.21 G: 16 UNITS x 50 SF	800 SF	0 UNITS @ > 3 HABITABLE ROOMS (175 SF) (0 UNITS) (175 SF) =	0 SF
TOTAL OPEN SPACE PROVIDED:	4,927 SF	TOTAL OPEN SPACE REQUIRED:	4,650 SF

ACTIONS REQUESTED

THE REQUEST CONFORMS TO THE FOLLOWING REQUIREMENTS AS FOLLOWS:

1. CODE SECTION FROM WHICH RELIEF IS REQUESTED: CODE SECTION WHICH AUTHORIZES RELIEF: 12.24 U 26

A CONDITIONAL USE PERMIT PURSUANT TO LAMC 12.24 U 26 TO PERMIT A DENSITY BONUS FOR A PROJECT FOR WHICH THE DENSITY INCREASE IS GREATER THAN THE MAXIMUM 35% PERMITTED IN LAMC SECTION 12.22 A 25; IN CONJUNCTION WITH THE CONSTRUCTION, USE, AND MAINTENANCE OF 43 FOR-RENT DWELLING UNITS IN LIEU OF THE 14 DWELLING UNITS OTHERWISE PERMITTED BY LAMC 12.22 A 25; WITH 11 DWELLING UNITS RESERVED FOR VERY LOW INCOME HOUSEHOLDS; AND PURSUANT TO LAMC SECTION 12.24 F:

INCENTIVES AND WAIVERS

• FROM MENU OF INCENTIVES (12.22 A 25):

- PERMIT A 10% DECREASE IN REQUIRED NORTHERLY SIDE YARD SETBACK TO ALLOW A 7-FOOT 2-INCH SIDE YARD SETBACK IN LIEU OF THE 8 FEET REQUIRED BY THE RD2 AND R3 ZONES PURSUANT TO LAMC 12.09.1. B.2.(A) AND LAMC 12.10. C 3, RESPECTIVELY.

- PERMIT A 10% DECREASE IN REQUIRED SOUTHERLY SIDE YARD SETBACK TO ALLOW A 7-FOOT 2-INCH SIDE YARD SETBACK IN LIEU OF THE 8 FEET REQUIRED BY THE RD2 AND R3 ZONES PURSUANT TO LAMC 12.09.1. B.2.(A) AND LAMC 12.10. C. 3, RESPECTIVELY.

- PERMIT THE AVERAGING OF ALLOWABLE DENSITY OVER THE PROJECT SITE LOCATED ON TWO CONTIGUOUS PARCELS ZONED RD2-1 AND R3-1.

• ON- AND OFF-MENU WAIVERS OF DEVELOPMENT STANDARDS:

- PERMIT A 17-FOOT INCREASE IN HEIGHT TO 62 FEET WITH NO 1-1 STEPBACK AFTER 45 FEET OF HEIGHT IN LIEU OF THE MAXIMUM 45 FEET ALLOWED IN THE RD2-1 AND R3-1 ZONES PURSUANT TO LAMC 12.21.1.

- PERMIT A 23% INCREASE IN ALLOWED FLOOR AREA RATIO TO ALLOW 39,011 SQUARE FEET OF FLOOR AREA IN LIEU OF THE 31,590 SQUARE FEET PERMITTED PURSUANT TO LAMC 12.21.1 A 1.

VICINITY MAP

CODE

BUILDING CODE:	2020 LABC, (TITLE 24, PART 2.5)
	BASED ON THE 2018 IRC (INCLUDES ACCESSIBILITY)
STRUCTURAL:	2020 LABC, VOL 2 (TITLE 24, PART 2, VOL 2)
	BASED ON THE 2018 IBC WITH ASCE 7-16
MECHANICAL CODE:	2019 CA MECHANICAL CODE, (TITLE 24, PART 4)
	BASED ON THE 2018 UNIFORM MECHANICAL CODE
PLUMBING CODE:	2019 CA PLUMBING CODE (TITLE 24, PART 5)
	BASED ON THE 2018 UNIFORM PLUMBING CODE
ELECTRICAL CODE:	2019 CA ELECTRICAL CODE (TITLE 24, PART 3)
	BASED ON THE 2017 NATIONAL ELECTRIC CODE
ENERGY CODE:	2019 CA ENERGY CODE (TITLE 24, PART 6) 2020 CITY OF LOS ANGELES GREEN BUILDING CODE

PROJECT DIRECTORY

OWNER
NAME: LG ALLIANCE CORP.
ADDRESS: 9440 SANTA MONICA BLVD., SUITE 301 BEVERLY HILLS, CA 90210

STRUCTURAL ENGINEER
NAME:
ADDRESS:
PHONE NO.:

TITLE 24
NAME:
ADDRESS:
PHONE NO.:

ARCHITECT
NAME: BREAKFORM DESIGN
ADDRESS: 127 ARENA STREET EL SEGUNDO, CA 90245
PHONE NO.: 310-322-3700

LAND SURVEYOR
NAME:
ADDRESS:
PHONE NO.:

TRACT MAP
NAME:
ADDRESS:
PHONE NO.:

RESIDENTIAL UNITS

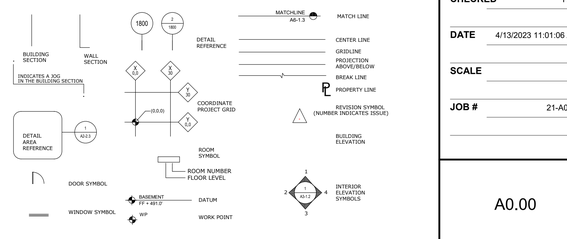
UNIT #	OCCUPANCY	S.F.	TYPE
101	1 BD / 1 BTH	974 SF	AFFORDABLE
103	/ 1 BTH	469 SF	MARKET RATE
104	/ 1 BTH	551 SF	AFFORDABLE
201	2 BD / 2 BTH	842 SF	MARKET RATE
202	/ 1 BTH	525 SF	MARKET RATE
203	2 BD / 2 BTH	988 SF	AFFORDABLE
204	1 BD / 1 BTH	712 SF	MARKET RATE
205	/ 1 BTH	434 SF	MARKET RATE
206	/ 1 BTH	434 SF	MARKET RATE
207	2 BD / 2 BTH	966 SF	AFFORDABLE
208	2 BD / 2 BTH	955 SF	MARKET RATE
209	/ 1 BTH	490 SF	AFFORDABLE
210	1 BD / 1 BTH	849 SF	MARKET RATE
301	2 BD / 2 BTH	842 SF	MARKET RATE
302	/ 1 BTH	525 SF	MARKET RATE
303	2 BD / 2 BTH	988 SF	MARKET RATE
304	1 BD / 1 BTH	712 SF	AFFORDABLE
305	/ 1 BTH	434 SF	MARKET RATE
306	/ 1 BTH	434 SF	AFFORDABLE
307	2 BD / 2 BTH	966 SF	MARKET RATE
308	2 BD / 2 BTH	955 SF	AFFORDABLE
309	/ 1 BTH	490 SF	MARKET RATE
310	1 BD / 1 BTH	849 SF	MARKET RATE
401	2 BD / 2 BTH	842 SF	MARKET RATE
402	/ 1 BTH	525 SF	MARKET RATE
403	2 BD / 2 BTH	988 SF	MARKET RATE
404	1 BD / 1 BTH	712 SF	MARKET RATE
405	/ 1 BTH	434 SF	MARKET RATE
406	/ 1 BTH	434 SF	AFFORDABLE
407	2 BD / 2 BTH	966 SF	MARKET RATE
408	2 BD / 2 BTH	955 SF	MARKET RATE
409	/ 1 BTH	490 SF	AFFORDABLE
410	1 BD / 1 BTH	849 SF	MARKET RATE
501	2 BD / 2 BTH	842 SF	AFFORDABLE
502	/ 1 BTH	525 SF	MARKET RATE
503	2 BD / 2 BTH	988 SF	MARKET RATE
504	1 BD / 1 BTH	709 SF	MARKET RATE
505	/ 1 BTH	434 SF	MARKET RATE
506	/ 1 BTH	434 SF	MARKET RATE
507	1 BD / 1 BTH	683 SF	MARKET RATE
508	1 BD / 1 BTH	656 SF	MARKET RATE
509	/ 1 BTH	490 SF	MARKET RATE
510	1 BD / 1 BTH	849 SF	MARKET RATE

SHEET INDEX

Sheet Number	Sheet Name
A0.30.2	Unnamed
ARCHITECTURAL	
A0.00	COVER
A0.01	GENERAL NOTES
A0.01B	TRANSIT STOP LOCATION & SCHEDULE
A0.02	APPROVAL LETTERS
A0.03	GREEN FORMS
A0.04	SPECS / RESEARCH REPORTS
A0.05	SPECS / RESEARCH REPORTS
A0.06	SPECS / RESEARCH REPORTS
A0.07	SPECS / RESEARCH REPORTS
A0.08	DOOR SCHEDULE
A0.09	DOOR SCHEDULE
A0.10	WINDOW SCHEDULE
A0.11	DOOR DETAILS
A0.12	WINDOW DETAILS
A0.13	WALL & FLOOR TYPES
A0.13.1	WALL & FLOOR TYPES
A0.14	GENERAL DETAILS
A0.15	GENERAL DETAILS
A0.16	GENERAL DETAILS
A0.17	ACCESSIBILITY NOTES & DETAILS
A0.18	ACCESSIBILITY NOTES & DETAILS
A0.19	ACCESSIBILITY NOTES & DETAILS
A0.20	FIRE LIFE SAFETY
A0.21	FIRE LIFE SAFETY
A0.23	EXISTING SITE SURVEY
A0.26	SQUARE FOOTAGE BREAKDOWNS
A0.26.1	SQUARE FOOTAGE BREAKDOWNS
A0.27	SQUARE FOOTAGE BREAKDOWNS
A0.27.1	SQUARE FOOTAGE BREAKDOWNS
A0.28	SQUARE FOOTAGE BREAKDOWNS
A0.29	OPEN SPACE AREA CALCULATIONS
A0.29.1	OPEN SPACE AREA CALCULATIONS
A0.30	F.A.R. CALCULATIONS
A0.30.1	F.A.R. CALCULATIONS
A0.32	SHADOW STUDY
A1.00	SITE PLAN
A2.00	PROPOSED PLANS
A2.10	PROPOSED PLANS
A2.20	PROPOSED PLANS
A2.30	PROPOSED PLANS
A2.40	PROPOSED PLANS
A2.50	PROPOSED PLANS
A3.10	ELEVATIONS
A3.20	ELEVATIONS
A3.30	ELEVATIONS
A3.40	ELEVATIONS
A3.50	ELEVATIONS COLOR
A3.60	ELEVATIONS COLOR
A3.70	ELEVATIONS COLOR
A3.80	ELEVATIONS COLOR
A4.10	PROPOSED SECTIONS
A4.20	PROPOSED SECTIONS
A4.30	PROPOSED SECTIONS
A4.40	PROPOSED SECTIONS
A4.50	PROPOSED SECTIONS
A5.00	ENLARGED PLANS
A5.10	ENLARGED PLANS
A7.00	REFLECTED CEILING PLAN
A7.01	LIGHTING SCHEDULE - SUBTERRANEAN PARKING
A7.10	REFLECTED CEILING PLAN
A7.11	LIGHTING SCHEDULE - FIRST FLOOR
A7.20	REFLECTED CEILING PLAN
A7.21	LIGHTING SCHEDULE - SECOND FLOOR
A7.30	REFLECTED CEILING PLAN
A7.31	LIGHTING SCHEDULE - THIRD FLOOR
A7.40	REFLECTED CEILING PLAN
A7.41	LIGHTING SCHEDULE - FOURTH FLOOR
A7.50	REFLECTED CEILING PLAN
A7.51	LIGHTING SCHEDULE - FIFTH FLOOR
A8.10	VERTICAL CIRCULATION
A8.20	VERTICAL CIRCULATION
A8.30	VERTICAL CIRCULATION
A8.40	VERTICAL CIRCULATION
A8.50	STAIR DETAILS
A8.60	ELEVATOR HOISTWAY SECTION
DEMO	DEMO PLAN
L1.00	LANDSCAPE PLAN
L1.10	LANDSCAPE PLAN
T24.1	TITLE 24
T24.2	TITLE 24

ABBREVIATIONS & SYMBOLS

&	And	BLKG	Blocking	D	Deep, Depth	EXP	Expansion	GALV	Galvinized	JT	Joint	N	North	P.T.D.	Paper Towel Dispenser	SIM	Similar	T.P.D.	Toilet Paper Dispenser
<	Angle	BM	Beam	DBL	Double	EXPO	Exposed	G.B.	Grab Bar			N.I.C.	Not in Contract	PTN	Partition	SLDG	Siding	T.S.	Top of Steel
@	At	BOT	Bottom	DET	Detail	EXT	Exterior	G.I.	Galvanized Iron	KIT	Kitchen	NO	Number			SPEC	Specification	TV	Television
℄	Centerline	BR	Bedroom	D.F.	Drinking Fountain			GL	Glass, Glazing			NOM	Nominal	Q.T.	Quarry Tile	SQ	Square	T.O.W.	Top of Wall
ø	Diameter or Round	BSMT	Basement	DIA	Diameter	F.A.	Fire Alarm	GND	Ground	LAM	Laminate	N.S.	No Scale			S.S.	Stainless Steel	TYP	Typical
⊥	Perpendicular	B.U.R.	Built Up Roofing	DIM	Dimension	F.D.	Floor Drain	GR	Grade	LAV	Lavatory	N.T.S.	Not to Scale	R	Riser	SSK	Service Sink		
#	Number			DISP	Dispenser	FDN	Foundation	GYP	Gypsum	L.F.	Lineal Foot			R&S	Radius	STD	Standard	UNF	Unfinished
(E)	Existing	CAB	Cabinet	DN	Down	F.E.	Fire Extinguisher			L.H.	Left Hand	O/	Over	R.D.	Roof Drain	STL	Steel	U.O.N.	Unless Otherwise Noted
		CARP	Carpet	D.O.	Door Opening	F.E.C.	Fire Extinguisher	H	High	LKR	Locker	OA	Overall	REF	Reference	STOR	Storage	UR	Urinal
AB	Anchor Bolt	C.B.	Catch Basin	DR	Door			H.B.	Hose Bib	L.R.	Living Room	OBSC	Obscure	REFR	Refrigerator	STRUCT	Structrual		
A/C	Air Conditioning	CEM	Cement	DS	Downspout	F.G.	Finish Grade	H.C.	Hollow Core	LT	Light	O.C.	On Center	REINF	Reinforced or Reinforcing	SUSP	Suspended	VERT	Vertical
A.C.	Asphaltic Concrete	CER	Ceramic	D.S.P.	Dry Standpipe	F.H.C.	Fire Hose Cabinet	HCP	Handicapped	LVR	Louver	O.D.	Outside Diameter	REQ	Required	SW	Switch	VEST	Vestibule
ACT	Acoustical Tile	C.I.	Cast Iron	DWG	Drawing	FIN	Finish	HDWR	Hardware			O.F.D.	Overflow Drain	RESIL	Resilient	SYM	Symmetrical	V.I.F.	Verify in Field
ACOUS	Acoustical	CLG	Ceiling	DWR	Drawer	FLASH	Flashing	HDWD	Hardwood	MATL	Material	OFF	Office	REV	Revised	SYS	System	VOL	Volume
ADJ	Adjustable	CLO	Closet			FLR	Floor	H.M.	Hollow Metal	MAX	Maximum	O.H.	Overhang	RFG	Roofing				
A.F.F.	Above Finish Floor	CLR	Clear	E	East	FLUOR	Fluorescent	HORIZ	Horizontal	M.B.	Machine Ball	OVHD	Overhead	R.H.	Right Hand	T	Tread	W	West
ALT	Alter or Alternate	CMU	Concrete Masonry Unit	EA	Each	F.O.C.	Face of Concrete	HR	Hour	MECH	Mechanical	OPNG	Opening	RM	Room	T.B.	Towel Bar	W/	With
ALUM	Aluminum	CNTR	Counter	EJ	Expansion Joint	F.O.F.	Face of Finish	HT	Height	MEMB	Membrane	OPP	Opposite	R.O.	Rough Opening	T&G	Tongue and Groove	W.H.	Water Heater
ANOD	Anodized	COL	Column	EL	Elevation	F.O.M.	Face of Masonry	HVAC	Heating, Ventilation and Air Conditioning	MET	Metal			RWD	Redwood	T.O.C.	Top of Curb	W/O	Without
A.P.	Access Panel	CONC	Concrete	ELEC	Electrical	F.O.S.	Face of Stud			MFR	Manufacture	PC	Piece			T.O.D.	Top of Drain	W.C.	Water Closet
APPROX	Approximate	CONN	Connection	ELEV	Elevation	FPRF	Fireproof	H.W.	Hot Water	MH	Manhole	P.D.	Planter Drain	S	South	TEL	Telephone	WD	Wood
ARCH	Architectural	CONST	Construction	EMER	Emergency	FR	Frame			MIN	Minimum	PL	Plate	S.C.	Solid Core	TEMP	Tempered, Temperature	WP	Waterproof
ASPH	Asphalt	CONT	Continuous	ENCL	Enclosure	FS	Full Size	I.D.	Inside Diameter	MIR	Mirror	P.L.	Property Line	SCHED	Schedule	TER	Terrazzo	WPM	Waterproof Membrane
		CONTR	Contractor	E.O.S.	Edge of Slab	FT	Foot, Feet	INCL	Including	MISC	Miscellaneous	PLMG	Plumbing	SECT	Section	THK	Thick, Thickness	WSCT	Wainscot
BD	Board	CORR	Corridor	EQ	Equal	FTG	Footing	INSUL	Insulation	M.O.	Masonry Opening	PLAM	Plastic Laminate	SEP	Separation, Separate	THR	Threshold	W.S.P.	Wet Standpipe
BITUM	Bituminous	C.T.	Ceramic Tile	EQUIP	Equipment	FURR	Furring, Furred	INT	Interior	M.R.	Moisture Resistant	PLAS	Plaster	SH	Shelf	TOIL	Toilet	WT	Weight
BLDG	Building	CTR	Center	E.W.	Each Way	FUT	Future			MTD	Mounted	PLYWD	Plywood	SHR	Shower	T.O.P.	Top of Pavement		
BLK	Block	CTSK	Countersunk	E.W.C.	Electric Water Cooler			JAN	Janitor	MUL	Mullion	PR	Pair	SHT	Sheet	T.O.S.	Top of Slab		
		C.W.	Cold Water	EXIST	Existing	GA	Gauge	JST	Joist			PT	Paint			T.P.D.	Toilet Paper Dispenser		



breakformdesign
127 arena street, el segundo, ca 90245
[o] 310.322.3700

LICENSED ARCHITECT
RAMSEY DAHAM
No. C-34257
10/31/23
RENEWAL DATE
STATE OF CALIFORNIA

3676 & 3704 KELTON AVE.
LOS ANGELES, CA 90034

Revision Schedule

Revision Number	Revision Date

COVER

DRAWN CR
CHECKED PNK
DATE 4/13/2023 11:01:06 AM
SCALE
JOB # 21-A001

A0.00

CITY OF LOS ANGELES
CALIFORNIA

BOARD OF BUILDING AND SAFETY COMMISSIONERS
VAN AMBATIELOS, PRESIDENT
E. FELICIA BRANNON, VICE PRESIDENT
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ERIC GARCETTI, MAYOR

DEPARTMENT OF BUILDING AND SAFETY
201 NORTH FIGUEROA
LOS ANGELES, CA 90012
FRANK M. BUSH, GENERAL MANAGER, SUPERINTENDENT OF BUILDING
OSAMA YOUNAN, P.E., EXECUTIVE OFFICER

SOPREMA, Inc.
310 Quadral Drive
Wadsworth, Ohio 44281

Attention: Glen N. Bestor
(330) 331-3070

Local Representative: Freddy Riofrio
(203) 262-9245

RESEARCH REPORT: RR 25908
(CSI # 07560)

Expires: May, 1, 2020
Issued Date: April 1, 2018
Code: 2017 LABC

GENERAL APPROVAL – Renewal and Clerical Modification -Soprema ALSAN RS 230 Field and RS 260 LO Field Liquid Applied Roof Covering System.

DETAILS

ALSAN RS 230 Field liquid applied roof covering system:

Deck: Concrete deck with maximum1-inch per foot slope.
Primer: ALSAN RS 276 primer applied at the rate of 1.0 gallon per 100 ft².
Base Coat: ALSAN RS 230 Field applied at the rate of 3.91 gallon per 100 ft².
Reinforcement: While the base coat of ALSAN RS 230 Field is wet, a layer of non-woven, needle punched polyester reinforcement fabric is applied and rolled so that the air bubbles are removed.
Top Coat: ALSAN RS 230 Field applied at the rate of 1.95 gallon per 100 ft².

ALSAN RS 260 LO Field liquid applied roof covering system:

Deck: Concrete deck with maximum 1-1/2-inch per foot slope.
Primer: ALSAN RS 276 primer applied at the rate of 1.0 gallon per 100 ft².
Base Coat: ALSAN RS 260 LO Field applied at the rate of 3.91 gallon per 100 ft².
Reinforcement: While the base coat of ALSAN RS 260 LO Field is wet, a layer of non-woven, needle punched polyester reinforcement fabric is applied and rolled so that the air bubbles are removed.
Top Coat: ALSAN RS 260 LO Field applied at the rate of 1.95 gallon per 100 ft².

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LADBS G-5 (Rev 08/14) AN EQUAL EMPLOYMENT OPPORTUNITY - AFFIRMATIVE ACTION EMPLOYER

Soprema, Inc.
RE: Soprema Alsan RS 230 Field and RS 260 LO Field Liquid Applied Roof Covering System.

The roof covering systems described above are approved as Class A roof coverings subject to the following conditions:

1. The roofing materials shall be delivered to the job site in sealed containers identified by the manufacturer's name and product designation.
2. Application of the components shall be on a concrete deck in accordance with the manufacturer's instructions consistent with the description and requirements herein. (A copy shall be available at the job site).

DISCUSSION

The clerical modification is to update the report to the 2017 Los Angeles City Building Code and to update the contact person and phone number.

The report is in compliance with the 2017 Los Angeles City Building Code.

The approval was based on tests in accordance with ICC Evaluation Services Acceptance Criteria for Membrane Roof Covering Systems (AC 75)

Addressee to whom this Research Report is issued is responsible for providing copies of it, complete with any attachments indicated, to architects, engineers and builders using items approved herein in design or construction which must be approved by Department of Building and Safety Engineers and Inspectors.

This general approval of an equivalent alternate to the Code is only valid where an engineer and/or inspector of this Department has determined that all conditions of this approval have been met in the project in which it is to be used.

QUAN NGHIEM, Chief
Engineering Research Section
201 N. Figueroa St., Room 880
Los Angeles, CA 90012
Phone- 213-202-9812
Fax- 213-202-9943

DR:
25908
TLR:0100073
RSS:26/18
150/13307.15

RR 25908
Page 2 of 2

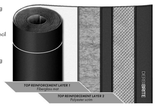
Technical Data Sheet

DERBIBRITE®

Modified Bitumen Waterproofing Membrane with a Unique Reflective Surfacing

ADVANTAGES

- Premium 3.5 mm [140 mil] Class A Fire Resistant Modified Bitumen Roofing Membrane that combines Derbigum's proven performance with a highly reflective white acrylic surface.
- Rigorously tested, Derbibrite exceeds Energy Star, Cool Roof Rating Council approved and contributes to LEED certification
- Manufactured with a fiberglass/polyester reinforcement that allows the acrylic to saturate and form an extremely tight molecular structure creating an impenetrable surface resistant to light oils, animals fats and fungus.
- May be used as both a roofing and flashing membrane
- Derbibrite is applied with PERMASTIC Cold Adhesive



TECHNICAL CHARACTERISTICS

CHARACTERISTIC	TESTED	ASTM Requirements Turf Methods D5147 / D6223-01 MACHINE DIRECTION	CROSS DIRECTION	DERBIBRITE Results* MACHINE DIRECTION	CROSS DIRECTION	UNITS
Tensile Strength	0°F	150	150	131	131	lbf/in
	73°F	65	65	99	102	lbf/in
Elongation	0°F	3.0	3.0	6.0	6.1	%
	73°F	3.0	3.0	11.1	8.5	%
Tear Resistance	73°F	120	120	156	156	lbf
Dimensional Stability		1	1	0.05	0.05	%
CHARACTERISTIC	**ASTM**	**INITIAL**	**AGED**	**INITIAL**	**AGED**	**UNITS**
Cold Flex	D6223	0	0	-21	-20	°C
CHARACTERISTIC	**ASTM**	**MINIMUM**		**MINIMUM**		**UNITS**
Compound Stability		275		300		°F
Mill		N/A		0		%
Fire Resistance	E108 & UL790	PASS/FAIL		Class A rated to 3" slope		
Fungus Resistance	G21	PASS/FAIL		0 (no growth of fungi)		

* All values shown are nominal and subject to normal manufacturing tolerances. Tested initially within 24 hours and again aged 28 days.

PRODUCT INFORMATION

CHARACTERISTIC	U.S. CUSTOMARY	METRIC
Thickness	140 mil	3.5 mm
Length	33 ft 4 in	10.2 m
Width	39 ¼ in	1.0 m
Weight	94 lb	42.6 kg
Net Coverage	100 sq ft	9.2 sq m

ENERGY AND ENVIRONMENT*

CHARACTERISTIC	ASTM	INITIAL	3-YEAR
Reflectivity	C1549	≥0.7b	≥0.68
Emissivity	E1371	≥0.81	≥0.83
Solar Reflective Index	E1980	93	81

* All values shown are nominal and subject to normal manufacturing tolerances.

PACKAGING & STORAGE

Packaged 20 rolls per pallet. Rolls should be stored on end, on raised platforms protected from the weather.

505 available on request (800) 727-9872 www.derbigum.us

DERBIGUM

4655 Blue Parkway, Kansas City, MO 64130 | PHONE: 800.727.9872 | **FAX:** 816.924.1542 | **WEBSITE:** www.derbigum.us | **EMAIL:** info@derbigum.us

STORMWATER OBSERVATION REPORT (SOR) FORM

Only to be used for Single Family Residences (4 units or less, <10,000 SF, <2,500 SF within a ESA)

LOW IMPACT DEVELOPMENT

IN THE EVENT THAT THE APPROVED STORMWATER BMP CANNOT BE BUILT PER PLANS (OR ANY MODIFICATION), CONSULT WITH BUREAU OF SANITATION STAFF PRIOR TO ANY PLAN MODIFICATIONS. FAILURE TO DO SO MAY DELAY OBTAINING A FINAL APPROVAL AND CERTIFICATE OF OCCUPANCY (C of O).

STORMWATER OBSERVATION *means the visual observation of the stormwater related Best Management Practices (BMPs) for conformance with the approved LID Plan at significant construction stages and at completion of the project. Stormwater observation does not include or waive the responsibility for the inspections required by Section 108 or other sections of the City of Los Angeles Building Code.*

STORMWATER OBSERVATION *must be performed by the contractor responsible for the approved LID Plan or designated staff in their employment. Homeowner can also perform the Stormwater Observation if no licensed contractor was involved* ***AS PART OF THE OBSERVATION, PROVIDE PRINTED PHOTOS OF THE BMPS TAKEN DURING VARIOUS CONSTRUCTION PHASES.***

STORMWATER OBSERVATION REPORT (SOR) *must be signed by the contractor responsible for the approved LID Plan and submitted to the City prior to the issuance to the certificate of occupancy. Homeowner can sign the Stormwater Observation Report if no licensed contractor was involved* ***PRIOR TO CERTIFICATE OF OCCUPANCY (C of O), SOR FORM, PRINTED PHOTOS OF THE BMPS TAKEN DURING VARIOUS CONSTRUCTION PHASES AND APPROVED STAMPED PLANS BY THE BUREAU OF SANITATION MUST BE SUBMITTED TO THE PUBLIC COUNTER FOR STAFF APPROVAL.***

Project Address:	**Building Permit No.:**
Contractor / Architect / Engineer responsible for construction of best management practices per approved LID Plan:	**Phone Number:**

I declare that the following statements are true to the best of my knowledge:

1. I am responsible for the approved LID Plan, and
2. I, or designated staff under my responsible charge, have performed the required site visits at each significant construction stage and at completion to verify that the best management practices as shown on the approved plan have been constructed and installed in accordance with the approved LID Plan.

Signature Date Contractor/Architect/Engineer License

Low Impact Development (LID)
Post Construction Stormwater Mitigation
Best Management Practices (BMPs)

STORMWATER BMP(s) VERIFICATION

Upon installation of the approved stormwater BMPs, a Stormwater Observation Report (SOR) Form shall be submitted to Department of Public Works, Bureau of Sanitation. 201 N. Figueroa, 3rd floor, station 18. The SOR Form must be with filed and approved by the Bureau of Sanitation prior to the issuance of a Certificate of Occupancy.

Project Address: _____

RESIDENTIAL (4 UNITS OR LESS, <10,000SF, < 2,500 SF within a ESA)

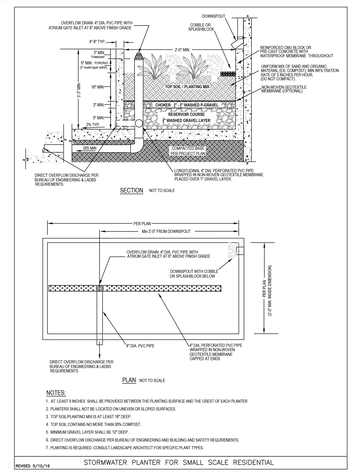
Item #	Stormwater BMP	Description (Units, total)	Reference Sheet(s)* (Sheet #)
1	Rain Tank(s) – 50 to 129 gal each		
2	Rain Tank(s) – > 130 gal min		
3	Shade Tree - min 15 gal		
4	Flow thru Planter(s)		
5	Permeable pavers / Porous concrete (min 10% open space)	☐ Incidental: _____ total SF ☐ Infiltration: _____ total SF	
6	Rain Garden	☐ # _____ - Lined: _____ total SF ☐ # _____ - Unlined: _____ total SF	
7	Dry Well		
8	SUMP Pump (modification was not required)		

ALL OTHER DEVELOPMENT

(Residential: 5-7 units, 10,000 ≥ SF, within a ESA and ≥2,500SF)

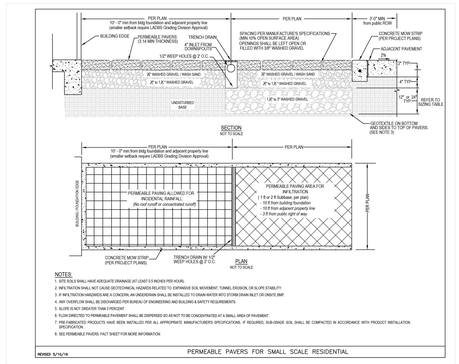
	Item #	Stormwater BMP	Description (Units, total)	Reference Sheet(s)* (Sheet #)
Infiltration	1	Infiltration Basin / Trench		
	2	Dry Well		
	3	Permeable pavers / Porous concrete (min 10% open space)	☐ Incidental: _____ total SF ☐ Infiltration: _____ total SF	
Capture & Use	4	Rain Tank(s) – 50 gal min		
	5	Cistern	☐ Above Grade ☐ Below Grade	
Treat & Discharge	6	Flow thru Planter(s)		
	7	Biofiltration	☐ # _____ - Lined: _____ total SF ☐ # _____ - Unlined: _____ total SF	
	8	Vegetative Swale / Filter Strip		
	9	Catch Basin Filter(s)		
	10	Trench Drain Filter(s)		
	11	Down Spout Filter(s)		
	12	SUMP Pump (modification was not required)		

* At a minimum: Site Plan, Architectural Elevations, Roof Plan, Civil Sheets and Detail



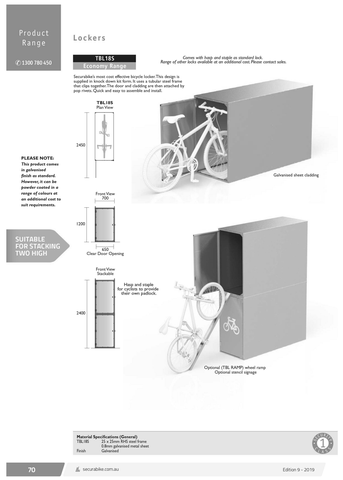
NOTES:
1. 18" LEAST 9 INCHES SHALL BE PROVIDED BETWEEN THE PLANTING SURFACE AND THE CREST OF EACH PLANTER
2. PLANTERS SHALL NOT BE LOCATED ON UNEVEN OR SLOPED SURFACES.
3. TOP SOIL/PLANTING MIX IS AT LEAST 18" DEEP.
4. TOP SOIL CONTAINS NO MORE THAN 30% COMPOST.
5. MINIMUM GRAVEL LAYER SHALL BE 12" DEEP.
6. DIRECT OVERFLOW DISCHARGE PER BUREAU OF ENGINEERING AND BUILDING AND SAFETY REQUIREMENTS.
7. PLANTING IS REQUIRED. CONSULT LANDSCAPE ARCHITECT FOR SPECIFIC PLANT TYPES.

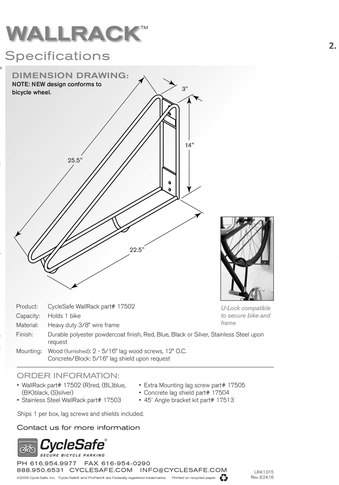
REVISED: 5/15/18 STORMWATER PLANTER FOR SMALL SCALE RESIDENTIAL



NOTES:
1. SITE SOILS SHALL HAVE ADEQUATE DRAINAGE (AT LEAST 0.5 INCHES PER HOUR).
2. INFILTRATION SHALL NOT CAUSE GEOTECHNICAL HAZARDS RELATED TO: EXPANSIVE SOIL MOVEMENT, TUNNEL EROSION, OR SLOPE STABILITY.
3. IF INFILTRATION HAZARDOUS ARE A CONCERN, AN IMPERVIOUS SHALL BE INSTALLED TO DRAIN WATER INTO STORM DRAIN INLET OR ONSITE BMP.
4. ANY OVERFLOW SHALL BE DISCHARGED PER BUREAU OF ENGINEERING AND BUILDING & SAFETY REQUIREMENTS.
5. SLOPE IS NOT GREATER THAN 5 PERCENT.
6. FLOW DIRECTED TO PERMEABLE PAVEMENT SHALL BE DISPERSED SO AS NOT TO BE CONCENTRATED AT A SMALL AREA OF PAVEMENT.
7. PRE-FABRICATED PRODUCTS HAVE BEEN INSTALLED PER ALL APPROPRIATE MANUFACTURERS SPECIFICATIONS. IF REQUIRED, SUB-GRADE SOIL SHALL BE COMPACTED IN ACCORDANCE WITH PRODUCT INSTALLATION SPECIFICATION.
8. SEE PERMEABLE PAVERS FACT SHEET FOR MORE INFORMATION.

REVISED: 5/15/18 PERMEABLE PAVERS FOR SMALL SCALE RESIDENTIAL

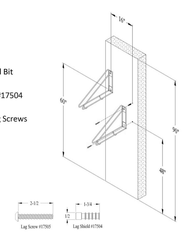




2. Concrete Wall Installation:

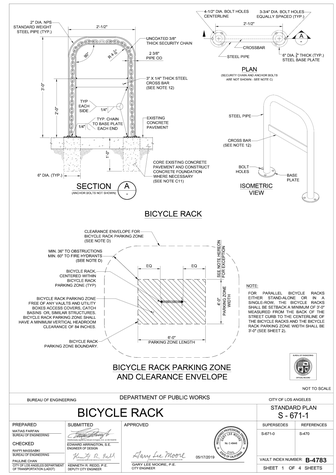
A. Tools & Fasteners

- 3/8" Impact Drill
- 1/2" x 3" Long Carbide Drill Bit
- T-30 Torx Bit
- (2) 5/16" Lag Shields Part #17504 (included)
- (2) 5/16" x 2-1/2" Long Lag Screws Part #17505 (included)
- Flat Head Screw Driver Bit
- Pencil
- Hammer
- Tape Measure
- Concrete Adhesive
- Blue Line or Laser Level



B. Installation:

a) Locate center of bottom hole placement of the first WallRack
b) Snap a line with the Blue Line or Laser and make a mark with pencil at approximately 60" from the floor to the first mounting hole position
c) Hold rack straight and center hole over mark position, the bottom hole and mark the upper hole at 12" center to center, which would be the 1st hole down from the top
d) Remove rack and drill approximately 2-1/2" deep into concrete with 1/2" x 4" Drill Bit on center
e) Squirt in some Concrete Adhesive in both holes
f) Insert the (2) Lag Shields #17504 into the holes and tap flush with the hammer
g) Align the rack with the holes and Sleeves and insert the (2) Lag Screws #17505 using the T-30 Torx Driver and tighten
h) Next offset the bottom rack hole down by 12" vertical and over 16" center-to-center horizontal, yielding approx. 48" from the rack bottom hole to the floor
i) Repeat steps "c" thru "g" completing on set of racks
j) Repeat steps "a" thru "i" as necessary to complete the wall installation using a Blue Line or Laser Level



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LICENSED ARCHITECT
RAMSEY DAHAM
No. C-34257
10/31/23
RENEWAL DATE
STATE OF CALIFORNIA

3676 & 3704 KELTON AVE.
LOS ANGELES, CA 90034

Revision Schedule

Revision Number	Revision Date

SPECS / RESEARCH REPORTS

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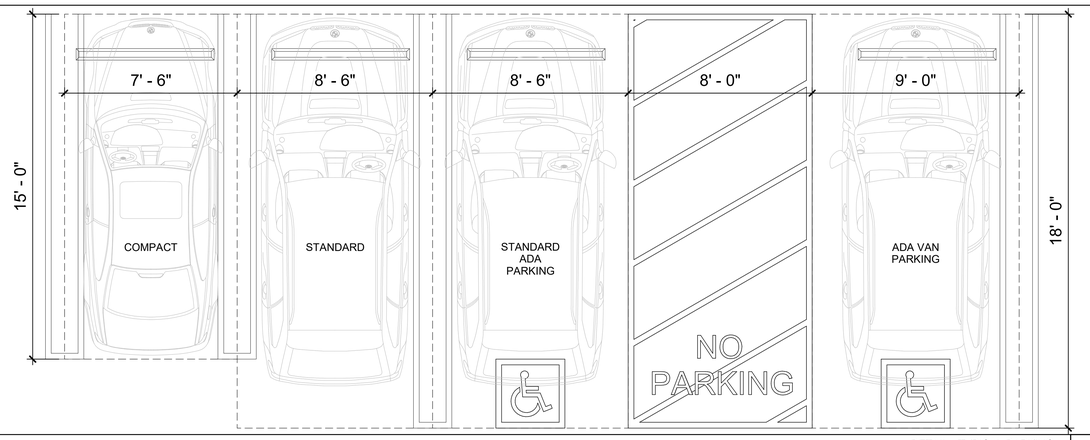
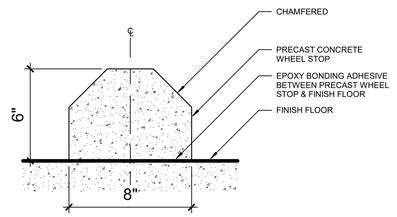
CHECKED PNK
DATE 4/13/2023 10:50:31 AM
SCALE
JOB # 21-A001

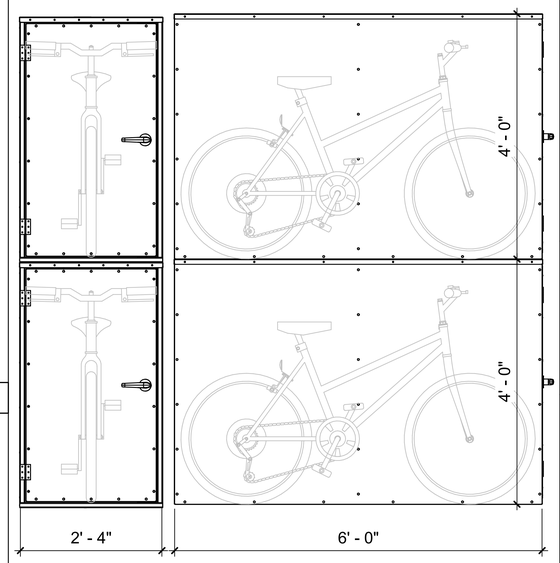
A0.06

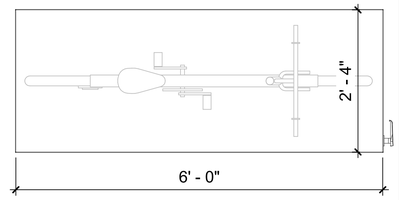


DETAIL - TYPICAL PARKING
3/8" = 1'-0" | 3

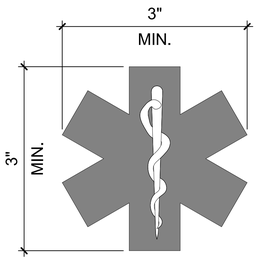


DETAIL - WHEEL STOP
3" = 1'-0" | 6





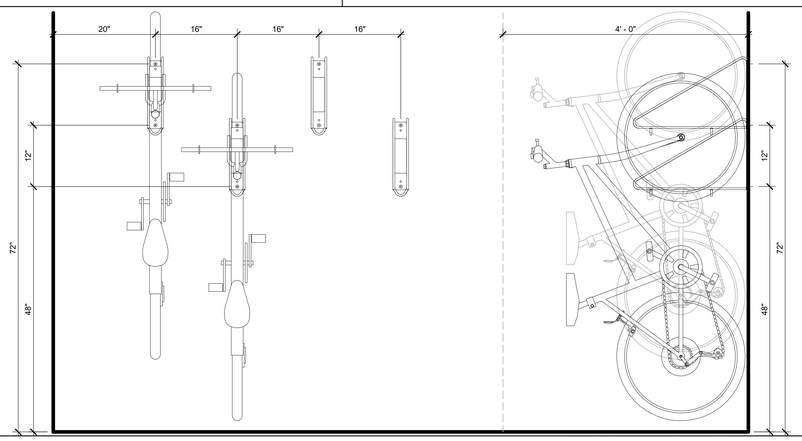
DETAIL - BICYCLE PARKING LONG TERM LOCKER
1" = 1'-0" | 2



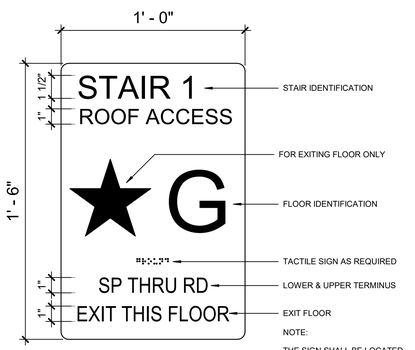
NOTE:

NTERNATIONAL SYMBOL FOR MEDICAL SERVICES (STAR OF LIFE). THE SYMBOL SHALL NOT BE LESS THAN 3 INCHES HIGH & SHALL BE PLACED INSIDE OF THE HOISTWAY DOOR FRAME.

DETAIL - MEDICAL SERVICES SYMBOL
12" = 1'-0" | 5



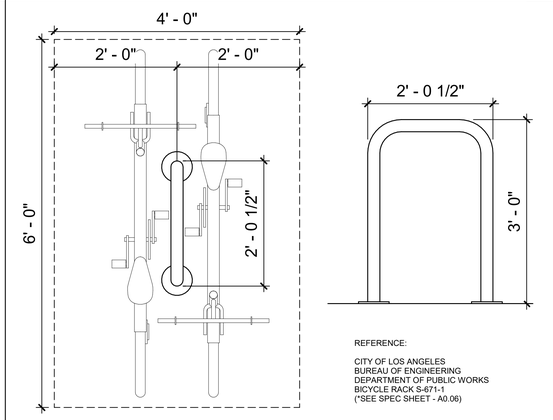
DETAIL - BICYCLE PARKING LONG TERM WALL RACK
1" = 1'-0" | 7



NOTE:

THE SIGN SHALL BE LOCATED 5 FEET ABOVE THE FLOOR LANDING IN A POSITION THAT IS READILY VISIBLE WHEN THE DOOR IS IN THE OPEN AND CLOSED POSITIONS.

DETAIL - FLOOR I.D. SIGN TYP.
3" = 1'-0" | 4



REFERENCE:

CITY OF LOS ANGELES
BUREAU OF ENGINEERING
DEPARTMENT OF PUBLIC WORKS
BICYCLE RACK S-671-1
(*SEE SPEC SHEET - A0.06)

DETAIL - BICYCLE PARKING SHORT TERM TYP.
1" = 1'-0" | 1

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No. C-34257
10/31/23
RENEWAL DATE
STATE OF CALIFORNIA

3676 & 3704 KELTON AVE.
LOS ANGELES, CA 90034

Revision Schedule

Revision Number	Revision Date

GENERAL DETAILS

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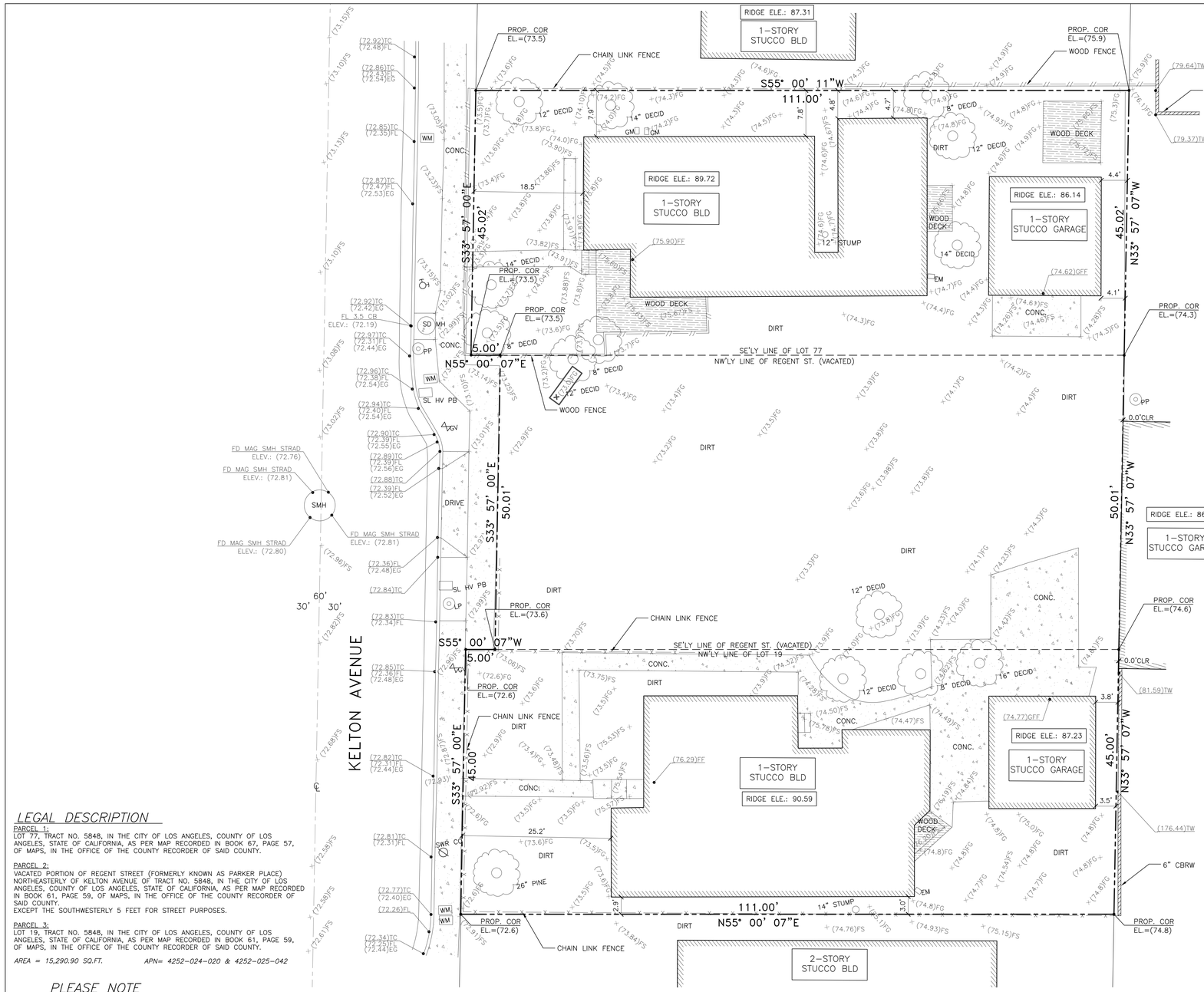
CHECKED	PNK
DATE	4/13/2023 10:50:35 AM
SCALE	As indicated
JOB #	21-A001

A0.16



LEGEND

A.C.	ASPHALT CONCRETE PAVEMENT
B.M.	BENCH MARK
C.B.W.	CONCRETE BLOCK WALL
C.B.R.W.	CONCRETE BLOCK RETAINING WALL
CONC.	CONCRETE
C/O	SEWER CLEANOUT
DWY.	CONC. DRIVEWAY APRON
E.G.	EDGE OF GUTTER
E.M.	ELECTRIC METER
E.P.	EDGE OF PAVEMENT
E.V.	ELECTRIC VALVE
F.F.	FINISHED FLOOR
F.G.	FINISHED GRADE
F.H.	FIRE HYDRANT
F.S.	FINISHED SURFACE
F.L.	FLOW LINE
G.M.	GAS METER
INV.	DRAIN INVERT
I.C.V.	IRRIGATION CONTROL VALVE
L.P.	LIGHT POLE
P.B.	PULL BOX
P.C.R.W.	POURED CONC. RETAINING WALL
P.P.	POWER POLE
R.R.W.	ROCK RETAINING WALL
S.S.M.H.	SEWER MANHOLE
S.D.M.	STORM DRAIN MANHOLE
T.C.	TOP OF CURB
T.G.	TOP OF DRAIN GRATE
T.W.	TOP OF WALL
W.M.H.	WATER MANHOLE
W.V.	WATER VALVE

OVERHANG

EASEMENT LIMIT

CONCRETE SURFACE

8" TREE AND TRUNK DIAMETER

WOOD FENCE

TOPOGRAPHIC NOTES

1. *CONTOUR LINES SHOWN ON THIS TOPOGRAPHIC SURVEY WERE GENERATED FROM RANDOM SPOT ELEVATIONS. THE EXPECTED LEVEL OF ACCURACY IS ±1/2 OF THE CONTOUR INTERVAL. ISOLATED ELEVATIONS, AS PLACED ON THE PLAT WERE PHYSICALLY MEASURED AT APPROXIMATELY THE LOCATIONS SHOWN. THEIR EXPECTED LEVEL OF ACCURACY IS ± 0.10 FOOT.*

2. *IF SPECIFIC ELEVATIONS ARE REQUIRED, THE USER SHOULD CONTACT PACIFIC LAND CONSULTANTS, INC., TEL: (310)544-8689 FOR SAME. INTERPOLATIONS MADE FROM THIS TOPOGRAPHIC SURVEY SHOULD BE DONE IN ACCORDANCE WITH STANDARD SURVEYING PRACTICES, AND LIABILITY FOR INTERPOLATIONS IS NOT ASSUMED BY PACIFIC LAND CONSULTANTS, INC.*

SURVEY CERTIFICATION

I HEREBY CERTIFY THAT I AM A REGISTERED LAND SURVEYOR OF THE STATE OF CALIFORNIA, THAT THIS PLAT CONSISTING OF ONE SHEET CORRECTLY REPRESENTS A SURVEY MADE UNDER MY SUPERVISION IN FEBRUARY, 2018; THAT ALL MONUMENTS SHOWN HEREON ACTUALLY EXIST AND THEIR POSITIONS ARE CORRECTLY SHOWN. THIS SURVEY DOES NOT INCLUDE EASEMENTS EXCEPT THOSE SPECIFICALLY DELINEATED HEREON.

CHRISTOPHER W. VASSALLO P.L.S. 8418 *DATE*

REGISTRATION EXPIRES 12-31-2022

PROFESSIONAL LAND SURVEYOR CHRISTOPHER W. VASSALLO P.L.S. 8418 EXP. 12/31/2022 STATE OF CALIFORNIA

0 4 8 FEET 16

1/8" = 1'-0"

LEGAL DESCRIPTION

PARCEL 1:
LOT 77, TRACT NO. 5848, IN THE CITY OF LOS ANGELES, COUNTY OF LOS ANGELES, STATE OF CALIFORNIA, AS PER MAP RECORDED IN BOOK 67, PAGE 57, OF MAPS, IN THE OFFICE OF THE COUNTY RECORDER OF SAID COUNTY.

PARCEL 2:
VACATED PORTION OF REGENT STREET (FORMERLY KNOWN AS PARKER PLACE) NORTHEASTERLY OF KELTON AVENUE OF TRACT NO. 5848, IN THE CITY OF LOS ANGELES, COUNTY OF LOS ANGELES, STATE OF CALIFORNIA, AS PER MAP RECORDED IN BOOK 61, PAGE 59, OF MAPS, IN THE OFFICE OF THE COUNTY RECORDER OF SAID COUNTY.
EXCEPT THE SOUTHWESTERLY 5 FEET FOR STREET PURPOSES.

PARCEL 3:
LOT 19, TRACT NO. 5848, IN THE CITY OF LOS ANGELES, COUNTY OF LOS ANGELES, STATE OF CALIFORNIA, AS PER MAP RECORDED IN BOOK 61, PAGE 59, OF MAPS, IN THE OFFICE OF THE COUNTY RECORDER OF SAID COUNTY.

AREA = 15,290.90 SQ.FT. *APN= 4252-024-020 & 4252-025-042*

PLEASE NOTE

IF THIS DRAWING IS PROVIDED IN AN ELECTRONIC FORMAT (VIA EMAIL OR ON COMPUTER DISC) AS A COURTESY TO OUR CLIENT, THE DELIVERY OF THE ELECTRONIC FILE DOES NOT CONSTITUTE THE DELIVERY OF OUR PROFESSIONAL WORK PRODUCT. IN THE EVENT THE ELECTRONIC FILE IS ALTERED, THE PRINT MUST BE REFERRED TO FOR THE ORIGINAL AND CORRECT SURVEY INFORMATION. PACIFIC LAND CONSULTANTS, INC. SHALL NOT BE RESPONSIBLE FOR ANY MODIFICATIONS MADE TO THE ELECTRONIC FILE OR FOR ANY PRODUCTS DERIVED FROM THE ELECTRONIC FILE WHICH ARE NOT REVIEWED, SIGNED AND SEALED BY PACIFIC LAND CONSULTANTS, INC.

UNDERGROUND UTILITIES

ALL INFORMATION SHOWN HEREON REGARDING UNDERGROUND UTILITIES WAS TAKEN FROM VISIBLE SURFACE EVIDENCE OR SOURCES NOT CONNECTED WITH THIS COMPANY AND WHILE SAID INFORMATION IS BELIEVED CORRECT, NO LIABILITY IS ASSUMED FOR THE ACCURACY OR COMPLETENESS OF SAID DATA.

BENCHMARK

BM# 13-02970
CUT SPIKE IN SE COR CB; 3.5FT E OF E CURB MIDVALE AVE; 22FT N OF VENICE BLVD
ELEV: 74.860' (NAVD-88)

PACIFIC LAND CONSULTANTS ESTABLISHED 1994

PACIFIC LAND CONSULTANTS, INC.
28441 HIGHRIDGE RD. SUITE 230
ROLLING HILLS ESTATES, CA 90274
(310) 544-8689

DRAWN BY:A.P.

PROJECT SITE:
3676 KELTON AVENUE
LOS ANGELES, CA

FILE NAME: 22027LS.DWG
DATE: 4-5-2022
SCALE: 1/8" = 1'-0"
JOB NO. 22027
SHEET 1 OF 1

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LICENSED ARCHITECT
RAMSEY DAHAM
No. C-34257
10/31/23
RENEWAL DATE
STATE OF CALIFORNIA

3676 & 3704 KELTON AVE.
LOS ANGELES, CA 90034

Revision Schedule

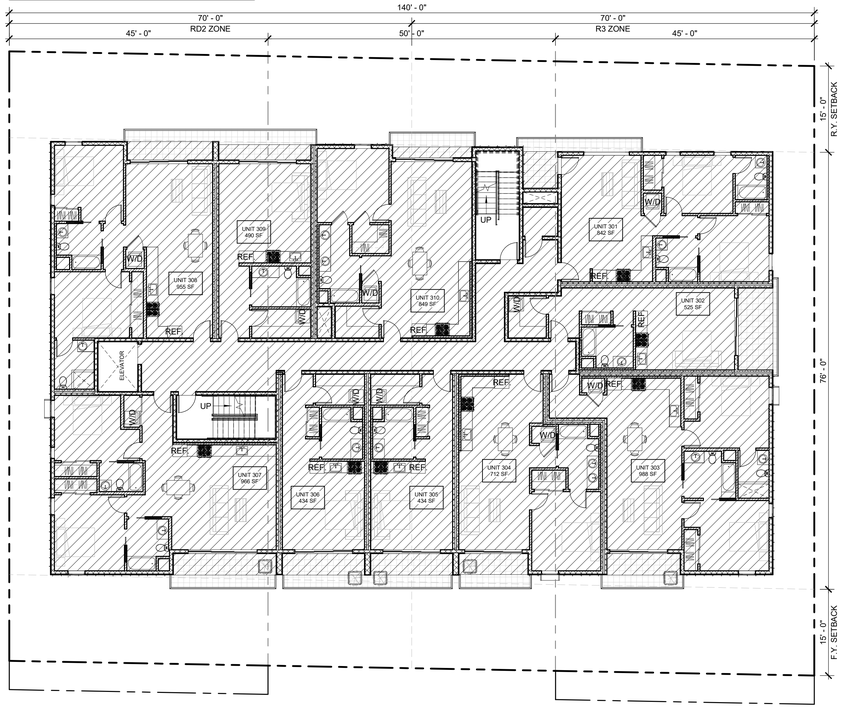
Revision Number	Revision Date

EXISTING SITE SURVEY

DRAWN CR
CHECKED PNK
DATE 4/13/2023 10:50:38 AM
SCALE
JOB # 21-A001

A0.23

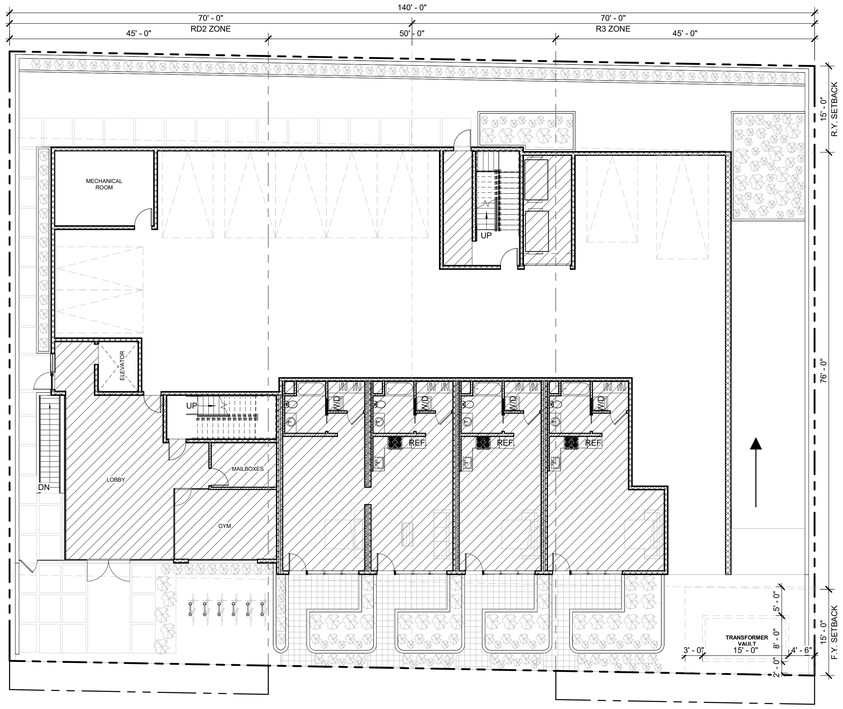
ZONING CODE FLOOR AREA	
PARKING LEVEL	379 SF
FIRST FLOOR	3,328 SF
SECOND FLOOR	8,680 SF
THIRD FLOOR	8,680 SF
FOURTH FLOOR	8,680 SF
FIFTH FLOOR	8,080 SF
TOTAL PROVIDED	**37,827 SF**



KELTON AVE

ZONING CODE - THIRD FLOOR
3/32" = 1'-0"

4



KELTON AVE

ZONING CODE - GROUND FLOOR
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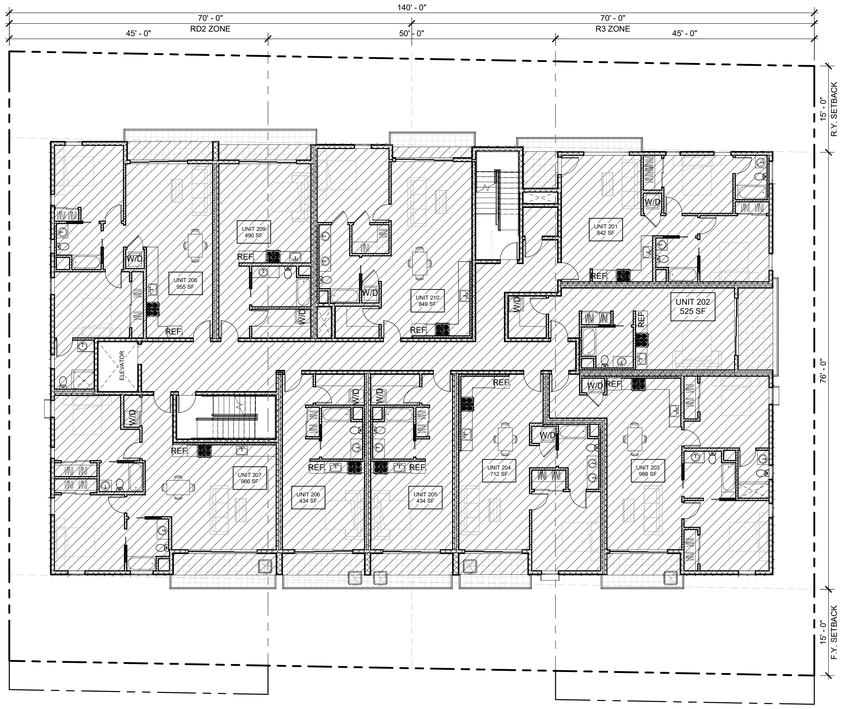
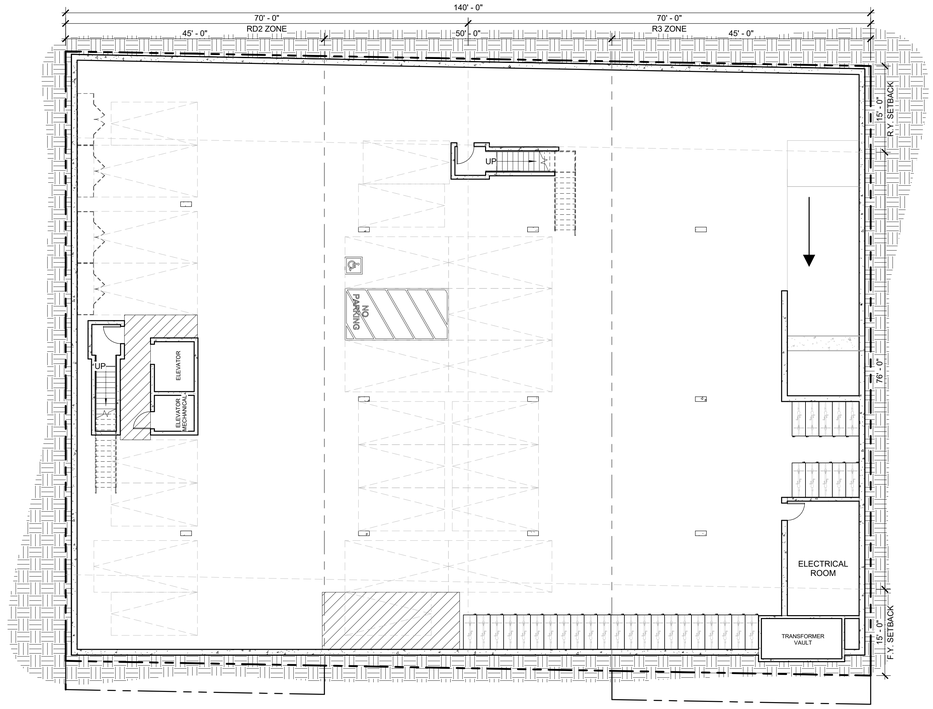
2



KELTON AVE

ZONING CODE - SECOND FLOOR
3/32" = 1'-0"

3



KELTON AVE

ZONING CODE - PARKING LEVEL
3/32" = 1'-0"

1

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LICENSED ARCHITECT
RAMSEY DAHAM
No. C-34257
10/31/23
RENEWAL DATE
STATE OF CALIFORNIA

3676 & 3704 KELTON AVE.
LOS ANGELES, CA 90034

Revision Schedule

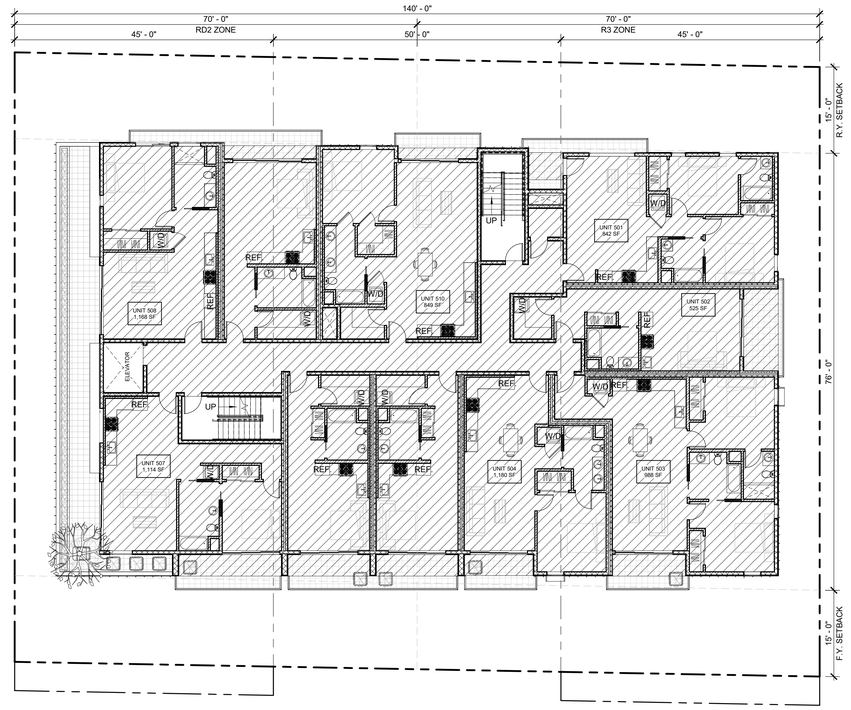
Revision Number	Revision Date

SQUARE FOOTAGE BREAKDOWNS

DRAWN	CR
CHECKED	PNK
DATE	4/13/2023 11:20:50 AM
SCALE	As indicated
JOB #	21-A001

A0.26

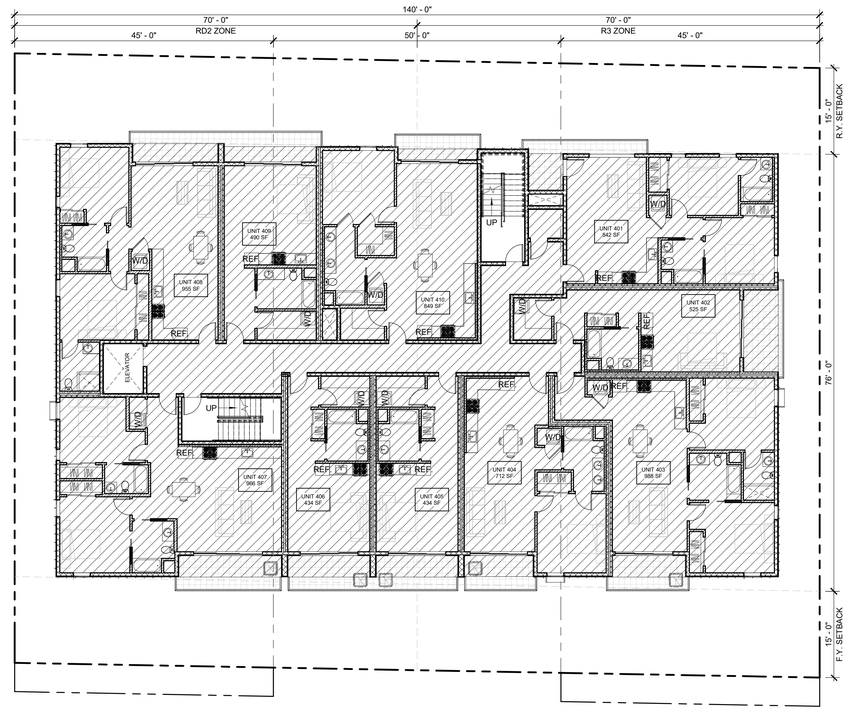
ZONING CODE FLOOR AREA	
PARKING LEVEL	379 SF
FIRST FLOOR	3,328 SF
SECOND FLOOR	8,680 SF
THIRD FLOOR	8,680 SF
FOURTH FLOOR	8,680 SF
FIFTH FLOOR	8,080 SF
TOTAL PROVIDED	**37,827 SF**



KELTON AVE

ZONING CODE - FIFTH FLOOR
3/32" = 1'-0"

2



KELTON AVE

ZONING CODE - FOURTH FLOOR
3/32" = 1'-0"

1

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127 arena street, el segundo, ca 90245
[o] 310.322.3700

LICENSED ARCHITECT
RAMSEY DAHAM
No. C-34257
10/31/23
RENEWAL DATE
STATE OF CALIFORNIA

3676 & 3704 KELTON AVE.
LOS ANGELES, CA 90034

Revision Schedule

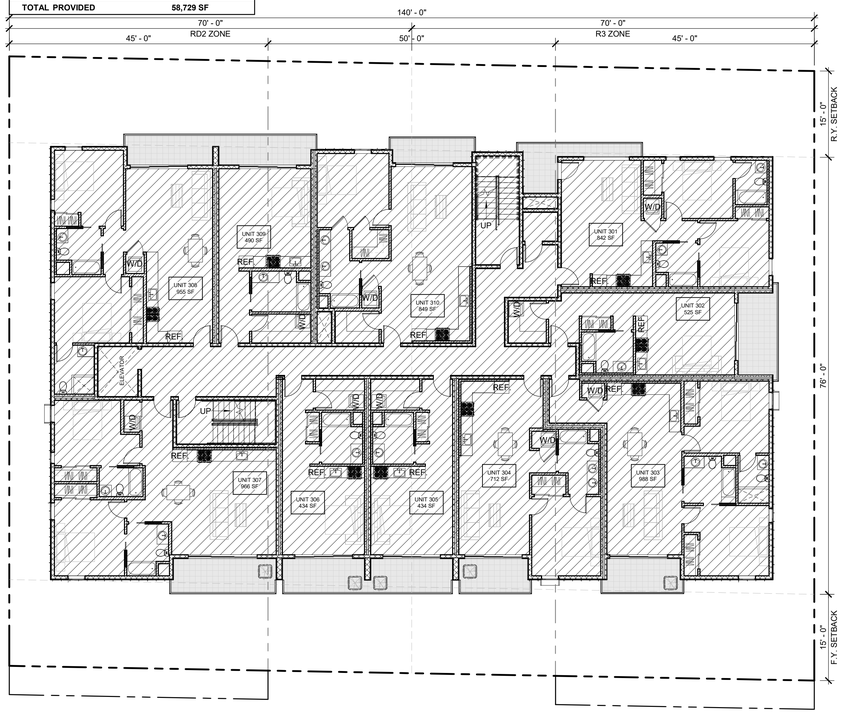
Revision Number	Revision Date

SQUARE FOOTAGE BREAKDOWNS

DRAWN	Author
CHECKED	Checker
DATE	4/13/2023 10:51:04 AM
SCALE	As indicated
JOB #	21-A001

A0.26.1

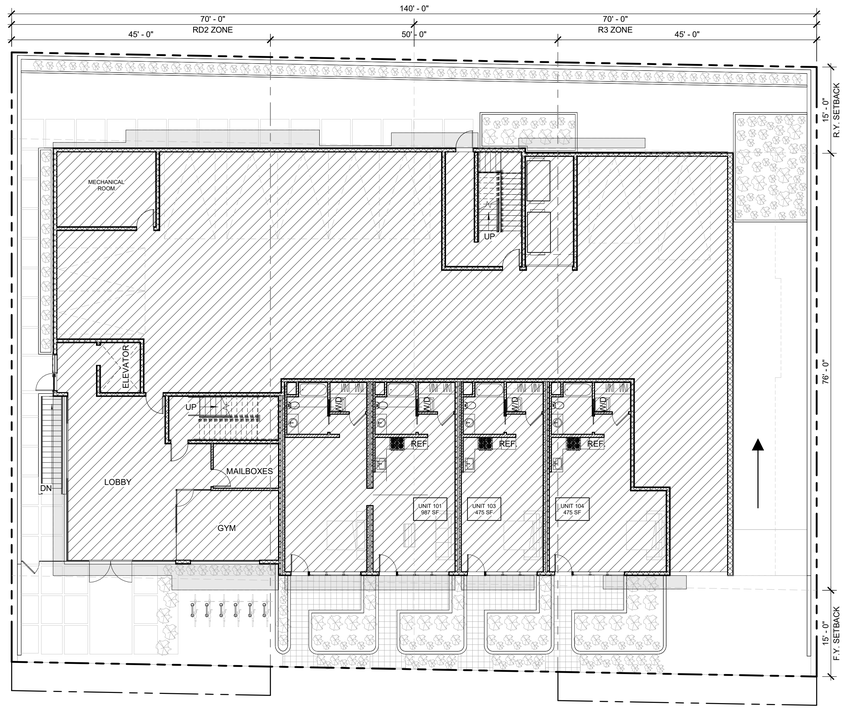
BUILDING CODE FLOOR AREA	
PARKING LEVEL	13,789 SF
-INCLUDED	N/A
FIRST FLOOR	8,383 SF
-INCLUDED	519 SF
SECOND FLOOR	8,527 SF
-INCLUDED	880 SF
THIRD FLOOR	8,527 SF
-INCLUDED	880 SF
FOURTH FLOOR	8,527 SF
-INCLUDED	880 SF
FIFTH FLOOR	7,921 SF
-INCLUDED	N/A SF
TOTAL PROVIDED	**58,729 SF**



KELTON AVE

BUILDING CODE - THIRD FLOOR
3/32" = 1'-0"

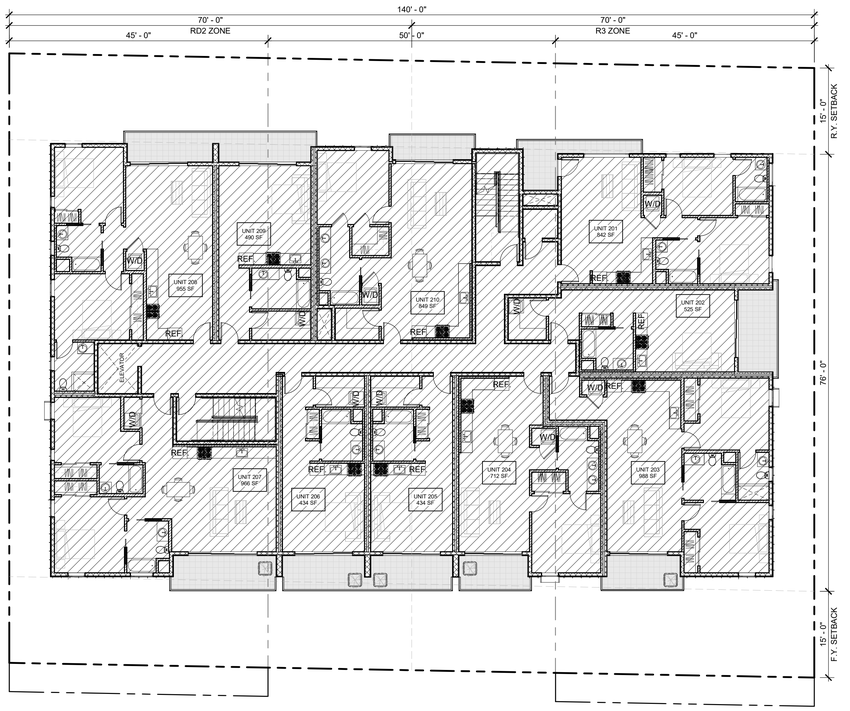
4



KELTON AVE

BUILDING CODE - GROUND FLOOR
3/32" = 1'-0"

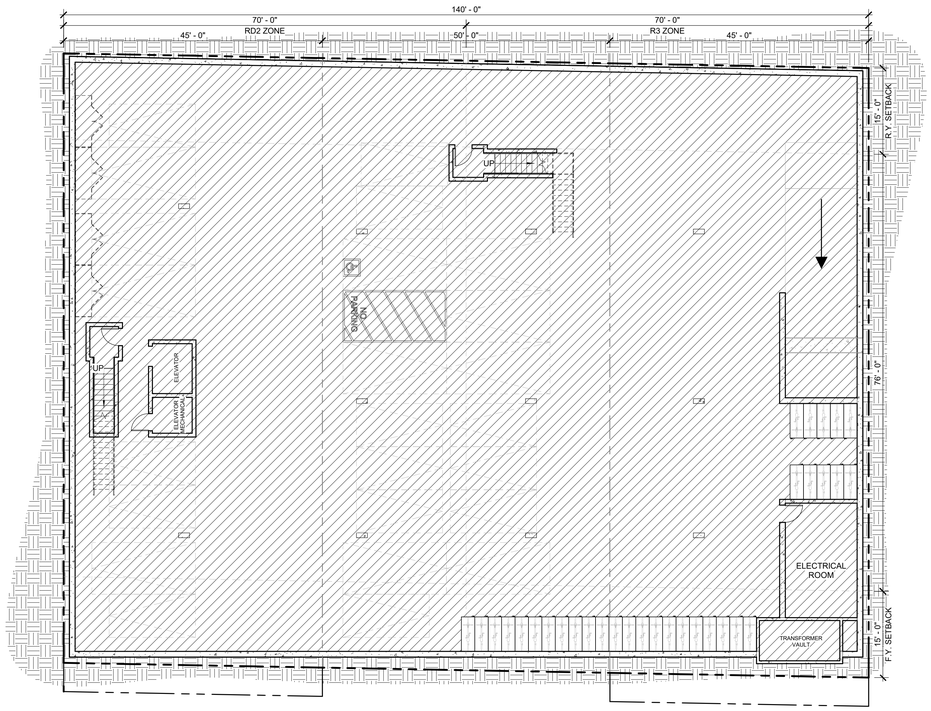
2



KELTON AVE

BUILDING CODE - SECOND FLOOR
3/32" = 1'-0"

3



KELTON AVE

BUILDING CODE - PARKING LEVEL
3/32" = 1'-0"

1

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LICENSED ARCHITECT
RAMSEY DAHAM
No. C-34257
10/31/23
RENEWAL DATE
STATE OF CALIFORNIA

3676 & 3704 KELTON AVE.
LOS ANGELES, CA 90034

Revision Schedule

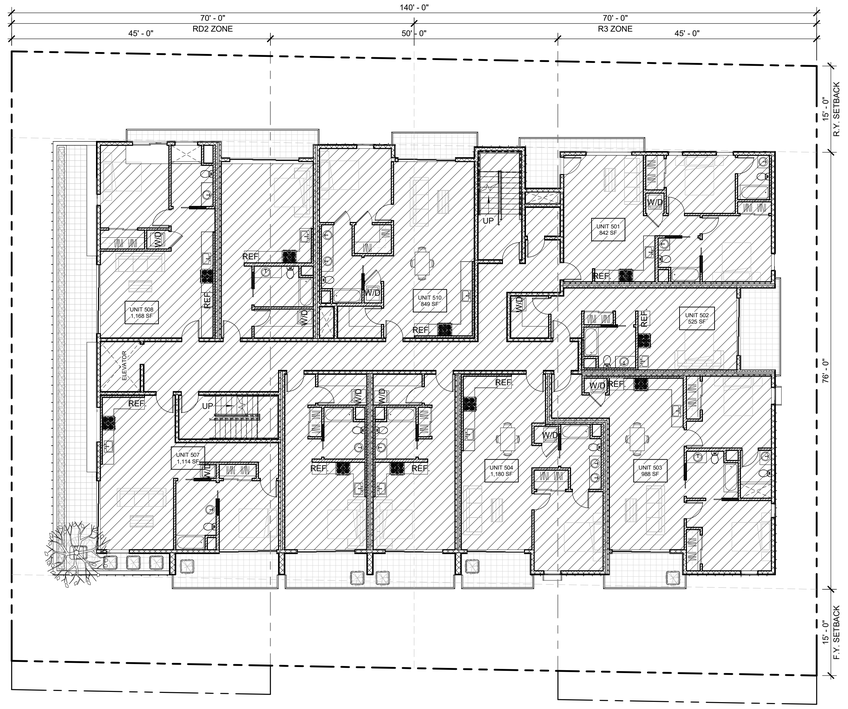
Revision Number	Revision Date

SQUARE FOOTAGE BREAKDOWNS

DRAWN	CR
CHECKED	PNK
DATE	4/13/2023 11:18:53 AM
SCALE	As indicated
JOB #	21-A001

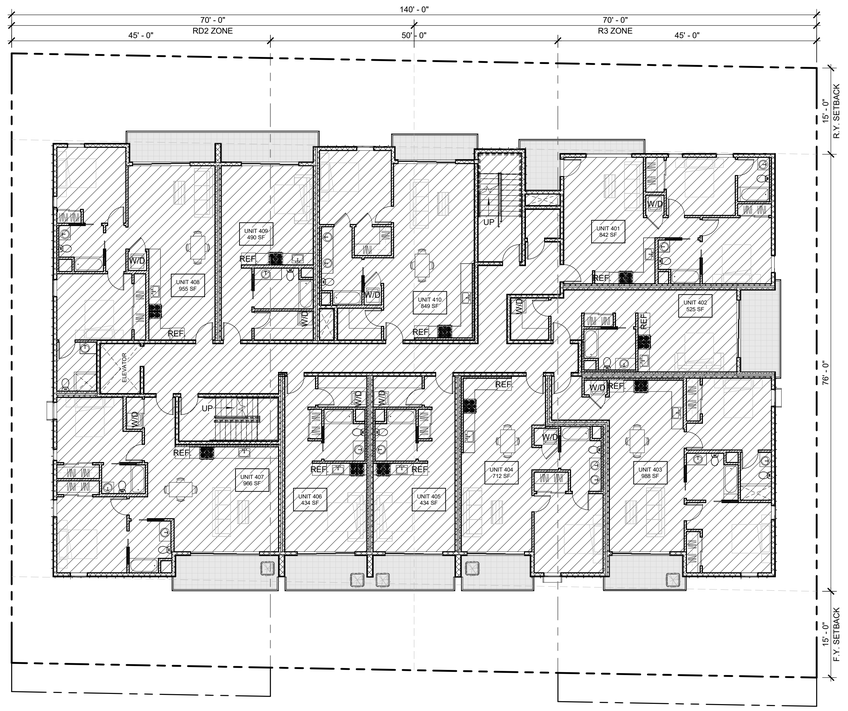
A0.27

BUILDING CODE FLOOR AREA	
PARKING LEVEL	13,789 SF
-INCLUDED	N/A
FIRST FLOOR	8,383 SF
-INCLUDED	519 SF
SECOND FLOOR	8,527 SF
-INCLUDED	880 SF
THIRD FLOOR	8,527 SF
-INCLUDED	880 SF
FOURTH FLOOR	8,527 SF
-INCLUDED	880 SF
FIFTH FLOOR	7,921 SF
-INCLUDED	N/A SF
TOTAL PROVIDED	**58,729 SF**



BUILDING CODE - FIFTH FLOOR
3/32" = 1'-0"

2



BUILDING CODE - FOURTH FLOOR
3/32" = 1'-0"

1

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LICENSED ARCHITECT
RAMSEY DAHAM
No. C-34257
10/31/23
RENEWAL DATE
STATE OF CALIFORNIA

3676 & 3704 KELTON AVE.
LOS ANGELES, CA 90034

Revision Schedule

Revision Number	Revision Date

SQUARE FOOTAGE BREAKDOWNS

DRAWN	Author
CHECKED	Checker
DATE	4/13/2023 10:51:29 AM
SCALE	As indicated
JOB #	21-A001

A0.27.1

OPEN SPACE PROVIDED VS. REQUIRED

PROVIDED		REQUIRED	
COMMON OPEN SPACE GROUND FLOOR:	1,977 SF	29 UNITS @ < 3 HABITABLE ROOMS (100 S.F.) (9 UNITS)(100 S.F.) =	2,900 SF
ROOF DECK RD2 ZONE: R3 ZONE:	1,767 SF 2,150 SF	14 UNITS @ 3 HABITABLE ROOMS (125 S.F.) (14 UNITS)(125 S.F.) =	1,750 SF
PRIVATE OPEN SPACE: RD2 ZONE: 27 UNITS x 50 SF R3 ZONE: 16 UNITS x 50 SF	1,350 SF 800 SF	0 UNITS @ > 3 HABITABLE ROOMS (175 S.F.) (0 UNITS)(175 S.F.) =	0 SF
TOTAL PROVIDED OPEN SPACE: RD2 ZONE: R3 ZONE:	3,117 SF 4,927SF	TOTAL REQUIRED OPEN SPACE:	4,650 SF

* (OPEN SPACE PROVIDED IN THE RD2 ZONE DOES NOT QUALIFY AS USABLE OPEN SPACE PER LAMC 12.21 G.2)

COMMON OPEN SPACE TREE COUNT			
PROVIDED		REQUIRED	
ON SITE	5 TREES	1 TREE PER EVERY 4 UNITS	
IN PARKWAY EXISTING NEW	2 TREES 4 TREES	43 UNITS / 4 =	10.75 TREES
TOTAL	**11 TREES**	**TOTAL**	**10.75 TREES**

COMMON OPEN SPACE PLANTING AREA

ROOF DECK COMMON OPEN SPACE: 2,150 SF
GROUND FLOOR COMMON OPEN SPACE: 965 SF

REQUIRED PLANTING AREA: 778.75 SF
25% OF THE COMMON OPEN SPACE

PROVIDED PLANTING AREA

ROOF DECK: 596 SF
GROUND FLOOR: 360 SF
TOTAL: **956 SF**

KELTON AVE — OPEN SPACE - FOURTH FLOOR 3/32" = 1'-0" — 4

KELTON AVE — OPEN SPACE - SECOND FLOOR 3/32" = 1'-0" — 2

KELTON AVE — OPEN SPACE - THIRD FLOOR 3/32" = 1'-0" — 3

KELTON AVE — OPEN SPACE - GROUND FLOOR 3/32" = 1'-0" — 1

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LICENSED ARCHITECT
RAMSEY DAHAM
No. C-34257
10/31/23
RENEWAL DATE
STATE OF CALIFORNIA

3676 & 3704 KELTON AVE.
LOS ANGELES, CA 90034

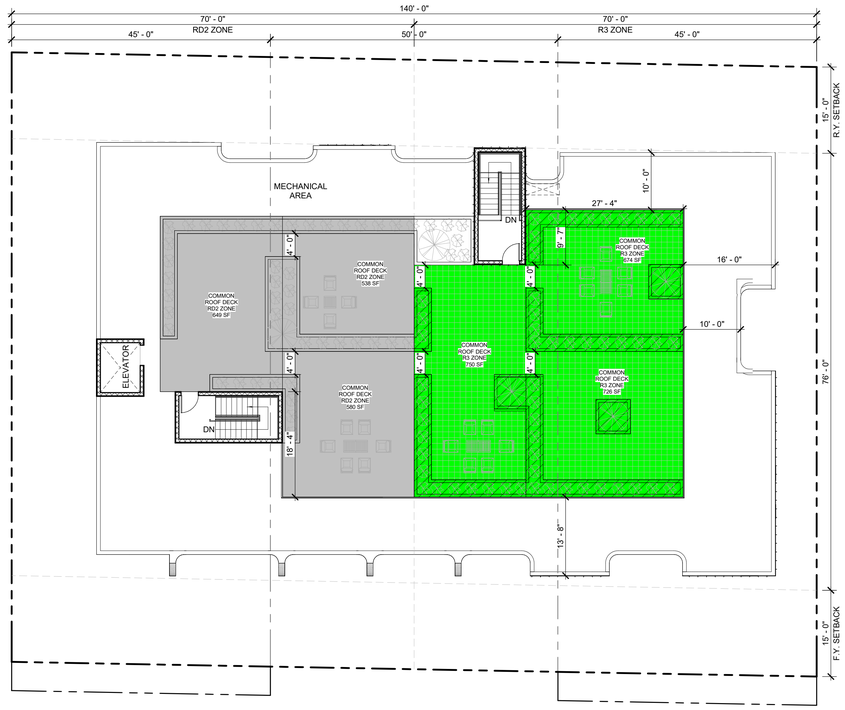
Revision Schedule

Revision Number	Revision Date

OPEN SPACE AREA CALCULATIONS

DRAWN	CR
CHECKED	PNK
DATE	4/13/2023 11:01:24 AM
SCALE	3/32" = 1'-0"
JOB #	21-A001

A0.29



KELTON AVE

OPEN SPACE - ROOF
3/32" = 1'-0"

2

KELTON AVE

OPEN SPACE - FIFTH FLOOR
3/32" = 1'-0"

1

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LICENSED ARCHITECT
RAMSEY DAHAM
No. C-34257
10/31/23
RENEWAL DATE
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3676 & 3704 KELTON AVE.
LOS ANGELES, CA 90034

Revision Schedule

Revision Number	Revision Date

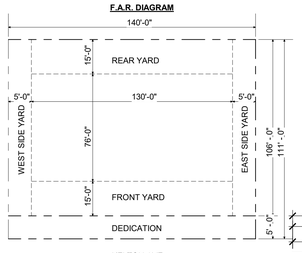
OPEN SPACE AREA CALCULATIONS

DRAWN	CR
CHECKED	PNK
DATE	4/13/2023 11:01:32 AM
SCALE	3/32" = 1'-0"
JOB #	21-A001

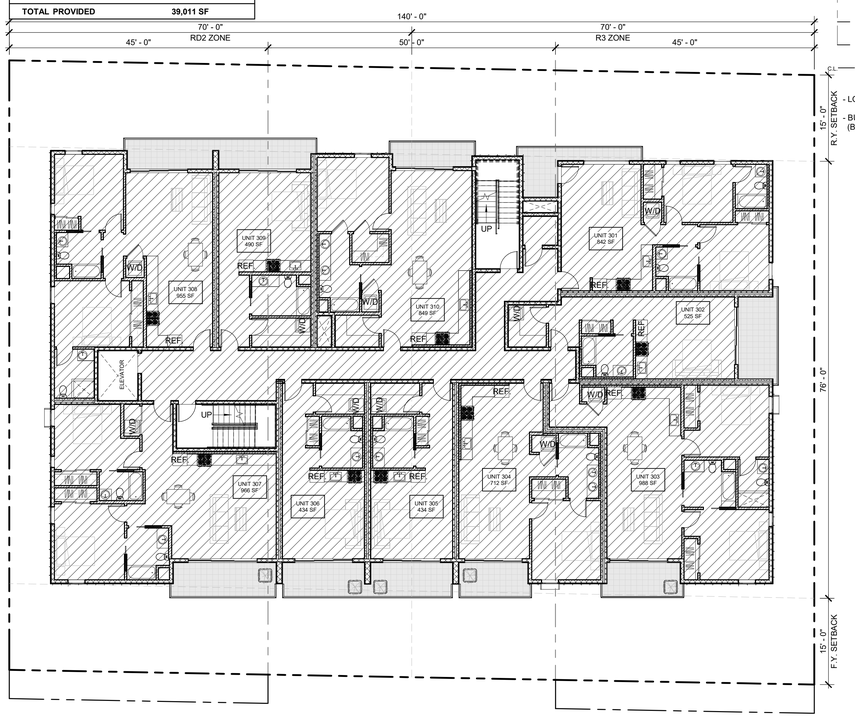
A0.29.1

F.A.R. CALCULATIONS	
PARKING LEVEL	379 SF
-INCLUDED	N/A
FIRST FLOOR	3,328 SF
-INCLUDED	519 SF
SECOND FLOOR	8,175 SF
-INCLUDED	880 SF
THIRD FLOOR	8,175 SF
-INCLUDED	880 SF
FOURTH FLOOR	8,175 SF
-INCLUDED	880 SF
FIFTH FLOOR	7,620 SF
-INCLUDED	N/A SF
TOTAL PROVIDED	**39,011 SF**

F.A.R. DIAGRAM



- LOT AREA: 15,334.6 S.F.
- BUILDABLE AREA: 10,530 S.F.
(BEFORE 5 FT DEDICATION)

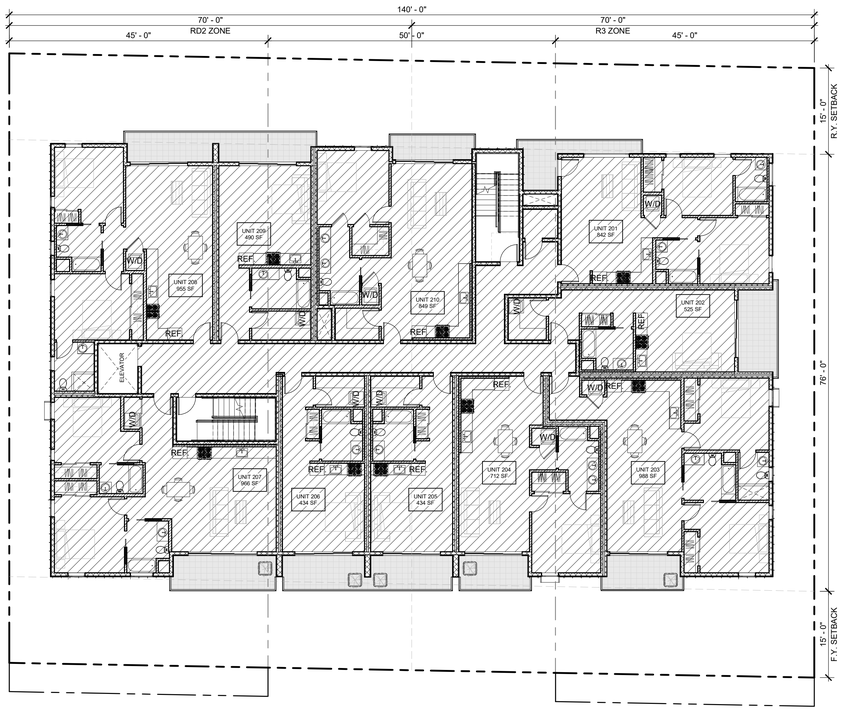


KELTON AVE

F.A.R. - THIRD FLOOR
3/32" = 1'-0"
4

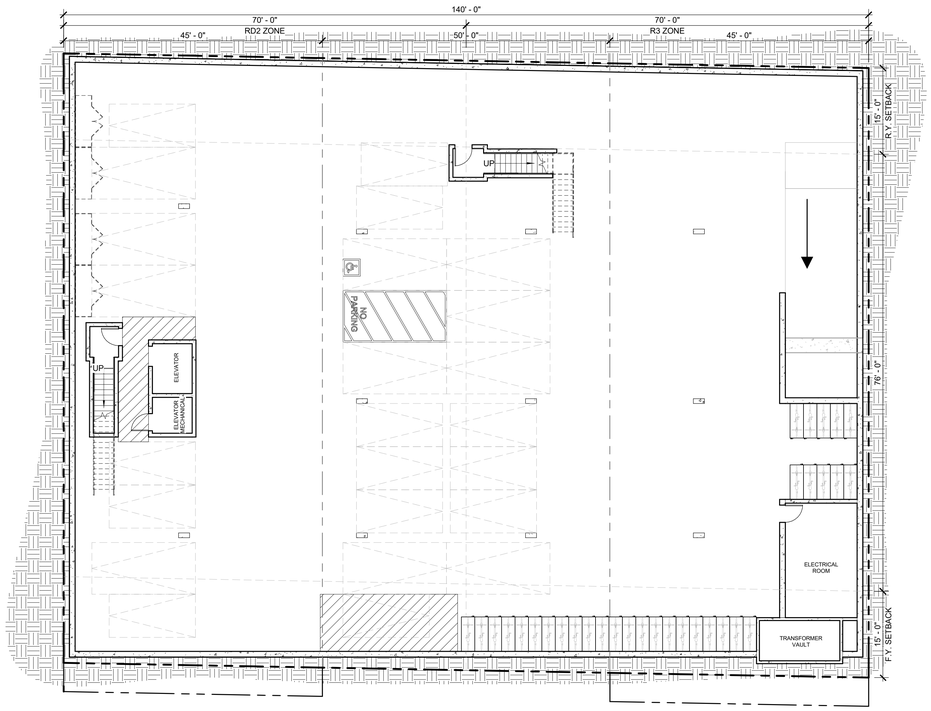
KELTON AVE

F.A.R. - GROUND FLOOR
3/32" = 1'-0"
2



KELTON AVE

F.A.R. - SECOND FLOOR
3/32" = 1'-0"
3



KELTON AVE

F.A.R. - PARKING LEVEL
3/32" = 1'-0"
1

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LICENSED ARCHITECT
RAMSEY DAHAM
No. C-34257
10/31/23
RENEWAL DATE
STATE OF CALIFORNIA

3676 & 3704 KELTON AVE.
LOS ANGELES, CA 90034

Revision Schedule

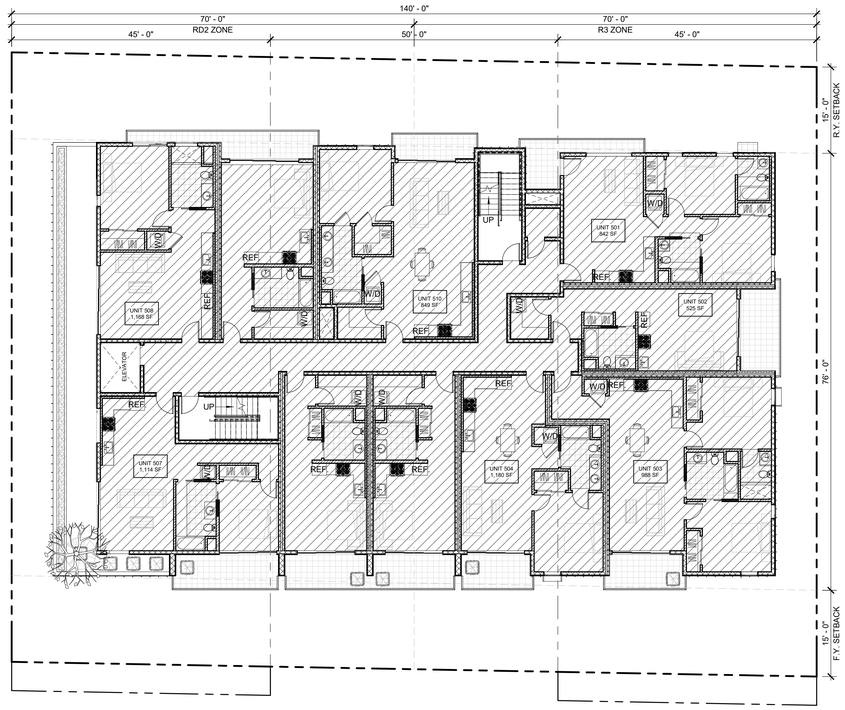
Revision Number	Revision Date

F.A.R. CALCULATIONS

DRAWN	CR
CHECKED	PNK
DATE	4/13/2023 11:16:57 AM
SCALE	As indicated
JOB #	21-A001

A0.30

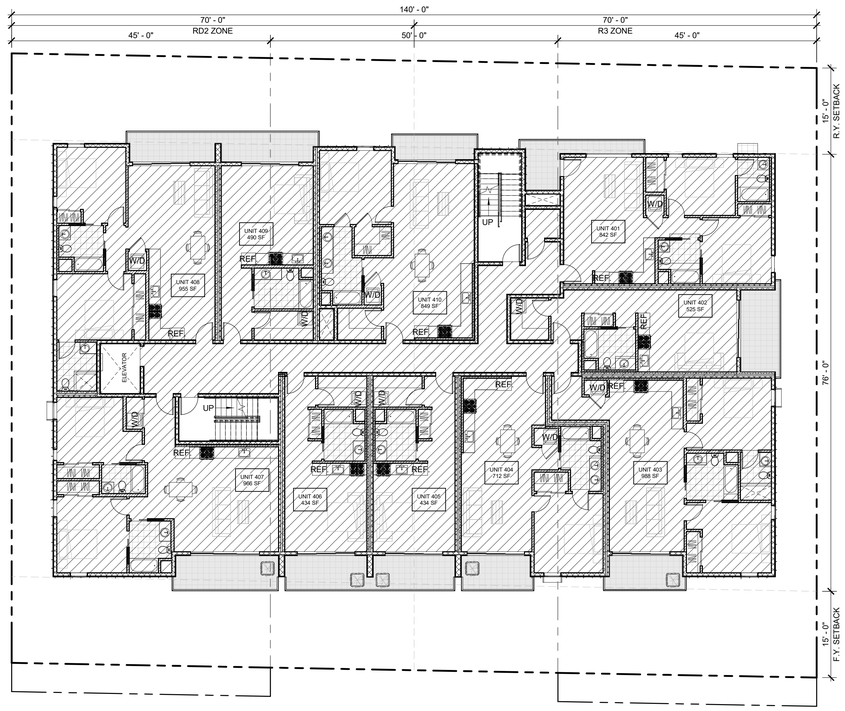
F.A.R. CALCULATIONS	
PARKING LEVEL	379 SF
-INCLUDED	N/A
FIRST FLOOR	3,328 SF
-INCLUDED	519 SF
SECOND FLOOR	8,175 SF
-INCLUDED	880 SF
THIRD FLOOR	8,175 SF
-INCLUDED	880 SF
FOURTH FLOOR	8,175 SF
-INCLUDED	880 SF
FIFTH FLOOR	7,620 SF
-INCLUDED	N/A SF
TOTAL PROVIDED	**39,011 SF**



KELTON AVE

F.A.R. - FIFTH FLOOR
3/32" = 1'-0"

2



KELTON AVE

F.A.R. - FOURTH FLOOR
3/32" = 1'-0"

1

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127 arena street, el segundo, ca 90245
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LICENSED ARCHITECT
RAMSEY DAHAM
No. C-34257
10/31/23
RENEWAL DATE
STATE OF CALIFORNIA

3676 & 3704 KELTON AVE.
LOS ANGELES, CA 90034

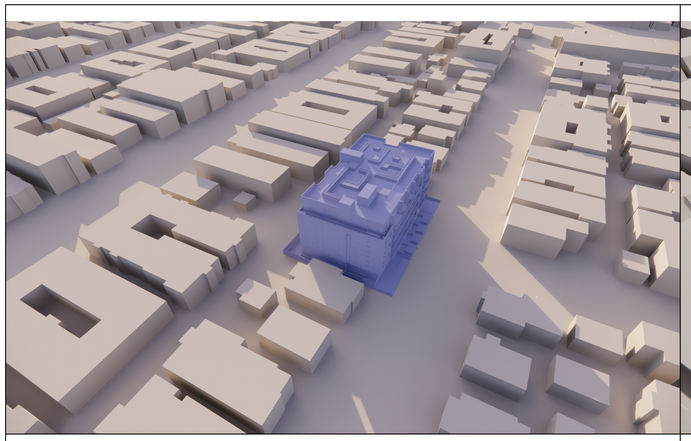
Revision Schedule

Revision Number	Revision Date

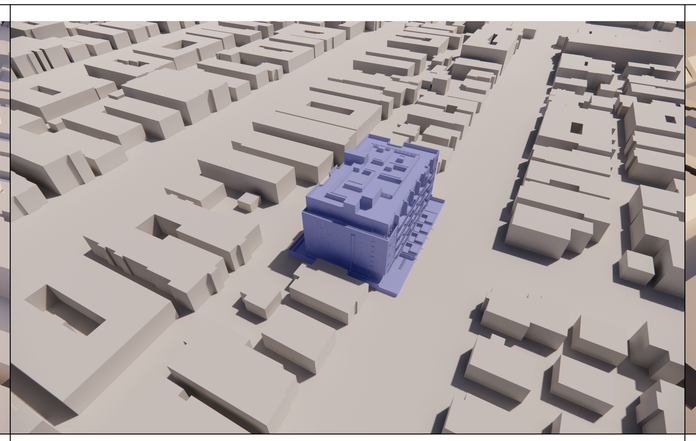
F.A.R. CALCULATIONS

DRAWN	Author
CHECKED	Checker
DATE	4/13/2023 10:52:24 AM
SCALE	As indicated
JOB #	21-A001

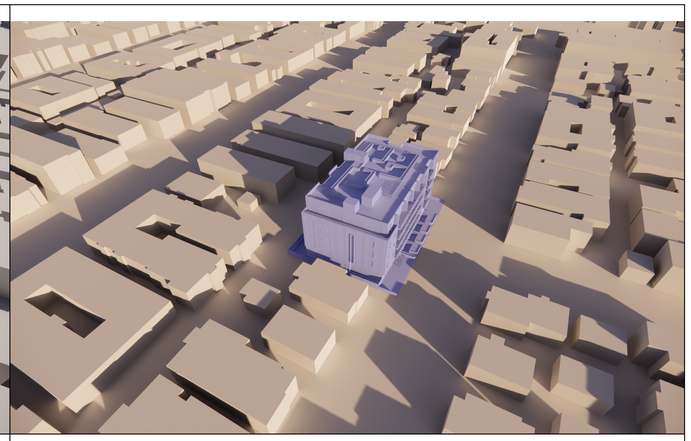
A0.30.1



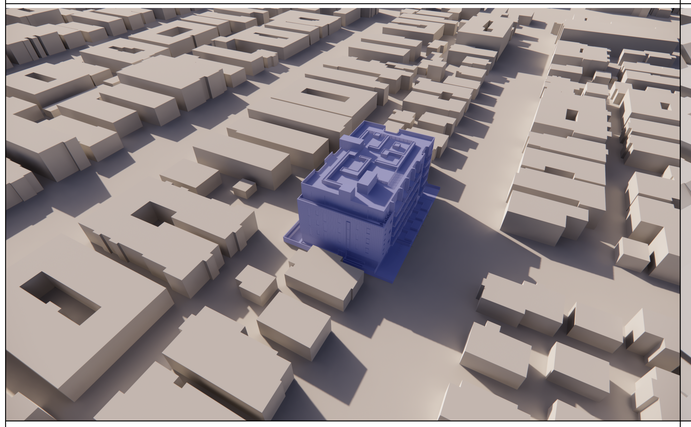
SPRING — MARCH 20 - 7:00 AM



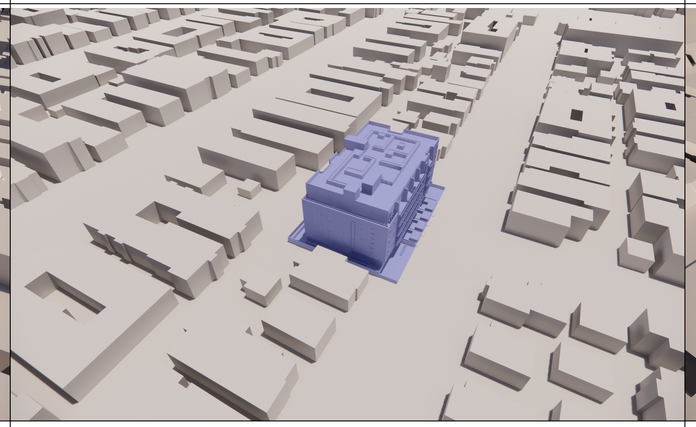
MARCH 20 - 12:00 PM



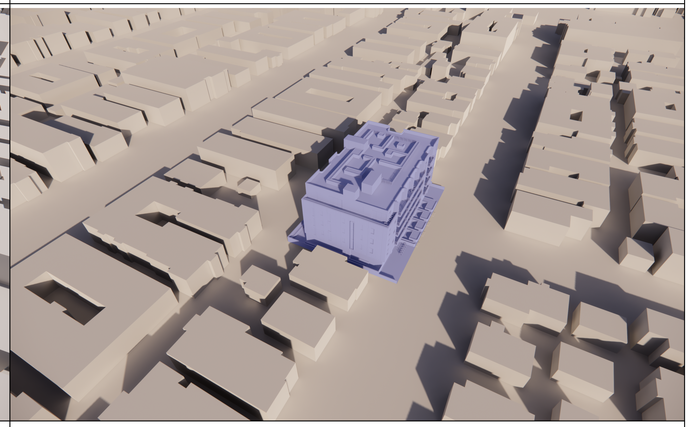
MARCH 20 - 5:00 PM



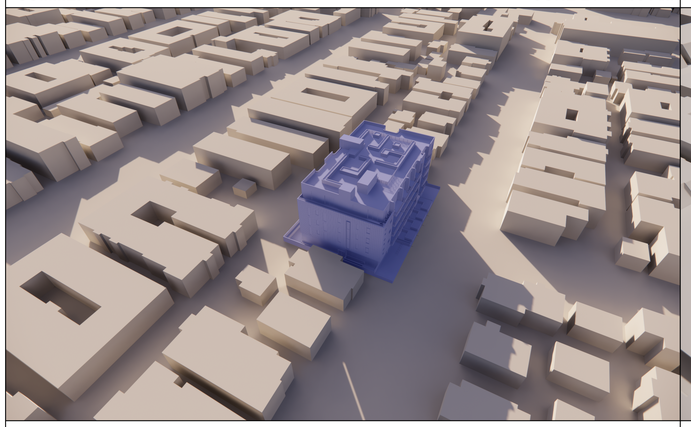
SUMMER — JUNE 21 - 7:00 AM



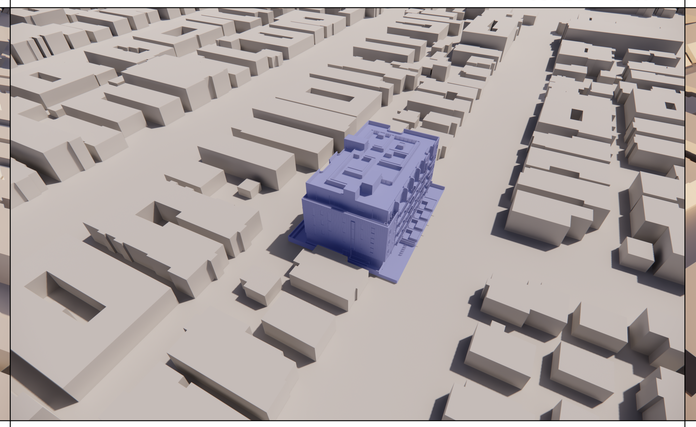
JUNE 21 - 12:00 PM



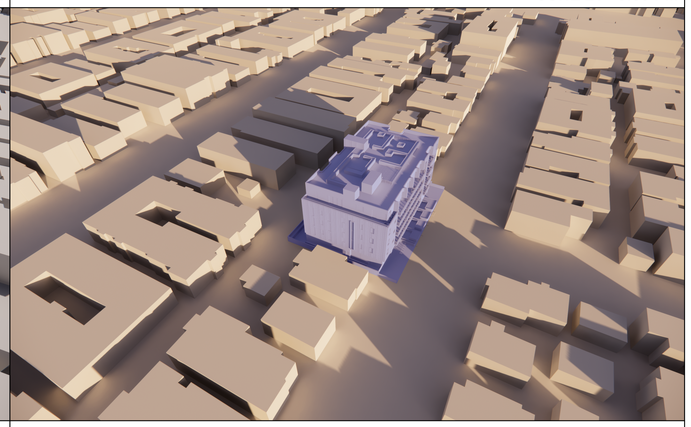
JUNE 21 - 5:00 PM



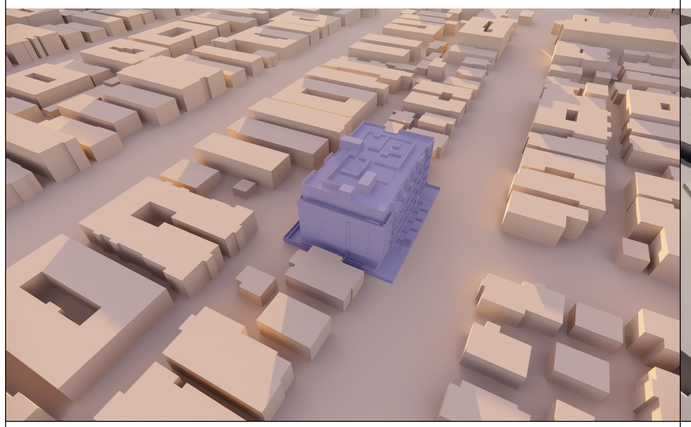
FALL — SEPTEMBER 22 - 7:00 AM



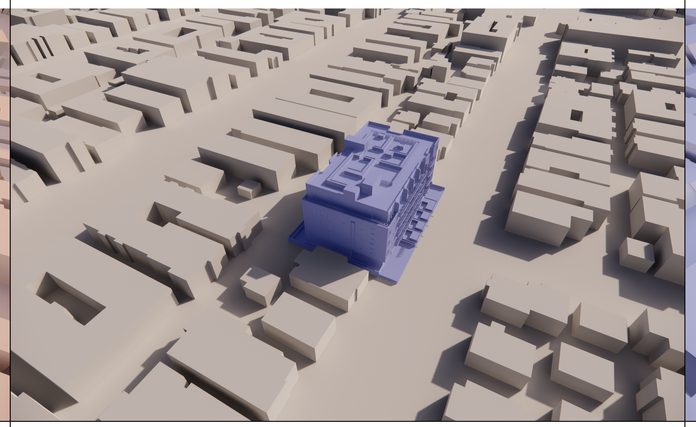
SEPTEMBER 22 - 12:00 PM



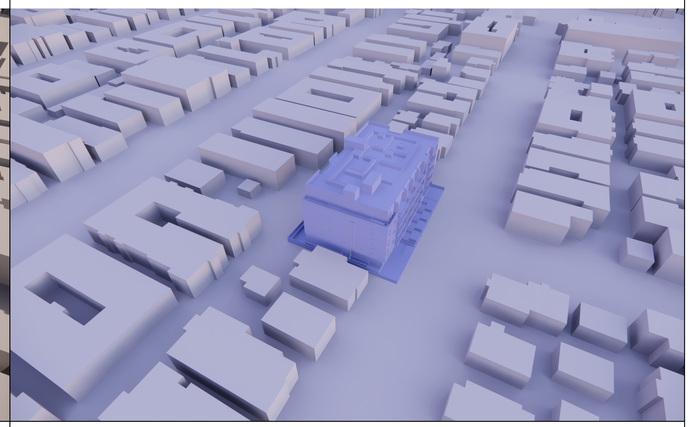
SEPTEMBER 22 - 5:00 PM



WINTER — DECEMBER 21 - 7:00 AM



DECEMBER 21 - 12:00 PM



DECEMBER 21 - 5:00 PM

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LICENSED ARCHITECT
RAMSEY DAHAM
No. C-34257
10/31/23
RENEWAL DATE
STATE OF CALIFORNIA

3676 & 3704 KELTON AVE.
LOS ANGELES, CA 90034

Revision Schedule

Revision Number	Revision Date

SHADOW STUDY

DRAWN	CR
CHECKED	PNK
DATE	4/13/2023 9:59:40 AM
SCALE	
JOB #	21-A001

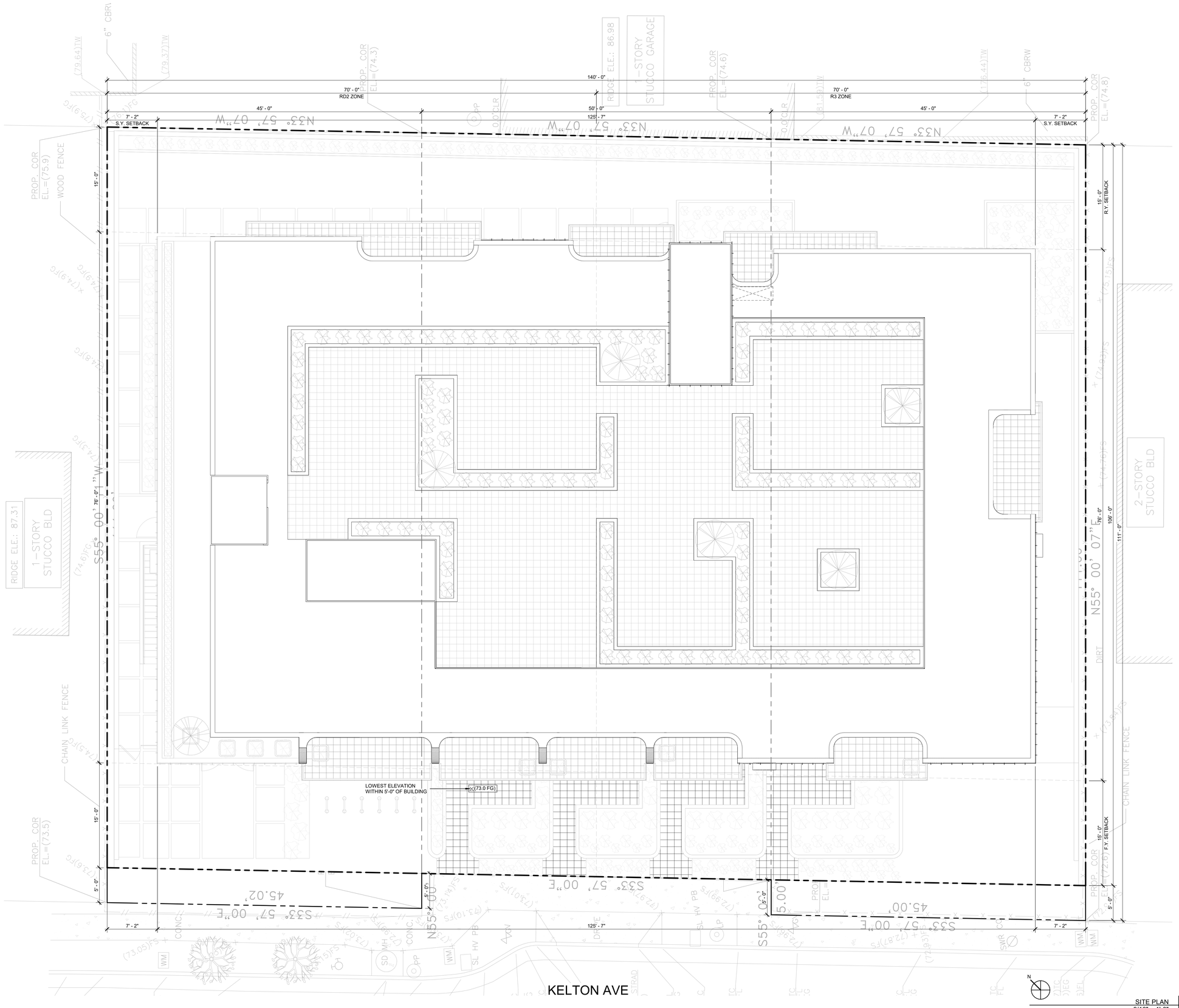
A0.32

SITE PLAN LEGEND

FLOOR TYPE

PROPERTY LINE

SETBACKS



SITE PLAN
3/16" = 1'-0"

1

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127 arena street, el segundo, ca 90245

[o] 310.322.3700

LICENSED ARCHITECT
RAMSEY DAHAM
No. C-34257
10/31/23
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3676 & 3704 KELTON AVE.
LOS ANGELES, CA 90034

Revision Schedule

Revision Number	Revision Date

SITE PLAN

DRAWN	CR
CHECKED	PNK
DATE	4/13/2023 10:52:29 AM
SCALE	As indicated
JOB #	21-A001

A1.00

ASSEMBLY TYPES

WALLS

1. 2x4 & 2x6 INTERIOR WALL ASSM. (1/A0.13)
2. 2x6 PLUMBING WALL ASSM. (2/A0.13)
3. 1-HR 2x6 INTERIOR WALL ASSM. (3/A0.13)
4. 1-HR 2x6 EXTERIOR WALL ASSM. (4/A0.13)
5. 1-HR DOUBLE WALL ASSM. (5/A0.13)
6. CONC. WALL PER STRC. (6/A0.13)
7. CONC. RETAINING WALL PER STRC. (7/A0.13)
8. CMU WALL PER STRC. (8/A0.13)
9. 2-HR INTERIOR CORRIDOR WALL ASSM. (9/A0.13)

FLOORS

- A1 CONC FLOOR PER STRC. W/ POLISHED CONC. FINISH (10/A0.13)
- A2 CONC FLOOR PER STRC. W/ WOOD FINISH (11/A0.13)
- A3 CONC FLOOR PER STRC. W/ TILE FINISH (12/A0.13)
- B1 WOOD JOIST PER STRC. W/ WOOD FINISH (13/A0.13)
- B2 WOOD JOIST PER STRC. W/ TILE FINISH (14/A0.13)
- B3 WOOD JOIST W/ WOOD DECK PER SPEC. (15/A0.13)
- B4 WOOD JOIST PER STRC. W/ METAL ROOFING PER SPEC. (16/A0.13)

FLOOR PLAN LEGEND

- FLOOR TYPE
- WINDOW TAG (A0.10 - SCHEDULE)
- DOOR TAG (A0.08, A0.09 - SCHEDULE)
- WALL TYPE
- ELEVATION MARKER
- PROPERTY LINE
- ACCESSIBLE ROUTE
- 1 HR
- 2 HR
- SD SMOKE DETECTOR
- CM CARBON MONOXIDE
- EXHAUST
- NFPA - 14 CLASS - I STANDPIPE
- ILLUMINATED EXIT SIGN
- MB MASTER BEDROOM
- BD BEDROOM
- BA BATHROOM
- LR LIVING ROOM
- KI KITCHEN
- DR DINING ROOM
- PWR POWDER ROOM
- CL CLOSET
- WIC WALK IN CLOSET
- LR LAUNDRY ROOM
- BC BALCONY
- EN ENTRY

EV PARKING CALCULATIONS

BASED ON 33 PROVIDED PARKING SPACES
STANDARD / COMPACT - 32
ADA - 1

EV PARKING SPACES

EV TOTAL:
32 x 30% = 9.6 → 10

EV READY:
32 x 25% = 8

EVCS:
32 x 10% = 3.2 → 4

ADA EV PARKING SPACES

ADA EV TOTAL:
1 x 30% = 0.30 → 1

ADA EV READY:
1 x 25% = 0.25 → 1

ADA EVCS:
1 x 10% = 0.10 → 1

PROVIDED EV SPACES

8 EV READY (9'x18' STALLS)

4 EVCS (9'x18' STALLS)

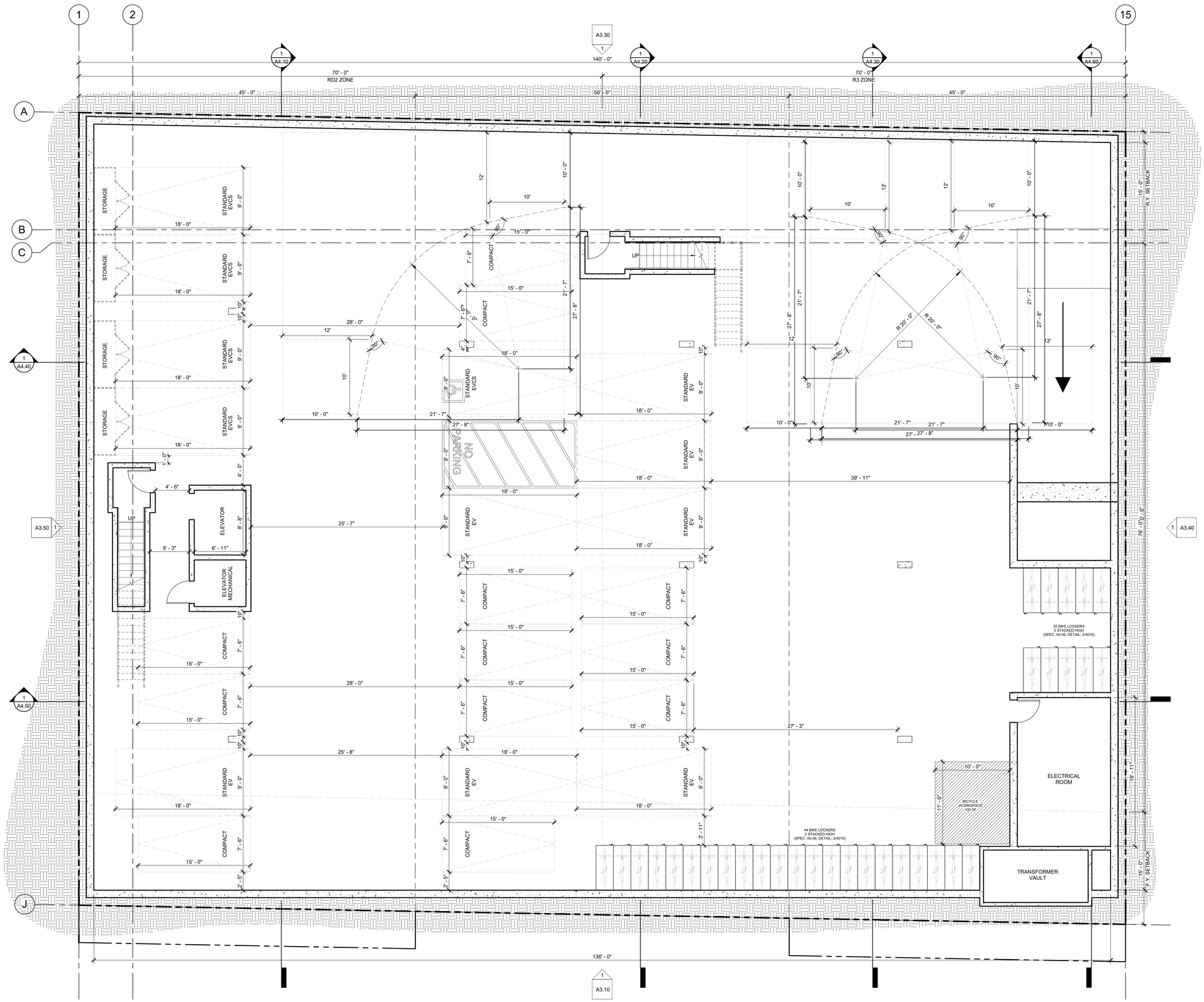
1 ADA EVCS (9'x18' STALL)

DEFINITIONS:

EVSE: ELECTRIC VEHICLE SUPPLY EQUIPMENT

EVCS: ELECTRIC VEHICLE CHARGING STATION

NOTE: DOUBLE STRIPING OF STALLS SHALL BE PER ZONING CODE SECTION 12.21A5 CHART NO. 5



KELTON AVE

N

SUBTERRANEAN PARKING
3/16" = 1'-0"

1

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LICENSED ARCHITECT
RAMSEY DAHAM
No. C-34257
10/31/23
RENEWAL DATE
STATE OF CALIFORNIA

3676 & 3704 KELTON AVE.
LOS ANGELES, CA 90034

Revision Schedule

Revision Number	Revision Date

PROPOSED PLANS

DRAWN	CR
CHECKED	PNK
DATE	4/13/2023 10:52:33 AM
SCALE	As indicated
JOB #	21-A001

A2.00

ASSEMBLY TYPES

WALLS

1. 2x4 & 2x6 INTERIOR WALL ASSM. (1/A0.13)
2. 2x6 PLUMBING WALL ASSM. (2/A0.13)
3. 1-HR 2x6 INTERIOR WALL ASSM. (3/A0.13)
4. 1-HR 2x6 EXTERIOR WALL ASSM. (4/A0.13)
5. 1-HR DOUBLE WALL ASSM. (5/A0.13)
6. CONC. WALL PER STRC. (6/A0.13)
7. CONC. RETAINING WALL PER STRC. (7/A0.13)
8. CMU WALL PER STRC. (8/A0.13)
9. 2-HR INTERIOR CORRIDOR WALL ASSM. (9/A0.13)

FLOORS

1. CONC FLOOR PER STRC. W/ POLISHED CONC. FINISH (10/A0.13)
2. CONC FLOOR PER STRC. W/ WOOD FINISH (11/A0.13)
3. CONC FLOOR PER STRC. W/ TILE FINISH (12/A0.13)
4. WOOD JOIST PER STRC. W/ WOOD FINISH (13/A0.13)
5. WOOD JOIST PER STRC. W/ TILE FINISH (14/A0.13)
6. WOOD JOIST W/ WOOD DECK PER SPEC. (15/A0.13)
7. WOOD JOIST PER STRC. W/ METAL ROOFING PER SPEC. (16/A0.13)

FLOOR PLAN LEGEND

- FLOOR TYPE
- WINDOW TAG (A0.10 - SCHEDULE)
- DOOR TAG (A0.08, A0.09 - SCHEDULE)
- WALL TYPE
- ELEVATION MARKER
- PROPERTY LINE
- ACCESSIBLE ROUTE
- 1 HR
- 2 HR
- SD — SMOKE DETECTOR
- CM — CARBON MONOXIDE
- EXHAUST
- NFPA - 14 CLASS - I STANDPIPE
- ILLUMINATED EXIT SIGN
- MB — MASTER BEDROOM
- BD — BEDROOM
- BA — BATHROOM
- LR — LIVING ROOM
- KI — KITCHEN
- DR — DINING ROOM
- PWR — POWDER ROOM
- CL — CLOSET
- WIC — WALK IN CLOSET
- LR — LAUNDRY ROOM
- BC — BALCONY
- EN — ENTRY

EV PARKING CALCULATIONS

BASED ON 33 PROVIDED PARKING SPACES
STANDARD / COMPACT - 32
ADA - 1

EV PARKING SPACES

EV TOTAL:
32 x 30% = 9.6 → 10

EV READY:
32 x 25% = 8

EVCS:
32 x 10% = 3.2 → 4

ADA EV PARKING SPACES

ADA EV TOTAL:
1 x 30% = 0.30 → 1

ADA EV READY:
1 x 25% = 0.25 → 1

ADA EVCS:
1 x 10% = 0.10 → 1

PROVIDED EV SPACES

8 EV READY (9'x18' STALLS)

4 EVCS (9'x18' STALLS)

1 ADA EVCS (9'x18' STALL)

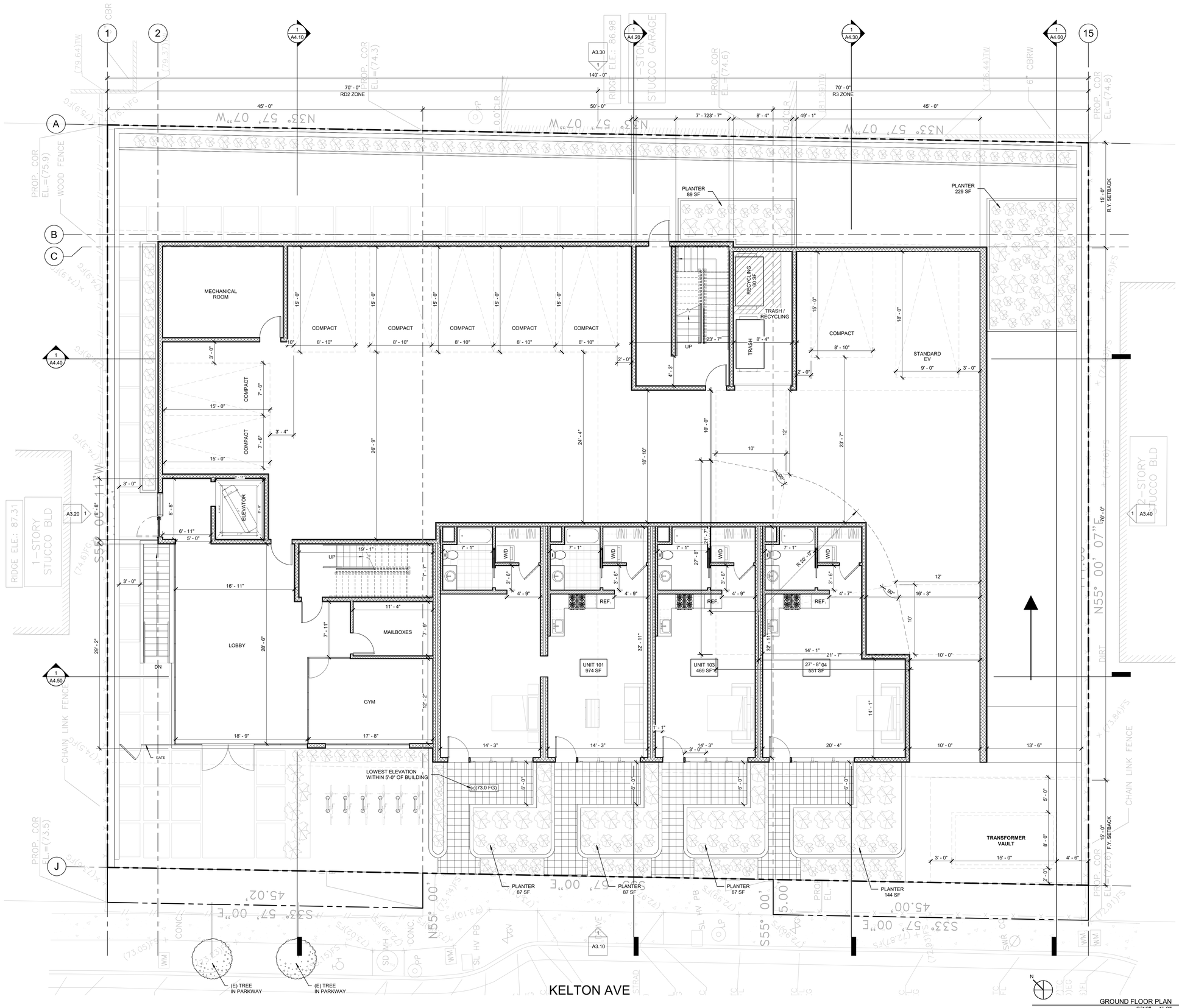
DEFINITIONS:

EVSE: ELECTRIC VEHICLE SUPPLY EQUIPMENT

EVCS: ELECTRIC VEHICLE CHARGING STATION

FENCES, PLANTERS, AND RETAINING WALLS SHALL NOT EXCEED A HEIGHT OF 6 FT. ABOVE THE NATURAL GROUND LEVEL IN THE REQUIRED SIDE YARD.

NOTE: DOUBLE STRIPING OF STALLS SHALL BE PER ZONING CODE SECTION 12.21A5 CHART NO. 5



GROUND FLOOR PLAN
3/16" = 1'-0"

1

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LICENSED ARCHITECT
RAMSEY DAHAM
No. C-34257
10/31/23
RENEWAL DATE
STATE OF CALIFORNIA

3676 & 3704 KELTON AVE.
LOS ANGELES, CA 90034

Revision Schedule

Revision Number	Revision Date

PROPOSED PLANS

DRAWN	CR
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DATE	4/13/2023 10:52:35 AM
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3676 & 3704 KELTON AVE.
LOS ANGELES, CA 90034

Revision Schedule	
Revision Number	Revision Date

PROPOSED PLANS

DRAWN	CR
CHECKED	PNK
DATE	4/13/2023 10:52:44 AM
SCALE	As indicated
JOB #	21-A001

A2.20

1 SECOND FLOOR PLAN
3/16" = 1'-0"

ASSEMBLY TYPES

WALLS
1. 2-HR 2x6 INTERIOR WALL ASSM. (1/A0.13)
2. 2x6 PLUMBING WALL ASSM. (2/A0.13)
3. 1-HR 2x6 INTERIOR WALL ASSM. (3/A0.13)
4. 1-HR 2x6 EXTERIOR WALL ASSM. (4/A0.13)
5. 1-HR DOUBLE WALL ASSM. (5/A0.13)
6. CONC. WALL PER STRC. (6/A0.13)
7. CONC. RETAINING WALL PER STRC. (7/A0.13)
8. CMU WALL PER STRC. (8/A0.13)
9. 2-HR INTERIOR CORRIDOR WALL ASSM. (9/A0.13)

FLOORS
1. CONC FLOOR PER STRC. W/ POLISHED CONC. FINISH (10/A0.13)
2. CONC FLOOR PER STRC. W/ WOOD FINISH (11/A0.13)
3. CONC FLOOR PER STRC. W/ TILE FINISH (12/A0.13)
4. WOOD JOIST PER STRC. W/ WOOD FINISH (13/A0.13)
5. WOOD JOIST PER STRC. W/ TILE FINISH (14/A0.13)
6. WOOD JOIST W/ WOOD DECK PER SPEC. (15/A0.13)
7. WOOD JOIST PER STRC. W/ METAL ROOFING PER SPEC. (16/A0.13)

FLOOR PLAN LEGEND

- FLOOR TYPE
- WINDOW TAG (A0.10 - SCHEDULE)
- DOOR TAG (A0.08, A0.09 - SCHEDULE)
- WALL TYPE
- ELEVATION MARKER
- PROPERTY LINE
- ACCESSIBLE ROUTE
- 1 HR
- 2 HR
- SMOKE DETECTOR
- CARBON MONOXIDE
- EXHAUST
- NFPA - 14 CLASS - I STANDPIPE
- ILLUMINATED EXIT SIGN
- MB MASTER BEDROOM
- BD BEDROOM
- BA BATHROOM
- LR LIVING ROOM
- KI KITCHEN
- DR DINING ROOM
- PWR POWDER ROOM
- CL CLOSET
- WIC WALK IN CLOSET
- LR LAUNDRY ROOM
- BC BALCONY
- EN ENTRY

ASSEMBLY TYPES

WALLS

1. 2x4 & 2x6 INTERIOR WALL ASSM. (1/A0.13)
2. 2x6 PLUMBING WALL ASSM. (2/A0.13)
3. 1-HR 2x6 INTERIOR WALL ASSM. (3/A0.13)
4. 1-HR 2x6 EXTERIOR WALL ASSM. (4/A0.13)
5. 1-HR DOUBLE WALL ASSM. (5/A0.13)
6. CONC. WALL PER STRC. (6/A0.13)
7. CONC. RETAINING WALL PER STRC. (7/A0.13)
8. CMU WALL PER STRC. (8/A0.13)
9. 2-HR INTERIOR CORRIDOR WALL ASSM. (9/A0.13)

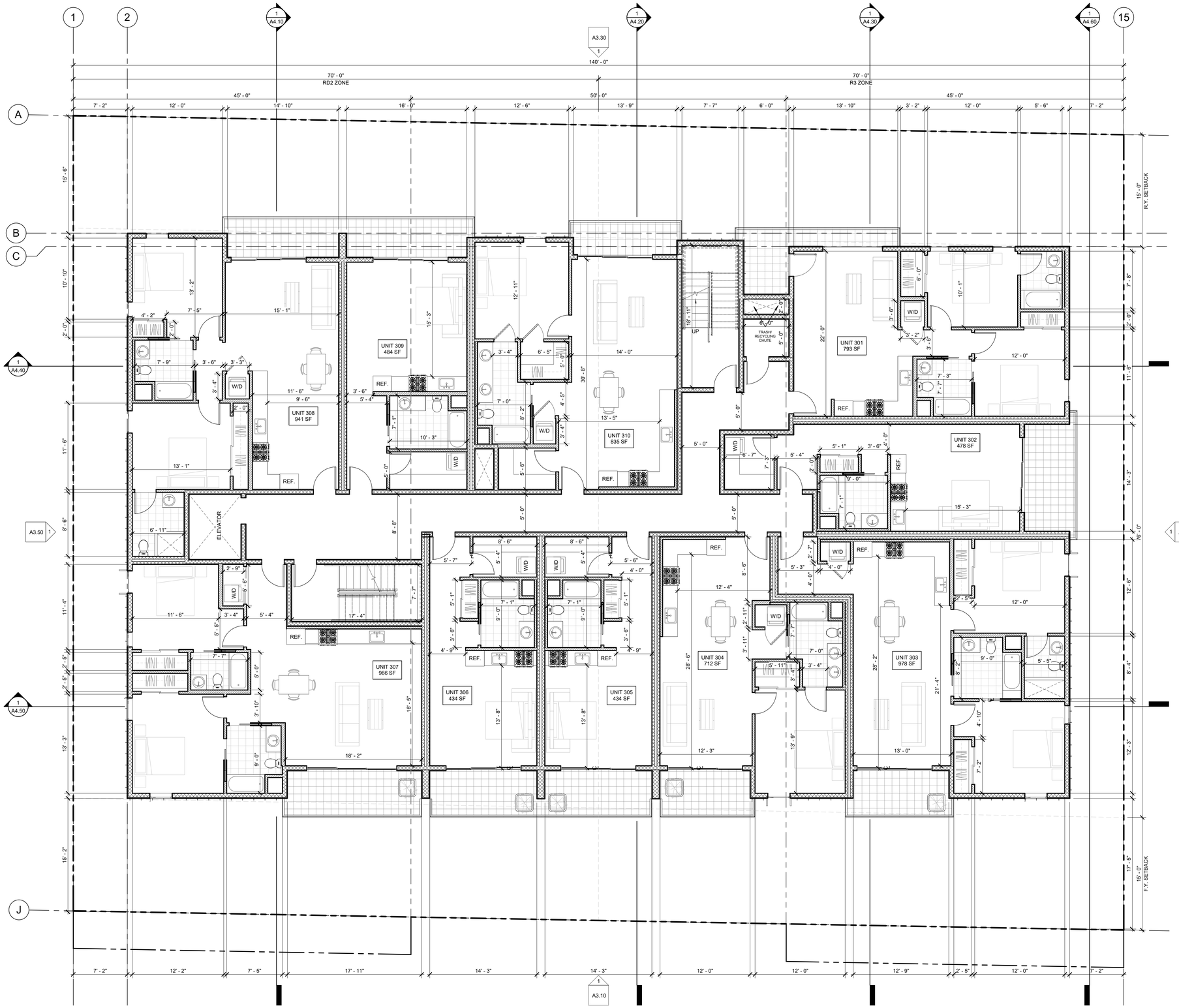
FLOORS

- A1 CONC FLOOR PER STRC. W/ POLISHED CONC. FINISH (10/A0.13)
- A2 CONC FLOOR PER STRC. W/ WOOD FINISH (11/A0.13)
- A3 CONC FLOOR PER STRC. W/ TILE FINISH (12/A0.13)
- B1 WOOD JOIST PER STRC. W/ WOOD FINISH (13/A0.13)
- B2 WOOD JOIST PER STRC. W/ TILE FINISH (14/A0.13)
- B3 WOOD JOIST W/ WOOD DECK PER SPEC. (15/A0.13)
- B4 WOOD JOIST PER STRC. W/ METAL ROOFING PER SPEC. (16/A0.13)

FLOOR PLAN LEGEND

- FLOOR TYPE
- WINDOW TAG (A0.10 - SCHEDULE)
- DOOR TAG (A0.08, A0.09 - SCHEDULE)
- WALL TYPE
- X-X" ELEVATION MARKER
- PROPERTY LINE
- ACCESSIBLE ROUTE
- 1 HR
- 2 HR
- SD SMOKE DETECTOR
- CM CARBON MONOXIDE
- EXHAUST
- S NFPA - 14 CLASS - I STANDPIPE
- ILLUMINATED EXIT SIGN

MB	MASTER BEDROOM
BD	BEDROOM
BA	BATHROOM
LR	LIVING ROOM
KI	KITCHEN
DR	DINING ROOM
PWR	POWDER ROOM
CL	CLOSET
WIC	WALK IN CLOSET
LR	LAUNDRY ROOM
BC	BALCONY
EN	ENTRY



N

THIRD FLOOR PLAN
3/16" = 1'-0"

1

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LICENSED ARCHITECT
RAMSEY DAHAM
No. C-34257
10/31/23
RENEWAL DATE
STATE OF CALIFORNIA

3676 & 3704 KELTON AVE.
LOS ANGELES, CA 90034

Revision Schedule

Revision Number	Revision Date

PROPOSED PLANS

DRAWN	CR
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DATE	4/13/2023 10:52:53 AM
SCALE	As indicated
JOB #	21-A001

A2.30

ASSEMBLY TYPES

WALLS

1. 2x4 & 2x6 INTERIOR WALL ASSM. (1/A0.13)
2. 2x6 PLUMBING WALL ASSM. (2/A0.13)
3. 1-HR 2x6 INTERIOR WALL ASSM. (3/A0.13)
4. 1-HR 2x6 EXTERIOR WALL ASSM. (4/A0.13)
5. 1-HR DOUBLE WALL ASSM. (5/A0.13)
6. CONC. WALL PER STRC. (6/A0.13)
7. CONC. RETAINING WALL PER STRC. (7/A0.13)
8. CMU WALL PER STRC. (8/A0.13)
9. 2-HR INTERIOR CORRIDOR WALL ASSM. (9/A0.13)

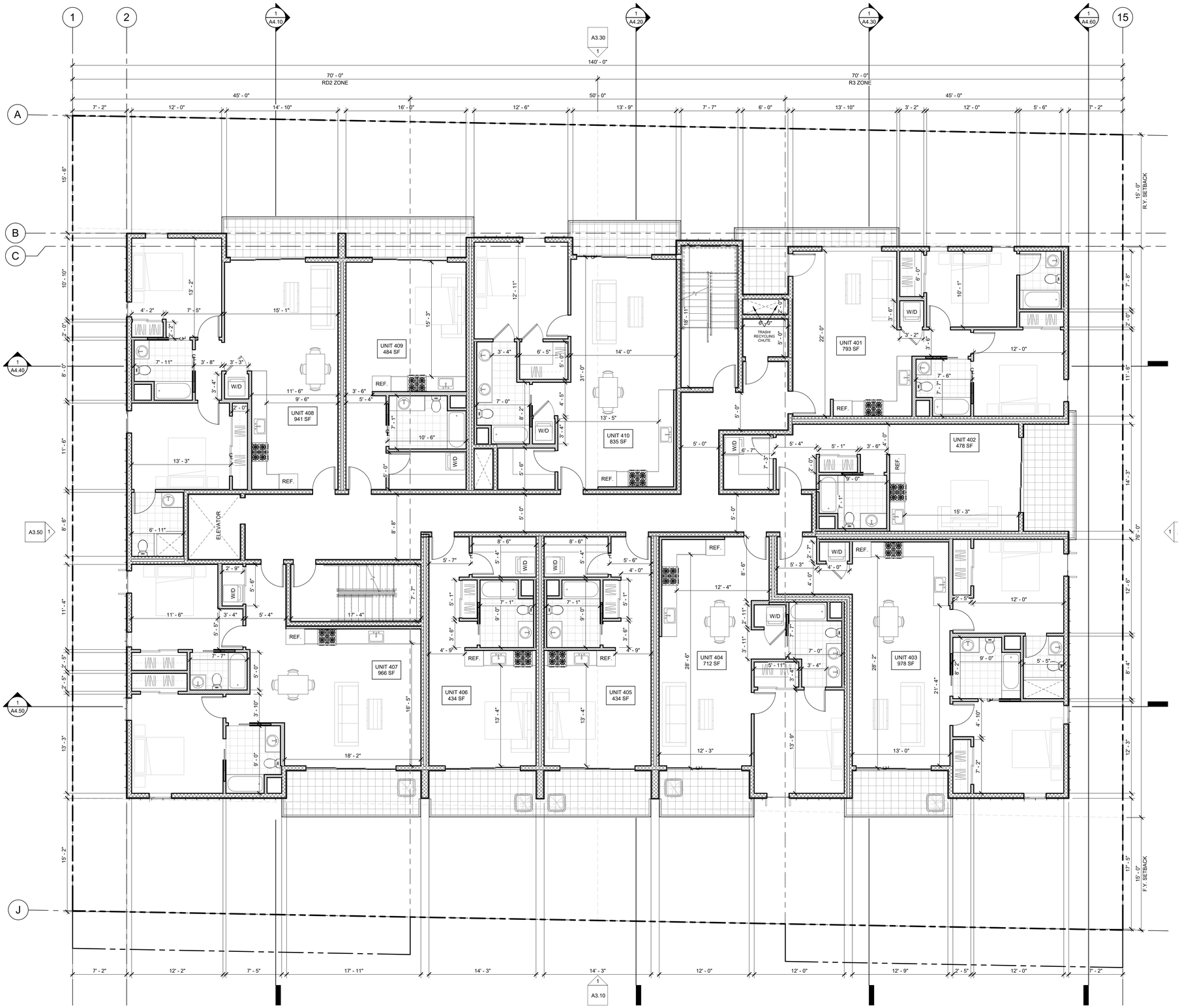
FLOORS

- A1 CONC FLOOR PER STRC. W/ POLISHED CONC. FINISH (10/A0.13)
- A2 CONC FLOOR PER STRC. W/ WOOD FINISH (11/A0.13)
- A3 CONC FLOOR PER STRC. W/ TILE FINISH (12/A0.13)
- B1 WOOD JOIST PER STRC. W/ WOOD FINISH (13/A0.13)
- B2 WOOD JOIST PER STRC. W/ TILE FINISH (14/A0.13)
- B3 WOOD JOIST W/ WOOD DECK PER SPEC. (15/A0.13)
- B4 WOOD JOIST PER STRC. W/ METAL ROOFING PER SPEC. (16/A0.13)

FLOOR PLAN LEGEND

- FLOOR TYPE
- WINDOW TAG (A0.10 - SCHEDULE)
- DOOR TAG (A0.08, A0.09 - SCHEDULE)
- WALL TYPE
- X-X" ELEVATION MARKER
- PROPERTY LINE
- ACCESSIBLE ROUTE
- 1 HR
- 2 HR
- SD SMOKE DETECTOR
- CM CARBON MONOXIDE
- EXHAUST
- S NFPA - 14 CLASS - I STANDPIPE
- ILLUMINATED EXIT SIGN

MB	MASTER BEDROOM
BD	BEDROOM
BA	BATHROOM
LR	LIVING ROOM
KI	KITCHEN
DR	DINING ROOM
PWR	POWDER POOL
CL	CLOSET
WIC	WALK IN CLOSET
LR	LAUNDRY ROOM
BC	BALCONY
EN	ENTRY



N

FOURTH FLOOR PLAN
3/16" = 1'-0"

1

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LICENSED ARCHITECT
RAMSEY DAHAM
No. C-34257
10/31/23
RENEWAL DATE
STATE OF CALIFORNIA

3676 & 3704 KELTON AVE.
LOS ANGELES, CA 90034

Revision Schedule

Revision Number	Revision Date

PROPOSED PLANS

DRAWN	CR
CHECKED	PNK
DATE	4/13/2023 10:53:02 AM
SCALE	As indicated
JOB #	21-A001

A2.40

ASSEMBLY TYPES

WALLS

1 2x4 & 2x6 INTERIOR WALL ASSM. (1/A0.13)
2 2x6 PLUMBING WALL ASSM. (2/A0.13)
3 1-HR 2x6 INTERIOR WALL ASSM. (3/A0.13)
4 1-HR 2x6 EXTERIOR WALL ASSM. (4/A0.13)
5 1-HR DOUBLE WALL ASSM. (5/A0.13)
6 CONC. WALL PER STRC. (6/A0.13)
7 CONC. RETAINING WALL PER STRC. (7/A0.13)
8 CMU WALL PER STRC. (8/A0.13)
9 2-HR INTERIOR CORRIDOR WALL ASSM. (9/A0.13)

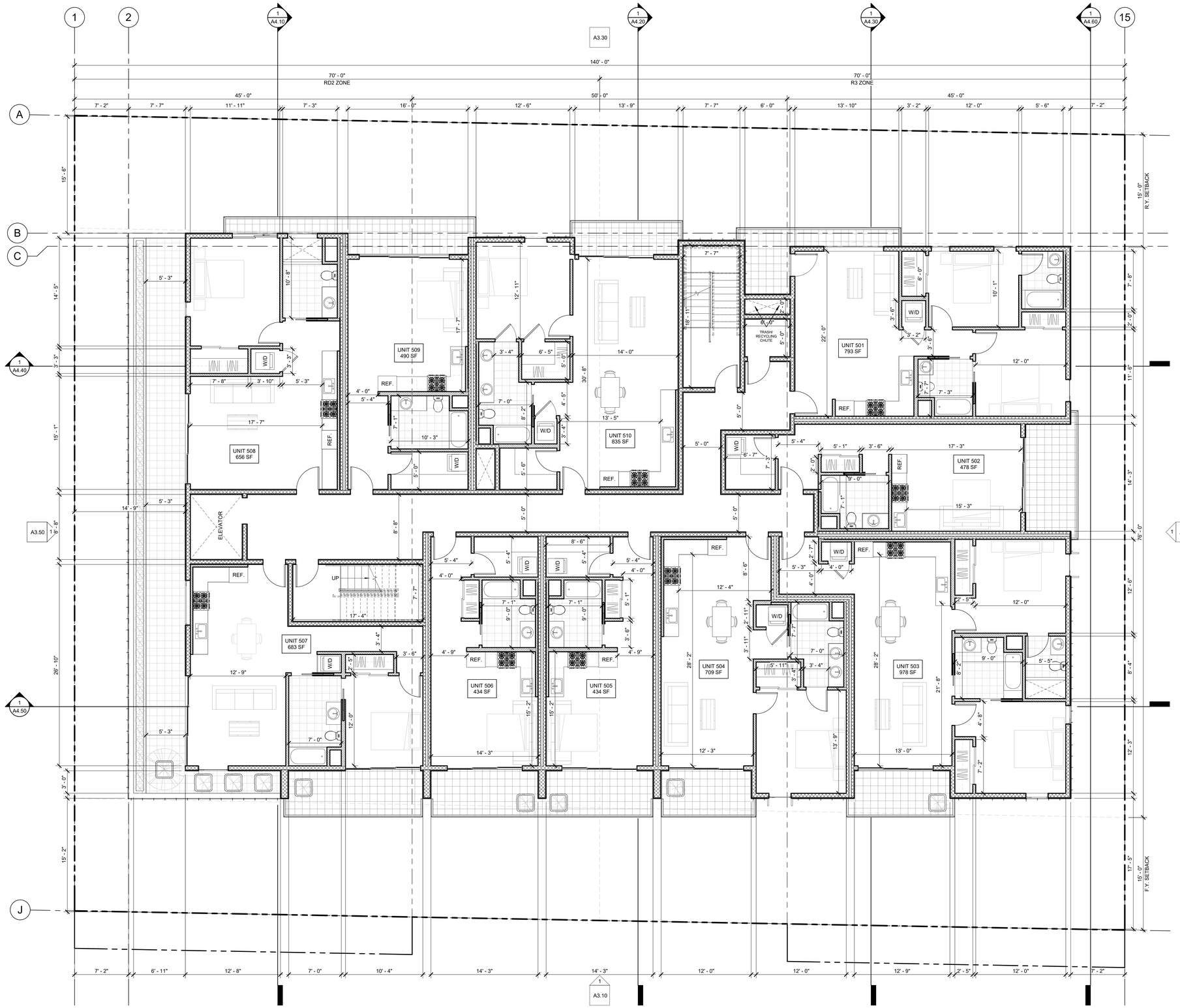
FLOORS

A1 CONC FLOOR PER STRC. W/ POLISHED CONC. FINISH (10/A0.13)
A2 CONC FLOOR PER STRC. W/ WOOD FINISH (11/A0.13)
A3 CONC FLOOR PER STRC. W/ TILE FINISH (12/A0.13)
B1 WOOD JOIST PER STRC. W/ WOOD FINISH (13/A0.13)
B2 WOOD JOIST PER STRC. W/ TILE FINISH (14/A0.13)
B3 WOOD JOIST W/ WOOD DECK PER SPEC. (15/A0.13)
B4 WOOD JOIST PER STRC. W/ METAL ROOFING PER SPEC. (16/A0.13)

FLOOR PLAN LEGEND

FLOOR TYPE
WINDOW TAG (A0.10 - SCHEDULE)
DOOR TAG (A0.08, A0.09 - SCHEDULE)
WALL TYPE
ELEVATION MARKER
PROPERTY LINE
ACCESSIBLE ROUTE
1 HR
2 HR
SD SMOKE DETECTOR
CM CARBON MONOXIDE
EXHAUST
S NFPA - 14 CLASS - I STANDPIPE
ILLUMINATED EXIT SIGN

MB MASTER BEDROOM
BD BEDROOM
BA BATHROOM
LR LIVING ROOM
KI KITCHEN
DR DINING ROOM
PWR POWDER ROOM
CL CLOSET
WIC WALK IN CLOSET
LR LAUNDRY ROOM
BC BALCONY
EN ENTRY



N

FIFTH FLOOR PLAN
3/16" = 1'-0"

1

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LICENSED ARCHITECT
RAMSEY DAHAM
No. C-34257
10/31/23
RENEWAL DATE
STATE OF CALIFORNIA

3676 & 3704 KELTON AVE.
LOS ANGELES, CA 90034

Revision Schedule

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SCALE	As indicated
JOB #	21-A001

A2.50

ASSEMBLY TYPES

WALLS

1. 2x4 & 2x6 INTERIOR WALL ASSM. (1/A0.13)
2. 2x6 PLUMBING WALL ASSM. (2/A0.13)
3. 1-HR 2x6 INTERIOR WALL ASSM. (3/A0.13)
4. 1-HR 2x6 EXTERIOR WALL ASSM. (4/A0.13)
5. 1-HR DOUBLE WALL ASSM. (5/A0.13)
6. CONC. WALL PER STRC. (6/A0.13)
7. CONC. RETAINING WALL PER STRC. (7/A0.13)
8. CMU WALL PER STRC. (8/A0.13)
9. 2-HR INTERIOR CORRIDOR WALL ASSM. (9/A0.13)

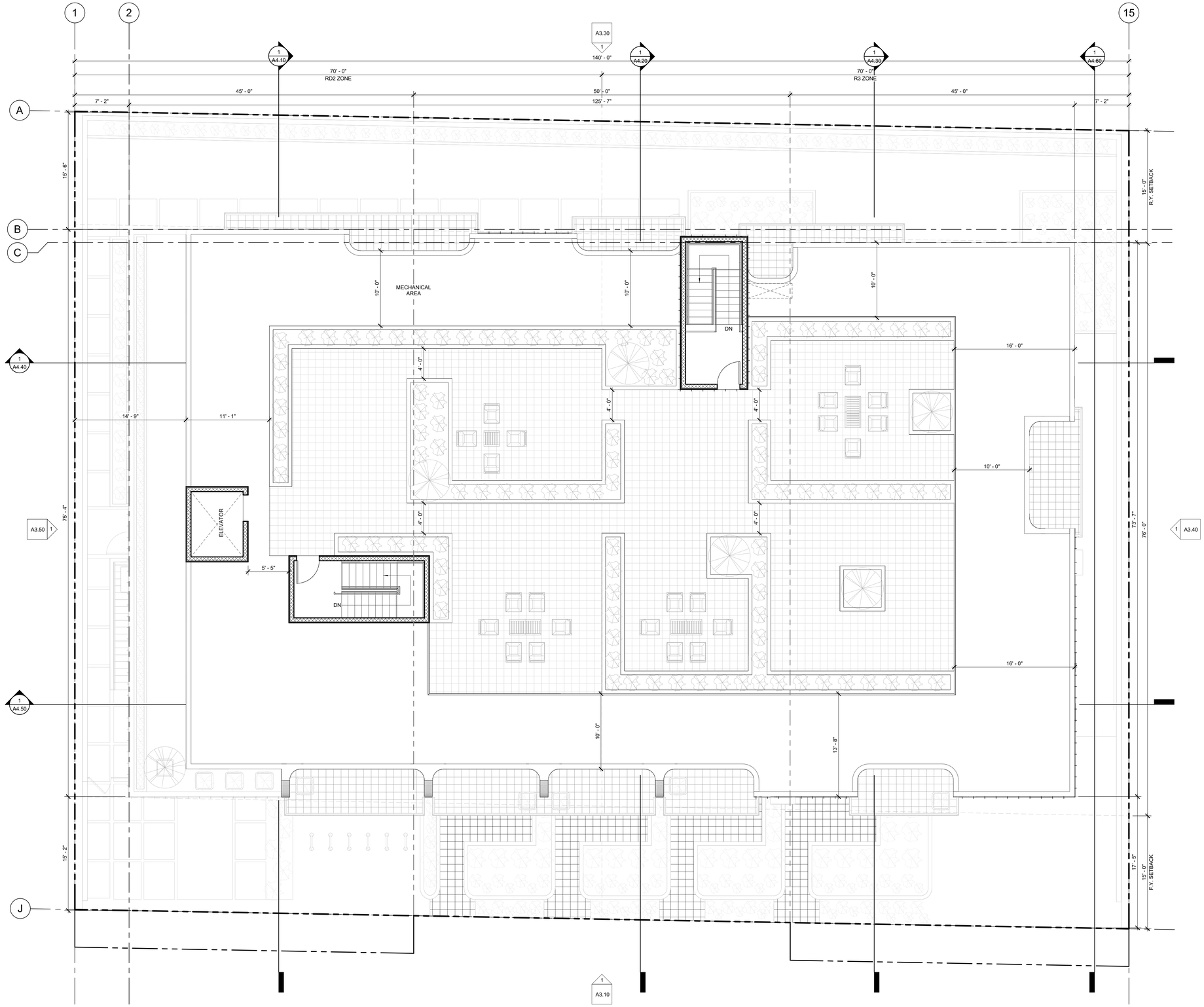
FLOORS

- A1 CONC FLOOR PER STRC. W/ POLISHED CONC. FINISH (10/A0.13)
- A2 CONC FLOOR PER STRC. W/ WOOD FINISH (11/A0.13)
- A3 CONC FLOOR PER STRC. W/ TILE FINISH (12/A0.13)
- B1 WOOD JOIST PER STRC. W/ WOOD FINISH (13/A0.13)
- B2 WOOD JOIST PER STRC. W/ TILE FINISH (14/A0.13)
- B3 WOOD JOIST W/ WOOD DECK PER SPEC. (15/A0.13)
- B3 WOOD JOIST PER STRC. W/ METAL ROOFING PER SPEC. (16/A0.13)

FLOOR PLAN LEGEND

- FLOOR TYPE
- WINDOW TAG (A0.10 - SCHEDULE)
- DOOR TAG (A0.08, A0.09 - SCHEDULE)
- WALL TYPE
- ELEVATION MARKER
- PROPERTY LINE
- ACCESSIBLE ROUTE
- 1 HR
- 2 HR
- SD SMOKE DETECTOR
- CM CARBON MONOXIDE
- EXHAUST
- NFPA - 14 CLASS - I STANDPIPE
- ILLUMINATED EXIT SIGN

MB	MASTER BEDROOM
BD	BEDROOM
BA	BATHROOM
LR	LIVING ROOM
KI	KITCHEN
DR	DINING ROOM
PWR	POWDER ROOM
CL	CLOSET
WIC	WALK IN CLOSET
LR	LAUNDRY ROOM
BC	BALCONY
EN	ENTRY



ROOF PLAN
3/16" = 1'-0"

1

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LICENSED ARCHITECT
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10/31/23
RENEWAL DATE
STATE OF CALIFORNIA

3676 & 3704 KELTON AVE.
LOS ANGELES, CA 90034

Revision Schedule

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PROPOSED PLANS

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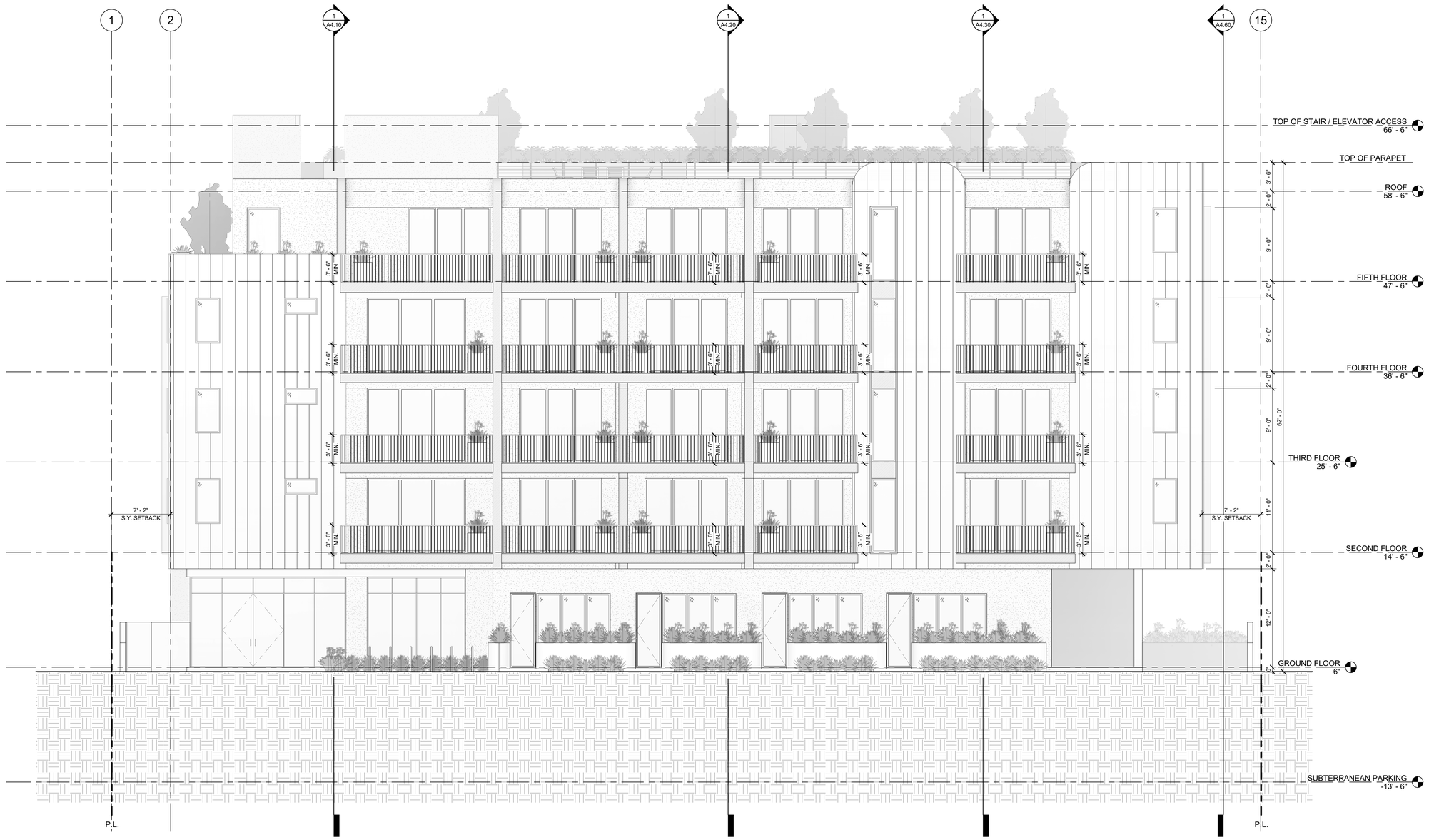
SCALE	As indicated
JOB #	21-A001

A2.60

ELEVATION LEGEND

	STANDING SEAM	— - — - —	1 HR
	STUCCO WHITE	- - - — - - -	2 HR
	DARK GRAY METAL	X	WINDOW TAG
		XXX	DOOR TAG
	PROPERTY LINE (PL)	X'-X"	ELEVATION MARKER



SOUTH ELEVATION
3/16" = 1'-0"

1

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LICENSED ARCHITECT
RAMSEY DAHAM
No. C-34257
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RENEWAL DATE
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LOS ANGELES, CA 90034

Revision Schedule

Revision Number	Revision Date

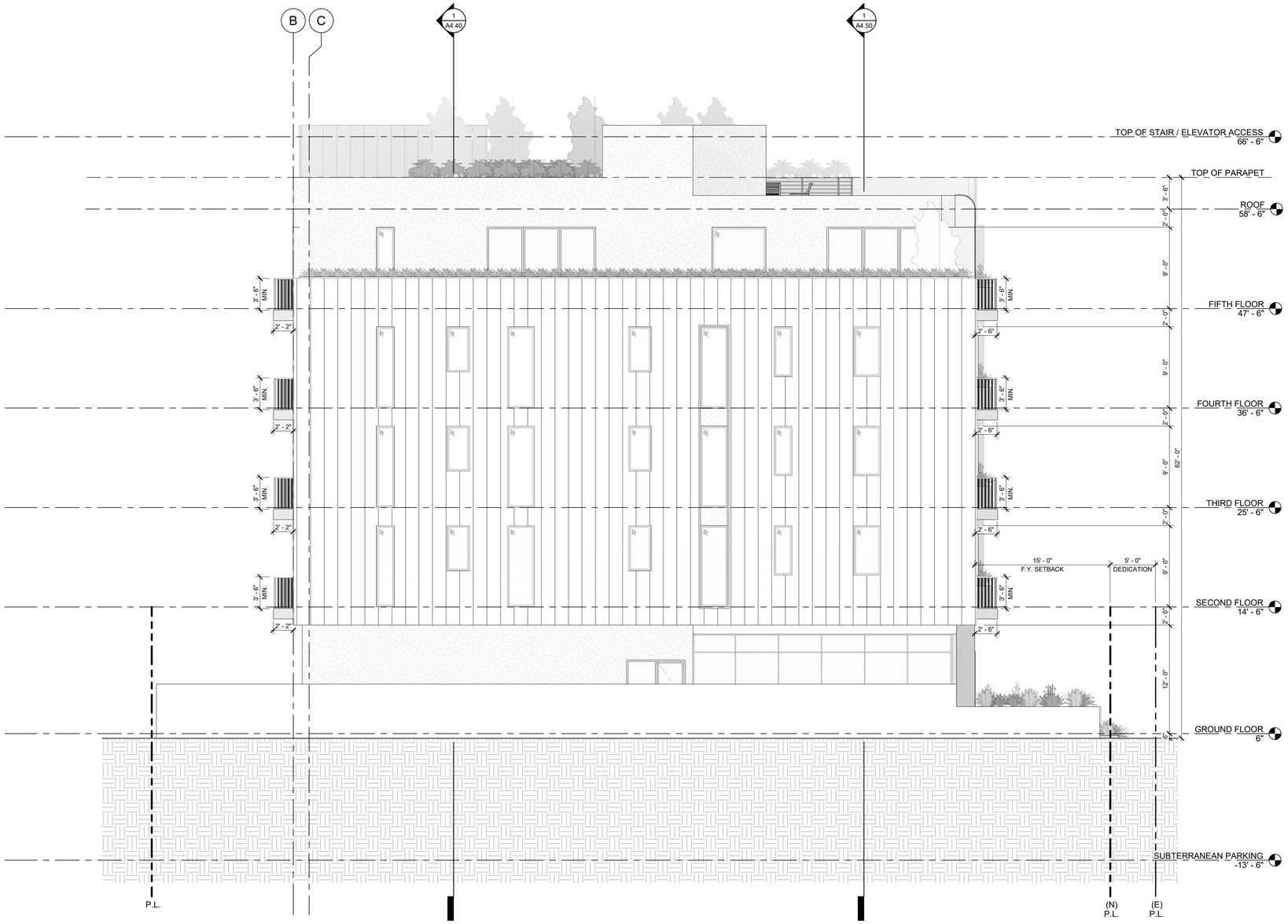
ELEVATIONS

DRAWN	CR
CHECKED	PNK
DATE	4/13/2023 10:54:05 AM
SCALE	As indicated
JOB #	21-A001

A3.10

ELEVATION LEGEND

STANDING SEAM		1 HR	
STUCCO WHITE		2 HR	
DARK GRAY METAL		X	WINDOW TAG
		XXX	DOOR TAG
PROPERTY LINE (PL)		X'-X"	ELEVATION MARKER



WEST ELEVATION
3/16" = 1'-0"

1

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LICENSED ARCHITECT
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No. C-34257
10/31/23
RENEWAL DATE
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LOS ANGELES, CA 90034

Revision Schedule

Revision Number	Revision Date

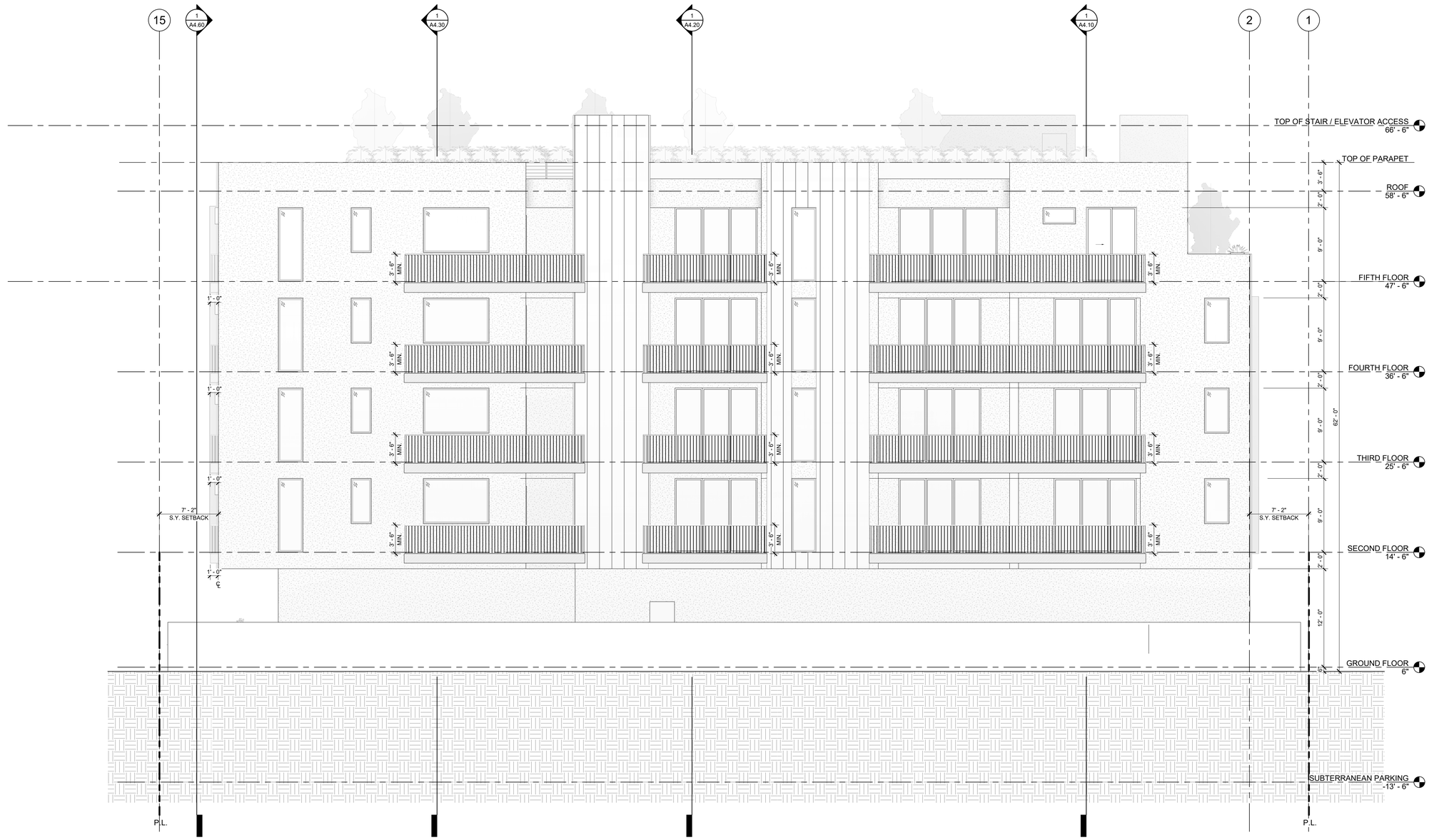
ELEVATIONS

DRAWN	CR
CHECKED	PNK
DATE	4/13/2023 10:54:59 AM
SCALE	As indicated
JOB #	21-A001

A3.20

ELEVATION LEGEND

	STANDING SEAM	— – — – —	1 HR
	STUCCO WHITE	– – — – –	2 HR
	DARK GRAY METAL	X	WINDOW TAG
		XXX	DOOR TAG
	PROPERTY LINE (PL)	X'-X"	ELEVATION MARKER



NORTH ELEVATION
3/16" = 1'-0"

1

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127 arena street, el segundo, ca 90245
[o] 310.322.3700

LICENSED ARCHITECT
RAMSEY DAHAM
No. C-34257
10/31/23
RENEWAL DATE
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3676 & 3704 KELTON AVE.
LOS ANGELES, CA 90034

Revision Schedule

Revision Number	Revision Date

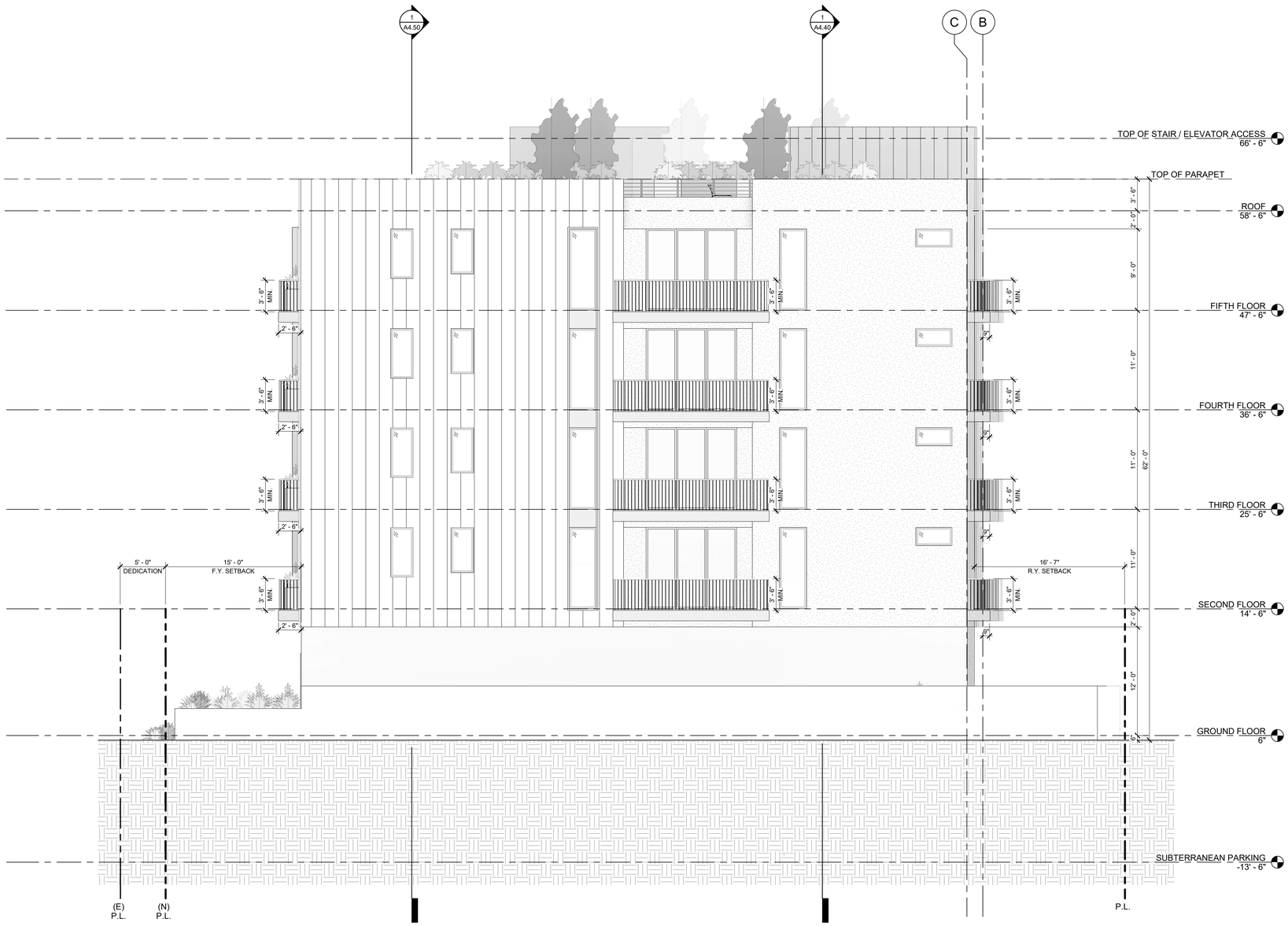
ELEVATIONS

DRAWN	CR
CHECKED	PNK
DATE	4/13/2023 10:55:51 AM
SCALE	As indicated
JOB #	21-A001

A3.30

ELEVATION LEGEND

	STANDING SEAM	— – — – —	1 HR
	STUCCO WHITE	– – – — – – –	2 HR
	DARK GRAY METAL	X	WINDOW TAG
		XXX	DOOR TAG
	PROPERTY LINE (PL)	X'-X"	ELEVATION MARKER



EAST ELEVATION
3/16" = 1'-0"

1

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127 arena street, el segundo, ca 90245
[o] 310.322.3700

LICENSED ARCHITECT
RAMSEY DAHAM
No. C-34257
10/31/23
RENEWAL DATE
STATE OF CALIFORNIA

3676 & 3704 KELTON AVE.
LOS ANGELES, CA 90034

Revision Schedule

Revision Number	Revision Date

ELEVATIONS

DRAWN	CR
CHECKED	PNK
DATE	4/13/2023 10:56:49 AM
SCALE	As indicated
JOB #	21-A001

A3.40

ASSEMBLY TYPES

WALLS

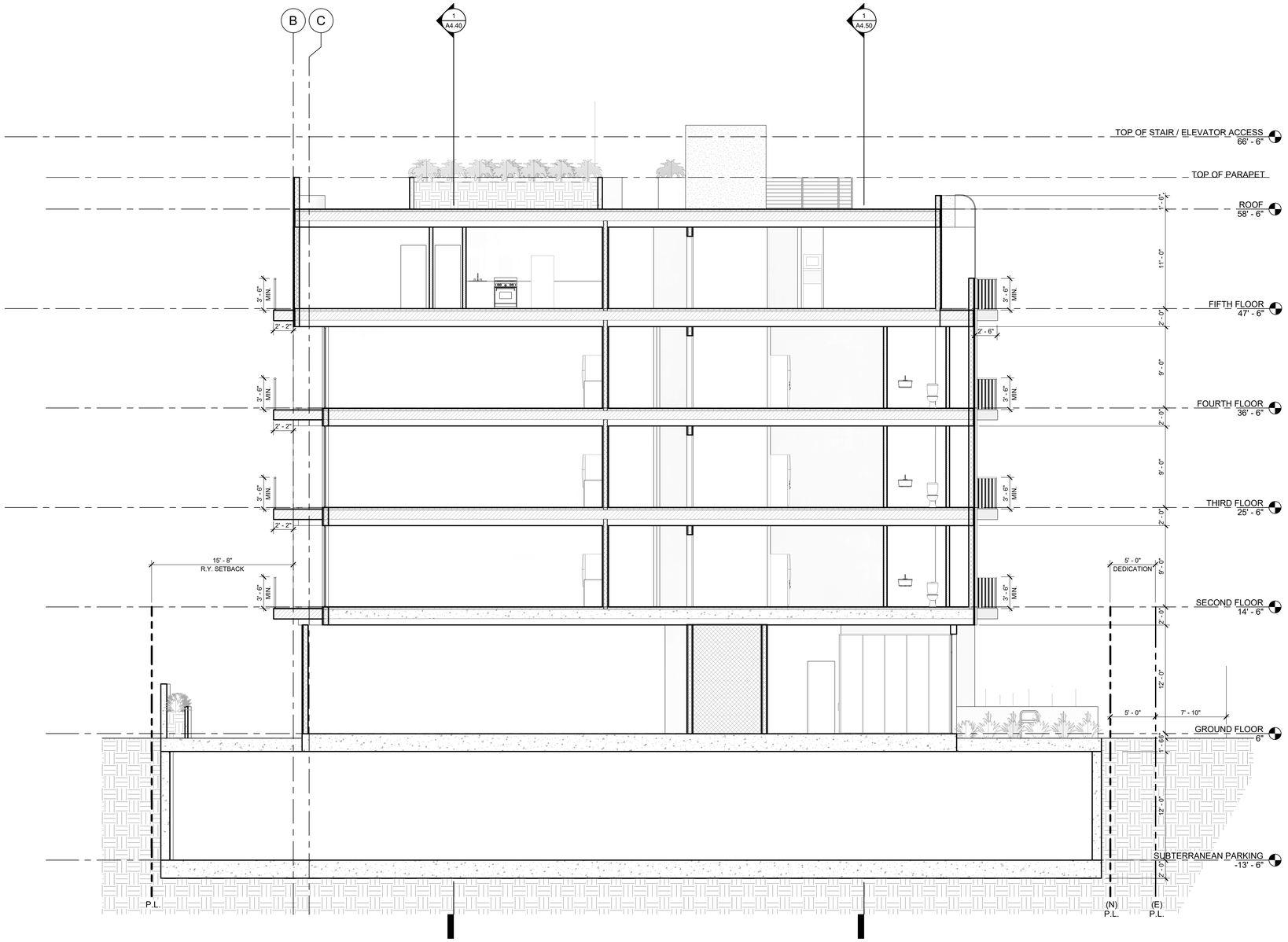
1. 2x4 & 2x6 INTERIOR WALL ASSM. (1/A0.13)
2. 2x6 PLUMBING WALL ASSM. (2/A0.13)
3. 1-HR 2x6 INTERIOR WALL ASSM. (3/A0.13)
4. 1-HR 2x6 EXTERIOR WALL ASSM. (4/A0.13)
5. 1-HR DOUBLE WALL ASSM. (5/A0.13)
6. CONC. WALL PER STRC. (6/A0.13)
7. CONC. RETAINING WALL PER STRC. (7/A0.13)
8. CMU WALL PER STRC. (8/A0.13)
9. 2-HR INTERIOR CORRIDOR WALL ASSM. (9/A0.13)

FLOORS

A1. CONC FLOOR PER STRC. W/ POLISHED CONC. FINISH (10/A0.13)
A2. CONC FLOOR PER STRC. W/ WOOD FINISH (11/A0.13)
A3. CONC FLOOR PER STRC. W/ TILE FINISH (12/A0.13)
B1. WOOD JOIST PER STRC. W/ WOOD FINISH (13/A0.13)
B2. WOOD JOIST PER STRC. W/ TILE FINISH (14/A0.13)
C1. WOOD JOIST W/ WOOD DECK PER SPEC. (15/A0.13)
C2. WOOD JOIST PER STRC. W/ METAL ROOFING PER SPEC. (16/A0.13)

SECTION LEGEND

- 1 HR
- 2 HR
- X'-X" ELEVATION MARKER
- PROPERTY LINE
- EXISTING GRADE



PROPOSED SECTION A
3/16" = 1'-0"

1

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LICENSED ARCHITECT
RAMSEY DAHAM
No. C-34257
10/31/23
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3676 & 3704 KELTON AVE.
LOS ANGELES, CA 90034

Revision Schedule

Revision Number	Revision Date

PROPOSED SECTIONS

DRAWN	CR
CHECKED	PNK
DATE	4/13/2023 10:57:00 AM
SCALE	As indicated
JOB #	21-A001

A4.10

ASSEMBLY TYPES

WALLS

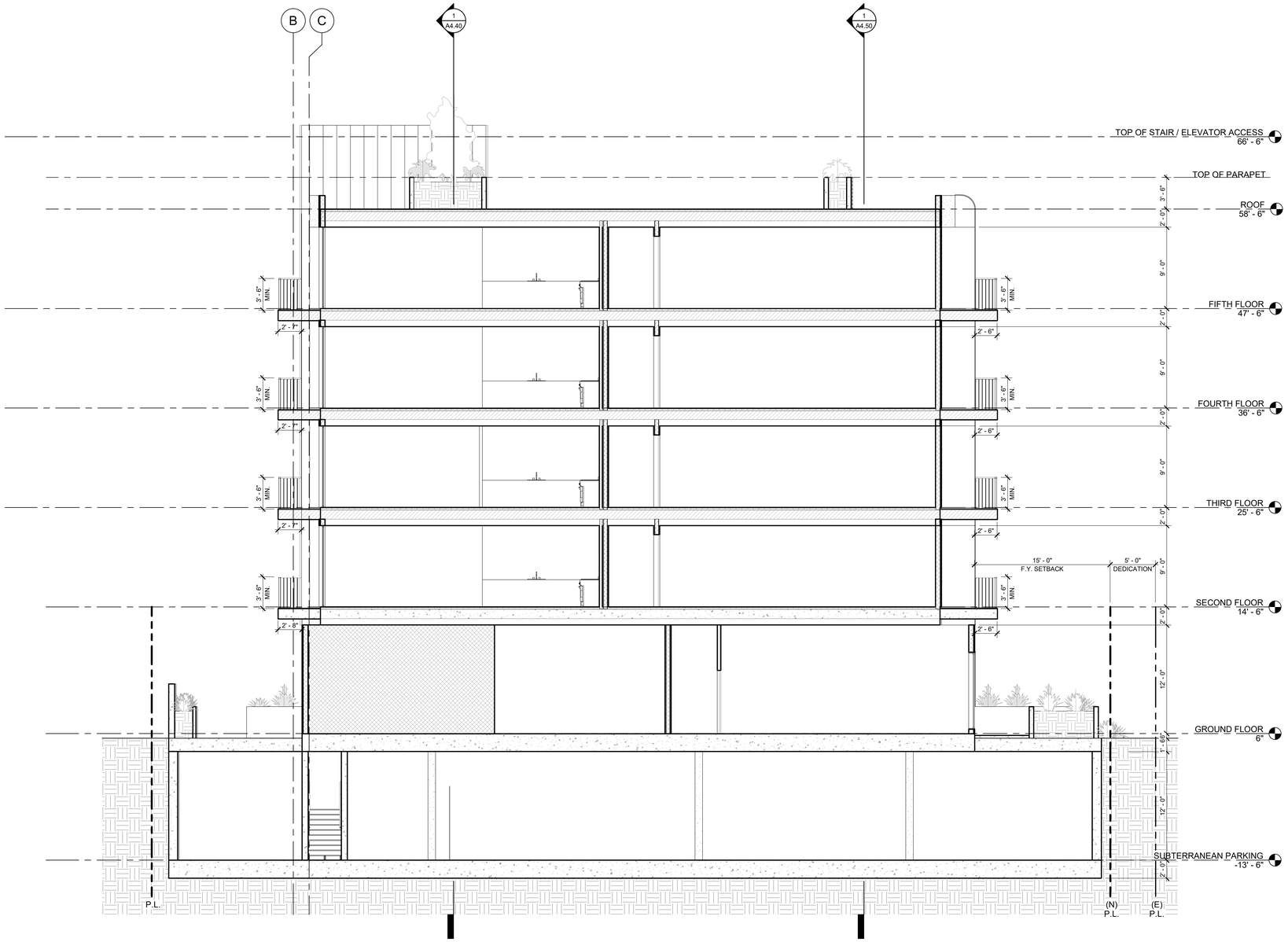
1. 2x4 & 2x6 INTERIOR WALL ASSM. (1/A0.13)
2. 2x6 PLUMBING WALL ASSM. (2/A0.13)
3. 1-HR 2x6 INTERIOR WALL ASSM. (3/A0.13)
4. 1-HR 2x6 EXTERIOR WALL ASSM. (4/A0.13)
5. 1-HR DOUBLE WALL ASSM. (5/A0.13)
6. CONC. WALL PER STRC. (6/A0.13)
7. CONC. RETAINING WALL PER STRC. (7/A0.13)
8. CMU WALL PER STRC. (8/A0.13)
9. 2-HR INTERIOR CORRIDOR WALL ASSM. (9/A0.13)

FLOORS

A.1. CONC FLOOR PER STRC. W/ POLISHED CONC. FINISH (10/A0.13)
A.2. CONC FLOOR PER STRC. W/ WOOD FINISH (11/A0.13)
A.3. CONC FLOOR PER STRC. W/ TILE FINISH (12/A0.13)
B.1. WOOD JOIST PER STRC. W/ WOOD FINISH (13/A0.13)
B.2. WOOD JOIST PER STRC. W/ TILE FINISH (14/A0.13)
B.3. WOOD JOIST W/ WOOD DECK PER SPEC. (15/A0.13)
B.4. WOOD JOIST PER STRC. W/ METAL ROOFING PER SPEC. (16/A0.13)

SECTION LEGEND

- 1 HR
- 2 HR
- X'-X" ELEVATION MARKER
- PROPERTY LINE
- EXISTING GRADE



PROPOSED SECTION B
3/16" = 1'-0"

1

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LICENSED ARCHITECT
RAMSEY DAHAM
No. C-34257
10/31/23
RENEWAL DATE
STATE OF CALIFORNIA

3676 & 3704 KELTON AVE.
LOS ANGELES, CA 90034

Revision Schedule

Revision Number	Revision Date

PROPOSED SECTIONS

DRAWN	CR
CHECKED	PNK
DATE	4/13/2023 10:57:10 AM
SCALE	As indicated
JOB #	21-A001

A4.20

ASSEMBLY TYPES

WALLS

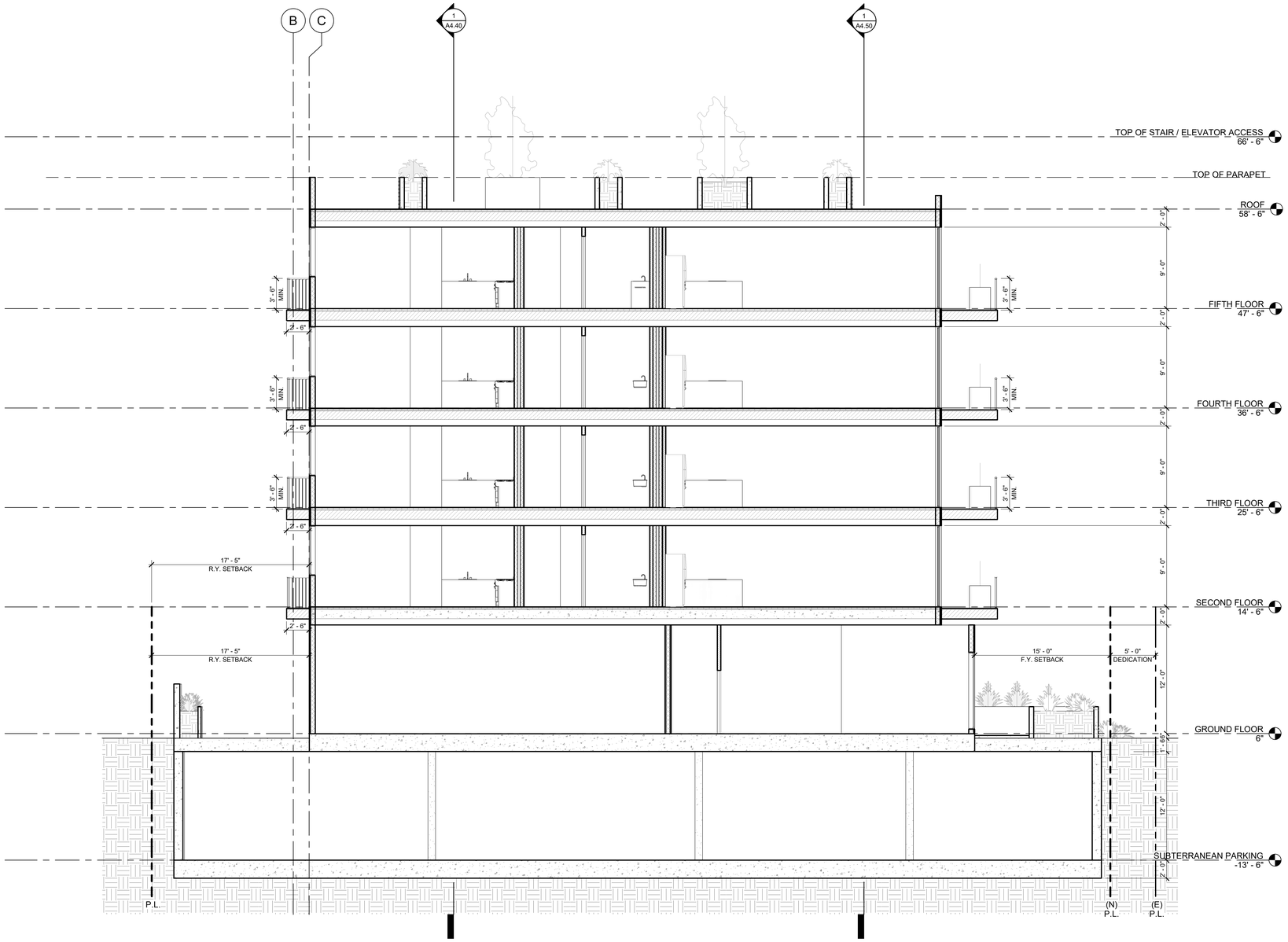
1. 2x4 & 2x6 INTERIOR WALL ASSM. (1/A0.13)
2. 2x6 PLUMBING WALL ASSM. (2/A0.13)
3. 1-HR 2x6 INTERIOR WALL ASSM. (3/A0.13)
4. 1-HR 2x6 EXTERIOR WALL ASSM. (4/A0.13)
5. 1-HR DOUBLE WALL ASSM. (5/A0.13)
6. CONC. WALL PER STRC. (6/A0.13)
7. CONC. RETAINING WALL PER STRC. (7/A0.13)
8. CMU WALL PER STRC. (8/A0.13)
9. 2-HR INTERIOR CORRIDOR WALL ASSM. (9/A0.13)

FLOORS

A1. CONC FLOOR PER STRC. W/ POLISHED CONC. FINISH (10/A0.13)
A2. CONC FLOOR PER STRC. W/ WOOD FINISH (11/A0.13)
A3. CONC FLOOR PER STRC. W/ TILE FINISH (12/A0.13)
B1. WOOD JOIST PER STRC. W/ WOOD FINISH (13/A0.13)
B2. WOOD JOIST PER STRC. W/ TILE FINISH (14/A0.13)
B3. WOOD JOIST W/ WOOD DECK PER SPEC. (15/A0.13)
B4. WOOD JOIST PER STRC. W/ METAL ROOFING PER SPEC. (16/A0.13)

SECTION LEGEND

- 1 HR
- 2 HR
- X'-X" ELEVATION MARKER
- PROPERTY LINE
- EXISTING GRADE



PROPOSED SECTION C
3/16" = 1'-0"

1

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LICENSED ARCHITECT
RAMSEY DAHAM
No. C-34257
10/31/23
RENEWAL DATE
STATE OF CALIFORNIA

3676 & 3704 KELTON AVE.
LOS ANGELES, CA 90034

Revision Schedule

Revision Number	Revision Date

PROPOSED SECTIONS

DRAWN	CR
CHECKED	PNK
DATE	4/13/2023 10:57:19 AM
SCALE	As indicated
JOB #	21-A001

A4.30

ASSEMBLY TYPES

WALLS

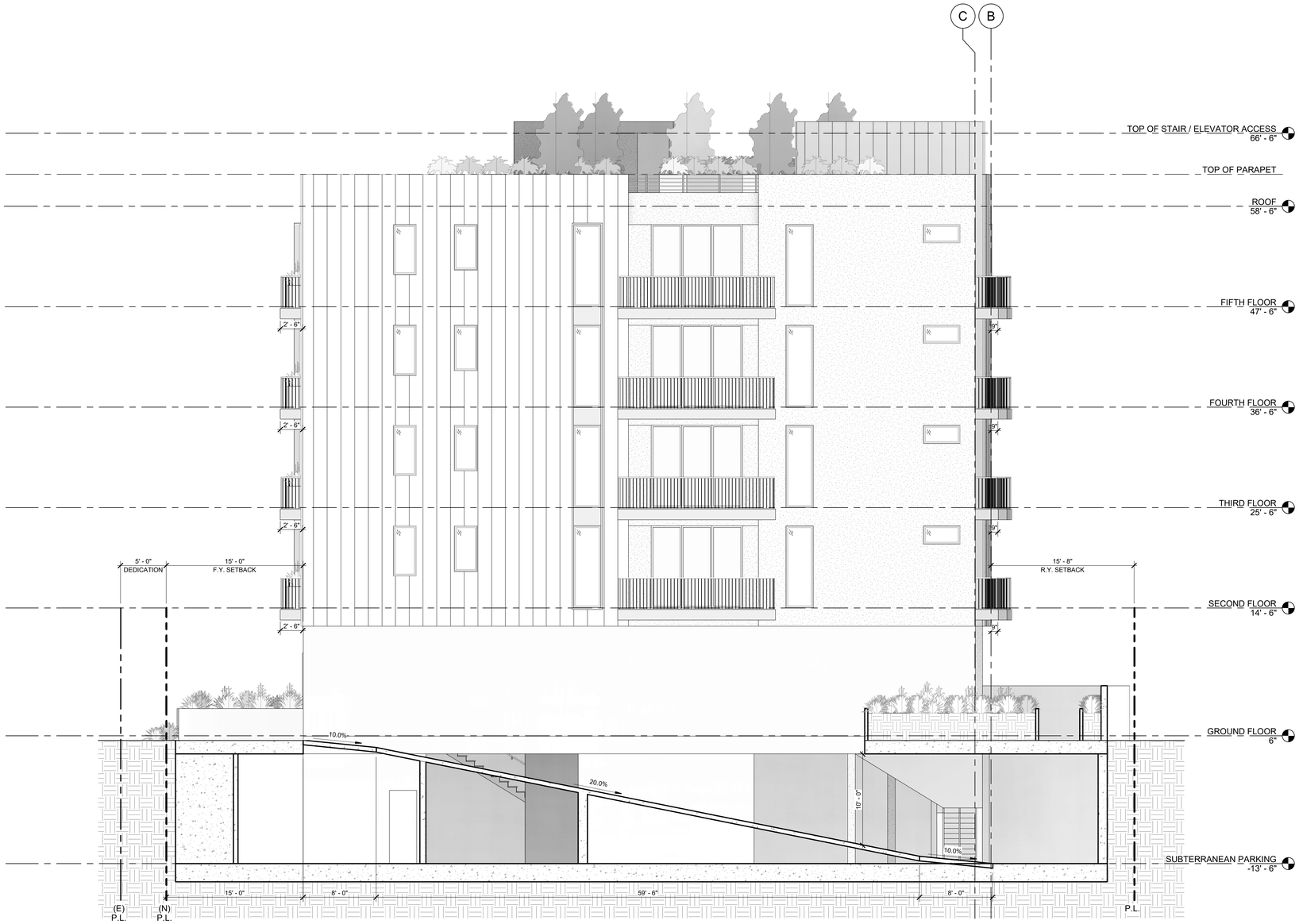
1. 2x4 & 2x6 INTERIOR WALL ASSM. (1/A0.13)
2. 2x6 PLUMBING WALL ASSM. (2/A0.13)
3. 1-HR 2x6 INTERIOR WALL ASSM. (3/A0.13)
4. 1-HR 2x6 EXTERIOR WALL ASSM. (4/A0.13)
5. 1-HR DOUBLE WALL ASSM. (5/A0.13)
6. CONC. WALL PER STRC. (6/A0.13)
7. CONC. RETAINING WALL PER STRC. (7/A0.13)
8. CMU WALL PER STRC. (8/A0.13)
9. 2-HR INTERIOR CORRIDOR WALL ASSM. (9/A0.13)

FLOORS

A1. CONC FLOOR PER STRC. W/ POLISHED CONC. FINISH (10/A0.13)
A2. CONC FLOOR PER STRC. W/ WOOD FINISH (11/A0.13)
A3. CONC FLOOR PER STRC. W/ TILE FINISH (12/A0.13)
B1. WOOD JOIST PER STRC. W/ WOOD FINISH (13/A0.13)
B2. WOOD JOIST PER STRC. W/ TILE FINISH (14/A0.13)
B3. WOOD JOIST W/ WOOD DECK PER SPEC. (15/A0.13)
B4. WOOD JOIST PER STRC. W/ METAL ROOFING PER SPEC. (16/A0.13)

SECTION LEGEND

- 1 HR
- 2 HR
- X'-X" ELEVATION MARKER
- PROPERTY LINE
- EXISTING GRADE



RAMP SECTION
3/16" = 1'-0"

1

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LICENSED ARCHITECT
RAMSEY DAHAM
No. C-34257
10/31/23
RENEWAL DATE
STATE OF CALIFORNIA

3676 & 3704 KELTON AVE.
LOS ANGELES, CA 90034

Revision Schedule

Revision Number	Revision Date

PROPOSED SECTIONS

DRAWN	CR
CHECKED	PNK
DATE	4/13/2023 10:58:25 AM
SCALE	As indicated
JOB #	21-A001

A4.60

ASSEMBLY TYPES

WALLS

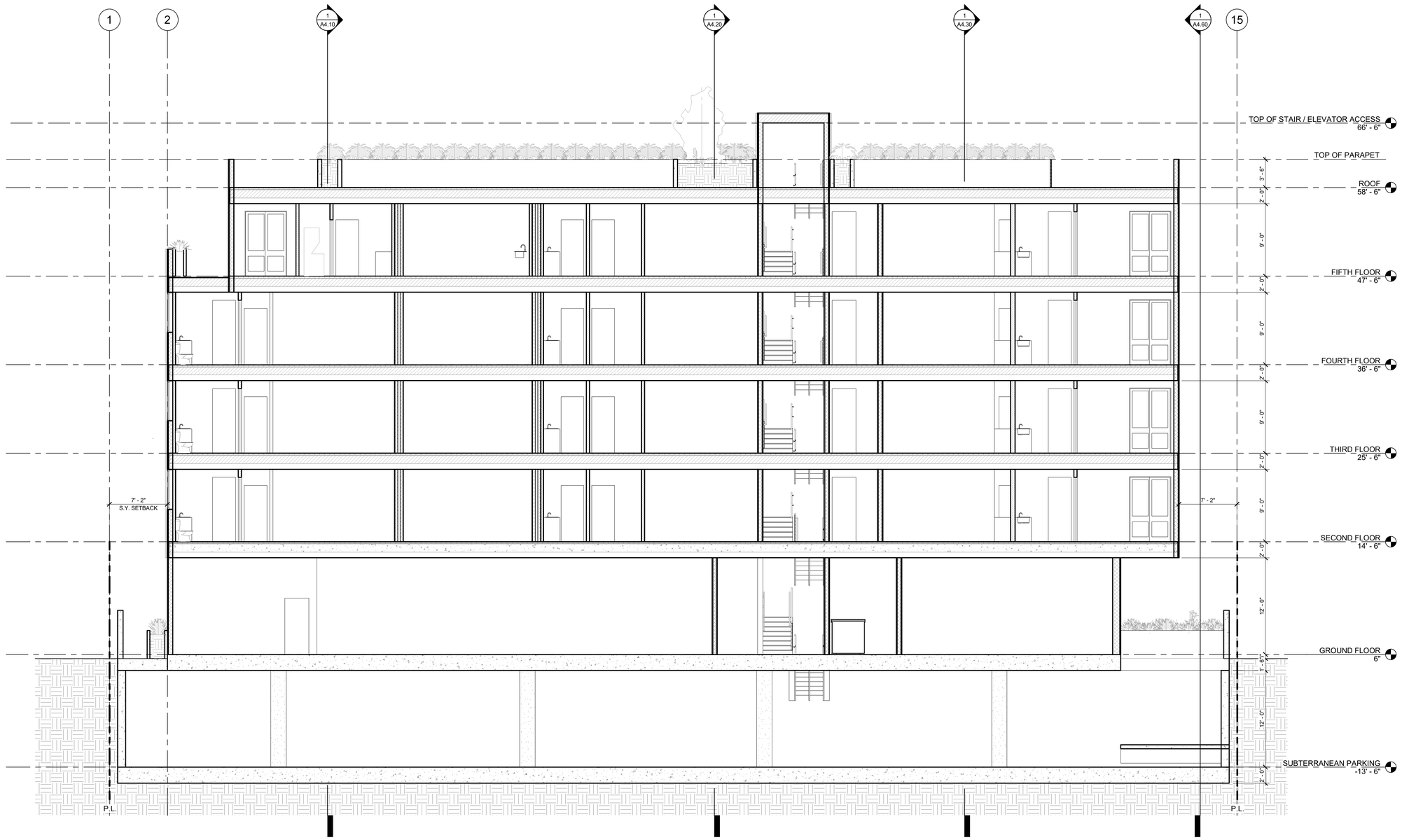
1. 2x4 & 2x6 INTERIOR WALL ASSM. (1/A0.13)
2. 2x6 PLUMBING WALL ASSM. (2/A0.13)
3. 1-HR 2x6 INTERIOR WALL ASSM. (3/A0.13)
4. 1-HR 2x6 EXTERIOR WALL ASSM. (4/A0.13)
5. 1-HR DOUBLE WALL ASSM. (5/A0.13)
6. CONC. WALL PER STRC. (6/A0.13)
7. CONC. RETAINING WALL PER STRC. (7/A0.13)
8. CMU WALL PER STRC. (8/A0.13)
9. 2-HR INTERIOR CORRIDOR WALL ASSM. (9/A0.13)

FLOORS

A-1. CONC FLOOR PER STRC. W/ POLISHED CONC. FINISH (10/A0.13)
A-2. CONC FLOOR PER STRC. W/ WOOD FINISH (11/A0.13)
A-3. CONC FLOOR PER STRC. W/ TILE FINISH (12/A0.13)
B-1. WOOD JOIST PER STRC. W/ WOOD FINISH (13/A0.13)
B-2. WOOD JOIST PER STRC. W/ TILE FINISH (14/A0.13)
B-3. WOOD JOIST W/ WOOD DECK PER SPEC. (15/A0.13)
B-4. WOOD JOIST PER STRC. W/ METAL ROOFING PER SPEC. (16/A0.13)

SECTION LEGEND

- 1 HR
- 2 HR
- X'-X" ELEVATION MARKER
- PROPERTY LINE
- EXISTING GRADE



PROPOSED SECTION D
3/16" = 1'-0"

1

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127 arena street, el segundo, ca 90245
[o] 310.322.3700

LICENSED ARCHITECT
RAMSEY DAHAM
No. C-34257
10/31/23
RENEWAL DATE
STATE OF CALIFORNIA

3676 & 3704 KELTON AVE.
LOS ANGELES, CA 90034

Revision Schedule

Revision Number	Revision Date

PROPOSED SECTIONS

DRAWN	CR
CHECKED	PNK
DATE	4/13/2023 10:58:34 AM
SCALE	As indicated
JOB #	21-A001

A4.40

ASSEMBLY TYPES

WALLS

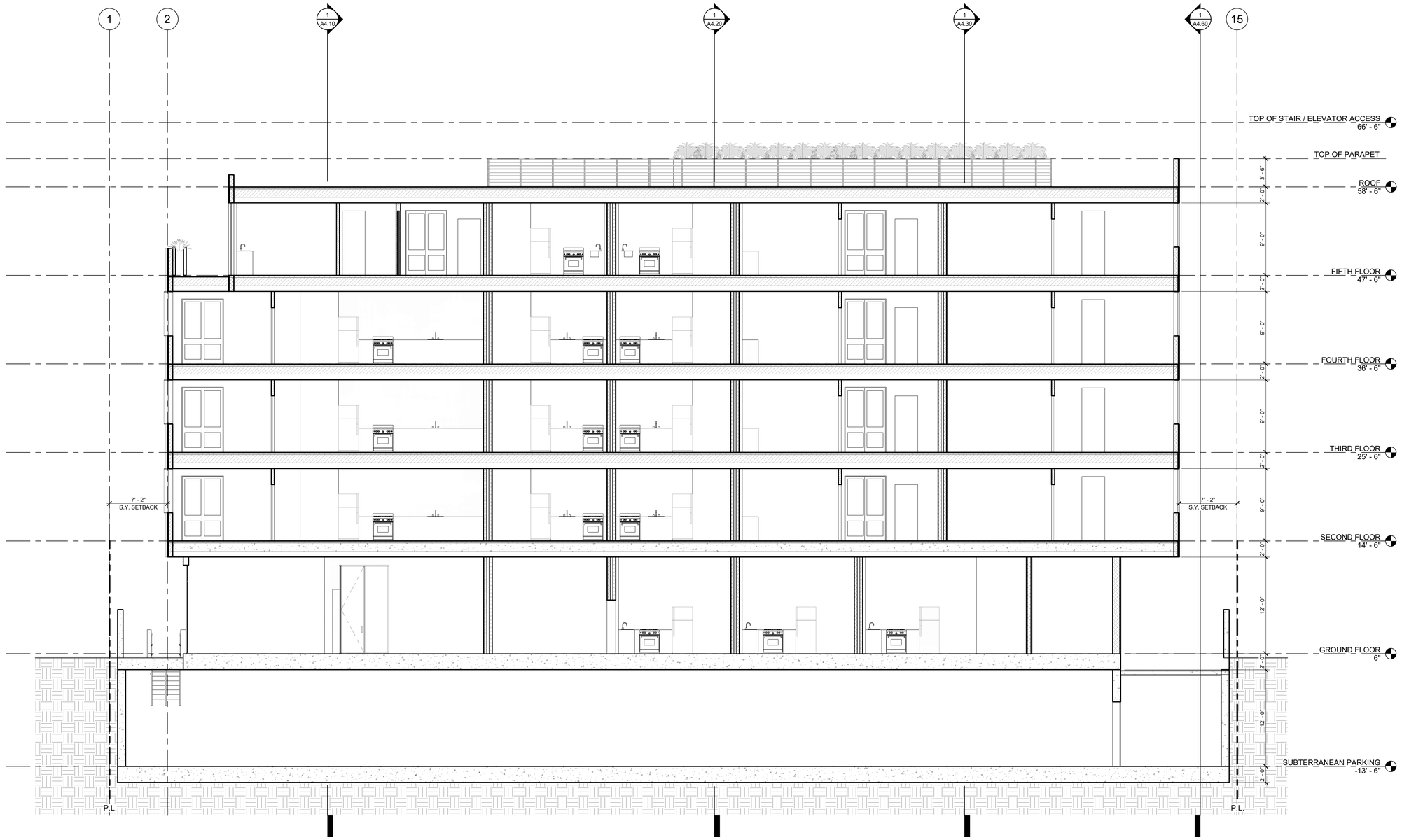
1. 2x4 & 2x6 INTERIOR WALL ASSM. (1/A0.13)
2. 2x6 PLUMBING WALL ASSM. (2/A0.13)
3. 1-HR 2x6 INTERIOR WALL ASSM. (3/A0.13)
4. 1-HR 2x6 EXTERIOR WALL ASSM. (4/A0.13)
5. 1-HR DOUBLE WALL ASSM. (5/A0.13)
6. CONC. WALL PER STRC. (6/A0.13)
7. CONC. RETAINING WALL PER STRC. (7/A0.13)
8. CMU WALL PER STRC. (8/A0.13)
9. 2-HR INTERIOR CORRIDOR WALL ASSM. (9/A0.13)

FLOORS

A-1. CONC FLOOR PER STRC. W/ POLISHED CONC. FINISH (10/A0.13)
A-2. CONC FLOOR PER STRC. W/ WOOD FINISH (11/A0.13)
A-3. CONC FLOOR PER STRC. W/ TILE FINISH (12/A0.13)
B-1. WOOD JOIST PER STRC. W/ WOOD FINISH (13/A0.13)
B-2. WOOD JOIST PER STRC. W/ TILE FINISH (14/A0.13)
B-3. WOOD JOIST W/ WOOD DECK PER SPEC. (15/A0.13)
B-4. WOOD JOIST PER STRC. W/ METAL ROOFING PER SPEC. (16/A0.13)

SECTION LEGEND

- 1 HR
- 2 HR
- X'-X" ELEVATION MARKER
- PROPERTY LINE
- EXISTING GRADE



PROPOSED SECTION E
3/16" = 1'-0"

1

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127 arena street, el segundo, ca 90245
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LICENSED ARCHITECT
RAMSEY DAHAM
No. C-34257
10/31/23
RENEWAL DATE
STATE OF CALIFORNIA

3676 & 3704 KELTON AVE.
LOS ANGELES, CA 90034

Revision Schedule

Revision Number	Revision Date

PROPOSED SECTIONS

DRAWN	CR
CHECKED	PNK
DATE	4/13/2023 10:58:52 AM
SCALE	As indicated
JOB #	21-A001

A4.50

PLANTING NOTES

1. QUANTITIES GIVEN FOR PLANT MATERIALS SPECIFIED FOR "ON CENTER" SPACING ARE SHOWN FOR CONVENIENCE ONLY AND ARE SUBORDINATE TO THE SPACING GIVEN. VERIFY AND SUPPLY SUFFICIENT NUMBER OF PLANTS TO FULFILL SPACING REQUIREMENTS.

2. ALL HEADER AND BAMBOO ROOT BARRIERS SHALL BE LOCATED BY THE ARCHITECT ON SITE.

3. ONTRACTOR SHALL INSTALL PLANT MATERIAL IN ACCORDANCE WITH THE SPECIFICATIONS, DRAWINGS AND DETAILS.

4. ONTRACTOR SHALL PROVIDE A MAINTENANCE PERIOD OF NOT LESS THAN 90 DAYS COMMENCING AT THE DATE OF FINAL ACCEPTANCE. SUCH MAINTENANCE SHALL INCLUDE ALL CARE PERTAINING TO ALL WORK INSTALLED AS PART OF THESE CONTRACT DOCUMENTS.

5. THE CONTRACTOR SHALL MAINTAIN A QUALIFIED SUPERVISOR ON THE SITE AT ALL TIMES DURING CONSTRUCTION THROUGH COMPLETION OF PICK-UP WORK.

6. THE CONTRACTOR SHALL VERIFY ALL PLANT MATERIAL QUANTITIES LISTED FOR CONVENIENCE OF CONTRACTOR. ACTUAL NUMBER OF SYMBOLS SHALL HAVE PRIORITY OVER QUANTITIES DESIGNATED.

7. REMOVE ALL DEBRIS, WEEDS, EXCESS MATERIAL AND ROCKS LARGER THAN 1" IN DIAMETER FROM PLANTING AREAS PRIOR TO PREPARATION & AGAIN PRIOR TO PLANTING.

8. SEE DETAILS AND SPECIFICATIONS FOR STAKING METHOD, PLANT PIT DIMENSIONS, SOIL PREPARATION, AND BACKFILL REQUIREMENTS.

9. ALL PLANT MATERIALS SHALL BE APPROVED BY THE LANDSCAPE ARCHITECT PRIOR TO INSTALLATION.

10. FINAL LOCATION OF ALL PLANT MATERIAL SHALL BE SUBJECT TO THE APPROVAL OF THE LANDSCAPE ARCHITECT.

11. CONTRACTOR SHALL NOTIFY LANDSCAPE ARCHITECT 48 HOURS PRIOR TO COMMENCEMENT OF WORK TO COORDINATE PROJECT OBSERVATION SCHEDULES.

12. GROUNDCOVER PLANTING SHALL BE CONTINUOUS UNDER ALL TREES AND SHRUBS. GROUNDCOVER SHALL BE PLANTED ACCORDING TO SPACING ON PLANT LEGEND.

13. TREES SHALL BE LOCATED A MINIMUM OF 5' FROM WALLS, OVERHEADS, WALKS, HEADERS, AND OTHER TREES WITHIN THE PROJECT. IF CONFLICTS ARISE BETWEEN SIZE OF AREAS AND PLANS, CONTRACTOR TO CONTACT LANDSCAPE ARCHITECT FOR RESOLUTION. FAILURE TO MAKE SUCH CONFLICTS KNOWN TO THE LANDSCAPE ARCHITECT WILL RESULT IN CONTRACTORS LIEABILITY TO RELOCATE THE MATERIALS.

14. ALL PLANTING AREAS SHALL BE LOOSENED TO A DEPTH OF 8". APPLY 4 C.Y. OF ORGANIC AMENDMENT AND 15 LBS. OF 10-10-10 FERTILIZER PER 1000 S.F. AND BLEND WITH THE TOP 8" OF SOIL. THIS AMENDMENT IS FOR BIDDING PURPOSES, AND SHALL BE SUPERCEDED BY RECOMMENDATIONS OF THE SOIL ANALYSIS REPORT.

15. FOR ALL TREES AND SHURB PLANTING, THE FOLLOWING PREPARED SOIL MIX SHALL BE USED FOR BACKFILL IN THE PLANTERS. THIS MIX IS FOR BIDDING PURPOSES, AND SHALL BE SUPERCEDED BY RECOMMENDATIONS OF THE SOIL ANALYSIS REPORT.
SITE SOIL - 6 PARTS BY VOLUME
ORGANIC AMENDMENT - 4 PARTS BY VOLUME
SOIL CONDITIONER / FERTILIZER 10-10-10-1LB. PER C.Y. OF MIX
IRON SULFATE - 2 LBS. PER C.Y.OF MIX

16. TURF IS NOT ALLOWED ON SLOPES GREATER THAN 25% WHERE THE TOE OF THE SLOPE IS ADJACENT TO AN IMPERMEABLE HARDSCAPE..

17. RECIRCULATING WATER SYSTEMS SHALL BE USED FOR WATER FEATURES.

18. A MINIMUM 3-INCH LAYER OF MULCH SHALL BE APPLIED ON ALL EXPOSED SOIL SURFACES OF PLANTING AREAS.EXCEPT TURF AREAS, CREEPING OR ROOTING GROUNDCOVER, OR DIRECT SEEDING APPLICATIONS WHERE MULCH IS CONTRAINDICATED.

19. FOR SOILS LESS THAN 6% ORGANIC MATTER IN THE TOP 6 INCHES OF SIL, COMPOST AT A RATE OF A MINIMUM OF FOUR CUBIC YARDS PER 1,000 SQUARE FEET OF PERMEABLE AREA SHALL BE INCORPORATED TO A DEPTH OF SIX INCHES INTO THE SOIL.

20. I AGREE TO COMPLY WITH THE REQUIREMENTS OF THE WATER EFFICIENT LANDSCAPE ORDINANCE AND SUBMIT A COMPLETE LANDSCAPE DOCUMENTATION PACKAGE THAT COMPLYS WITH THE PERFORMANCE APPROACH.

DATE_____ SIGNED_____

21. AT THE TIME OF FINAL INSPECTION THE PERMIT APPLICANT MUST PROVIDE THE OWNER OF THE PROPERTY WITH A CERTIFICATE OF COMPLETION, CERTIFICATE OF INSTALLATION, IRRIGATION SCHEDULE AND SCHEDULE OF LANDSCAPE AND IRRIGATION MAINTENANCE.

IRRIGATION NOTES

1. CONTRACTOR IS TO AUGMENT EXISTING IRRIGATION SYSTEM. CONTRACTOR SHALL REPAIR OR REPLACE ANY EXISTING LANDSCAPE AND IRRIGATION DAMAGED FROM CONSTRAUCTION TO AN ACCEPTABLE LANDSCAPE CONDITION WITH A FULLY FUNCTIONAL AND EFFICIENT IRRIGATION SYSTEM PER THE CONTAINED CONDITIONS.

2. ALL NEW TREES REQUIRE INDIVIDUAL POP-UP STREAM BUBBLERS, MIN. 2 PER TREE, WITHIN 4' OF TREE. TREE IRRIGATION SHALL BE ON A SEPARATE VALVE.

3. SPRAY OR ROTOR HEADS SHALL BE ON POP-UPS. 6" FOR LAWN, LOW GROUNDCOVER OR PARKED CAR OVERHANG AREAS, 12" FOR SHRUB AREAS. HEADS ON RISERS ARE ONLY ALLOWED ADJACENT TO WALLS WITH LIMITED SPACE FOR POP-UPS.

4. LOCATE SPRAY HEADS 24" FROM NON-PERVIOUS PAVING TO PREVENT OVERSPRAY. EXCEPTION ALLOWED IF ADJACENT SURFACE IS PERMEABLE OR IF USING ALTERNATIVE TECHNOLOGY IRRIGATION. ROTATOR OR ROTARY HEADS MAYBE LOCATED 12" FROM PAVING.

5. CONTRACTOR SHALL REPLACE ANY EXISTING IRRIGATION CONTROLLER WITH A MODULE AND SENSOR TO PROVIDE WEATHER BASED INFORMATIONTHAT WILL AUTOMATE THE IRRIGATION RUNTIMES BASED ON WEATHER. SEE HUNTER SOLAR SYNC, RAINBIRD ET MANAGER OR EQUIVALENT.

6. THE PLANTING AND IRRIGATION SYSTEM SHALL BE COMPLETED BY THE DEVELOPER/BUILDER PRIOR TO THE CLOSE OF ESCROW OF 50 PERCENT OF THE UNITS OF THE PROJECT OR PHASE.

7. SIXTY DAYS AFTER TLANDSCAPE AND IRRIGATION INSTALLATION. THE LANDSCAPE PROFESSIONAL SHALL SUBMIT TO THE HOMEOWNERS/PROPERTY OWNERS ASSOCIATION A CERTIFICATE OF SUBSTANTIAL COMPLETION (12.40 G LAMC.)

8. THE DEVELOPER/BUILDER SHALL GUARANTEE ALL TRESS AND IRRIGATION FOR A PERIOD OF SIX MONTHS AND ALL OTHER PLANS FOR A PERIOD OF 60 DAYS AFTER LANDSCAPE AND IRRIGATION INSTALLATION.

STATEMENTS AND CERTIFICATION

1. I HAVE COMPLIED WITH THE CRITERIA OF THE ORDINANCE AND APPLIED THEM FOR THE EFFICIENT USE OF WATER IN THE LANDSCAPE DESIGN PLANS.

2. A DIAGRAM OF THE IRRIGATION PLAN SHOWING HYDROZONES SHALL BE KEPT WITH THE IRRIGATION CONTROLLER FOR SUBSEQUENT MANAGEMENT PURPOSES.

3. A CERTIFICATE OF COMPLETION SHALL BE FILLED OUT AND CERTIFIED BY EITHER THE SIGNER OF THE LANDSCAPE PLANS, THE SIGNER OF THE IRRIGATION PLANS, OR THE LICENSED LANDSCAPE CONTRACTOR FOR THE PROJECT.

4. AN IRRIGATION AUDIT REPORT SHALL BE COMPLETED AT THE TIME OF FINAL INSPECTION.

"THE SUBDIVIDER SHALL RECORD A COVENANT AND AGREEMENT SATISFACTORY TO THE ADVISORY AGENCY GUARANTEEING THAT:

A. THE PLANTING AND IRRIGATION SYSTEM SHALL BE COMPLEATED BY THE DEVELOPER/BUILDER PRIOR TO THE CLOSE OF ESCROW OF 50 PERCENT OF THE UNITS OF THE PROJECT OR PHASE.

B. SIXTY DAYS AFTER LANDSCAPE AND IRRIGATION INSTALLATION. THE LANDSCAPE PROFESSIONAL SHALL SUBMIT TO THE HOMEOWNERS/PROPERTY OWNERS ASSOCIATION A CERTIFICATE OF SUBSTANTIAL COMPLETION.

C. THE DEVELOPER/BUILDER SHALL MAINTAIN THE LANDSCAPING AND IRRIGATION FOR 60 DAYS AFTER COMPLETION OF THE LANDSCAPE AND IRRIGATION INSTALLATION.

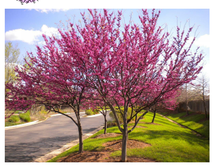
D. THE DEVELOPER/BUILDER SHALL GUARANTEE ALL TREES AND IRRIGATION FOR A PERIOD OF SIX MONTHS AND ALL OTHER PLANTS FOR A PERIOD OF 60 DAYS AFTER LANDSCAPE AND IRRIGATION INSTALLATION."

PROJECT SUMMARY:	NEW 5 STORY RESIDENTIAL BUILDING WITH 43 APARTMENT UNITS (32 MARKET RATE, 11 AFFORDABLE) + 1 FLOOR OF SUBTERRANEAN PARKING PROJECT UTILIZES INCENTIVES PER LAMC 12.22A.25
PROJECT ADDRESS:	3676 & 3704 KELTON AVE., LOS ANGELES, CA 90034
LOT SIZE:	5,053.9 SF, 5,286.4 SF, 4,994.3 SF
LOT AREA:	15,334.6 SF
ASSESSOR'S PARCEL #:	4252024020, 4252025042
TRACT:	TR 5848
MAP REFERENCE:	M B 67-57 (SHT 2), M B 61-59 (SHT 1)
BLOCK:	NONE
LOT:	77, VAC 20090809493, 19
CENSUS TRACT:	2718.04,
ZONING:	RD2-1, R3-1

PLANTING LEGEND

SYMBOL	QTY.	SIZE	SPREAD	BOTANICAL NAME / COMMON NAME	NATIVE	WUCOLS WATER USE TYPES	WATER USE VALUES	HYDRO ZONE
	9	10'	10'	CERCIS OCCIDENTALIS / WESTERN REDBUD	NO	LOW	0.4	1
	100	12"	6" - 9"	DASYLIRION SPP. / DESERT SPOON	NO	VERY LOW	0.2	1
	186	24" - 36"	24" - 48"	SANSEVIERIA SPP. / MOTHER-IN-LAW'S TONGUE	NO	LOW	0.3	1
	71	36" - 48"	24" - 36"	TRADESCANTIA PALLIDA / SPIDERWORT	NO	MEDIUM	0.4	2
	73	12"	3' - 5'	PENNISETUM SETACEUM / FOUNTAIN GRASS	NO	LOW	0.3	1

CERCIS OCCIDENTALIS / WESTERN REDBUD

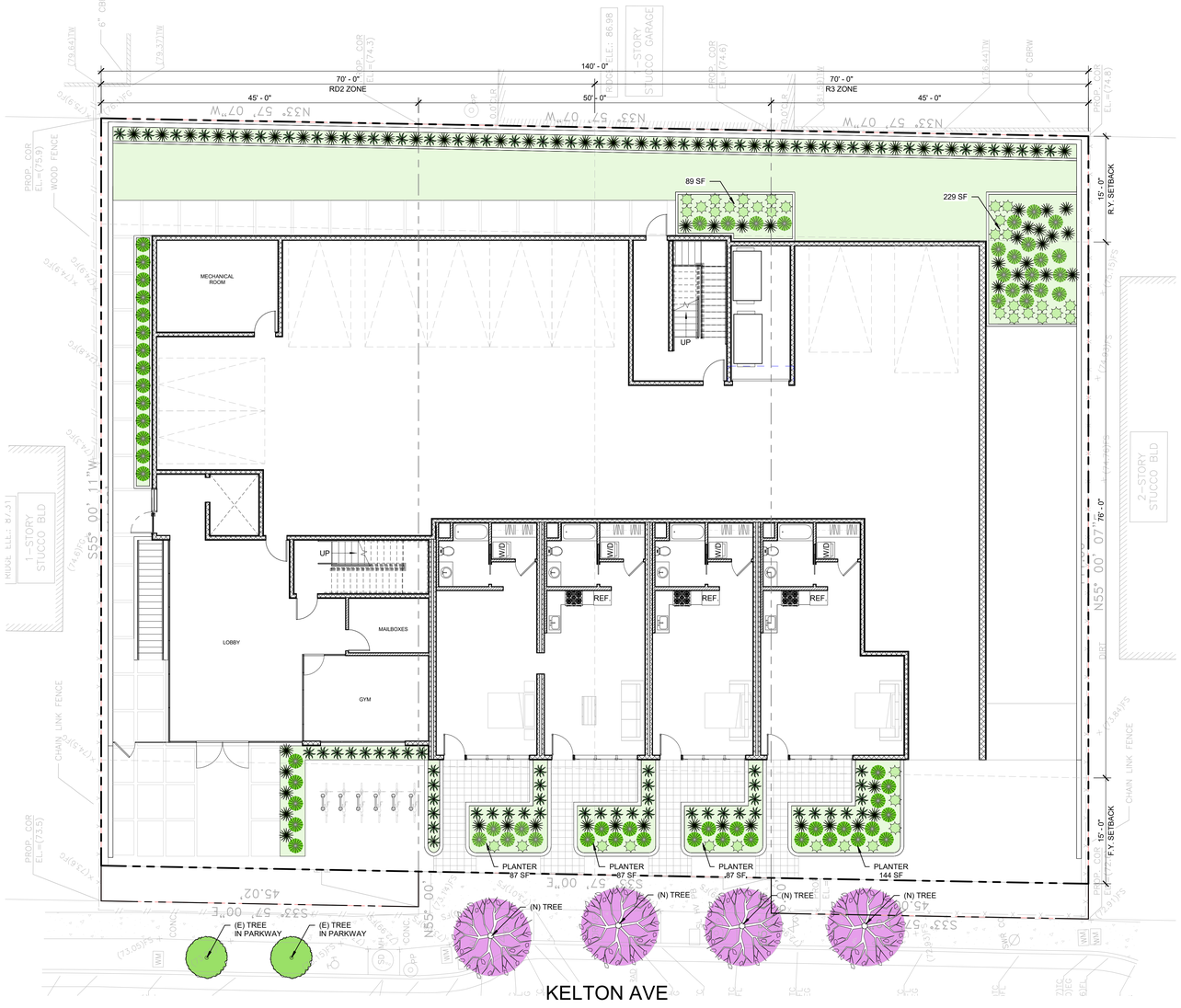


CANYON PRINCE WILD RYE / ELYMUS CONDENSATUS

POZO BLUE SAGE / SALVIA 'POZO BLUE'

BLADDERPOD / PERITOMA ARBOREA

CALIFORNIA GRAY RUSH - JUNCUS PATENS



LANDSCAPE PLAN
1/8" = 1'-0"

1

breakformdesign
127 arena street, el segundo, ca 90245
[o] 310.322.3700

LICENSED ARCHITECT
RAMSEY DAHAM
No. C-34257
10/31/23
RENEWAL DATE
STATE OF CALIFORNIA

3676 & 3704 KELTON AVE.
LOS ANGELES, CA 90034

Revision Schedule

Revision Number	Revision Date

LANDSCAPE PLAN

DRAWN	CR
CHECKED	PNK
DATE	4/13/2023 11:04:11 AM
SCALE	As indicated
JOB #	21-A001

L1.00

PLANTING NOTES

1. QUANTITIES GIVEN FOR PLANT MATERIALS SPECIFIED FOR "ON CENTER" SPACING ARE SHOWN FOR CONVENIENCE ONLY AND ARE SUBORDINATE TO THE SPACING GIVEN. VERIFY AND SUPPLY SUFFICIENT NUMBER OF PLANTS TO FULFILL SPACING REQUIREMENTS.

2. ALL HEADER AND BAMBOO ROOT BARRIERS SHALL BE LOCATED BY THE ARCHITECT ON SITE.

3. ONTRACTOR SHALL INSTALL PLANT MATERIAL IN ACCORDANCE WITH THE SPECIFICATIONS, DRAWINGS AND DETAILS.

4. ONTRACTOR SHALL PROVIDE A MAINTENANCE PERIOD OF NOT LESS THAN 90 DAYS COMMENCING AT THE DATE OF FINAL ACCEPTANCE. SUCH MAINTENANCE SHALL INCLUDE ALL CARE PERTAINING TO ALL WORK INSTALLED AS PART OF THESE CONTRACT DOCUMENTS.

5. THE CONTRACTOR SHALL MAINTAIN A QUALIFIED SUPERVISOR ON THE SITE AT ALL TIMES DURING CONSTRUCTION THROUGH COMPLETION OF PICK-UP WORK.

6. THE CONTRACTOR SHALL VERIFY ALL PLANT MATERIAL QUANTITIES LISTED FOR CONVENIENCE OF CONTRACTOR. ACTUAL NUMBER OF SYMBOLS SHALL HAVE PRIORITY OVER QUANTITIES DESIGNATED.

7. REMOVE ALL DEBRIS, WEEDS, EXCESS MATERIAL AND ROCKS LARGER THAN 1" IN DIAMETER FROM PLANTING AREAS PRIOR TO PREPARATION & AGAIN PRIOR TO PLANTING.

8. SEE DETAILS AND SPECIFICATIONS FOR STAKING METHOD, PLANT PIT DIMENSIONS, SOIL PREPARATION, AND BACKFILL REQUIREMENTS.

9. ALL PLANT MATERIALS SHALL BE APPROVED BY THE LANDSCAPE ARCHITECT PRIOR TO INSTALLATION.

10. FINAL LOCATION OF ALL PLANT MATERIAL SHALL BE SUBJECT TO THE APPROVAL OF THE LANDSCAPE ARCHITECT.

11. CONTRACTOR SHALL NOTIFY LANDSCAPE ARCHITECT 48 HOURS PRIOR TO COMMENCEMENT OF WORK TO COORDINATE PROJECT OBSERVATION SCHEDULES.

12. GROUNDCOVER PLANTING SHALL BE CONTINUOUS UNDER ALL TREES AND SHRUBS. GROUNDCOVER SHALL BE PLANTED ACCORDING TO SPACING ON PLANT LEGEND.

13. TREES SHALL BE LOCATED A MINIMUM OF 5' FROM WALLS, OVERHEADS, WALKS, HEADERS, AND OTHER TREES WITHIN THE PROJECT. IF CONFLICTS ARISE BETWEEN SIZE OF AREAS AND PLANS, CONTRACTOR TO CONTACT LANDSCAPE ARCHITECT FOR RESOLUTION. FAILURE TO MAKE SUCH CONFLICTS KNOWN TO THE LANDSCAPE ARCHITECT WILL RESULT IN CONTRACTORS LIEABILITY TO RELOCATE THE MATERIALS.

14. ALL PLANTING AREAS SHALL BE LOOSENED TO A DEPTH OF 8". APPLY 4 C.Y. OF ORGANIC AMENDMENT AND 15 LBS. OF 10-10-10 FERTILIZER PER 1000 S.F. AND BLEND WITH THE TOP 8" OF SOIL. THIS AMENDMENT IS FOR BIDDING PURPOSES, AND SHALL BE SUPERCEDED BY RECOMMENDATIONS OF THE SOIL ANALYSIS REPORT.

15. FOR ALL TREES AND SHURB PLANTING, THE FOLLOWING PREPARED SOIL MIX SHALL BE USED FOR BACKFILL IN THE PLANTERS. THIS MIX IS FOR BIDDING PURPOSES, AND SHALL BE SUPERCEDED BY RECOMMENDATIONS OF THE SOIL ANALYSIS REPORT.
SITE SOIL - 6 PARTS BY VOLUME
ORGANIC AMENDMENT - 4 PARTS BY VOLUME
SOIL CONDITIONER / FERTILIZER 10-10-10-1LB. PER C.Y. OF MIX
IRON SULFATE - 2 LBS. PER C.Y.OF MIX

16. TURF IS NOT ALLOWED ON SLOPES GREATER THAN 25% WHERE THE TOE OF THE SLOPE IS ADJACENT TO AN IMPERMEABLE HARDSCAPE..

17. RECIRCULATING WATER SYSTEMS SHALL BE USED FOR WATER FEATURES.

18. A MINIMUM 3-INCH LAYER OF MULCH SHALL BE APPLIED ON ALL EXPOSED SOIL SURFACES OF PLANTING AREAS.EXCEPT TURF AREAS, CREEPING OR ROOTING GROUNDCOVER, OR DIRECT SEEDING APPLICATIONS WHERE MULCH IS CONTRAINDICATED.

19. FOR SOILS LESS THAN 6% ORGANIC MATTER IN THE TOP 6 INCHES OF SIL, COMPOST AT A RATE OF A MINIMUM OF FOUR CUBIC YARDS PER 1,000 SQUARE FEET OF PERMEABLE AREA SHALL BE INCORPORATED TO A DEPTH OF SIX INCHES INTO THE SOIL.

20. I AGREE TO COMPLY WITH THE REQUIREMENTS OF THE WATER EFFICIENT LANDSCAPE ORDINANCE AND SUBMIT A COMPLETE LANDSCAPE DOCUMENTATION PACKAGE THAT COMPLYS WITH THE PERFORMANCE APPROACH.

DATE_____ SIGNED_____

21. AT THE TIME OF FINAL INSPECTION THE PERMIT APPLICANT MUST PROVIDE THE OWNER OF THE PROPERTY WITH A CERTIFICATE OF COMPLETION, CERTIFICATE OF INSTALLATION, IRRIGATION SCHEDULE AND SCHEDULE OF LANDSCAPE AND IRRIGATION MAINTENANCE.

IRRIGATION NOTES

1. CONTRACTOR IS TO AUGMENT EXISTING IRRIGATION SYSTEM. CONTRACTOR SHALL REPAIR OR REPLACE ANY EXISTING LANDSCAPE AND IRRIGATION DAMAGED FROM CONSTRAUCTION TO AN ACCEPTABLE LANDSCAPE CONDITION WITH A FULLY FUNCTIONAL AND EFFICIENT IRRIGATION SYSTEM PER THE CONTAINED CONDITIONS.

2. ALL NEW TREES REQUIRE INDIVIDUAL POP-UP STREAM BUBBLERS, MIN. 2 PER TREE, WITHIN 4' OF TREE. TREE IRRIGATION SHALL BE ON A SEPARATE VALVE.

3. SPRAY OR ROTOR HEADS SHALL BE ON POP-UPS. 6" FOR LAWN, LOW GROUNDCOVER OR PARKED CAR OVERHANG AREAS, 12" FOR SHRUB AREAS. HEADS ON RISERS ARE ONLY ALLOWED ADJACENT TO WALLS WITH LIMITED SPACE FOR POP-UPS.

4. LOCATE SPRAY HEADS 24" FROM NON-PERVIOUS PAVING TO PREVENT OVERSPRAY. EXCEPTION ALLOWED IF ADJACENT SURFACE IS PERMEABLE OR IF USING ALTERNATIVE TECHNOLOGY IRRIGATION. ROTATOR OR ROTARY HEADS MAYBE LOCATED 12" FROM PAVING.

5. CONTRACTOR SHALL REPLACE ANY EXISTING IRRIGATION CONTROLLER WITH A MODULE AND SENSOR TO PROVIDE WEATHER BASED INFORMATIONTHAT WILL AUTOMATE THE IRRIGATION RUNTIMES BASED ON WEATHER. SEE HUNTER SOLAR SYNC, RAINBIRD ET MANAGER OR EQUIVALENT.

6. THE PLANTING AND IRRIGATION SYSTEM SHALL BE COMPLETED BY THE DEVELOPER/BUILDER PRIOR TO THE CLOSE OF ESCROW OF 50 PERCENT OF THE UNITS OF THE PROJECT OR PHASE.

7. SIXTY DAYS AFTER TLANDSCAPE AND IRRIGATION INSTALLATION. THE LANDSCAPE PROFESSIONAL SHALL SUBMIT TO THE HOMEOWNERS/PROPERTY OWNERS ASSOCIATION A CERTIFICATE OF SUBSTANTIAL COMPLETION (12.40 G LAMC.)

8. THE DEVELOPER/BUILDER SHALL GUARANTEE ALL TRESS AND IRRIGATION FOR A PERIOD OF SIX MONTHS AND ALL OTHER PLANS FOR A PERIOD OF 60 DAYS AFTER LANDSCAPE AND IRRIGATION INSTALLATION.

STATEMENTS AND CERTIFICATION

1. I HAVE COMPLIED WITH THE CRITERIA OF THE ORDINANCE AND APPLIED THEM FOR THE EFFICIENT USE OF WATER IN THE LANDSCAPE DESIGN PLANS.

2. A DIAGRAM OF THE IRRIGATION PLAN SHOWING HYDROZONES SHALL BE KEPT WITH THE IRRIGATION CONTROLLER FOR SUBSEQUENT MANAGEMENT PURPOSES.

3. A CERTIFICATE OF COMPLETION SHALL BE FILLED OUT AND CERTIFIED BY EITHER THE SIGNER OF THE LANDSCAPE PLANS, THE SIGNER OF THE IRRIGATION PLANS, OR THE LICENSED LANDSCAPE CONTRACTOR FOR THE PROJECT.

4. AN IRRIGATION AUDIT REPORT SHALL BE COMPLETED AT THE TIME OF FINAL INSPECTION.

PLANTING LEGEND

SYMBOL	QTY.	SIZE	SPREAD	BOTANICAL NAME / COMMON NAME	NATIVE	WUCOLS WATER USE TYPES	WATER USE VALUES	HYDRO ZONE
	9	10'	10'	CERCIS OCCIDENTALIS / WESTERN REDBUD	NO	LOW	0.4	1
	100	12"	6" - 9"	DASYLIRION SPP. / DESERT SPOON	NO	VERY LOW	0.2	1
	186	24" - 36"	24" - 48"	SANSEVIERIA SPP. / MOTHER-IN-LAW'S TONGUE	NO	LOW	0.3	1
	71	36" - 48"	24" - 36"	TRADESCANTIA PALLIDA / SPIDERWORT	NO	MEDIUM	0.4	2
	73	12"	3' - 5'	PENNISETUM SETACEUM / FOUNTAIN GRASS	NO	LOW	0.3	1

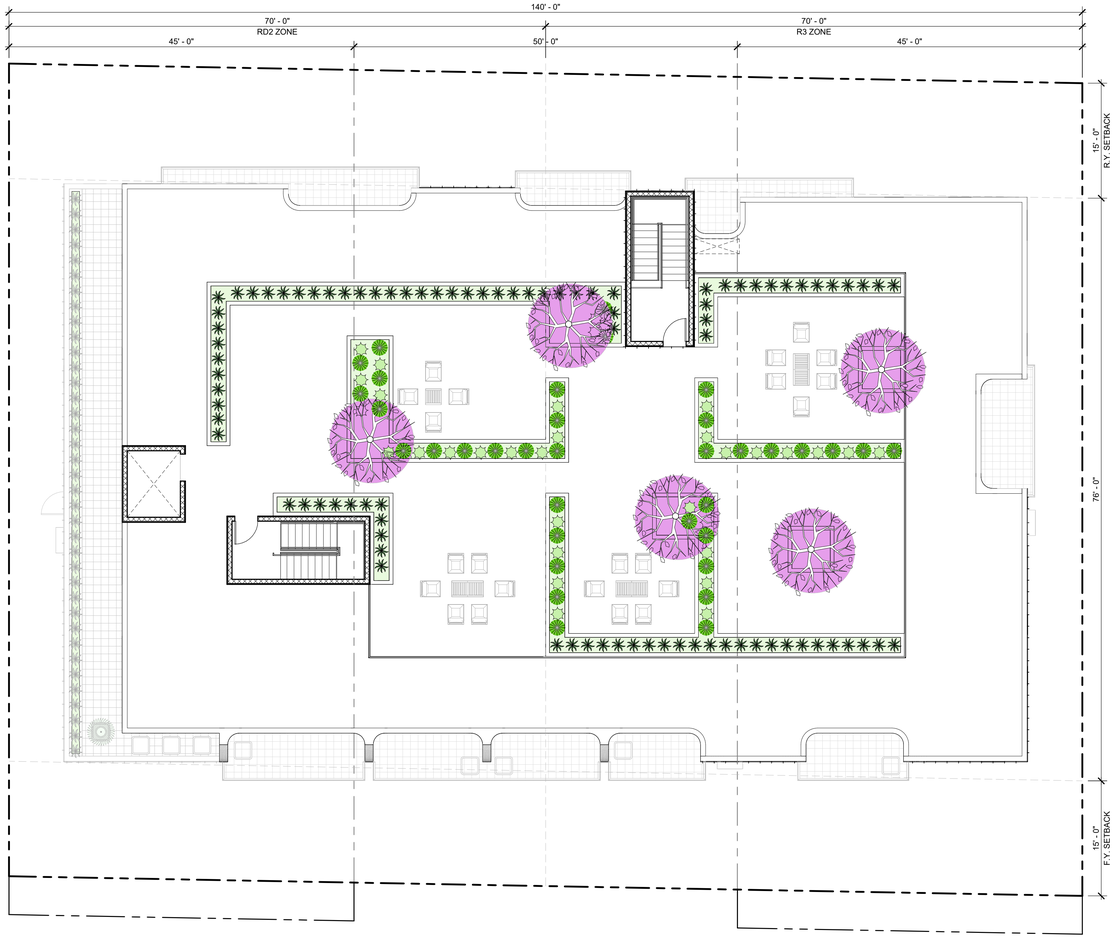
CERCIS OCCIDENTALIS / WESTERN REDBUD

CANYON PRINCE WILD RYE / ELYMUS CONDENSATUS

POZO BLUE SAGE / SALVIA 'POZO BLUE'

BLADDERPOD / PERITOMA ARBOREA

CALIFORNIA GRAY RUSH - JUNCUS PATENS



KELTON AVE

LANDSCAPE PLAN - ROOF
1/8" = 1'-0"

1

breakformdesign
127 arena street, el segundo, ca 90245
[o] 310.322.3700

LICENSED ARCHITECT
RAMSEY DAHAM
No. C-34257
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L1.10

COUNTY CLERK'S USE

CITY OF LOS ANGELES
OFFICE OF THE CITY CLERK
200 NORTH SPRING STREET, ROOM 395
LOS ANGELES, CALIFORNIA 90012

CALIFORNIA ENVIRONMENTAL QUALITY ACT

NOTICE OF EXEMPTION

(PRC Section 21152; CEQA Guidelines Section 15062)

Pursuant to Public Resources Code § 21152(b) and CEQA Guidelines § 15062, the notice should be posted with the County Clerk by mailing the form and posting fee payment to the following address: Los Angeles County Clerk/Recorder, Environmental Notices, P.O. Box 1208, Norwalk, CA 90650. Pursuant to Public Resources Code § 21167 (d), the posting of this notice starts a 35-day statute of limitations on court challenges to reliance on an exemption for the project. Failure to file this notice as provided above, results in the statute of limitations being extended to 180 days.

PARENT CASE NUMBER(S) / REQUESTED ENTITLEMENTS
CPC-2023-0582-CU-DB-HCA-PHP

LEAD CITY AGENCY	CASE NUMBER
City of Los Angeles (Department of City Planning)	ENV-2023-0583-CE
PROJECT TITLE	**COUNCIL DISTRICT**
3704 Kelton Avenue	5

PROJECT LOCATION (Street Address and Cross Streets and/or Attached Map) ☐ Map attached.
3676-3704 South Kelton Avenue; 10845 West Regent Street

PROJECT DESCRIPTION: ☒ Additional page(s) attached.
Demolition of existing improvements. Construction, use, maintenance of a multifamily building with 43 dwelling units, including 11 Very Low Income. 62 feet in height and has 42,222 square feet of floor area; 33 parking spaces; parking provided within a subterranean garage. The project includes any additional actions as deemed necessary or desirable, including but not limited to demolition, grading, excavation, haul route, street tree removal, on-site tree removal, and building permits. The amount of materials exported will be up to approximately 6,941 cubic yards.

NAME OF APPLICANT / OWNER:
Kelton Avenue Investments, LLC

CONTACT PERSON (If different from Applicant/Owner above)	(AREA CODE) TELEPHONE NUMBER | EXT.
Jesi Harris, Brian Silveira & Associates	704.277.7332

EXEMPT STATUS: (Check all boxes, and include all exemptions, that apply and provide relevant citations.)

STATE CEQA STATUTE & GUIDELINES

☐ STATUTORY EXEMPTION(S)

Public Resources Code Section(s) _____

☒ CATEGORICAL EXEMPTION(S) (State CEQA Guidelines Sec. 15301-15333 / Class 1-Class 33)

CEQA Guideline Section(s) / Class(es) **15332 / Class 32**

☐ OTHER BASIS FOR EXEMPTION (E.g., CEQA Guidelines Section 15061(b)(3) or (b)(4) or Section 15378(b))

_____

JUSTIFICATION FOR PROJECT EXEMPTION: ☒ Additional page(s) attached

☒ None of the exceptions in CEQA Guidelines Section 15300.2 to the categorical exemption(s) apply to the Project.
☐ The project is identified in one or more of the list of activities in the City of Los Angeles CEQA Guidelines as cited in the justification.

IF FILED BY APPLICANT, ATTACH CERTIFIED DOCUMENT ISSUED BY THE CITY PLANNING DEPARTMENT STATING THAT THE DEPARTMENT HAS FOUND THE PROJECT TO BE EXEMPT.
If different from the applicant, the identity of the person undertaking the project.

CITY STAFF USE ONLY:

CITY STAFF NAME AND SIGNATURE	STAFF TITLE
Renata Ooms	City Planner

ENTITLEMENTS APPROVED
Density Bonus, Site Plan Review, Specific Plan conformance.

DISTRIBUTION: County Clerk, Agency Record
Rev. 6-22-2021

DEPARTMENT OF CITY PLANNING

COMMISSION OFFICE
(213) 978-1300

CITY PLANNING COMMISSION

SAMANTHA MILLMAN
PRESIDENT

CAROLINE CHOE
VICE-PRESIDENT

MARIA CABILDO
ILISSA GOLD
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CITY OF LOS ANGELES
CALIFORNIA

KAREN BASS
MAYOR

EXECUTIVE OFFICES
200 N. SPRING STREET, ROOM 525
LOS ANGELES, CA 90012-4801
(213) 978-1271

VINCENT P. BERTONI, AICP
DIRECTOR

SHANA M.M. BONSTIN
DEPUTY DIRECTOR

ARTHI L. VARMA, AICP
DEPUTY DIRECTOR

LISA M. WEBBER, AICP
DEPUTY DIRECTOR

JUSTIFICATION FOR PROJECT EXEMPTION CASE NO. ENV-2023-0583-CE

On July 17, 2023, the Planning Department determined that the City of Los Angeles Guidelines for the implementation of the California Environmental Quality Act of 1970 and the State CEQA Guidelines designate the subject project as Categorically Exempt under Article 19, Section 15332, Class 32.

A project qualifies for a Class 32 Categorical Exemption if it is developed on an infill site and meets the following criteria:

(a) The project is consistent with the applicable general plan designation and all applicable general plan policies as well as with the applicable zoning designation and regulations;
(b) The proposed development occurs within city limits on a project site of no more than five acres substantially surrounded by urban uses;
(c) The project site has no value as habitat for endangered, rare or threatened species;
(d) Approval of the project would not result in any significant effects relating to traffic, noise, air quality, or water quality; and
(e) The site can be adequately served by all required utilities and public services.

The project is for a five story 43-unit apartment building (11 Very Low Income units) with 33 parking spaces located at 3676-3704 South Kelton Avenue and 10845 West Regent Street. As a multifamily building and a project which is characterized as in-fill development, the project qualifies for the Class 32 Categorical Exemption.

The site is zoned RD2-1 and R3-1 and has a General Plan Land Use Designation of Low Medium Residential and Medium Residential. As shown in the case file, the project is consistent with the applicable West Los Angeles Community Plan designation and policies and all applicable zoning designations and regulations. The subject site is wholly within the City of Los Angeles, on a site that is approximately 0.4 acres. Lots adjacent to the subject site are developed with the following urban uses: single- and multi-family residential uses. The site is previously disturbed and surrounded by development and therefore is not, and has no value as, a habitat for endangered, rare or threatened species. There are no protected trees on the site, as identified in the Tree Report prepared by The Tree Resource on June 2023. Non-protected trees on-site and two street trees are proposed to be removed from the subject site. However, as explained in the project description, the applicant will be required to improve the right-of-way. Prior to any work on the right-of-way, the applicant will be required to obtain approved plans from the Department of Public Works. As there currently is no approved right-of-way improvement plan and for purposes of

conservative analysis and the requirements of CEQA, Planning has analyzed the worst-case potential for removal of all street trees. Note, no street tree or protected tree may be removed without prior approval of the Board of Public Works/Urban Forestry (BPW) under LAMC Sections 62.161 - 62.171. At the time of preparation of this CE, no approvals have been given for any tree removals on-site or in the right-of-way by BPW. Based on the above, Planning has required a Tree Report to identify all trees on the project site and in the right-of-way that could be impacted by the Project and to consider the potential removal of two street trees.

The project will be subject to Regulatory Compliance Measures (RCMs), which require compliance with the City of Los Angeles Noise Ordinance and regulations related to pollutant discharge, dewatering, stormwater mitigation, and Best Management Practices for stormwater runoff. These RCMs will ensure the project will not have significant impacts on noise and water. Furthermore, the project does not exceed the threshold criteria established by LADOT for preparing a transportation study. Interim thresholds were developed by DCP staff based on CalEEMod model runs relying on reasonable assumptions, consulting with AQMD staff, and surveying published air quality studies for which criteria air pollutants did not exceed the established SCAQMD construction and operational thresholds. The project site will be adequately served by all public utilities and services given that the construction of s 43 unit residential building will be on a site which has been previously developed and is consistent with the General Plan. Therefore, the project meets all of the Criteria for the Class 32.

Exceptions Narrative for Class 32 Categorical Exemption

There are five (5) Exceptions which must be considered in order to find a project exempt under Class 32: (a) Cumulative Impacts; (b) Significant Effect; (c) Scenic Highways; (d) Hazardous Waste Sites; and (e) Historical Resources.

There is not a succession of known projects of the same type and in the same place as the subject project. As mentioned, the project proposes 43 unit project in an area zoned and designated for multifamily development. All adjacent lots are developed with single and multi-family uses, and the subject site is of a similar size and slope to nearby properties. The project proposes a Floor Area Ratio (FAR) of 3.7:1 on a site that is permitted to have a maximum FAR of 3:1, which is allowed pursuant to the Density Bonus menu of incentives. It is not unusual for the vicinity of the subject site, and is similar in scope to other existing multifamily in the area. Thus, there are no unusual circumstances which may lead to a significant effect on the environment. Additionally, the only State Scenic Highway within the City of Los Angeles is the Topanga Canyon State Scenic Highway, State Route 27, which travels through a portion of Topanga State Park is not near the site. Therefore the subject site will not create any impacts within a designated as a state scenic highway. Furthermore, according to Envirostor, the State of California's database of Hazardous Waste Sites, neither the subject site, nor any site in the vicinity, is identified as a hazardous waste site. The project site has not been identified as a historic resource by local or state agencies, and the project site has not been determined to be eligible for listing in the National Register of Historic Places, California Register of Historical Resources, the Los Angeles Historic-Cultural Monuments Register, and/or any local register; and was not found to be a potential historic resource based on the City's HistoricPlacesLA website or SurveyLA, the citywide survey of Los Angeles. Finally, the City does not choose to treat the site as a historic resource. Based on this, the project will not result in a substantial adverse change to the significance of a historic resource and this exception does not apply.

Assessment of 3676-3704 S Kelton Avenue; 10845 W Regent Street
Project Eligibility for a Categorical Exemption as a Class 32 In-Fill Development

California Environmental Quality Act Class 32 Categorical Exemption Evaluation

This assessment evaluates whether the proposed 3676-3704 S Kelton Avenue Project (project) located in the City of Los Angeles (City) qualifies for a Class 32 Categorical Exemption under the California Environmental Quality Act (CEQA) as an eligible infill development.

CEQA defines categorical exemptions for various types of projects the Secretary of the Resources Agency of the State of California has determined would not have a significant effect on the environment, and therefore are not subject to further environmental review under CEQA. The Class 32 exemption (Section 15332 of the State CEQA Guidelines) is intended to promote infill development within urbanized areas. The class consists of environmentally benign infill projects consistent with local general plan and zoning requirements.

Pursuant to Section 15332 of the State CEQA Guidelines, for a project to be eligible for a Categorical Exemption as Class 32 In-fill Development, a project must meet the following conditions, or criteria:

Criteria

(a) The project is consistent with the applicable general plan designation and all applicable general plan policies as well as with applicable zoning designation and regulations.

(b) The proposed development occurs within city limits on a project site of no more than five (5) acres substantially surrounded by urban uses.

(c) The project site has no value as habitat for endangered, rare or threatened species.

(d) Approval of the project would not result in any significant effects relating to traffic, noise, air quality, or water quality.

(e) The site can be adequately served by all required utilities and public services.

In addition, projects seeking this Categorical Exemption cannot fall under certain specified exceptions, as follows.

Exceptions

(a) The project and successive projects of the same type in the same place will result in cumulative impacts.

(b) There are unusual circumstances creating the reasonable possibility of significant effects.

(c) The project may result in damage to scenic resources, including, but not limited to, trees, historic buildings, rock outcroppings, or similar resources, within an officially designated scenic highway.

(d) The project is located on a site that the Department of Toxic Substances Control and the Secretary of the Environmental Protection have identified, pursuant to Government code section 65962.5, as being affected by hazardous wastes or clean-up problems.

(e) The project may cause a substantial adverse change in the significance of an historical resource.

The justification for use of a Class 32 Categorical Exemption as an infill project in compliance with CEQA and the City's Class 32 Requirements is provided below in the following format: I. Project Description, II. Evaluation of Class 32 Exemption Criteria, III. Consideration of Exemptions, and IV. Conclusion.

I. PROJECT DESCRIPTION

The subject property consists of two (2) whole existing parcels containing three (3) lots. The parcels are currently developed with two (2) multi-family residential duplexes of approximately 750 square feet per unit. The project proposes a five-story, 43,349 square foot apartment building with 43 units, 11 of which are designated affordable housing, above a subterranean parking level containing 24 spaces, located on a 15,334.6 square foot lot. The project site is surrounded by urban development, consisting of single- and multi-family residential and commercial land uses. The project would remove the existing multi-family residences on the subject property. Project Plans are included as Attachment A.

II. EVALUATION OF CLASS 32 EXEMPTION CRITERIA

The following subsections provide discussion and analysis of the project's consistency with the criteria listed in Section 15332 of the State CEQA Guidelines, for a project to be eligible for a Categorical Exemption as a Class 32 In-fill Development project.

Written justification that the proposed Project meets the following criteria:

(a) The project is consistent with the applicable general plan designation and all applicable general plan policies as well as with applicable zoning designation and regulations.

The proposed residential project is consistent with the subject properties' existing General Plan designations, as specified in the Palms-Mar Vista-Del Rey Community Plan, a component of the City's General Plan, which designates the site "Low Medium Residential" and "Medium Residential." The site is spread between two lots, one zoned RD2-1 and one zoned R3-1. The project would therefore not require a General Plan Amendment or Zoning Change. Multiple dwelling units are consistent with RD2 and R3 uses as outlined in the Los Angeles Municipal Code (LAMC) Sections12.09.1 and 12.10. Under the existing zoning of RD2-1 and R3-1, the minimum lot area per dwelling unit is 2,000 square feet for the RD2 portions and 800 square feet for the R3 portions. After accounting for a 5-foot dedication to the public right-of-way, the total lot remaining area lot area is comprised of 7,472 square feet with the RD2 zoning designation and 7,412 square feet with the R3 zoning designation. Therefore, the lot would allow fourteen (14) units on the project site. The project is providing a 79 percent of its base density of 14 units as affordable housing set-aside for Very Low Income households, which would allow for an additional 29 units per the LAMC 12.21 A 25 and LAMC 12.24 U 26 for a combined total of 43

allowable units. Therefore, the project is within the parameters of the density allowed for projects in the RD2 and R3 zones with its rate and depth of affordability.

Additionally, the project's on-menu incentives allow the base density to be averaged between the RD2 and R3 zones, resulting in a 14-unit base density upon which the density bonus is based pursuant to LAMC 12.22. A.25. The project's other on-menu incentives allow for 10 percent decreases to the north and south side yard setbacks, respectively. The project also requests off-menu waivers of development standard which allow it to increase the building's allowable height by 17 feet, provide 26 parking spaces in lieu of 46, increase its allowable floor-area-ration by 37 percent, and reduce its required usable open space by 35 percent.

The construction of a 43-unit apartment building would be consistent with the General Plan designation and zoning. The project is also located within the West Los Angeles Transportation Improvement Mitigation Specific Plan Area, where most development projects require a Transportation Impact Assessment Fee (TIA Fee) as calculated by Los Angeles Department of Transportation (LADOT). The Applicant would comply with the TIA payment procedures and pay the required fee as determined by the LADOT.

Therefore, the project would be consistent with all applicable general plan designation, general plan policies and applicable zoning designation and regulations.

(b) The proposed development occurs within city limits on a project site of no more than five acres substantially surrounded by urban uses.

The project site is located within the city limits of the City of Los Angeles. The project site consists of approximately 15, 334.6 square feet of land, or 0.35 acres, and is surrounded by existing urban uses, including single- and multi-family residential uses to the east and west, single-family residential to the north and multi-family residential uses to the south. Therefore, the project is consistent with this condition.

(c) The project site has no value as habitat for endangered, rare or threatened species.

The project site is located within a highly urbanized portion of the City of Los Angeles. The surrounding urban landscape including the project site has been developed for decades. The project site is currently developed with residential buildings along with soft and hardscape landscaping. The subject property does not have reported occurrences of special-status species in the California Natural Diversity Database (CNDDB) maintained by the California Department of Fish and Wildlife (CDFW). The project site does not include riparian areas or other sensitive plant communities. Therefore, the project site has no substantive value as a habitat for endangered, rare, or threatened species.

(d) Approval of the project would not result in any significant effects relating to traffic, noise, air quality, or water quality.

Transportation Effects

The project would have a significant impact if the project would conflict or be inconsistent with CEQA Guidelines Section 15064.3(b)(1), relating to Vehicle Miles Traveled (VMT). CEQA

Guidelines Section 15064.3(b)(1) applies to land use projects and states, "Vehicle miles traveled exceeding an applicable threshold of significance may indicate a significant impact. Generally, projects within one-half mile of either an existing major transit stop or a stop along an existing high-quality transit corridor should be presumed to cause a less than significant transportation impact. Projects that decrease vehicle miles traveled in the project area compared to existing conditions should be presumed to have a less than significant transportation impact." Both of the following City of Los Angeles Transportation Assessment Guidelines (TAG) screening criteria must be met in order to require further analysis of a land use project's VMT contribution: the land use project would both generate a net increase of 250 or more daily vehicle trips and the project would generate a net increase in daily VMT.

In order to determine if both criteria are triggered by the project, a basic run of the City of Los Angeles VMT Calculator was performed. The VMT Calculator, (included as Attachment B) run determined that the project's 43 new multi-family residences would generate 200 average daily trips (ADT), and 1,261 daily VMT. Additionally, the proposed project would remove the four (4) existing multi-family residences, which currently generate a combined total of 20 ADT and 128 daily VMT. Therefore, the proposed project would result in a project-related net increase of 180 ADT and 1,133 daily VMT, which would be below the City's screening criterion of 250 ADT for a VMT analysis to be required. As such, the VMT generated by the project would not result in a significant effect relating to transportation, and further analysis of the project's VMT contribution would not be warranted.

Air Quality Effects

The project's potential air quality effects were evaluated by estimating the potential construction and operational emissions of criteria pollutants and comparing those levels to significance thresholds provided by the Southern California Air Quality Management District (SCAQMD). The project's emissions were estimated using the CalEEMod 2020.4.0 model provided by SCAQMD for the purposes of evaluating air quality impacts of proposed projects.

Projects in the SCAQMD with daily emissions that exceed any of the emission thresholds provided in Table I, SCAQMD Daily Maximum Emissions Thresholds, may be considered significant under CEQA guidelines.

At forty-three (43) dwelling units, zero (0) square feet of non-residential use, and less than 20,000 cubic yards of soil export, the proposed project is not expected to exceed any of the emission thresholds set for air quality effects during either the project's construction or operation phases.

Table I - SCAQMD Daily Maximum Emissions Thresholds

Pollutant	Construction (lbs./day)	Operation (lbs./day)
ROG	75 lbs/day	55 lbs/day
NO_x	100 lbs/day	55 lbs/day
CO	550 lbs/day	550 lbs/day
SO_x	150 lbs/day	150 lbs/day
PM-10	150 lbs/day	150 lbs/day
PM-2.5	55 lbs/day	55 lbs/day

SCAQMD Air Quality Significance Thresholds, Revision: April 2019

Construction activity emissions considered demolition of existing structures, site preparation, grading, building construction, paving, and architectural coating (including painting or other surface treatments). Following construction, emission from operations of the project would result primarily from mobile sources (light duty vehicle use). Table II, Maximum Daily Emissions, summarizes the project's maximum daily emissions estimated by CalEEMod for short-term construction and long-term operations (model outputs provided in Attachment C).

Table II - Project Maximum Daily Emissions

Daily Emissions (lbs/day)	ROG	NO_x	CO	SO_x	PM-10	PM-2.5
Construction						
Max. Daily Construction Emissions	0.73	2.91	3.17	0.007	0.35	0.18
SCAQMD Thresholds	75	100	550	150	150	55
Significant Impact (Y/N)	N	N	N	N	N	N
Operations						
Max. Daily Operations Emissions	1.6	0.83	8.94	0.02	1.60	0.45
SCAQMD Thresholds	55	55	550	150	150	55
Significant Impact (Y/N)	N	N	N	N	N	N

As shown in Table II, the project would not exceed SCAQMD significance thresholds and would therefore not result in a significant effect relating to air quality.

Localized Significance Thresholds (LSTs) were developed to evaluate ambient air quality on a local level in addition to the more regional emissions-based thresholds of significance. The LST methodology addresses specific emissions, namely oxides of nitrogen (NOx), carbon monoxide (CO), and particulate matter (PM-10 and PM-2.5). LSTs represent the maximum emissions from a project that are not expected to cause or contribute to an exceedance of the most stringent applicable federal or state ambient air quality standard, and they are developed based on the ambient concentrations of that pollutant for each source receptor area and distance to the nearest sensitive receptor.

For the proposed project, LST impacts were evaluated using SCAQMD screening table thresholds for a 1-acre site with a source-receptor distance of 25 meters, the most stringent parameter for which the screening tables provide thresholds. This evaluation is based on maximum daily onsite construction emissions that would occur during any phase of project construction. Daily emissions would typically be lower than the reported maximum amounts. The table below shows the relevant threshold and the estimated peak daily onsite emissions for each pollutant during project construction to establish the highest level of onsite emissions to be evaluated for LST impacts. As shown in Table III, Project Related LST Evaluation, the project's maximum daily onsite construction emissions would not exceed the relevant LST screening table thresholds for LST-related criteria pollutants, and impacts would be less than significant.

1 acre/25 meters/Northwest Coastal LA County	Project LST Emissions (lbs/day)			
	NO_x	CO	PM-10	PM-2.5
LST Threshold	103	562	4	3
Peak Onsite Daily Emissions	2.91	3.17	0.35	0.18
Significant Impact? (Yes/No)	No	No	No	No

Noise Effects

Based upon the size, scope and features of this project and the project site, it is not likely that the City will require additional documentation or analysis to provide substantial evidence supporting a determination that the project will not have significant impacts related to noise.

Construction

Construction activities and associated noise would be temporary and be restricted to daytime hours pursuant to Los Angeles Municipal Code (LAMC) Section 41.40. The maximum noise level of construction equipment is regulated by LAMC Section 112.05 to 75 dB at 50 feet from the source; however, the LAMC indicates such restrictions do not apply where technically infeasible despite the use of mufflers, shields, sound barriers and/or other noise reduction device or techniques during the operation of the equipment. The table below is based on the L_{max} noise levels of construction equipment provided in the Federal Highway Administration Construction Noise Handbook, Construction Noise Levels – Regulatory Compliance provides construction equipment noise levels with the use of mufflers and sound barriers required by LAMC Section 112.05.

Phase	Equipment	#	Type	Lmax at 50 ft (dBA)	LAMC Sec. 112.05 Complance	Reduced Lmax at 50 ft (dBA)
Demolition	Concrete Industrial Saws	1	Stationary	90	Barrier	70
	Rubber Tired Dozers	1	Mobile	82	Muffler	67
	Tractors/Loaders/Backhoes	2	Mobile	80	Muffler	65
Site Preparation	Graders	1	Mobile	85	Muffler	75
	Tractors/Loaders/Backhoes	1	Mobile	80	Muffler	65
Grading	Graders	1	Mobile	85	Muffler	75
	Rubber Tired Dozers	1	Mobile	82	Muffler	67
	Tractors/Loaders/Backhoes	1	Mobile	78	Muffler	65
Building Construction	Cranes	1	Mobile	81	Muffler	66
	Forklifts	2	Mobile	75	None	75
	Tractors/Loaders/Backhoes	2	Mobile	80	Muffler	65

Paving	Cement and Mortar Mixers	4	Mobile	79	Muffler	64
	Pavers	1	Mobile	77	Muffler	62
	Rollers	1	Mobile	80	Muffler	65
	Tractors/Loaders/Backhoes	1	Mobile	78	Muffler	65
Architectural Coating	Air Compressors	1	Stationary	78	Barrier	58

As shown in the final column of Table II-1, regulatory compliance with LAMC Section 112.05 standards, requiring mufflers, shields, sound barriers and/or other noise reduction device or techniques during the operation of the equipment) would reduce the construction noise levels to less than 75 dBA at 50 feet through industrial-grade mufflers on mobile equipment and barriers or enclosures formed by sound transmission obscuring products around stationary equipment. Mufflers and sound transmission obscuring products, like barriers or enclosures, are available from a variety of manufacturers. Therefore, construction related temporary noise level increases would be less than significant with regulatory compliance measures incorporated.

Operations

Pursuant to LAMC Section 112.02, the project would be considered to exceed operational noise ordinance standards if it would increase the ambient noise level on another property by more than 5 dBA.

This project does not propose to develop commercial, industrial, manufacturing, or institutional facilities that are associated with loud stationary noise sources. The project would introduce new stationary noise sources in the form of Heating, Ventilation, and Air Conditioning (HVAC) units. It is assumed that the project would include rooftop HVAC units for each of the 43 dwelling units, one for the lobby, and one for the ground floor gym for a total of forty-five (45) HVAC units. Based on noise levels for HVAC units similar to those expected to be used in the project, each HVAC unit would produce a noise level of 68 dBA Leq at 3.3 ft.

This analysis assumes all 45 roof-mounted HVAC units are in simultaneous use as a "worst-case" scenario although actual HVAC use would depend on weather conditions and tenant occupancy. Addition of the reference noise levels for the 45 HVAC units would result in a composite reference noise level of 84.5 dBA at 3.3 feet, a value that is used to calculate noise levels at greater distances. Of the nearby sensitive land uses, the property which would experience the greatest level of noise from HVAC operation would be the single-family residence to the north at 3670 S Kelton Avenue, approximately 12 feet of horizontal distance and 47 feet of vertical distance from the nearest portion of the project rooftop area in which HVAC units could potentially be placed. At this distance, noise levels would be reduced by 23.355 dBA to 61.14 dBA based on the equation for distance attenuation of a point source. In addition, the parapet and roofline would decrease noise levels by a further 10 dBA based on the Federal Transit Administration (FTA) methodology for calculating barrier insertion loss for a final noise level of 51.14 dBA.

LAMC Section 111.03 establishes a presumed ambient noise level of 50 dBA during the day and 40 dB at night for the RD and R3 zones. Based on the formula for the addition of decibels, the

addition of 51.14 dBA from the 45 proposed HVAC units to the ambient daytime noise level would result in an increase of 1.1 dBA above the presumed daytime ambient noise level of 50 dBA for the RD and R3 zones, which the residences are within. At nighttime, the HVAC would result in a 1.2 dBA increase above the presumed nighttime ambient noise level of 40 dBA. All other property boundaries would experience lower levels of HVAC noise. Therefore, operational HVAC noise would not exceed the ambient noise level by more than 5 dBA in compliance with LAMC Section 112.02. In addition, noise levels would potentially be further reduced by the structural and architectural materials of nearby source receptors.

Generally, it takes a doubling of traffic volumes to increase traffic noise levels by 3 dBA, which is the level at which changes are barely perceptible to the human ear. The major source of traffic noise in the project vicinity is Venice Boulevard. Based on City of Los Angeles VMT Calculator, the project would generate a net increase of 200 ADT. A traffic volume increase of 200 ADT on Venice Boulevard would far less than double traffic volumes and would therefore result in a noise level increase far below 3 dBA. As such, the additional traffic generated by the project would not be expected to result in a significant noise impact.

Water Quality Effects

The proposed infill development would replace existing residential land uses with new, higher density residential uses, which would not significantly differ in potential water quality effects.

(e) The site can be adequately served by all required utilities and public services.

The project site is located in an urbanized area of the City's Palms-Mar Vista-Del Rey Community Plan Area and consists of parcels currently developed with two (2) multi-family structures served by existing utility and public service providers. The proposed project would replace the four (4) existing residences with a 43-unit apartment building. The proposed project would be served by the same utility and public service providers that serve the site and surrounding vicinity under existing conditions, including:

- Los Angeles Fire Department Station 43
- Los Angeles Police Department West Bureau
- City of Los Angeles Department of Public Works
- City of Los Angeles Department of Recreation and Parks

The proposed project would add a net increase of 39 new dwelling units to the site, consistent with existing planning and zoning (as described in Section II.a), on which utilities and public service agencies base their service and facility planning. The project would be served by existing public service providers, is consistent with existing planning and zoning, would not substantially increase demand for utilities or public services over existing conditions. As described in Section III.b., below, the project's 43 new apartments would provide housing for an estimated net 129 persons. The City projects the City's future population for the year 2040 to increase by 763,900, accommodating growth, such as the project's added population, that utilities and public service agencies use for planning purposes. As the increase in units would not be substantial and would be within the projected City growth, the project would be adequately served by required utilities and public services.

III. CONSIDERATION OF EXCEPTIONS

Section 15300.2 of the CEQA Statutes and Guidelines provides a list of exceptions for consideration of a project as categorically exempt. The exemptions that apply to the project are listed and discussed below:

(a) Cumulative Impact. All exemptions for these classes are inapplicable when the cumulative impact of successive projects of the same type in the same place, over time is significant.

This project proposes an infill development of residential uses within an urban setting surrounded by existing residential and commercial uses. The project's environmental effects regarding traffic, noise, and air quality would be less than significant, as discussed above. According to the Southern California Association of Governments (SCAG) 2016-2040 Regional Transportation Plan/Sustainable Communities Strategy (2016 RTP/SCS or Plan) Demographics & Growth Forecast, the population of the City of Los Angeles in 2012 was 3,845,500 with 1,325,500 households. Based on this data, the City's average household size is approximately three (3) persons per dwelling unit, and therefore, the project's 43 new apartment units would provide housing for an estimated 129 persons. The proposed removal of the four (4) existing multi-family residential units from the site would result in a net increase of 39 dwelling units and approximately 117 additional persons residing within the site, which would represent an increase of less than 0.003 percent in the City's housing and population totals for the year 2012.

SCAG projects the City's future population and housing supply for the year 2040 in the 2016 RTP/SCS to increase by 763,900 and 364,800, respectively, over the 2012 estimates. As such, the project's net increase of 117 persons and 39 residential units on the site would represent less than 0.02 percent increase of the projected increases of population and .01 percent of the projected City increases of housing over that time period. The project's net increases of a small fraction of one percent of the projected growth in housing and population for the City would have a less than cumulatively considerable contribution to projected growth in the City and any associated population related impacts such as increases in demand for municipal services that would arise from other foreseeable development. In addition, the project site is located within an urbanized area and is already developed with existing residential uses, and would not have any significant impacts, as evaluated in this Categorical Exemption analysis. Therefore, the proposed development of an 43-unit apartment building and removal of 4 multifamily residences on the project site would not be expected to result in a cumulatively considerable contribution to impacts involving other past, present, or future projects in the area.

(b) Significant Effect. A categorical exemption shall not be used for an activity where there is a reasonable possibility that the activity will have a significant effect on the environment due to unusual circumstances.

The construction and operation of a proposed five-story apartment building with 43 units surrounded by existing residential uses would not have a significant effect on the environment due to unusual circumstances. As discussed in Section II, the project would not have a significant effect on the environment, and there are no unusual site conditions or issues that would warrant further environmental analysis.

(c) Scenic Highways. A categorical exemption shall not be used for a project which may result in damage to scenic resources, including but not limited to, trees, historic buildings, rock outcroppings, or similar resources, within a highway officially designated as a state scenic highway.

According to the Mobility Plan 2035, an Element of the City's General Plan, the project is located approximately 745 feet from a Boulevard II, Venice Boulevard (Blvd). However, the project site is not visible from Venice Blvd and would not result in damage to scenic resources as the site is located in an urbanized area and is infill development. Therefore, the project would not impact resources located within an officially designated state scenic highway.

(d) Hazardous Waste Sites. A categorical exemption shall not be used for a project located on a site which is included on any list compiled pursuant to Section 65962.5 of the Government Code.

The project is not located within a site which is included in any list compiled pursuant to Section 65962.5 of the Government Code, commonly referred to as the Cortese List. The site is not listed on the California Department of Toxic Substances Control maintained EnviroStor online data management system for tracking cleanup, permitting, enforcement, and investigation efforts at hazardous waste facilities and sites with known or suspected contamination issues and is not listed on the State Water Resources Control Board GeoTracker online data management system for tracking sites that require cleanup, such as Leaking Underground Storage Tanks (LUSTs). The South Coast Air Quality Management District (SCAQMD) Rule 1403 regulates the removal and disposal of asbestos containing materials, and the Occupational Safety and Health Administration (OSHA) requirements provides safety requirements regarding removal of lead-based paint. Therefore, the project is not identified as a hazardous waste site and would not be in conflict with this exception for a Class 32 In-Fill Development Categorical Exemption.

(e) Historical Resources. A categorical exemption shall not be used for a project which may cause a substantial adverse change in the significance of a historical resource.

The project site was not identified on Historic Places LA, the Los Angeles Historic Resources Inventory, or in the City's Zone Information and Map Access System (ZIMAS) as a Los Angeles Historical Cultural Monument, Los Angeles Historic Preservation Overlay Zone, National Register of Historic Places, Potential Historic Multi-Family Resident, Existing or Potential Residential Historic District or National Historic Landmark. Based on Historic Places LA, the ZIMAS database and site plans, the project would not cause a substantial adverse change in the significance of a historical resource.

IV. CONCLUSION

Based on the above information and above documentation, this analysis shows that development of the proposed 3676-3704 S Kelton Avenue Project would be consistent with the criteria for a Class 32 Categorical Exemption under CEQA Statute Section 15332.

K E L T O N A V E Attachment A

PROJECT INFORMATION

PROJECT SUMMARY:	NEW 5 STORY RESIDENTIAL BUILDING WITH 43 APARTMENT UNITS (32 MARKET RATE, 11 AFFORDABLE) + 1 FLOOR OF SUBTERRANEAN PARKING PROJECT UTILIZES INCENTIVES PER LAMC 12.22A.25
PROJECT ADDRESS:	3676 & 3704 KELTON AVE., LOS ANGELES, CA 90034
LOT SIZE:	5,053.9 SF, 5,286.4 SF, 4,994.3 SF
LOT AREA:	15,334.6 SF
ASSESSOR'S PARCEL #:	4252024020, 4252025042
TRACT:	TR 5848
MAP REFERENCE:	M B 67-57 (SHT 2), M B 61-59 (SHT 1)
BLOCK:	NONE
LOT:	77, VAC 20090809493, 19
CENSUS TRACT:	2718.04,
ZONING:	RD2-1, R3-1
HILLSIDE AREA:	NO
SPECIFIC PLAN AREA:	WEST LOS ANGELES TRANSPORTATION IMPROVEMENT & MITIGATION
METHANE HAZARD SITE:	NONE
SPECIAL GRADING AREA:	YES
USE:	RESIDENTIAL
HEIGHT LIMIT:	45'-0"
BUILDING FOOTPRINT:	
BUILDING SQUARE FOOTAGE:	42,490 SF
BUILDING HEIGHT:	62'-0"
TYPE OF CONSTRUCTION:	
OCCUPANCY GROUP:	R2 & S-2
NUMBER OF STORY:	5 + 1 FLOOR SUBTERRANEAN
PARKING REQUIREMENT:	46 PARKING SPACES
PARKING PROVIDED:	26 PARKING SPACES
SPRINKLER:	NFPA-13

F.A.R. PROVIDED VS. REQUIRED

PROVIDED		ALLOWED	
RESIDENTIAL FLOOR AREA:	43,349 SF	TOTAL LOT AREA:	15,334.6 SF
(F.A.R.) x 10,530 SF = 43,349 SF		BUILDABLE AREA:	10,530 SF
F.A.R. PROVIDED:	4.12:1	F.A.R. ALLOWED:	3:1
		(3) 10,530 SF =	31,590 SF
TOTAL PROVIDED:	**43,349 SF**	**TOTAL ALLOWED:**	**31,590 SF**

RESIDENTIAL DENSITY PROVIDED VS. REQUIRED

PROVIDED		ALLOWED	
DWELLING UNITS:	43 DWELLING UNITS	RD2 LOT AREA:	5,053.9 + 2,643.2 - 225 = 7,472 SF
MARKET RATE:	32	7,472 / 2,000 =	3.7
VERY LOW INCOME:	11	R3 LOT AREA:	4,994.3 + 2,643.2 - 225 = 7,412 SF
		7,412 / 800 =	9.3
		3.7 + 9.3 =	14
TOTAL PROVIDED:	**43 DWELLING UNITS**	**TOTAL ALLOWED:**	**14 DWELLING UNITS**

PARKING PROVIDED VS. REQUIRED

PROVIDED		REQUIRED	
TENANT PARKING:	26 SPACES	TENANT PARKING:	
		16 @ < 3 HABITABLE ROOMS (1 SPACE / UNIT) (16 UNITS) (1 SPACE) =	16 SPACES
		10 @ 3 HABITABLE ROOMS (1.5 SPACES / UNIT) (10 UNITS) (1.5 SPACES) =	15 SPACES
		17 @ > 3 HABITABLE ROOMS (2 SPACES) (17 UNITS) (2 SPACES) =	34 SPACES
		TOTAL =	**65 SPACES**
		BICYCLE ORD. 30% PARKING REDUCTION (19 SPACES REPLACED BY BICYCLE PARKING)	
TOTAL PROVIDED:	**26 SPACES**	**TOTAL:**	**46 SPACES**

BICYCLE PARKING PROVIDED VS. REQUIRED

PROVIDED		REQUIRED	
LONG TERM BICYCLE PARKING:		LONG TERM BICYCLE PARKING:	
RESIDENTIAL:	64 SPACES	RESIDENTIAL: 1 / 1.0 UNITS 25 / 1.0 =	25 SPACES
SHORT TERM BICYCLE PARKING:		1 / 1.5 UNITS 18 / 1.5 =	12 SPACES
RESIDENTIAL:	12 SPACES		**37 SPACES**
		SHORT TERM BICYCLE PARKING:	
		RESIDENTIAL: 1 / 10 UNITS 25 / 10 =	2.5 → 3 SPACES
		1 / 15 UNITS 18 / 15 =	1.2 → 2 SPACES
			5 SPACES
TOTAL LONG TERM ROVIDED:	64 SPACES	TOTAL LONG TERM REQUIRED:	37 SPACES
TOTAL SHORT TERM PROVIDED:	12 SPACES	TOTAL SHORT TERM REQUIRED:	5 SPACES

OPEN SPACE PROVIDED VS. REQUIRED

PROVIDED		REQUIRED	
COMMON OPEN SPACE:		26 UNITS @ < 3 HABITABLE ROOMS (100 SF) (26 UNITS) (100 SF) =	2,600 SF
COMMON ROOF DECK:	2,279 SF	17 UNITS @ 3 HABITABLE ROOMS (125 SF) (17 UNITS) (125 SF) =	2,125 SF
PRIVATE OPEN SPACE TOWARDS CALCULATION BASED ON ZONING CODE 12.21 G:		0 UNITS @ > 3 HABITABLE ROOMS (175 SF) (0 UNITS) (175 SF) =	0 SF
16 UNITS x 50 SF	800 SF		
TOTAL OPEN SPACE PROVIDED:	3,079 SF	TOTAL OPEN SPACE REQUIRED:	4,725 SF

ACTIONS REQUESTED

THE REQUEST CONFORMS TO THE FOLLOWING REQUIREMENTS AS FOLLOWS:

1. CODE SECTION FROM WHICH RELIEF IS REQUESTED: CODE SECTION WHICH AUTHORIZES RELIEF: 12.24 U 26 A CONDITIONAL USE PERMIT PURSUANT TO LAMC 12.24 U 26 TO PERMIT A DENSITY BONUS FOR A PROJECT FOR WHICH THE DENSITY INCREASE IS GREATER THAN THE MAXIMUM 35% PERMITTED IN LAMC SECTION 12.22 A 25; IN CONJUNCTION WITH THE CONSTRUCTION, USE, AND MAINTENANCE OF 43 FOR-RENT DWELLING UNITS IN LIEU OF THE 14 DWELLING UNITS OTHERWISE PERMITTED BY LAMC 12.22 A 25; WITH 11 DWELLING UNITS RESERVED FOR VERY LOW INCOME HOUSEHOLDS; AND PURSUANT TO LAMC SECTION 12.24 F.

INCENTIVES AND WAIVERS

FROM MENU OF INCENTIVES (12.22 A 25):
- PERMIT A 10% DECREASE IN REQUIRED NORTHERLY SIDE YARD SETBACK TO ALLOW A 7-FOOT 2-INCH SIDE YARD SETBACK IN LIEU OF THE 8 FEET REQUIRED BY THE RD2 AND R3 ZONES PURSUANT TO LAMC 12.09.1. B.2.(A) AND LAMC 12.10. C 3, RESPECTIVELY.

- PERMIT A 10% DECREASE IN REQUIRED SOUTHERLY SIDE YARD SETBACK TO ALLOW A 7-FOOT 2-INCH SIDE YARD SETBACK IN LIEU OF THE 8 FEET REQUIRED BY THE RD2 AND R3 ZONES PURSUANT TO LAMC 12.09.1. B.2.(A) AND LAMC 12.10. C. 3, RESPECTIVELY.

- PERMIT THE AVERAGING OF ALLOWABLE DENSITY OVER THE PROJECT SITE LOCATED ON TWO CONTIGUOUS PARCELS ZONED RD2-1 AND R3-1.

OFF-MENU WAIVERS OF DEVELOPMENT STANDARDS:
- PERMIT A 17-FOOT INCREASE IN HEIGHT TO 62 FEET WITH NO 1-1 STEPBACK AFTER 45 FEET OF HEIGHT IN LIEU OF THE MAXIMUM 45 FEET ALLOWED IN THE RD2-1 AND R3-1 ZONES PURSUANT TO LAMC 12.21.1.

- PERMIT A 37% INCREASE IN ALLOWED FLOOR AREA RATIO TO ALLOW 43,349 SQUARE FEET OF FLOOR AREA IN LIEU OF THE 31,590 SQUARE FEET PERMITTED PURSUANT TO LAMC 12.21.1 A 1.

- PERMIT THE PROVISION OF 26 PARKING SPACES IN LIEU OF THE 46 PARKING SPACES REQUIRED PURSUANT TO LAMC 12.21 A. 4.

- PERMIT THE PROVISION OF 10 COMPACT AUTOMOBILE PARKING SPACES AND 16 STANDARD AUTOMOBILE PARKING SPACES IN LIEU OF ONE STANDARD SPACE PER DWELLING UNIT REQUIRED PURSUANT TO 12.21 A.5.C.

- PERMIT A 35% DECREASE IN REQUIRED USABLE OPEN SPACE TO ALLOW THE PROVISION OF 3,079 SQUARE FEET OF USABLE OPEN SPACE IN LIEU OF THE 4,725 SQUARE FEET REQUIRED BY LAMC 12.21. G.2.

RESIDENTIAL UNITS

UNIT #	OCCUPANCY	S.F.	TYPE
101	1 BD / 2 BTH	987 SF	MARKET RATE
103	/ 1 BTH	475 SF	AFFORDABLE
104	/ 1 BTH	475 SF	MARKET RATE
105	1 BD / 1 BTH	841 SF	AFFORDABLE
106	/ 1 BTH	475 SF	MARKET RATE
107	1 BD / 2 BTH	987 SF	MARKET RATE
201	2 BD / 2 BTH	842 SF	AFFORDABLE
202	/ 1 BTH	525 SF	MARKET RATE
203	2 BD / 2 BTH	988 SF	MARKET RATE
204	1 BD / 1 BTH	712 SF	MARKET RATE
205	/ 1 BTH	434 SF	AFFORDABLE
206	/ 1 BTH	434 SF	MARKET RATE
207	2 BD / 2 BTH	966 SF	MARKET RATE
208	2 BD / 2 BTH	955 SF	MARKET RATE
209	/ 1 BTH	490 SF	AFFORDABLE
210	1 BD / 1 BTH	849 SF	MARKET RATE
301	2 BD / 2 BTH	842 SF	MARKET RATE
302	/ 1 BTH	525 SF	MARKET RATE
303	2 BD / 2 BTH	988 SF	MARKET RATE
304	1 BD / 1 BTH	712 SF	AFFORDABLE
305	/ 1 BTH	434 SF	MARKET RATE
306	/ 1 BTH	434 SF	AFFORDABLE
307	2 BD / 2 BTH	966 SF	MARKET RATE
308	2 BD / 2 BTH	955 SF	AFFORDABLE
309	/ 1 BTH	490 SF	MARKET RATE
310	1 BD / 1 BTH	849 SF	MARKET RATE
401	2 BD / 2 BTH	842 SF	AFFORDABLE
402	/ 1 BTH	525 SF	MARKET RATE
403	2 BD / 2 BTH	988 SF	MARKET RATE
404	1 BD / 1 BTH	712 SF	MARKET RATE
405	/ 1 BTH	434 SF	AFFORDABLE
406	/ 1 BTH	434 SF	MARKET RATE
407	2 BD / 2 BTH	966 SF	MARKET RATE
408	2 BD / 2 BTH	955 SF	MARKET RATE
409	/ 1 BTH	490 SF	MARKET RATE
410	1 BD / 1 BTH	849 SF	MARKET RATE
501	2 BD / 2 BTH	842 SF	MARKET RATE
502	/ 1 BTH	525 SF	MARKET RATE
503	2 BD / 2 BTH	988 SF	MARKET RATE
504	2 BD / 2 BTH	1,180 SF	AFFORDABLE
507	2 BD / 2 BTH	1,114 SF	MARKET RATE
508	2 BD / 2 BTH	1,168 SF	MARKET RATE
510	1 BD / 1 BTH	849 SF	MARKET RATE

VICINITY MAP

CODE

BUILDING CODE:	2020 LABC, (TITLE 24, PART 2.5)
	BASED ON THE 2018 IRC (INCLUDES ACCESSIBILITY)
STRUCTURAL:	2020 LABC, VOL 2 (TITLE 24, PART 2, VOL 2)
	BASED ON THE 2018 IBC WITH ASCE 7-16
MECHANICAL CODE:	2019 CA MECHANICAL CODE, (TITLE 24, PART 4)
	BASED ON THE 2018 UNIFORM MECHANICAL CODE
PLUMBING CODE:	2019 CA PLUMBING CODE (TITLE 24, PART 5)
	BASED ON THE 2018 UNIFORM PLUMBING CODE
ELECTRICAL CODE:	2019 CA ELECTRICAL CODE (TITLE 24, PART 3)
	BASED ON THE 2017 NATIONAL ELECTRIC CODE
ENERGY CODE:	2019 CA ENERGY CODE (TITLE 24, PART 6)
	2020 CITY OF LOS ANGELES GREEN BUILDING CODE

PROJECT DIRECTORY

OWNER
NAME: LG ALLIANCE CORP.
ADDRESS: 9440 SANTA MONICA BLVD., SUITE 301 BEVERLY HILLS, CA 90210

STRUCTURAL ENGINEER
NAME:
ADDRESS:
PHONE NO.:

TITLE 24
NAME:
ADDRESS:
PHONE NO.:

ARCHITECT
NAME: BREAKFORM DESIGN
ADDRESS: 127 ARENA STREET EL SEGUNDO, CA 90245
PHONE NO.: 310-322-3700

LAND SURVEYOR
NAME:
ADDRESS:
PHONE NO.:

TRACT MAP
NAME:
ADDRESS:
PHONE NO.:

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A0.01	GENERAL NOTES
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A0.03	GREEN FORMS
A0.04	SPECS / RESEARCH REPORTS
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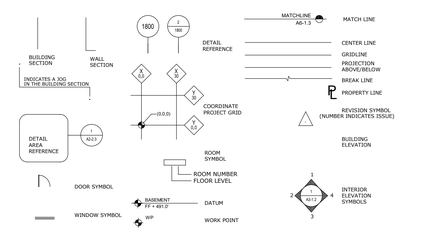
ABREVIATIONS & SYMBOLS

&	And	BLKG	Blocking	D	Deep, Depth	EXP	Expansion	GALV	Galvinized	JT	Joint	N	North	P.T.D.	Paper Towel Dispenser	SIM	Similar	T.P.D.	Toilet Paper Dispenser
<	Angle	BM	Beam	DBL	Double	EXPO	Exposed	G.B.	Grab Bar			N.I.C.	Not in Contract	PTN	Partition	SLDG	Siding	T.S.	Top of Steel
@	At	BOT	Bottom	DET	Detail	EXT	Exterior	G.I.	Galvanized Iron	KIT	Kitchen	NO	Number			SPEC	Specification	TV	Television
℄	Centerline	BR	Bedroom	D.F.	Drinking Fountain			GL	Glass, Glazing			NOM	Nominal	Q.T.	Quarry Tile	SQ	Square	T.O.W.	Top of Wall
ø	Diameter or Round	BSMT	Basement	DIA	Diameter	F.A.	Fire Alarm	GND	Ground	LAM	Laminate	N.S.	No Scale			S.S.	Stainless Steel	TYP	Typical
⊥	Perpendicular	B.U.R.	Built Up Roofing	DIM	Dimension	F.D.	Floor Drain	GR	Grade	LAV	Lavatory	N.T.S.	Not to Scale	R	Riser	SSK	Service Sink		
#	Number			DISP	Dispenser	FDN	Foundation	GYP	Gypsum	L.F.	Lineal Foot			RAD	Radius	STD	Standard	UNF	Unfinished
(E)	Existing	CAB	Cabinet	DN	Down	F.E.	Fire Extinguisher			L.H.	Left Hand	O/	Over	R.D.	Roof Drain	STL	Steel	U.O.N.	Unless Otherwise Noted
		CARP	Carpet	D.O.	Door Opening	F.E.C.	Fire Extinguisher	H	High	LKR	Locker	OA	Overall	REF	Reference	STOR	Storage	UR	Urinal
AB	Anchor Bolt	C.B.	Catch Basin	DR	Door	Cab.		H.B.	Hose Bib	L.R.	Living Room	OBSC	Obscure	REFR	Refrigerator	STRUCT	Structrual		
A/C	Air Conditioning	CEM	Cement	DS	Downspout	F.G.	Finish Grade	H.C.	Hollow Core	LT	Light	O.C.	On Center	REINF	Reinforced or Reinforcing	SUSP	Suspended	VERT	Vertical
A.C.	Asphaltic Concrete	CER	Ceramic	D.S.P.	Dry Standpipe	F.H.C.	Fire Hose Cabinet	HCP	Handicapped	LVR	Louver	O.D.	Outside Diameter	REQ	Required	SW	Switch	VEST	Vestibule
ACT	Acoustical Tile	C.I.	Cast Iron	DWG	Drawing	FIN	Finish	HDWR	Hardware			O.F.D.	Overflow Drain	RESIL	Resilient	SYM	Symmetrical	V.I.F.	Verify in Field
ACOUS	Acoustical	CLG	Ceiling	DWR	Drawer	FLASH	Flashing	HDWD	Hardwood	MATL	Material	OFF	Office	REV	Revised	SYS	System	VOL	Volume
ADJ	Adjustable	CLO	Closet			FLR	Floor	H.M.	Hollow Metal	MAX	Maximum	O.H.	Overhang	RFG	Roofing				
A.F.F.	Above Finish Floor	CLR	Clear	E	East	FLUOR	Fluorescent	HORIZ	Horizontal	M.B.	Machine Ball	OVHD	Overhead	R.H.	Right Hand	T	Tread	W	West
ALT	Alter or Alternate	CMU	Concrete Masonry Unit	EA	Each	F.O.C.	Face of Concrete	HR	Hour	MECH	Mechanical	OPNG	Opening	RM	Room	T.B.	Towel Bar	W/	With
ALUM	Aluminum	CNTR	Counter	EJ	Expansion Joint	F.O.F.	Face of Finish	HT	Height	MEMB	Membrane	OPP	Opposite	R.O.	Rough Opening	T&G	Tongue and Groove	W.H.	Water Heater
ANOD	Anodized	COL	Column	EL	Elevation	F.O.M.	Face of Masonry	HVAC	Heating, Ventilation and Air Conditioning	MET	Metal			RWD	Redwood	T.O.C.	Top of Curb	W/O	Without
A.P.	Access Panel	CONC	Concrete	ELEC	Electrical	F.O.S.	Face of Stud			MFR	Manufacture	PC	Piece			T.O.D.	Top of Drain	W.C.	Water Closet
APPROX	Approximate	CONN	Connection	ELEV	Elevation	FPRF	Fireproof	H.W.	Hot Water	MH	Manhole	P.D.	Planter Drain	S	South	TEL	Telephone	WD	Wood
ARCH	Architectural	CONST	Construction	EMER	Emergency	FR	Frame			MIN	Minimum	PL	Plate	S.C.	Solid Core	TEMP	Tempered, Temperature	WP	Waterproof
ASPH	Asphalt	CONT	Continuous	ENCL	Enclosure	FS	Full Size	I.D.	Inside Diameter	MIR	Mirror	P.L.	Property Line	SCHED	Schedule	TER	Terrazzo	WPM	Waterproof Membrane
		CONTR	Contractor	E.O.S.	Edge of Slab	FT	Foot, Feet	INCL	Including	MISC	Miscellaneous	PLMG	Plumbing	SECT	Section	THK	Thick, Thickness	WSCT	Wainscot
BD	Board	CORR	Corridor	EQ	Equal	FTG	Footing	INSUL	Insulation	M.O.	Masonry Opening	PLAM	Plastic Laminate	SEP	Separation, Separate	THR	Threshold	W.S.P.	Wet Standpipe
BITUM	Bituminous	C.T.	Ceramic Tile	EQUIP	Equipment	FURR	Furring, Furred	INT	Interior	M.R.	Moisture Resistant	PLAS	Plaster	SH	Shelf	TOIL	Toilet	WT	Weight
BLDG	Building	CTR	Center	E.W.	Each Way	FUT	Future			MTD	Mounted	PLYWD	Plywood	SHR	Shower	T.O.P.	Top of Pavement		
BLK	Block	CTSK	Countersunk	E.W.C.	Electric Water Cooler			JAN	Janitor	MUL	Mullion	PR	Pair	SHT	Sheet	T.O.S.	Top of Slab		
		C.W.	Cold Water	EXIST	Existing	GA	Gauge	JST	Joist			PT	Paint			T.P.D.	Toilet Paper Dispenser		



breakformdesign
127 arena street, el segundo, ca 90245
[o] 310.322.3700

LICENSED ARCHITECT
RAMSEY DAHAM
No. C-34257
10/31/23
RENEWAL DATE
STATE OF CALIFORNIA

3676 & 3704 KELTON AVE.
LOS ANGELES, CA 90034

Revision Schedule

Revision Number	Revision Date

COVER

DRAWN	CR
CHECKED	PNK
DATE	11/9/2022 2:55:27 PM
SCALE	
JOB #	21-A001

A0.00

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LOS ANGELES, CA 90012

FRANK M. BUSH
GENERAL MANAGER
SUPERINTENDENT OF BUILDING

OSAMA YOUNAN, P.E.
EXECUTIVE OFFICER

SOPREMA, Inc.
310 Quadral Drive
Wadsworth, Ohio 44281

Attention: Glen N. Bestor
(330) 331-3070

Local Representative: Freddy Riofrio
(203) 262-9245

RESEARCH REPORT: RR 25908
(CSI # 07560)

Expires: May, 1, 2020
Issued Date: April 1, 2018
Code: 2017 LABC

GENERAL APPROVAL – Renewal and Clerical Modification -Soprema ALSAN RS 230 Field and RS 260 LO Field Liquid Applied Roof Covering System.

DETAILS

ALSAN RS 230 Field liquid applied roof covering system:

Deck: Concrete deck with maximum1-inch per foot slope.
Primer: ALSAN RS 276 primer applied at the rate of 1.0 gallon per 100 ft².
Base Coat: ALSAN RS 230 Field applied at the rate of 3.91 gallon per 100 ft².
Reinforcement: While the base coat of ALSAN RS 230 Field is wet, a layer of non-woven, needle punched polyester reinforcement fabric is applied and rolled so that the air bubbles are removed.
Top Coat: ALSAN RS 230 Field applied at the rate of 1.95 gallon per 100 ft².

ALSAN RS 260 LO Field liquid applied roof covering system:

Deck: Concrete deck with maximum 1-1/2-inch per foot slope.
Primer: ALSAN RS 276 primer applied at the rate of 1.0 gallon per 100 ft².
Base Coat: ALSAN RS 260 LO Field applied at the rate of 3.91 gallon per 100 ft².
Reinforcement: While the base coat of ALSAN RS 260 LO Field is wet, a layer of non-woven, needle punched polyester reinforcement fabric is applied and rolled so that the air bubbles are removed.
Top Coat: ALSAN RS 260 LO Field applied at the rate of 1.95 gallon per 100 ft².

RR 25908
Page 1of 2

LADBS G-5 (Rev 08/14) AN EQUAL EMPLOYMENT OPPORTUNITY - AFFIRMATIVE ACTION EMPLOYER

Soprema, Inc.
RE: Soprema Alsan RS 230 Field and RS 260 LO Field Liquid Applied Roof Covering System.

The roof covering systems described above are approved as Class A roof coverings subject to the following conditions:

1. The roofing materials shall be delivered to the job site in sealed containers identified by the manufacturer's name and product designation.
2. Application of the components shall be on a concrete deck in accordance with the manufacturer's instructions consistent with the description and requirements herein. (A copy shall be available at the job site).

DISCUSSION

The clerical modification is to update the report to the 2017 Los Angeles City Building Code and to update the contact person and phone number.

The report is in compliance with the 2017 Los Angeles City Building Code.

The approval was based on tests in accordance with ICC Evaluation Services Acceptance Criteria for Membrane Roof Covering Systems (AC 75)

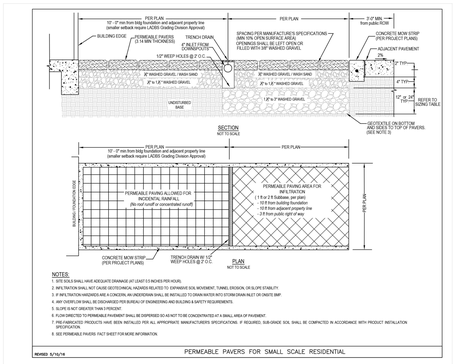
Addressee to whom this Research Report is issued is responsible for providing copies of it, complete with any attachments indicated, to architects, engineers and builders using items approved herein in design or construction which must be approved by Department of Building and Safety Engineers and Inspectors.

This general approval of an equivalent alternate to the Code is only valid where an engineer and/or inspector of this Department has determined that all conditions of this approval have been met in the project in which it is to be used.

QUAN NGHIEM, Chief
Engineering Research Section
201 N. Figueroa St., Room 880
Los Angeles, CA 90012
Phone- 213-202-9812
Fax- 213-202-9943

DR:
25908
TLR/HMRTS
R05/26/18
1505/1507.15

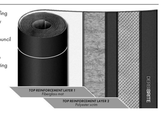
RR 25908
Page 2 of 2


PER PLAN
10' - 0" min from bldg foundation and adjacent property line (smaller setback require LADBS Grading Division Approval)
BUILDING EDGE
PERMEABLE PAVERS (2 1/4 MIN THICKNESS)
TRENCH DRAIN 6" WIDE FLUSH W/ CONCRETE
SPACING PER MANUFACTURER'S SPECIFICATIONS (MIN 10% OPEN SURFACE AREA). OPENINGS SHALL BE LEFT OPEN OR FILLED WITH 3/8" WASHED GRAVEL
CONCRETE MOW STRIP (PER PROJECT PLANS)
2'-0" MIN. from patio ROW
ADJACENT PAVEMENT
2%
12" DEEP HOLES @ 9' O.C.
4" THK
8" WASHED GRAVEL / WASH SAND
6" x 1.5" WASHED GRAVEL
12" or 24" RISING TABLE
REFER TO PLANTING TABLE
UNDISTURBED SOIL
GEOTEXTILE ON BOTTOM AND SIDES TO TOP OF PAVERS. (SEE NOTE 5)
SECTION
NOT TO SCALE
PERMEABLE PAVING ALLOWED FOR INCIDENTAL RAINFALL (Non-roof runoff or stormwater runoff)
PERMEABLE PAVING AREA FOR STORMWATER
1 ft or 2 ft bollards, per plan)
- 10 ft from building foundation
- 10 ft from adjacent property line
- 3 ft from public right of way
BUILDING FOUNDATION WALL
PER PLAN
CONCRETE MOW STRIP (PER PROJECT PLANS)
TRENCH DRAIN W/ 12" WIDE HOLES @ 9' O.C.
PLAN
NOT TO SCALE
NOTES:
1. SITE SOILS SHALL HAVE ADEQUATE DRAINAGE (AT LEAST 0.5 INCHES PER HOUR).
2. INFILTRATION SHALL NOT CAUSE GEOTECHNICAL HAZARDS RELATED TO: EXPANSIVE SOIL MOVEMENT, TUNNEL EROSION, OR SLOPE STABILITY.
3. IF INFILTRATION HAZARDOUS ARE A CONCERN, AN IMPERVIOUS SHALL BE INSTALLED TO DRAIN WATER INTO STORM DRAIN INLET OR ONSITE BMP.
4. ANY OVERFLOW SHALL BE DISCHARGED PER BUREAU OF ENGINEERING AND BUILDING & SAFETY REQUIREMENTS.
5. SLOPE IS NOT GREATER THAN 5 PERCENT.
6. FLOW DIRECTED TO PERMEABLE PAVEMENT SHALL BE DISPERSED SO AS NOT TO BE CONCENTRATED AT A SMALL AREA OF PAVEMENT.
7. PRE-FABRICATED PRODUCTS HAVE BEEN INSTALLED PER ALL APPROPRIATE MANUFACTURERS SPECIFICATIONS. IF REQUIRED, SUB-GRADE SOIL SHALL BE COMPACTED IN ACCORDANCE WITH PRODUCT INSTALLATION SPECIFICATION.
8. SEE PERMEABLE PAVERS FACT SHEET FOR MORE INFORMATION.
REVISED 5/15/18
PERMEABLE PAVERS FOR SMALL SCALE RESIDENTIAL

Technical Data Sheet

DERBIBRITE®
Modified Bitumen Waterproofing Membrane with a Unique Reflective Surfacing

ADVANTAGES
- Premium 3.5 mm [140 mil] Class A Fire Resistant Modified Bitumen Roofing Membrane that combines Derbigum's proven performance with a highly reflective white acrylic surface.
- Rigorously tested, Derbibrite exceeds Energy Star, Cool Roof Rating Council approved and contributes to LEED certification
- Manufactured with a fiberglass/polyester reinforcement that allows the acrylic to saturate and form an extremely tight molecular structure creating an impenetrable surface resistant to light oils, animals fats and fungus.
- May be used as both a roofing and flashing membrane
- Derbibrite is applied with PERMASTIC Cold Adhesive



TECHNICAL CHARACTERISTICS

CHARACTERISTIC	TESTED	ASTM Requirements Turf Methods D5147 / D6223-01 MACHINE DIRECTION	CROSS DIRECTION	DERBIBRITE Results* MACHINE DIRECTION	CROSS DIRECTION	UNITS
Tensile Strength	0°F	150	150	131	131	lbf/in
	73°F	65	65	99	102	lbf/in
Elongation	0°F	3.0	3.0	6.0	6.1	%
	73°F	3.0	3.0	11.1	8.5	%
Tear Resistance	73°F	120	120	156	156	lbf
Dimensional Stability		1	1	0.05	0.05	%
CHARACTERISTIC	ASTM	INITIAL	AGED	INITIAL	AGED	UNITS
Cold Flex	D6223	0	0	-21	-20	°C
CHARACTERISTIC	ASTM	MINIMUM		MINIMUM		UNITS
Compound Stability		275		300		°F
Mill		N/A		0		%
Fire Resistance	E108 & UL790	PASS/FAIL		Class A rated to 3" slope 0 (no growth of fungi)		
Fungus Resistance	G21	PASS/FAIL				

* All values shown are nominal and subject to normal manufacturing tolerances. Tested initially within 24 hours and again aged 28 days.

PRODUCT INFORMATION

CHARACTERISTIC	U.S. CUSTOMARY	METRIC
Thickness	140 mil	3.5 mm
Length	33 ft 4 in	10.2 m
Width	39 ¼ in	1.0 m
Weight	94 lb	42.6 kg
Net Coverage	100 sq ft	9.2 sq m

ENERGY AND ENVIRONMENT*

CHARACTERISTIC	ASTM	INITIAL	3-YEAR
Reflectivity	C1549	≥0.7b	≥0.68
Emissivity	E1371	≥0.81	≥0.83
Solar Reflective Index	E1980	93	81

* All values shown are nominal and subject to normal manufacturing tolerances.

PACKAGING & STORAGE

Packaged 20 rolls per pallet. Rolls should be stored on end, on raised platforms protected from the weather.

505 available on request (800) 727-9972 www.derbigum.us

DERBIGUM

4655 Blue Parkway, Kansas City, MO 64130 | PHONE: 800.727.9972 | FAX: 816.924.1542 | WEBSITE: www.derbigum.us | EMAIL: info@derbigum.us

STORMWATER OBSERVATION REPORT (SOR) FORM
Only to be used for Single Family Residences (4 units or less, <10,000 SF, <2,500 SF within a ESA)

LOW IMPACT DEVELOPMENT

IN THE EVENT THAT THE APPROVED STORMWATER BMP CANNOT BE BUILT PER PLANS (OR ANY MODIFICATION), CONSULT WITH BUREAU OF SANITATION STAFF PRIOR TO ANY PLAN MODIFICATIONS. FAILURE TO DO SO MAY DELAY OBTAINING A FINAL APPROVAL AND CERTIFICATE OF OCCUPANCY (C of O).

STORMWATER OBSERVATION *means the visual observation of the stormwater related Best Management Practices (BMPs) for conformance with the approved LID Plan at significant construction stages and at completion of the project. Stormwater observation does not include or waive the responsibility for the inspections required by Section 108 or other sections of the City of Los Angeles Building Code.*

STORMWATER OBSERVATION *must be performed by the contractor responsible for the approved LID Plan or designated staff in their employment. Homeowner can also perform the Stormwater Observation if no licensed contractor was involved* ***AS PART OF THE OBSERVATION, PROVIDE PRINTED PHOTOS OF THE BMPS TAKEN DURING VARIOUS CONSTRUCTION PHASES.***

STORMWATER OBSERVATION REPORT (SOR) *must be signed by the contractor responsible for the approved LID Plan and submitted to the City prior to the issuance to the certificate of occupancy. Homeowner can sign the Stormwater Observation Report if no licensed contractor was involved* ***PRIOR TO CERTIFICATE OF OCCUPANCY (C of O), SOR FORM, PRINTED PHOTOS OF THE BMPS TAKEN DURING VARIOUS CONSTRUCTION PHASES AND APPROVED STAMPED PLANS BY THE BUREAU OF SANITATION MUST BE SUBMITTED TO THE PUBLIC COUNTER FOR STAFF APPROVAL.***

Project Address:	**Building Permit No.:**
Contractor / Architect / Engineer responsible for construction of best management practices per approved LID Plan:	**Phone Number:**

I declare that the following statements are true to the best of my knowledge:

1. I am responsible for the approved LID Plan, and

2. I, or designated staff under my responsible charge, have performed the required site visits at each significant construction stage and at completion to verify that the best management practices as shown on the approved plan have been constructed and installed in accordance with the approved LID Plan.

Signature Date Contractor/Architect/Engineer License

Low Impact Development (LID)
Post Construction Stormwater Mitigation
Best Management Practices (BMPs)

STORMWATER BMP(s) VERIFICATION

Upon installation of the approved stormwater BMPs, a Stormwater Observation Report (SOR) Form shall be submitted to Department of Public Works, Bureau of Sanitation. 201 N. Figueroa, 3rd floor, station 18. The SOR Form must be with filed and approved by the Bureau of Sanitation prior to the issuance of a Certificate of Occupancy.

Project Address: _____

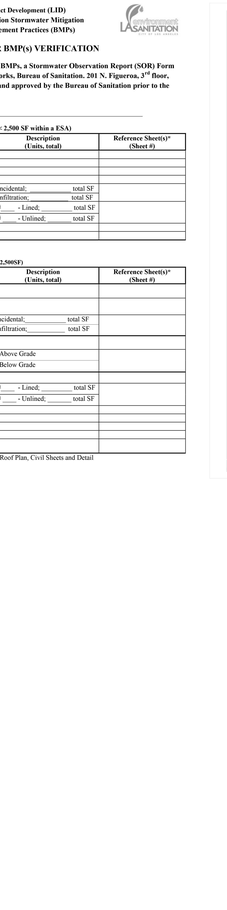
RESIDENTIAL (4 UNITS OR LESS, <10,000SF, < 2,500 SF within a ESA)

Item #	Stormwater BMP	Description (Units, total)	Reference Sheet(s)* (Sheet #)
1	Rain Tank(s) – 50 to 129 gal each		
2	Rain Tank(s) – > 130 gal min		
3	Shade Tree - min 15 gal		
4	Flow thru Planter(s)		
5	Permeable pavers / Porous concrete (min 10% open space)	☐ Incidental: _____ total SF ☐ Infiltration: _____ total SF	
6	Rain Garden	☐ # ____ - Lined: _____ total SF ☐ # ____ - Unlined: _____ total SF	
7	Dry Well		
8	SUMP Pump (modification was not required)		

ALL OTHER DEVELOPMENT
(Residential: 5-2 units, 10,000 ≥ SF, within a ESA and ≥2,500SF)

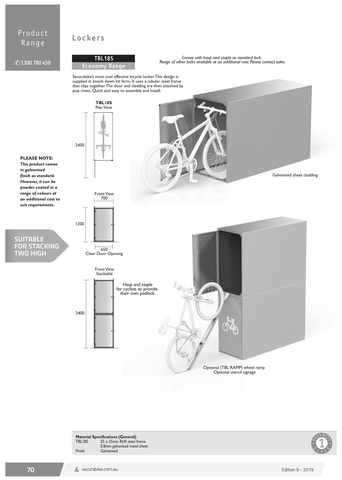
	Item #	Stormwater BMP	Description (Units, total)	Reference Sheet(s)* (Sheet #)
Infiltration	1	Infiltration Basin / Trench		
	2	Dry Well		
	3	Permeable pavers / Porous concrete (min 10% open space)	☐ Incidental: _____ total SF ☐ Infiltration: _____ total SF	
Capture & Use	4	Rain Tank(s) – 50 gal min		
	5	Cistern	☐ Above Grade ☐ Below Grade	
Treat & Discharge	6	Flow thru Planter(s)		
	7	Biofiltration	☐ # ____ - Lined: _____ total SF ☐ # ____ - Unlined: _____ total SF	
	8	Vegetative Swale / Filter Strip		
	9	Catch Basin Filter(s)		
	10	Trench Drain Filter(s)		
	11	Down Spout Filter(s)		
	12	SUMP Pump (modification was not required)		

* At a minimum: Site Plan, Architectural Elevations, Roof Plan, Civil Sheets and Detail



NOTES:
1. MT LEAST 6 INCHES SHALL BE PROVIDED BETWEEN THE PLANTING SURFACE AND THE CREST OF EACH PLANTER
2. PLANTERS SHALL NOT BE LOCATED ON UNEVEN OR SLOPED SURFACES.
3. TOP SOIL/PLANTING MIX IS AT LEAST 18" DEEP.
4. TOP SOIL CONTAINS NO MORE THAN 30% COMPOST.
5. MINIMUM GRAVEL LAYER SHALL BE 12" DEEP.
6. DIRECT OVERFLOW DISCHARGE PER BUREAU OF ENGINEERING AND BUILDING AND SAFETY REQUIREMENTS.
7. PLANTING IS REQUIRED. CONSULT LANDSCAPE ARCHITECT FOR SPECIFIC PLANT TYPES.

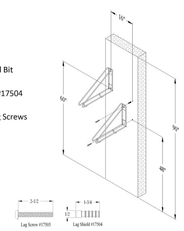
REVISED 5/15/18
STORMWATER PLANTER FOR SMALL SCALE RESIDENTIAL



2. Concrete Wall Installation:

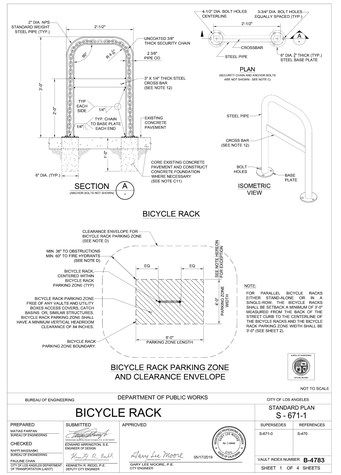
A. Tools & Fasteners

- 3/8" Impact Drill
- 1/2" x 3" Long Carbide Drill Bit
- T-30 Torx Bit
- (2) 5/16" Lag Shields Part #17504 (included)
- (2) 5/16" x 2-1/2" Long Lag Screws Part #17505 (included)
- Flat Head Screw Driver Bit
- Pencil
- Hammer
- Tape Measure
- Concrete Adhesive
- Blue Line or Laser Level



B. Installation:

a) Locate center of bottom hole placement of the first WallRack
b) Snap a line with the Blue Line or Laser and make a mark with pencil at approximately 60" from the floor to the first mounting hole position
c) Hold rack straight and center hole over mark position, the bottom hole and mark the upper hole at 12" center to center, which would be the 1st hole down from the top
d) Remove rack and drill approximately 2-1/2" deep into concrete with 1/2" x 4" Drill Bit on center
e) Squirt in some Concrete Adhesive in both holes
f) Insert the (2) Lag Shields #17504 into the holes and tap flush with the hammer
g) Align the rack with the holes and Sleeves and insert the (2) Lag Screws #17505 using the T-30 Torx Driver and tighten
h) Next offset the bottom rack hole down by 12" vertical and over 16" center-to-center horizontal, yielding approx. 48" from the rack bottom hole to the floor
i) Repeat steps "c" thru "g" completing on set of racks
j) Repeat steps "a" thru "i" as necessary to complete the wall installation using a Blue Line or Laser Level



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LICENSED ARCHITECT
RAMSEY DAHAM
No. C-34257
10/31/23
RENEWAL DATE
STATE OF CALIFORNIA

3676 & 3704 KELTON AVE.
LOS ANGELES, CA 90034

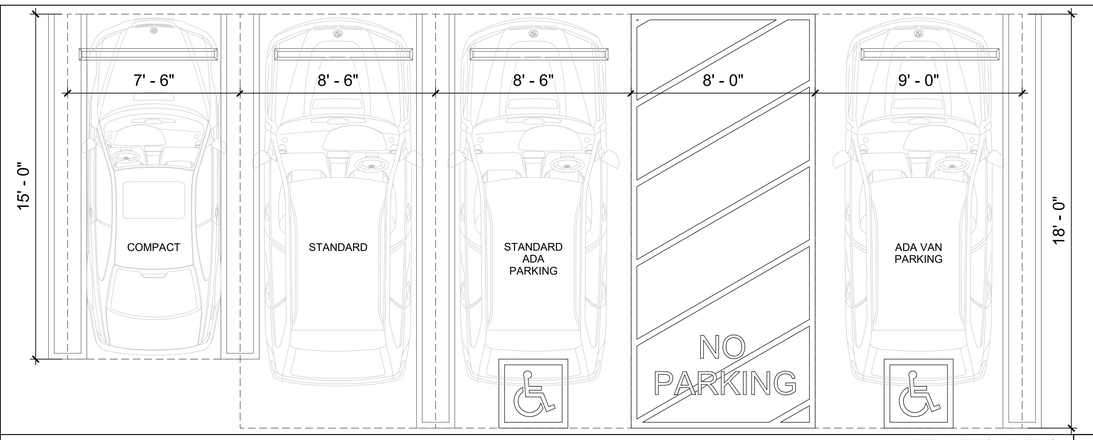
Revision Schedule

Revision Number	Revision Date

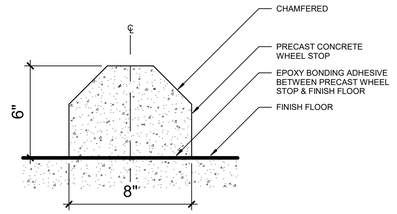
SPECS / RESEARCH REPORTS

DRAWN CR
CHECKED PNK
DATE 10/31/2022 1:16:22 PM
SCALE
JOB # 21-A001

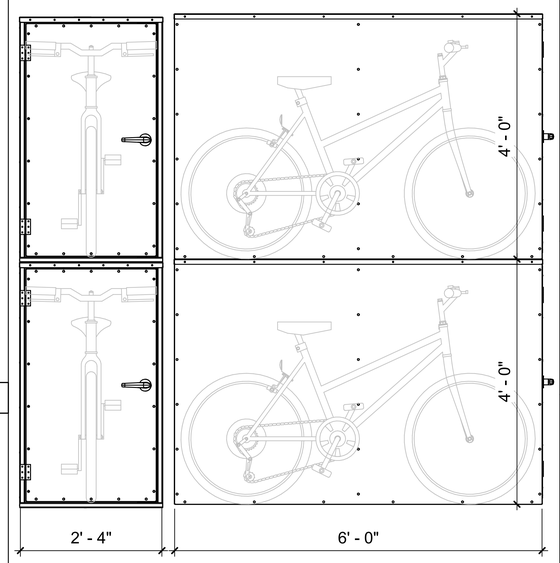
A0.06

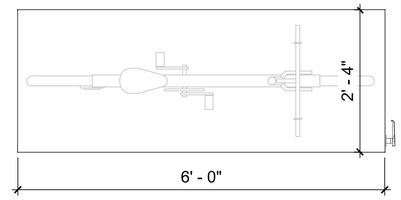


DETAIL - TYPICAL PARKING
3/8" = 1'-0" | 3

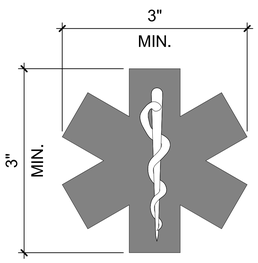


DETAIL - WHEEL STOP
3" = 1'-0" | 6





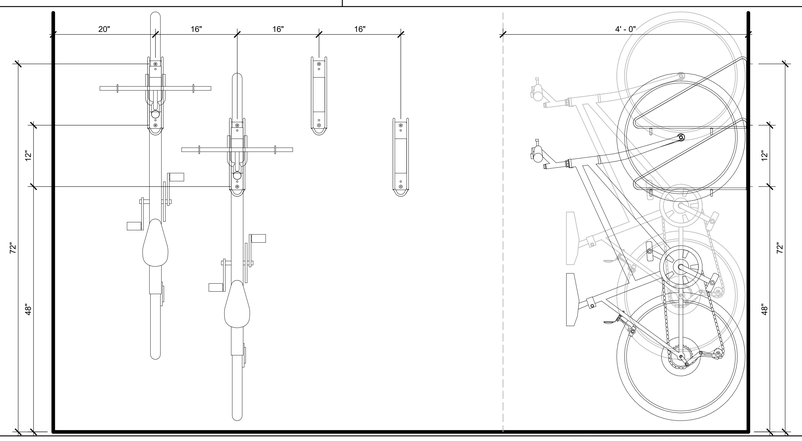
DETAIL - BICYCLE PARKING LONG TERM LOCKER
1" = 1'-0" | 2



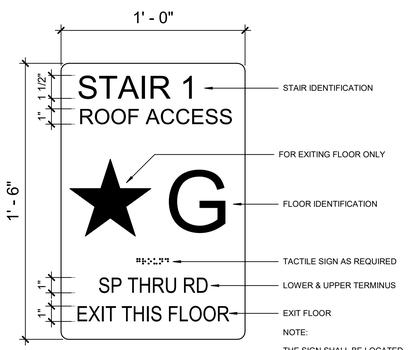
NOTE:

NTERNATIONAL SYMBOL
FOR MEDICAL SERVICES
(STAR OF LIFE). THE SYMBOL
SHALL NOT BE LESS THAN
3 INCHES HIGH & SHALL BE
PLACED INSIDE OF THE
HOISTWAY DOOR FRAME.

DETAIL - MEDICAL SERVICES SYMBOL
12" = 1'-0" | 5



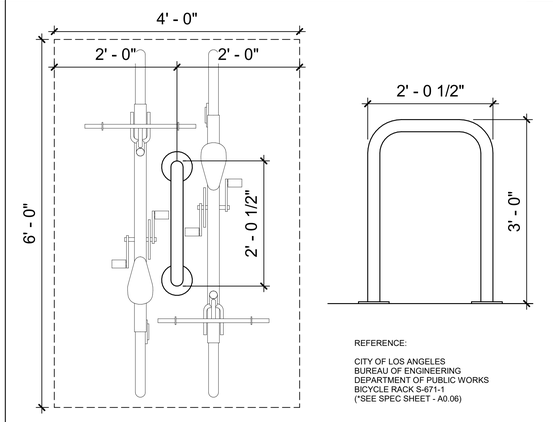
DETAIL - BICYCLE PARKING LONG TERM WALL RACK
1" = 1'-0" | 7



NOTE:

THE SIGN SHALL BE LOCATED
5 FEET ABOVE THE FLOOR
LANDING IN A POSITION THAT
IS READILY VISIBLE WHEN THE
DOOR IS IN THE OPEN AND
CLOSED POSITIONS.

DETAIL - FLOOR I.D. SIGN TYP.
3" = 1'-0" | 4



REFERENCE:

CITY OF LOS ANGELES
BUREAU OF ENGINEERING
DEPARTMENT OF PUBLIC WORKS
BICYCLE RACK S-671-1
(*SEE SPEC SHEET - A0.06)

DETAIL - BICYCLE PARKING SHORT TERM TYP.
1" = 1'-0" | 1

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No. C-34257
10/31/23
RENEWAL DATE
STATE OF CALIFORNIA

3676 & 3704 KELTON AVE.
LOS ANGELES, CA 90034

Revision Schedule

Revision Number	Revision Date

GENERAL DETAILS

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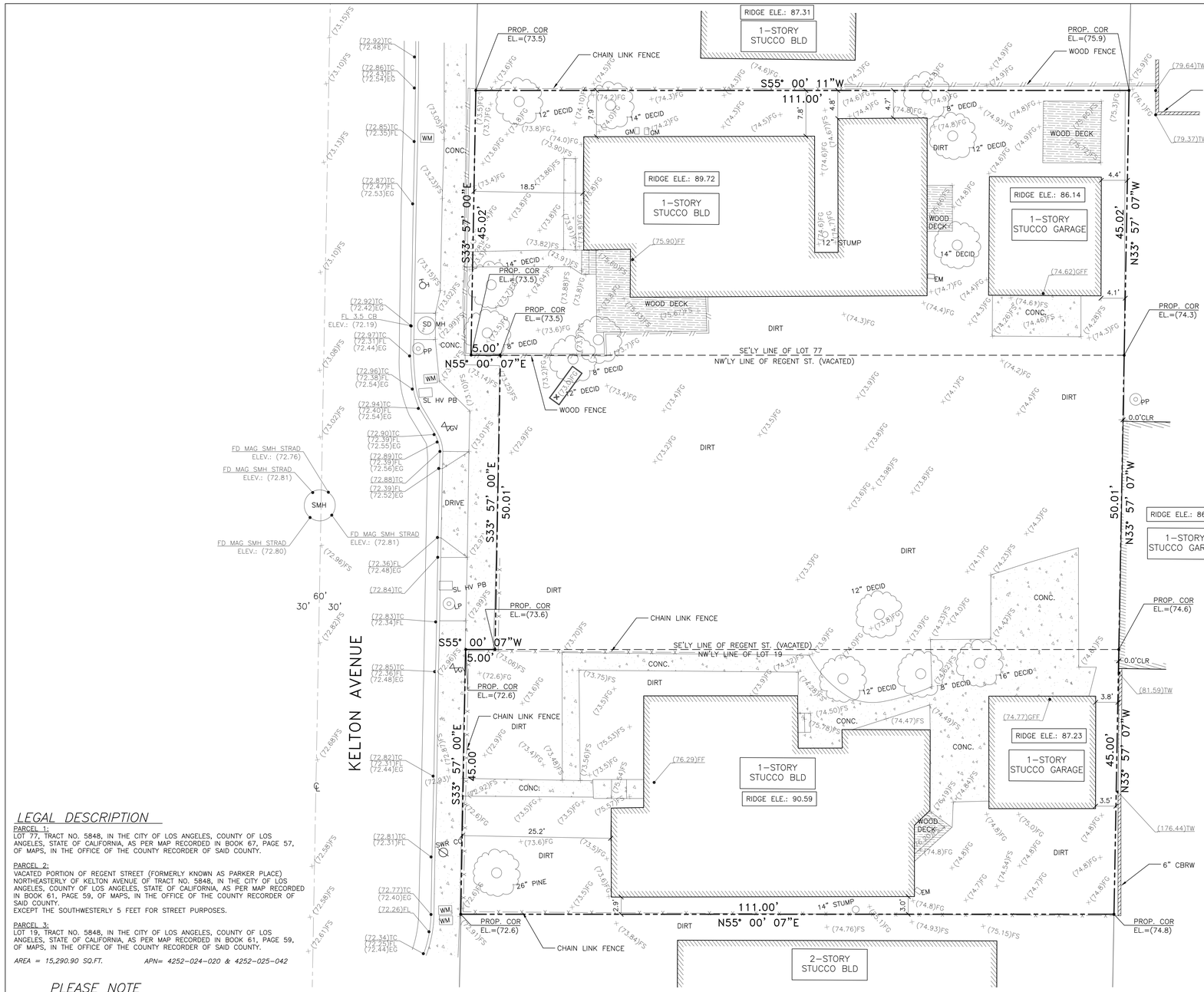
CHECKED	PNK
DATE	10/31/2022 1:16:25 PM
SCALE	As indicated
JOB #	21-A001

A0.16



LEGEND

A.C.	ASPHALT CONCRETE PAVEMENT
B.M.	BENCH MARK
C.B.W.	CONCRETE BLOCK WALL
C.B.R.W.	CONCRETE BLOCK RETAINING WALL
CONC.	CONCRETE
C/O	SEWER CLEANOUT
DWY.	CONC. DRIVEWAY APRON
E.G.	EDGE OF GUTTER
E.M.	ELECTRIC METER
E.P.	EDGE OF PAVEMENT
E.V.	ELECTRIC VALVE
F.F.	FINISHED FLOOR
F.G.	FINISHED GRADE
F.H.	FIRE HYDRANT
F.S.	FINISHED SURFACE
F.L.	FLOW LINE
G.M.	GAS METER
INV.	DRAIN INVERT
I.C.V.	IRRIGATION CONTROL VALVE
L.P.	LIGHT POLE
P.B.	PULL BOX
P.C.R.W.	POURED CONC. RETAINING WALL
P.P.	POWER POLE
R.R.W.	ROCK RETAINING WALL
S.S.M.H.	SEWER MANHOLE
S.D.M.	STORM DRAIN MANHOLE
T.C.	TOP OF CURB
T.G.	TOP OF DRAIN GRATE
T.W.	TOP OF WALL
W.M.H.	WATER MANHOLE
W.V.	WATER VALVE

OVERHANG

EASEMENT LIMIT

CONCRETE SURFACE

8" TREE AND TRUNK DIAMETER

WOOD FENCE

TOPOGRAPHIC NOTES

1. *CONTOUR LINES SHOWN ON THIS TOPOGRAPHIC SURVEY WERE GENERATED FROM RANDOM SPOT ELEVATIONS. THE EXPECTED LEVEL OF ACCURACY IS ±1/2 OF THE CONTOUR INTERVAL. ISOLATED ELEVATIONS, AS PLACED ON THE PLAT WERE PHYSICALLY MEASURED AT APPROXIMATELY THE LOCATIONS SHOWN. THEIR EXPECTED LEVEL OF ACCURACY IS ± 0.10 FOOT.*

2. *IF SPECIFIC ELEVATIONS ARE REQUIRED, THE USER SHOULD CONTACT PACIFIC LAND CONSULTANTS, INC., TEL: (310)544-8689 FOR SAME. INTERPOLATIONS MADE FROM THIS TOPOGRAPHIC SURVEY SHOULD BE DONE IN ACCORDANCE WITH STANDARD SURVEYING PRACTICES, AND LIABILITY FOR INTERPOLATIONS IS NOT ASSUMED BY PACIFIC LAND CONSULTANTS, INC.*

SURVEY CERTIFICATION

I HEREBY CERTIFY THAT I AM A REGISTERED LAND SURVEYOR OF THE STATE OF CALIFORNIA, THAT THIS PLAT CONSISTING OF ONE SHEET CORRECTLY REPRESENTS A SURVEY MADE UNDER MY SUPERVISION IN FEBRUARY, 2018; THAT ALL MONUMENTS SHOWN HEREON ACTUALLY EXIST AND THEIR POSITIONS ARE CORRECTLY SHOWN. THIS SURVEY DOES NOT INCLUDE EASEMENTS EXCEPT THOSE SPECIFICALLY DELINEATED HEREON.

CHRISTOPHER W. VASSALLO P.L.S. 8418 *DATE*

REGISTRATION EXPIRES 12-31-2022

PROFESSIONAL LAND SURVEYOR CHRISTOPHER W. VASSALLO P.L.S. 8418 EXP. 12/31/2022 STATE OF CALIFORNIA

0 4 8 FEET 16

1/8" = 1'-0"

LEGAL DESCRIPTION

PARCEL 1:
LOT 77, TRACT NO. 5848, IN THE CITY OF LOS ANGELES, COUNTY OF LOS ANGELES, STATE OF CALIFORNIA, AS PER MAP RECORDED IN BOOK 67, PAGE 57, OF MAPS, IN THE OFFICE OF THE COUNTY RECORDER OF SAID COUNTY.

PARCEL 2:
VACATED PORTION OF REGENT STREET (FORMERLY KNOWN AS PARKER PLACE) NORTHEASTERLY OF KELTON AVENUE OF TRACT NO. 5848, IN THE CITY OF LOS ANGELES, COUNTY OF LOS ANGELES, STATE OF CALIFORNIA, AS PER MAP RECORDED IN BOOK 61, PAGE 59, OF MAPS, IN THE OFFICE OF THE COUNTY RECORDER OF SAID COUNTY.
EXCEPT THE SOUTHWESTERLY 5 FEET FOR STREET PURPOSES.

PARCEL 3:
LOT 19, TRACT NO. 5848, IN THE CITY OF LOS ANGELES, COUNTY OF LOS ANGELES, STATE OF CALIFORNIA, AS PER MAP RECORDED IN BOOK 61, PAGE 59, OF MAPS, IN THE OFFICE OF THE COUNTY RECORDER OF SAID COUNTY.

AREA = 15,290.90 SQ.FT. *APN= 4252-024-020 & 4252-025-042*

PLEASE NOTE

IF THIS DRAWING IS PROVIDED IN AN ELECTRONIC FORMAT (VIA EMAIL OR ON COMPUTER DISC) AS A COURTESY TO OUR CLIENT, THE DELIVERY OF THE ELECTRONIC FILE DOES NOT CONSTITUTE THE DELIVERY OF OUR PROFESSIONAL WORK PRODUCT. IN THE EVENT THE ELECTRONIC FILE IS ALTERED, THE PRINT MUST BE REFERRED TO FOR THE ORIGINAL AND CORRECT SURVEY INFORMATION. PACIFIC LAND CONSULTANTS, INC. SHALL NOT BE RESPONSIBLE FOR ANY MODIFICATIONS MADE TO THE ELECTRONIC FILE OR FOR ANY PRODUCTS DERIVED FROM THE ELECTRONIC FILE WHICH ARE NOT REVIEWED, SIGNED AND SEALED BY PACIFIC LAND CONSULTANTS, INC.

UNDERGROUND UTILITIES

ALL INFORMATION SHOWN HEREON REGARDING UNDERGROUND UTILITIES WAS TAKEN FROM VISIBLE SURFACE EVIDENCE OR SOURCES NOT CONNECTED WITH THIS COMPANY AND WHILE SAID INFORMATION IS BELIEVED CORRECT, NO LIABILITY IS ASSUMED FOR THE ACCURACY OR COMPLETENESS OF SAID DATA.

BENCHMARK

BM# 13-02970
CUT SPIKE IN SE COR CB; 3.5FT E OF E CURB MIDVALE AVE; 22FT N OF VENICE BLVD
ELEV: 74.860' (NAVD-88)

PACIFIC LAND CONSULTANTS ESTABLISHED 1994

PACIFIC LAND CONSULTANTS, INC.
28441 HIGHRIDGE RD. SUITE 230
ROLLING HILLS ESTATES, CA 90274
(310) 544-8689

DRAWN BY:A.P.

PROJECT SITE:
3676 KELTON AVENUE
LOS ANGELES, CA

FILE NAME: 22027LS.DWG
DATE: 4-5-2022
SCALE: 1/8" = 1'-0"
JOB NO. 22027
SHEET 1 OF 1

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LICENSED ARCHITECT
RAMSEY DAHAM
No. C-34257
10/31/23
RENEWAL DATE
STATE OF CALIFORNIA

3676 & 3704 KELTON AVE.
LOS ANGELES, CA 90034

Revision Schedule

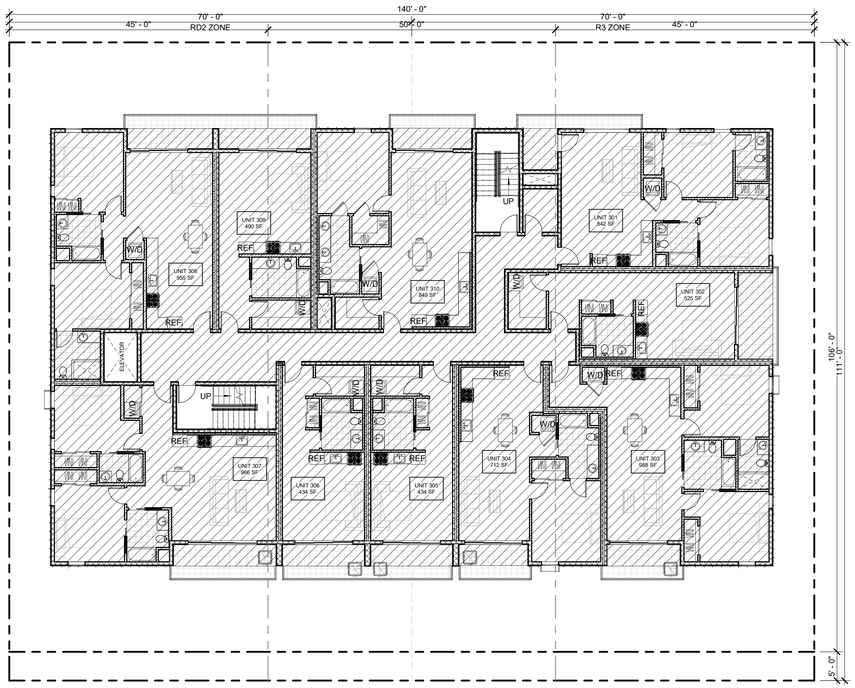
Revision Number	Revision Date

EXISTING SITE SURVEY

DRAWN CR
CHECKED PNK
DATE 10/31/2022 1:16:28 PM
SCALE
JOB # 21-A001

A0.23

ZONING CODE FLOOR AREA	
PARKING LEVEL	471 SF
FIRST FLOOR	6,480 SF
SECOND FLOOR	8,892 SF
THIRD FLOOR	8,892 SF
FOURTH FLOOR	8,892 SF
FIFTH FLOOR	8,206 SF
TOTAL PROVIDED	**41,833 SF**

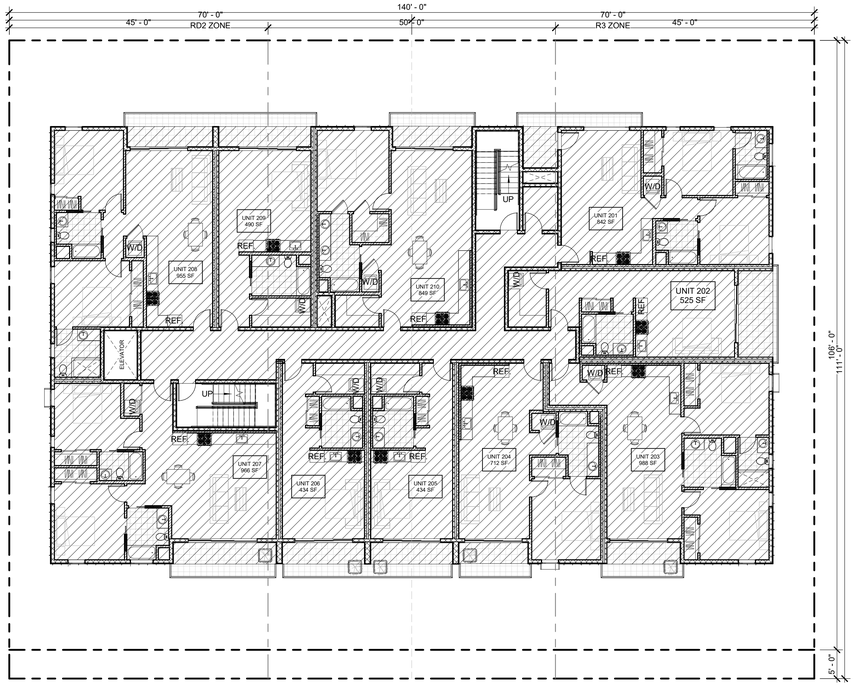


KELTON AVE

ZONING CODE - THIRD FLOOR
3/32" = 1'-0" 4

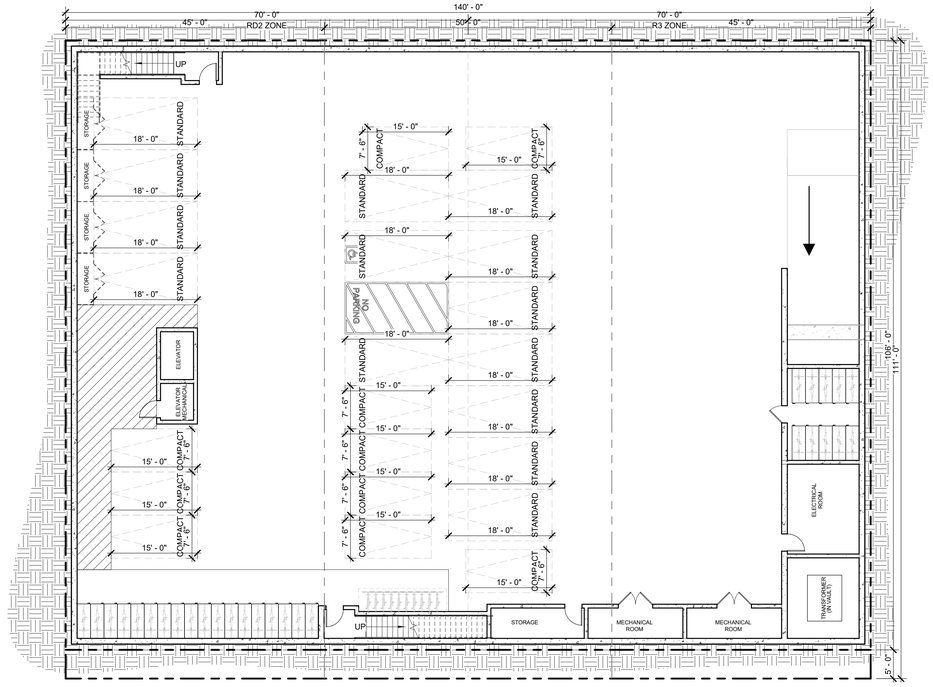
KELTON AVE

ZONING CODE - GROUND FLOOR
3/32" = 1'-0" 2



KELTON AVE

ZONING CODE - SECOND FLOOR
3/32" = 1'-0" 3



KELTON AVE

ZONING CODE - PARKING LEVEL
3/32" = 1'-0" 1

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Revision Schedule

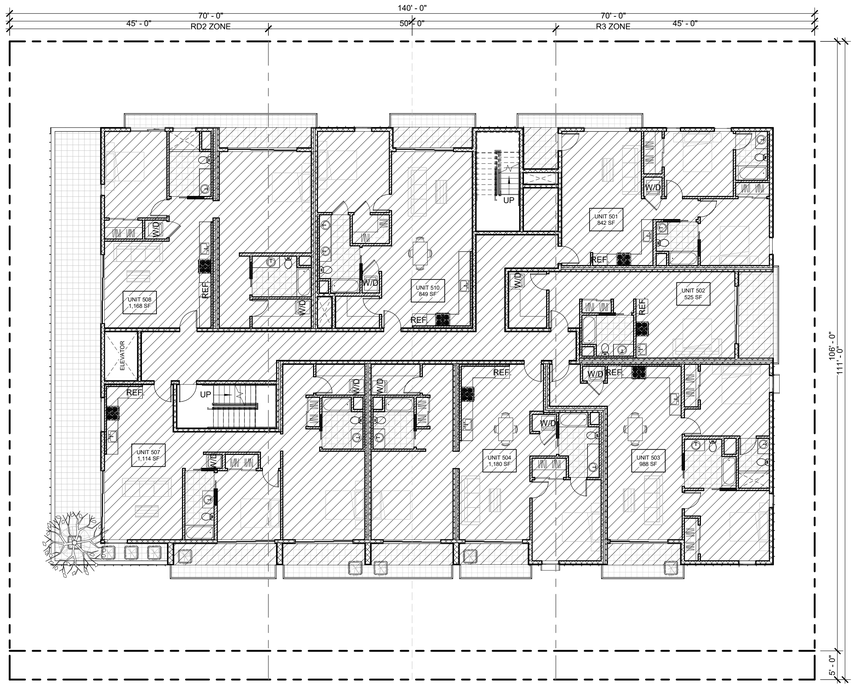
Revision Number	Revision Date

SQUARE FOOTAGE BREAKDOWNS

DRAWN	CR
CHECKED	PNK
DATE	10/31/2022 1:16:43 PM
SCALE	As indicated
JOB #	21-A001

A0.26

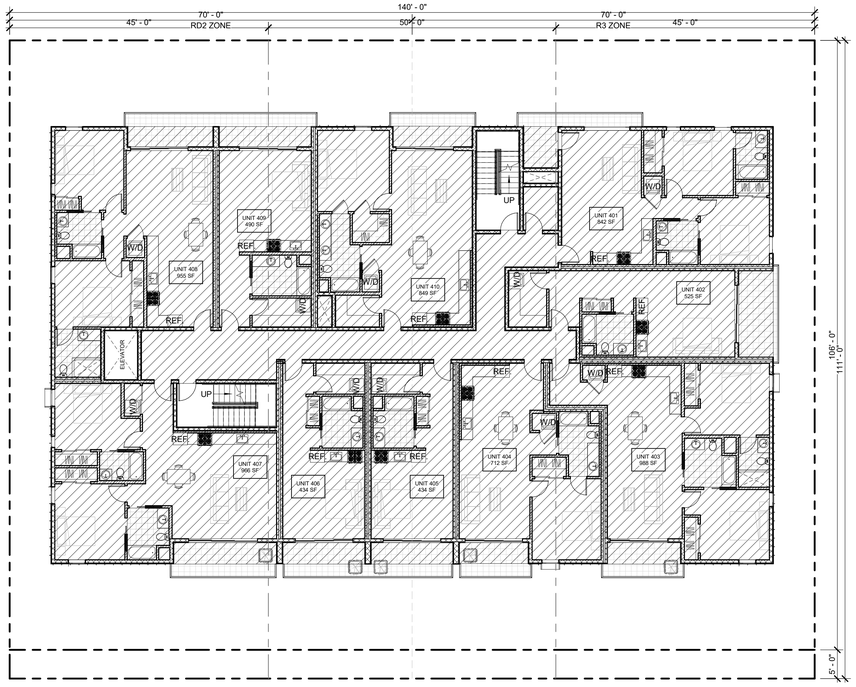
ZONING CODE FLOOR AREA	
PARKING LEVEL	471 SF
FIRST FLOOR	6,480 SF
SECOND FLOOR	8,892 SF
THIRD FLOOR	8,892 SF
FOURTH FLOOR	8,892 SF
FIFTH FLOOR	8,206 SF
TOTAL PROVIDED	**41,833 SF**



KELTON AVE

ZONING CODE - FIFTH FLOOR
3/32" = 1'-0"

2



KELTON AVE

ZONING CODE - FOURTH FLOOR
3/32" = 1'-0"

1

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Revision Schedule

Revision Number	Revision Date

SQUARE FOOTAGE BREAKDOWNS

DRAWN	Author
CHECKED	Checker
DATE	10/31/2022 1:16:53 PM
SCALE	As indicated
JOB #	21-A001

A0.26.1

BUILDING CODE FLOOR AREA	
PARKING LEVEL	13,872 SF
-INCLUDED	N/A
FIRST FLOOR	8,427 SF
-INCLUDED	645 SF
SECOND FLOOR	8,775 SF
-INCLUDED	929 SF
THIRD FLOOR	8,775 SF
-INCLUDED	929 SF
FOURTH FLOOR	8,775 SF
-INCLUDED	929 SF
FIFTH FLOOR	8,142 SF
-INCLUDED	N/A SF
TOTAL PROVIDED	**60,198 SF**

KELTON AVE

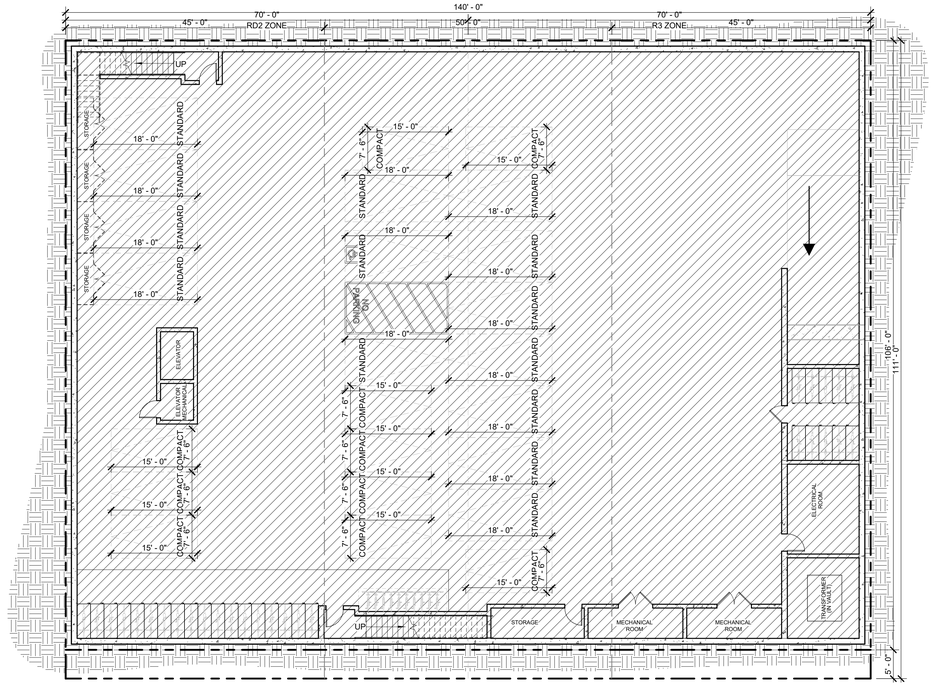
BUILDING CODE - THIRD FLOOR
3/32" = 1'-0" 4

KELTON AVE

BUILDING CODE - GROUND FLOOR
3/32" = 1'-0" 2

KELTON AVE

BUILDING CODE - SECOND FLOOR
3/32" = 1'-0" 3



KELTON AVE

BUILDING CODE - PARKING LEVEL
3/32" = 1'-0" 1

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Revision Schedule

Revision Number	Revision Date

SQUARE FOOTAGE BREAKDOWNS

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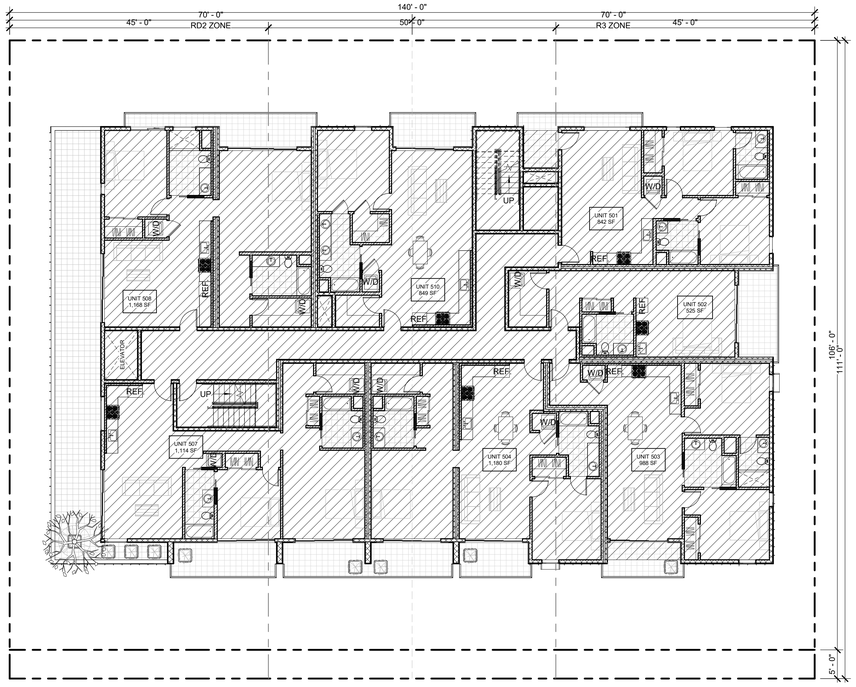
CHECKED	PNK
DATE	10/31/2022 1:17:08 PM
SCALE	As indicated
JOB #	21-A001

A0.27

BUILDING CODE FLOOR AREA	
PARKING LEVEL	13,872 SF
-INCLUDED	N/A
FIRST FLOOR	8,427 SF
-INCLUDED	645 SF
SECOND FLOOR	8,775 SF
-INCLUDED	929 SF
THIRD FLOOR	8,775 SF
-INCLUDED	929 SF
FOURTH FLOOR	8,775 SF
-INCLUDED	929 SF
FIFTH FLOOR	8,142 SF
-INCLUDED	N/A SF
TOTAL PROVIDED	**60,198 SF**



KELTON AVE

BUILDING CODE - FIFTH FLOOR
3/32" = 1'-0"

2

KELTON AVE

BUILDING CODE - FOURTH FLOOR
3/32" = 1'-0"

1

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SQUARE FOOTAGE BREAKDOWNS

DRAWN	Author
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DATE	10/31/2022 1:17:17 PM
SCALE	As indicated
JOB #	21-A001

A0.27.1

OPEN SPACE PROVIDED VS. REQUIRED

PROVIDED		REQUIRED	
COMMON OPEN SPACE:		26 UNITS @ < 3 HABITABLE ROOMS (100 S.F.) (26 UNITS)(100 S.F.) =	2,600 SF
COMMON ROOF DECK			
RD2 ZONE:	1,767 SF	17 UNITS @ 3 HABITABLE ROOMS (125 S.F.) (17 UNITS)(125 S.F.) =	2,125 SF
R3 ZONE:	2,279 SF		
PRIVATE OPEN SPACE:		0 UNITS @ > 3 HABITABLE ROOMS (175 S.F.) (0 UNITS)(175 S.F.) =	0 SF
RD2 ZONE: 27 UNITS x 50 SF	1,350 SF		
R3 ZONE: 16 UNITS x 50 SF	800 SF		
TOTAL PROVIDED OPEN SPACE:		TOTAL REQUIRED OPEN SPACE:	4,725 SF
RD2 ZONE:	3,117 SF		
R3 ZONE:	3,079 SF		

* (OPEN SPACE PROVIDED IN THE RD2 ZONE DOES NOT QUALIFY AS USABLE OPEN SPACE PER LAMC 12.21 G.2)

COMMON OPEN SPACE TREE COUNT			
PROVIDED		REQUIRED	
ON SITE	9 TREES	1 TREE PER EVERY 4 UNITS	
IN PARKWAY	3 TREES	43 UNITS / 4 =	10.75 TREES
TOTAL	**12 TREES**	**TOTAL**	**10.75 TREES**

COMMON OPEN SPACE PLANTING AREA

ROOF DECK COMMON OPEN SPACE:	2,279 SF
REQUIRED PLANTING AREA: 25% OF THE COMMON OPEN SPACE	570 SF
PROVIDED PLANTING AREA:	570 SF

KELTON AVE — OPEN SPACE - FOURTH FLOOR 3/32" = 1'-0" — 4

OPEN SPACE - SECOND FLOOR 3/32" = 1'-0" — 2

KELTON AVE — OPEN SPACE - THIRD FLOOR 3/32" = 1'-0" — 3

KELTON AVE — OPEN SPACE - GROUND FLOOR 3/32" = 1'-0" — 1

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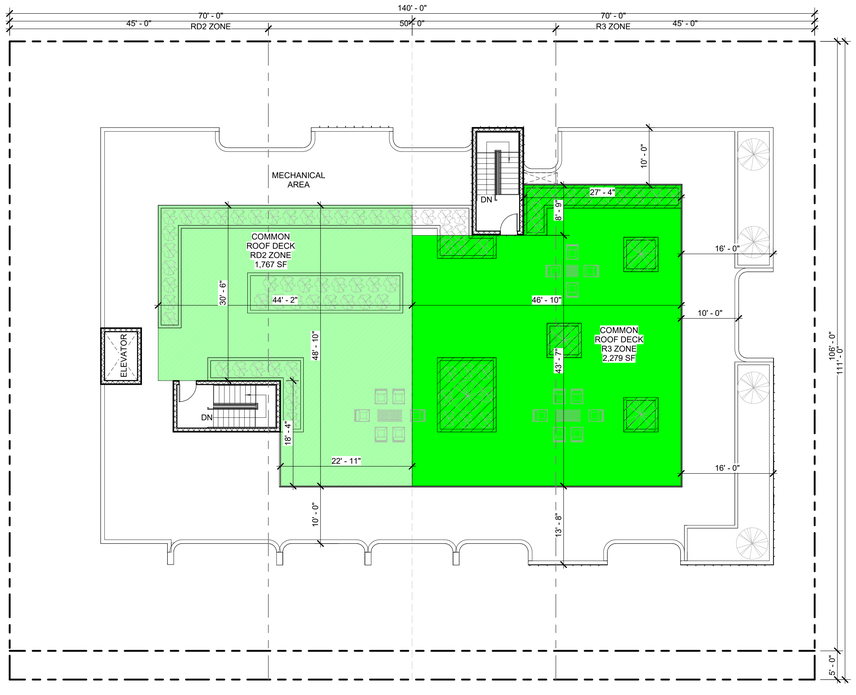
Revision Schedule

Revision Number	Revision Date

OPEN SPACE AREA CALCULATIONS

DRAWN	CR
CHECKED	PNK
DATE	11/3/2022 4:07:43 PM
SCALE	3/32" = 1'-0"
JOB #	21-A001

A0.29



KELTON AVE

OPEN SPACE - ROOF
3/32" = 1'-0"

2

KELTON AVE

OPEN SPACE - FIFTH FLOOR
3/32" = 1'-0"

1

breakformdesign

127 arena street, el segundo, ca 90245
[o] 310.322.3700

LICENSED ARCHITECT
RAMSEY DAHAM
No. C-34257
10/31/23
RENEWAL DATE
STATE OF CALIFORNIA

3676 & 3704 KELTON AVE.
LOS ANGELES, CA 90034

Revision Schedule

Revision Number	Revision Date

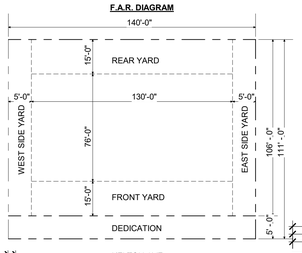
OPEN SPACE AREA CALCULATIONS

DRAWN	CR
CHECKED	PNK
DATE	11/3/2022 4:07:49 PM
SCALE	3/32" = 1'-0"
JOB #	21-A001

A0.29.1

F.A.R. CALCULATIONS	
PARKING LEVEL -INCLUDED	665 SF N/A
FIRST FLOOR -INCLUDED	6,480 SF 645 SF
SECOND FLOOR -INCLUDED	8,338 SF 929 SF
THIRD FLOOR -INCLUDED	8,338 SF 929 SF
FOURTH FLOOR -INCLUDED	8,338 SF 929 SF
FIFTH FLOOR -INCLUDED	7,758 SF N/A SF
TOTAL PROVIDED	**43,349 SF**

F.A.R. DIAGRAM



- LOT AREA: 15,334.6 S.F.
- BUILDABLE AREA: 10,530 S.F.
(BEFORE 5 FT DEDICATION)

KELTON AVE

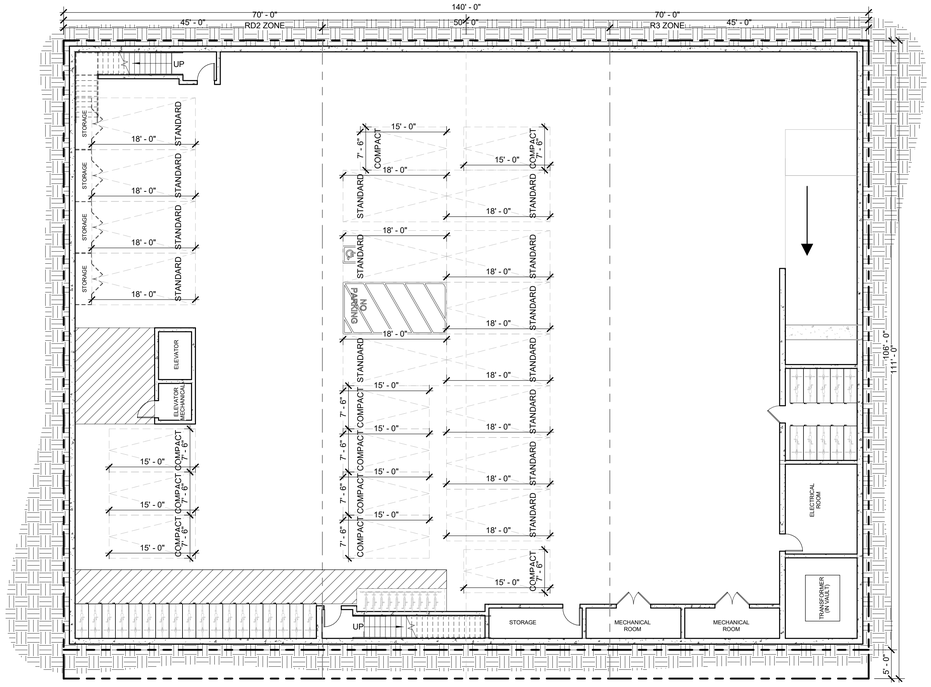
F.A.R. - THIRD FLOOR
3/32" = 1'-0" 4

KELTON AVE

F.A.R. - GROUND FLOOR
3/32" = 1'-0" 2

KELTON AVE

F.A.R. - SECOND FLOOR
3/32" = 1'-0" 3



KELTON AVE

F.A.R. - PARKING LEVEL
3/32" = 1'-0" 1

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LICENSED ARCHITECT
RAMSEY DAHAM
No. C-34257
10/31/23
RENEWAL DATE
STATE OF CALIFORNIA

3676 & 3704 KELTON AVE.
LOS ANGELES, CA 90034

Revision Schedule

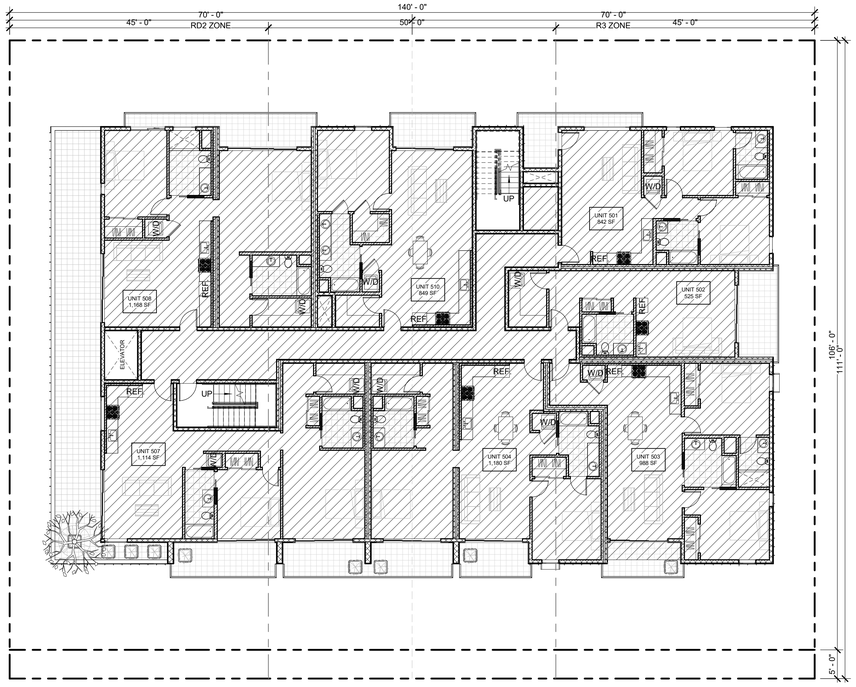
Revision Number	Revision Date

F.A.R. CALCULATIONS

DRAWN	CR
CHECKED	PNK
DATE	10/31/2022 1:18:07 PM
SCALE	As indicated
JOB #	21-A001

A0.30

F.A.R. CALCULATIONS	
PARKING LEVEL	665 SF
-INCLUDED	N/A
FIRST FLOOR	6,480 SF
-INCLUDED	645 SF
SECOND FLOOR	8,338 SF
-INCLUDED	929 SF
THIRD FLOOR	8,338 SF
-INCLUDED	929 SF
FOURTH FLOOR	8,338 SF
-INCLUDED	929 SF
FIFTH FLOOR	7,758 SF
-INCLUDED	N/A SF
TOTAL PROVIDED	**43,349 SF**



KELTON AVE

F.A.R. - FIFTH FLOOR
3/32" = 1'-0"

2

KELTON AVE

F.A.R. - FOURTH FLOOR
3/32" = 1'-0"

1

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LICENSED ARCHITECT
RAMSEY DAHAM
No. C-34257
10/31/23
RENEWAL DATE
STATE OF CALIFORNIA

3676 & 3704 KELTON AVE.
LOS ANGELES, CA 90034

Revision Schedule

Revision Number	Revision Date

F.A.R. CALCULATIONS

DRAWN	Author
CHECKED	Checker
DATE	10/31/2022 1:18:17 PM
SCALE	As indicated
JOB #	21-A001

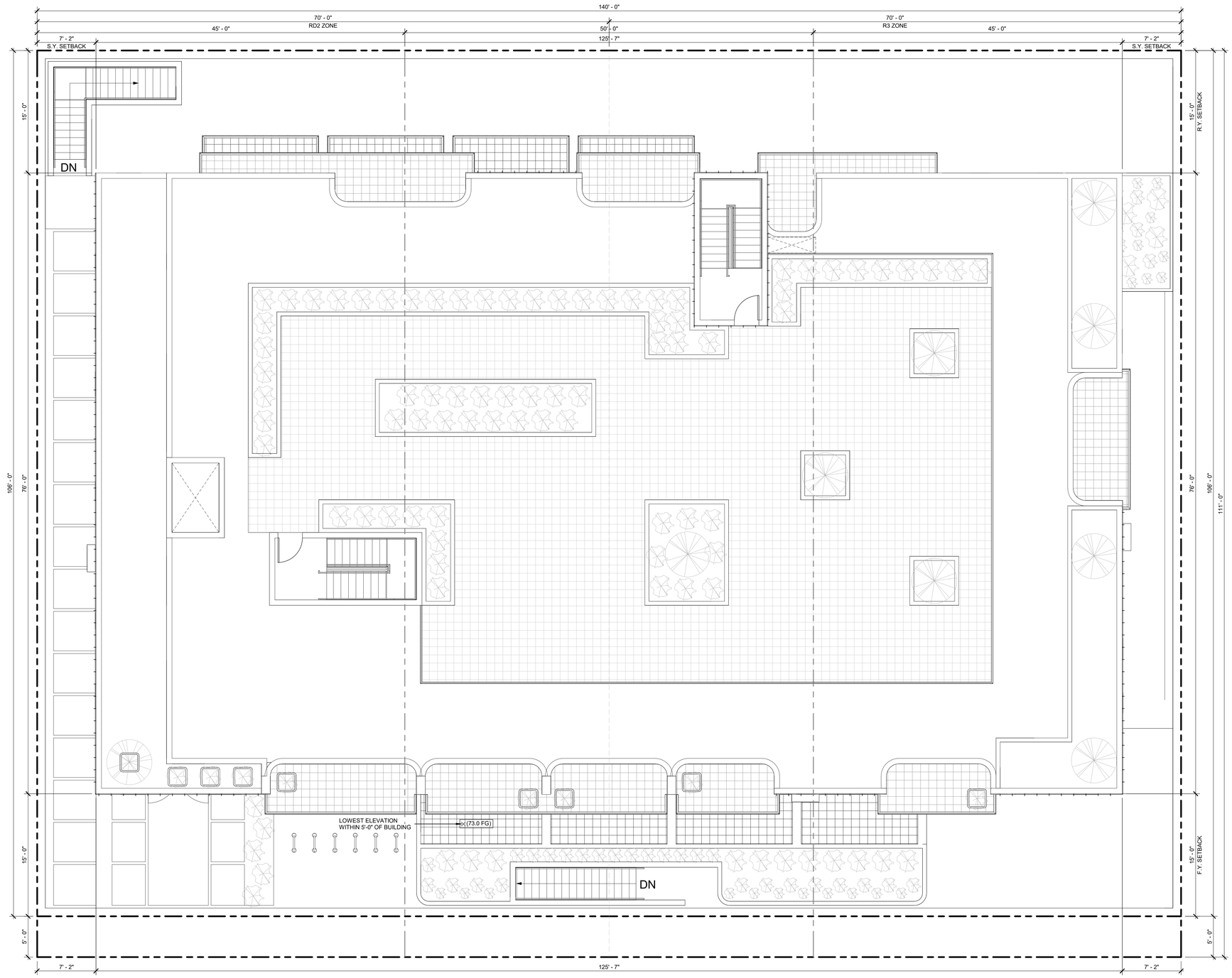
A0.30.1

SITE PLAN LEGEND

FLOOR TYPE

PROPERTY LINE

SETBACKS



KELTON AVE

N

SITE PLAN
3/16" = 1'-0"

1

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LICENSED ARCHITECT
RAMSEY DAHAM
No. C-34257
10/31/23
RENEWAL DATE
STATE OF CALIFORNIA

3676 & 3704 KELTON AVE.
LOS ANGELES, CA 90034

Revision Schedule

Revision Number	Revision Date

SITE PLAN

DRAWN	CR
CHECKED	PNK
DATE	10/31/2022 1:18:21 PM
SCALE	As indicated
JOB #	21-A001

A1.00

ASSEMBLY TYPES

WALLS

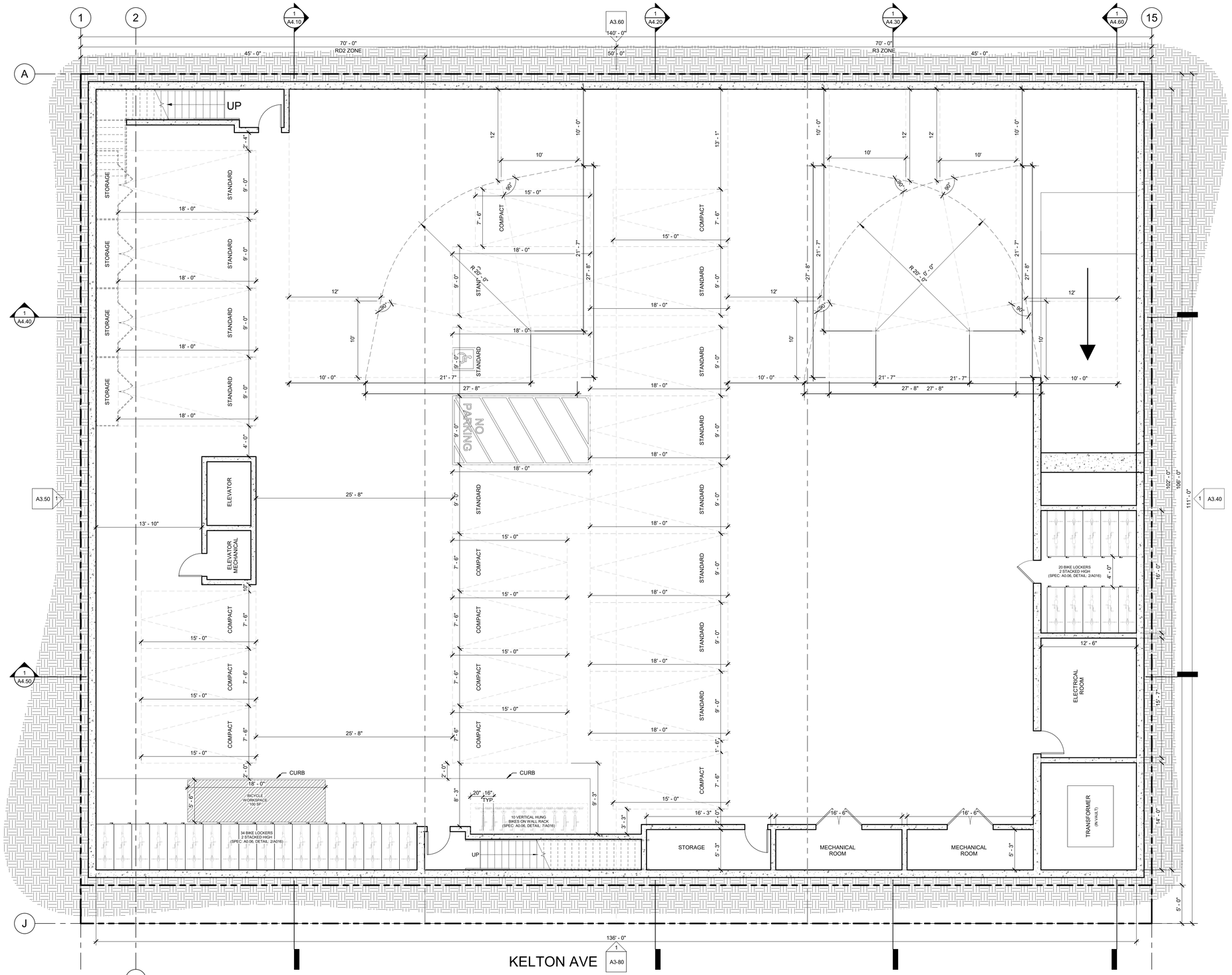
1. 2x4 & 2x6 INTERIOR WALL ASSM. (1/A0.13)
2. 2x6 PLUMBING WALL ASSM. (2/A0.13)
3. 1-HR 2x6 INTERIOR WALL ASSM. (3/A0.13)
4. 1-HR 2x6 EXTERIOR WALL ASSM. (4/A0.13)
5. 1-HR DOUBLE WALL ASSM. (5/A0.13)
6. CONC. WALL PER STRC. (6/A0.13)
7. CONC. RETAINING WALL PER STRC. (7/A0.13)
8. CMU WALL PER STRC. (8/A0.13)
9. 2-HR INTERIOR CORRIDOR WALL ASSM. (9/A0.13)

FLOORS

- A-1: CONC FLOOR PER STRC. W/ POLISHED CONC. FINISH (10/A0.13)
- A-2: CONC FLOOR PER STRC. W/ WOOD FINISH (11/A0.13)
- A-3: CONC FLOOR PER STRC. W/ TILE FINISH (12/A0.13)
- B-1: WOOD JOIST PER STRC. W/ WOOD FINISH (13/A0.13)
- B-2: WOOD JOIST PER STRC. W/ TILE FINISH (14/A0.13)
- B-3: WOOD JOIST W/ WOOD DECK PER SPEC. (15/A0.13)
- B-4: WOOD JOIST PER STRC. W/ METAL ROOFING PER SPEC. (16/A0.13)

FLOOR PLAN LEGEND

- FLOOR TYPE
- X: WINDOW TAG (A0.10 - SCHEDULE)
- XXX: DOOR TAG (A0.08, A0.09 - SCHEDULE)
- X: WALL TYPE
- X-X": ELEVATION MARKER
- PROPERTY LINE
- ACCESSIBLE ROUTE
- 1 HR
- 2 HR
- SD: SMOKE DETECTOR
- CM: CARBON MONOXIDE
- EXHAUST
- S: NFPA - 14 CLASS - I STANDPIPE
- ILLUMINATED EXIT SIGN
- MB: MASTER BEDROOM
- BD: BEDROOM
- BA: BATHROOM
- LR: LIVING ROOM
- KI: KITCHEN
- DR: DINING ROOM
- PWR: POWDER ROOM
- CL: CLOSET
- WIC: WALK IN CLOSET
- LR: LAUNDRY ROOM
- BC: BALCONY
- EN: ENTRY



NOTE: DOUBLE STRIPING OF STALLS SHALL BE PER ZONING CODE SECTION 12.21A5 CHART NO. 5

N

SUBTERRANEAN PARKING
3/16" = 1'-0"

1

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LICENSED ARCHITECT
RAMSEY DAHAM
No. C-34257
10/31/23
RENEWAL DATE
STATE OF CALIFORNIA

3676 & 3704 KELTON AVE.
LOS ANGELES, CA 90034

Revision Schedule

Revision Number	Revision Date

PROPOSED PLANS

DRAWN	CR
CHECKED	PNK
DATE	10/31/2022 1:18:24 PM
SCALE	As indicated
JOB #	21-A001

A2.00

ASSEMBLY TYPES

WALLS

1. 2x4 & 2x6 INTERIOR WALL ASSM. (1/A0.13)
2. 2x6 PLUMBING WALL ASSM. (2/A0.13)
3. 1-HR 2x6 INTERIOR WALL ASSM. (3/A0.13)
4. 1-HR 2x6 EXTERIOR WALL ASSM. (4/A0.13)
5. 1-HR DOUBLE WALL ASSM. (5/A0.13)
6. CONC. WALL PER STRC. (6/A0.13)
7. CONC. RETAINING WALL PER STRC. (7/A0.13)
8. CMU WALL PER STRC. (8/A0.13)
9. 2-HR INTERIOR CORRIDOR WALL ASSM. (9/A0.13)

FLOORS

- A1 CONC FLOOR PER STRC. W/ POLISHED CONC. FINISH (10/A0.13)
- A2 CONC FLOOR PER STRC. W/ WOOD FINISH (11/A0.13)
- A3 CONC FLOOR PER STRC. W/ TILE FINISH (12/A0.13)
- B1 WOOD JOIST PER STRC. W/ WOOD FINISH (13/A0.13)
- B2 WOOD JOIST PER STRC. W/ TILE FINISH (14/A0.13)
- B3 WOOD JOIST W/ WOOD DECK PER SPEC. (15/A0.13)
- B4 WOOD JOIST PER STRC. W/ METAL ROOFING PER SPEC. (16/A0.13)

FLOOR PLAN LEGEND

- FLOOR TYPE
- WINDOW TAG (A0.10 - SCHEDULE)
- DOOR TAG (A0.08, A0.09 - SCHEDULE)
- WALL TYPE
- ELEVATION MARKER
- PROPERTY LINE
- ACCESSIBLE ROUTE
- 1 HR
- 2 HR
- SD SMOKE DETECTOR
- CM CARBON MONOXIDE
- EXHAUST
- S NFPA - 14 CLASS - I STANDPIPE
- ILLUMINATED EXIT SIGN

MB	MASTER BEDROOM
BD	BEDROOM
BA	BATHROOM
LR	LIVING ROOM
KI	KITCHEN
DR	DINING ROOM
PWR	POWDER ROOM
CL	CLOSET
WIC	WALK IN CLOSET
LR	LAUNDRY ROOM
BC	BALCONY
EN	ENTRY

FENCES, PLANTERS, AND RETAINING WALLS SHALL NOT EXCEED A HEIGHT OF 6 FT. ABOVE THE NATURAL GROUND LEVEL IN THE REQUIRED SIDE YARD.

NOTE: DOUBLE STRIPING OF STALLS SHALL BE PER ZONING CODE SECTION 12.21A5 CHART NO. 5

N

GROUND FLOOR PLAN
3/16" = 1'-0"

1

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LICENSED ARCHITECT
RAMSEY DAHAM
No. C-34257
10/31/23
RENEWAL DATE
STATE OF CALIFORNIA

3676 & 3704 KELTON AVE.
LOS ANGELES, CA 90034

Revision Schedule

Revision Number	Revision Date

PROPOSED PLANS

DRAWN	CR
CHECKED	PNK
DATE	10/31/2022 1:18:27 PM
SCALE	As indicated
JOB #	21-A001

A2.10

ASSEMBLY TYPES

WALLS

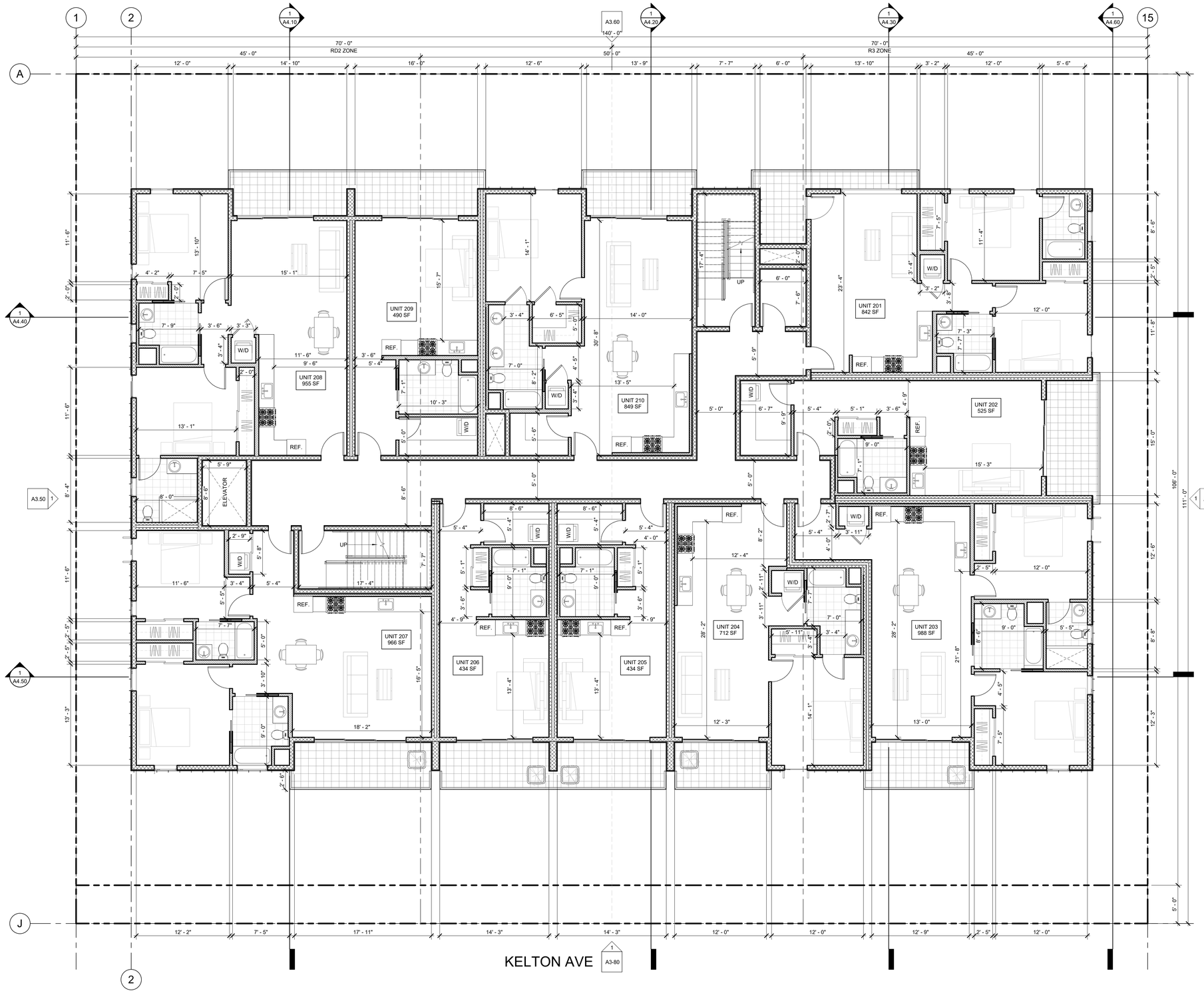
1. 2x4 & 2x6 INTERIOR WALL ASSM. (1/A0.13)
2. 2x6 PLUMBING WALL ASSM. (2/A0.13)
3. 1-HR 2x6 INTERIOR WALL ASSM. (3/A0.13)
4. 1-HR 2x6 EXTERIOR WALL ASSM. (4/A0.13)
5. 1-HR DOUBLE WALL ASSM. (5/A0.13)
6. CONC. WALL PER STRC. (6/A0.13)
7. CONC. RETAINING WALL PER STRC. (7/A0.13)
8. CMU WALL PER STRC. (8/A0.13)
9. 2-HR INTERIOR CORRIDOR WALL ASSM. (9/A0.13)

FLOORS

- A1 CONC FLOOR PER STRC. W/ POLISHED CONC. FINISH (10/A0.13)
- A2 CONC FLOOR PER STRC. W/ WOOD FINISH (11/A0.13)
- A3 CONC FLOOR PER STRC. W/ TILE FINISH (12/A0.13)
- B1 WOOD JOIST PER STRC. W/ WOOD FINISH (13/A0.13)
- B2 WOOD JOIST PER STRC. W/ TILE FINISH (14/A0.13)
- B3 WOOD JOIST W/ WOOD DECK PER SPEC. (15/A0.13)
- B4 WOOD JOIST PER STRC. W/ METAL ROOFING PER SPEC. (16/A0.13)

FLOOR PLAN LEGEND

- FLOOR TYPE
- WINDOW TAG (A0.10 - SCHEDULE)
- DOOR TAG (A0.08, A0.09 - SCHEDULE)
- WALL TYPE
- ELEVATION MARKER
- PROPERTY LINE
- ACCESSIBLE ROUTE
- 1 HR
- 2 HR
- SD SMOKE DETECTOR
- CM CARBON MONOXIDE
- EXHAUST
- S NFPA - 14 CLASS - I STANDPIPE
- ILLUMINATED EXIT SIGN
- MB MASTER BEDROOM
- BD BEDROOM
- BA BATHROOM
- LR LIVING ROOM
- KI KITCHEN
- DR DINING ROOM
- PWR POWDER ROOM
- CL CLOSET
- WIC WALK IN CLOSET
- LR LAUNDRY ROOM
- BC BALCONY
- EN ENTRY



SECOND FLOOR PLAN
3/16" = 1'-0"

1

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LICENSED ARCHITECT
RAMSEY DAHAM
No. C-34257
10/31/23
RENEWAL DATE
STATE OF CALIFORNIA

3676 & 3704 KELTON AVE.
LOS ANGELES, CA 90034

Revision Schedule

Revision Number	Revision Date

PROPOSED PLANS

DRAWN	CR
CHECKED	PNK
DATE	10/31/2022 1:18:39 PM
SCALE	As indicated
JOB #	21-A001

A2.20

ASSEMBLY TYPES

WALLS

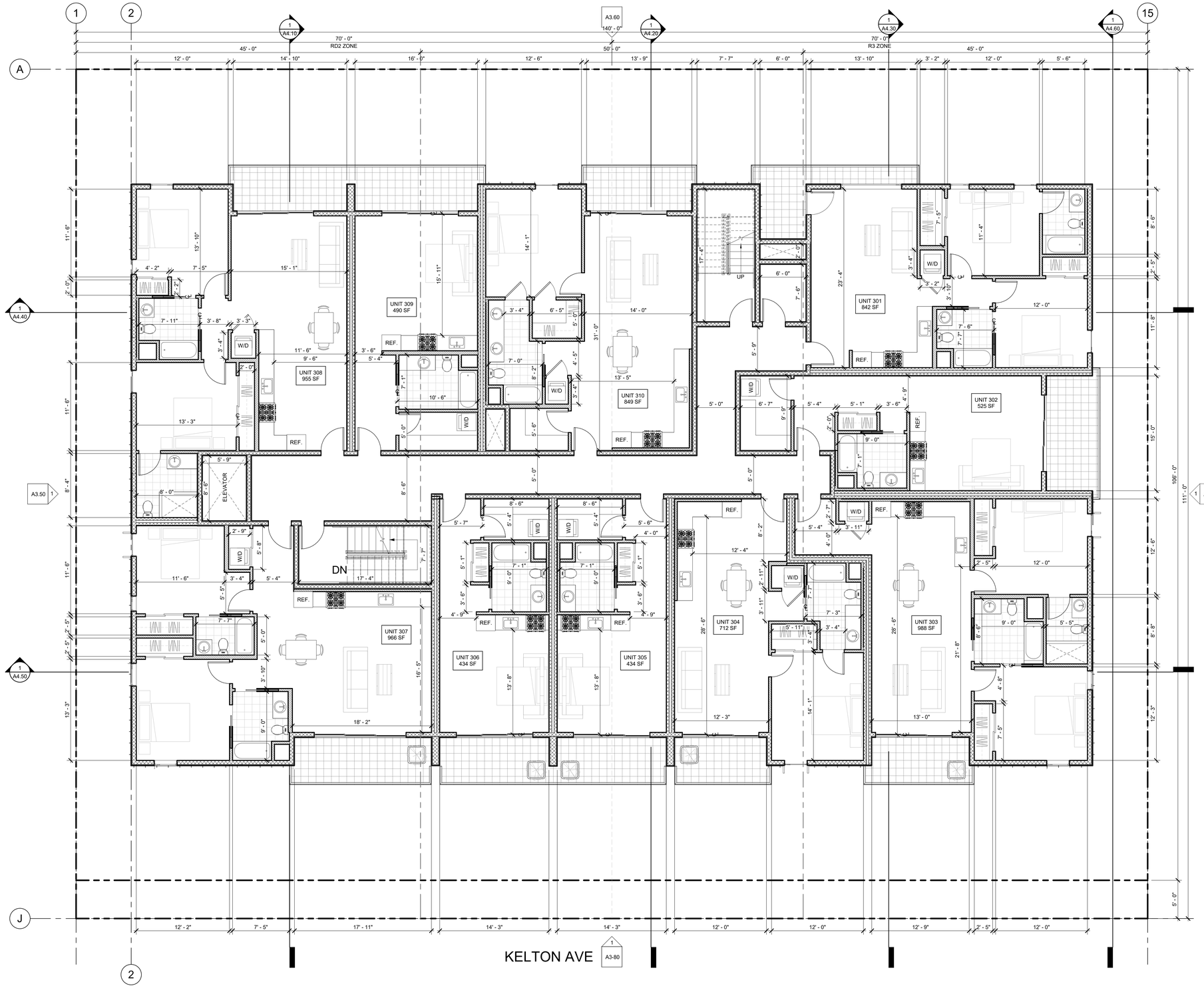
1. 2x4 & 2x6 INTERIOR WALL ASSM. (1/A0.13)
2. 2x6 PLUMBING WALL ASSM. (2/A0.13)
3. 1-HR 2x6 INTERIOR WALL ASSM. (3/A0.13)
4. 1-HR 2x6 EXTERIOR WALL ASSM. (4/A0.13)
5. 1-HR DOUBLE WALL ASSM. (5/A0.13)
6. CONC. WALL PER STRC. (6/A0.13)
7. CONC. RETAINING WALL PER STRC. (7/A0.13)
8. CMU WALL PER STRC. (8/A0.13)
9. 2-HR INTERIOR CORRIDOR WALL ASSM. (9/A0.13)

FLOORS

- CONC FLOOR PER STRC. W/ POLISHED CONC. FINISH (10/A0.13)
- CONC FLOOR PER STRC. W/ WOOD FINISH (11/A0.13)
- CONC FLOOR PER STRC. W/ TILE FINISH (12/A0.13)
- WOOD JOIST PER STRC. W/ WOOD FINISH (13/A0.13)
- WOOD JOIST PER STRC. W/ TILE FINISH (14/A0.13)
- WOOD JOIST W/ WOOD DECK PER SPEC. (15/A0.13)
- WOOD JOIST PER STRC. W/ METAL ROOFING PER SPEC. (16/A0.13)

FLOOR PLAN LEGEND

- FLOOR TYPE
- WINDOW TAG (A0.10 - SCHEDULE)
- DOOR TAG (A0.08, A0.09 - SCHEDULE)
- WALL TYPE
- X-X" ELEVATION MARKER
- PROPERTY LINE
- ACCESSIBLE ROUTE
- 1 HR
- 2 HR
- SD SMOKE DETECTOR
- CM CARBON MONOXIDE
- EXHAUST
- S NFPA - 14 CLASS - I STANDPIPE
- ILLUMINATED EXIT SIGN
- MB MASTER BEDROOM
- BD BEDROOM
- BA BATHROOM
- LR LIVING ROOM
- KI KITCHEN
- DR DINING ROOM
- PWR POWDER ROOM
- CL CLOSET
- WIC WALK IN CLOSET
- LR LAUNDRY ROOM
- BC BALCONY
- EN ENTRY



N

THIRD FLOOR PLAN
3/16" = 1'-0"

1

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LICENSED ARCHITECT
RAMSEY DAHAM
No. C-34257
10/31/23
RENEWAL DATE
STATE OF CALIFORNIA

3676 & 3704 KELTON AVE.
LOS ANGELES, CA 90034

Revision Schedule

Revision Number	Revision Date

PROPOSED PLANS

DRAWN	CR
CHECKED	PNK
DATE	10/31/2022 1:18:52 PM
SCALE	As indicated
JOB #	21-A001

A2.30

ASSEMBLY TYPES

WALLS

1. 2x4 & 2x6 INTERIOR WALL ASSM. (1/A0.13)
2. 2x6 PLUMBING WALL ASSM. (2/A0.13)
3. 1-HR 2x6 INTERIOR WALL ASSM. (3/A0.13)
4. 1-HR 2x6 EXTERIOR WALL ASSM. (4/A0.13)
5. 1-HR DOUBLE WALL ASSM. (5/A0.13)
6. CONC. WALL PER STRC. (6/A0.13)
7. CONC. RETAINING WALL PER STRC. (7/A0.13)
8. CMU WALL PER STRC. (8/A0.13)
9. 2-HR INTERIOR CORRIDOR WALL ASSM. (9/A0.13)

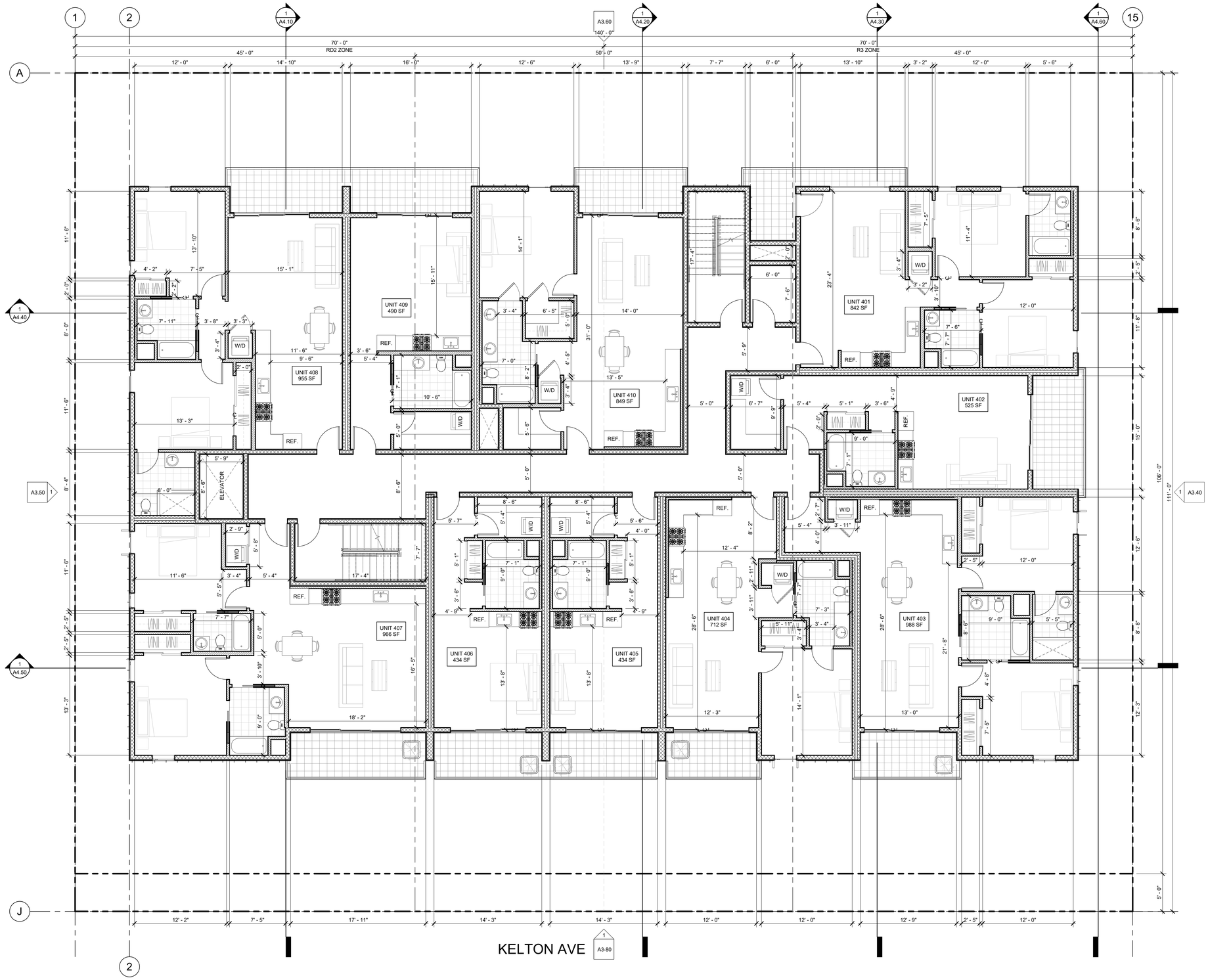
FLOORS

- A1 CONC FLOOR PER STRC. W/ POLISHED CONC. FINISH (10/A0.13)
- A2 CONC FLOOR PER STRC. W/ WOOD FINISH (11/A0.13)
- A3 CONC FLOOR PER STRC. W/ TILE FINISH (12/A0.13)
- B1 WOOD JOIST PER STRC. W/ WOOD FINISH (13/A0.13)
- B2 WOOD JOIST PER STRC. W/ TILE FINISH (14/A0.13)
- B3 WOOD JOIST W/ WOOD DECK PER SPEC. (15/A0.13)
- B4 WOOD JOIST PER STRC. W/ METAL ROOFING PER SPEC. (16/A0.13)

FLOOR PLAN LEGEND

- FLOOR TYPE
- WINDOW TAG (A0.10 - SCHEDULE)
- DOOR TAG (A0.08, A0.09 - SCHEDULE)
- WALL TYPE
- X-X" ELEVATION MARKER
- PROPERTY LINE
- ACCESSIBLE ROUTE
- 1 HR
- 2 HR
- SD SMOKE DETECTOR
- CM CARBON MONOXIDE
- EXHAUST
- S NFPA - 14 CLASS - I STANDPIPE
- ILLUMINATED EXIT SIGN

MB	MASTER BEDROOM
BD	BEDROOM
BA	BATHROOM
LR	LIVING ROOM
KI	KITCHEN
DR	DINING ROOM
PWR	POWDER ROOM
CL	CLOSET
WIC	WALK IN CLOSET
LR	LAUNDRY ROOM
BC	BALCONY
EN	ENTRY



N

FOURTH FLOOR PLAN
3/16" = 1'-0"

1

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LICENSED ARCHITECT
RAMSEY DAHAM
No. C-34257
10/31/23
RENEWAL DATE
STATE OF CALIFORNIA

3676 & 3704 KELTON AVE.
LOS ANGELES, CA 90034

Revision Schedule

Revision Number	Revision Date

PROPOSED PLANS

DRAWN	CR
CHECKED	PNK
DATE	10/31/2022 1:19:00 PM
SCALE	As indicated
JOB #	21-A001

A2.40

ASSEMBLY TYPES

WALLS

1. 2x4 & 2x6 INTERIOR WALL ASSM. (1/A0.13)
2. 2x6 PLUMBING WALL ASSM. (2/A0.13)
3. 1-HR 2x6 INTERIOR WALL ASSM. (3/A0.13)
4. 1-HR 2x6 EXTERIOR WALL ASSM. (4/A0.13)
5. 1-HR DOUBLE WALL ASSM. (5/A0.13)
6. CONC. WALL PER STRC. (6/A0.13)
7. CONC. RETAINING WALL PER STRC. (7/A0.13)
8. CMU WALL PER STRC. (8/A0.13)
9. 2-HR INTERIOR CORRIDOR WALL ASSM. (9/A0.13)

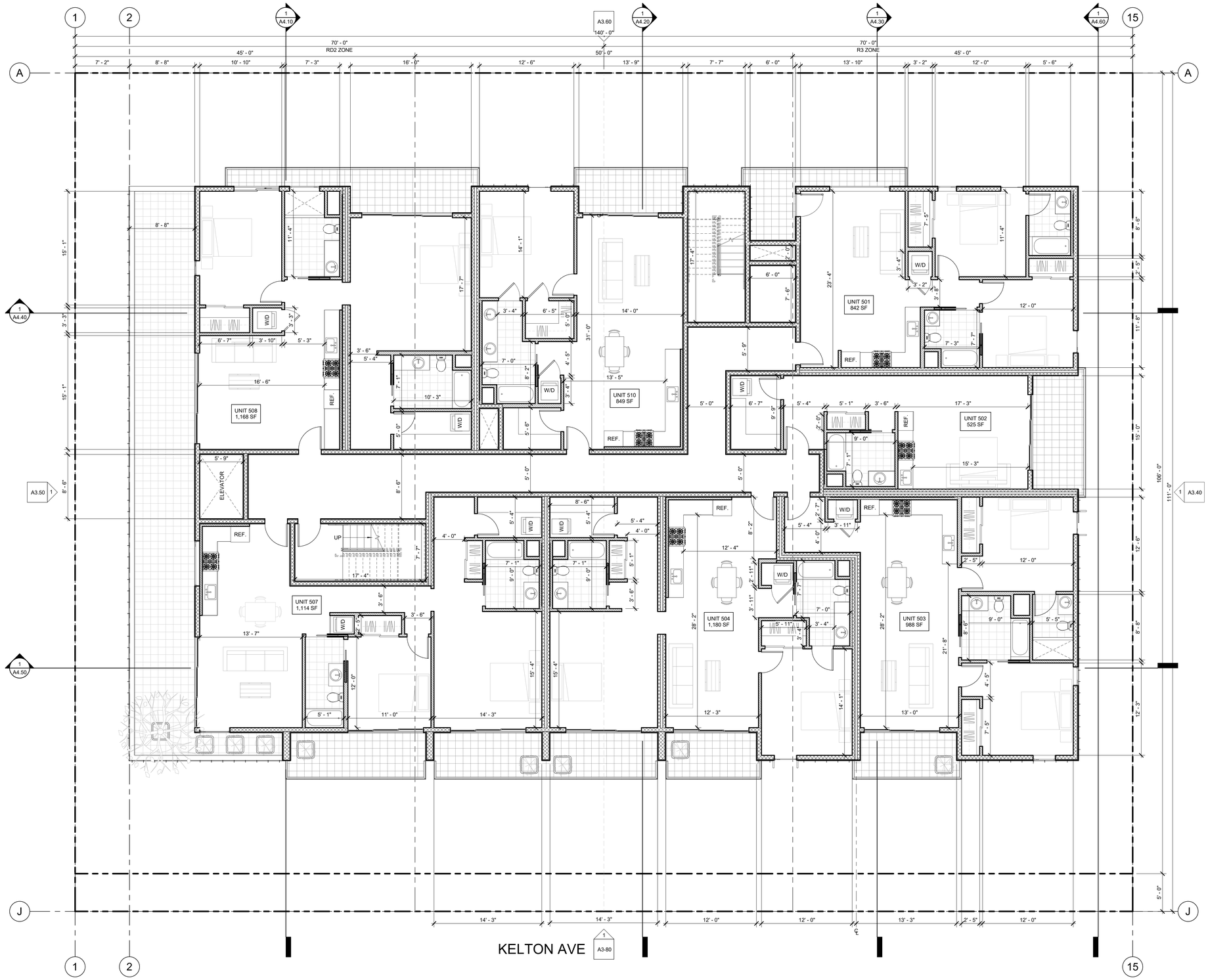
FLOORS

- A1 CONC FLOOR PER STRC. W/ POLISHED CONC. FINISH (10/A0.13)
- A2 CONC FLOOR PER STRC. W/ WOOD FINISH (11/A0.13)
- A3 CONC FLOOR PER STRC. W/ TILE FINISH (12/A0.13)
- B1 WOOD JOIST PER STRC. W/ WOOD FINISH (13/A0.13)
- B2 WOOD JOIST PER STRC. W/ TILE FINISH (14/A0.13)
- B3 WOOD JOIST W/ WOOD DECK PER SPEC. (15/A0.13)
- B4 WOOD JOIST PER STRC. W/ METAL ROOFING PER SPEC. (16/A0.13)

FLOOR PLAN LEGEND

- FLOOR TYPE
- X WINDOW TAG (A0.10 - SCHEDULE)
- XXX DOOR TAG (A0.08, A0.09 - SCHEDULE)
- X WALL TYPE
- X-X" ELEVATION MARKER
- PROPERTY LINE
- ACCESSIBLE ROUTE
- 1 HR
- 2 HR
- SD SMOKE DETECTOR
- CM CARBON MONOXIDE
- EXHAUST
- S NFPA - 14 CLASS - I STANDPIPE
- ILLUMINATED EXIT SIGN

MB	MASTER BEDROOM
BD	BEDROOM
BA	BATHROOM
LR	LIVING ROOM
KI	KITCHEN
DR	DINING ROOM
PWR	POWDER ROOM
CL	CLOSET
WIC	WALK IN CLOSET
LR	LAUNDRY ROOM
BC	BALCONY
EN	ENTRY



FIFTH FLOOR PLAN
3/16" = 1'-0"

1

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LICENSED ARCHITECT
RAMSEY DAHAM
No. C-34257
10/31/23
RENEWAL DATE
STATE OF CALIFORNIA

3676 & 3704 KELTON AVE.
LOS ANGELES, CA 90034

Revision Schedule

Revision Number	Revision Date

PROPOSED PLANS

DRAWN	CR
CHECKED	PNK
DATE	10/31/2022 1:19:06 PM
SCALE	As indicated
JOB #	21-A001

A2.50

ASSEMBLY TYPES

WALLS

1. 2x4 & 2x6 INTERIOR WALL ASSM. (1/A0.13)
2. 2x6 PLUMBING WALL ASSM. (2/A0.13)
3. 1-HR 2x6 INTERIOR WALL ASSM. (3/A0.13)
4. 1-HR 2x6 EXTERIOR WALL ASSM. (4/A0.13)
5. 1-HR DOUBLE WALL ASSM. (5/A0.13)
6. CONC. WALL PER STRC. (6/A0.13)
7. CONC. RETAINING WALL PER STRC. (7/A0.13)
8. CMU WALL PER STRC. (8/A0.13)
9. 2-HR INTERIOR CORRIDOR WALL ASSM. (9/A0.13)

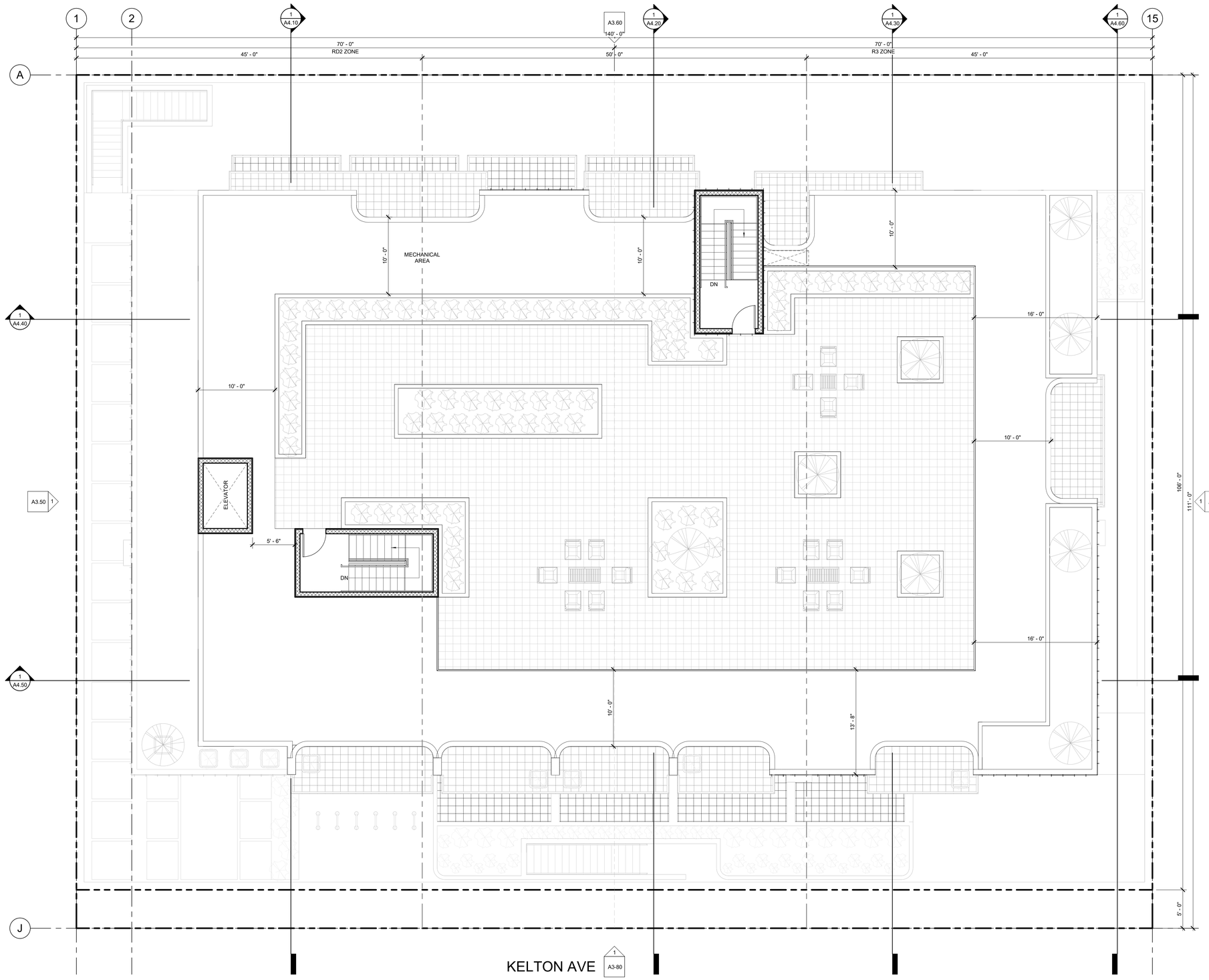
FLOORS

- A1: CONC FLOOR PER STRC. W/ POLISHED CONC. FINISH (10/A0.13)
- A2: CONC FLOOR PER STRC. W/ WOOD FINISH (11/A0.13)
- A3: CONC FLOOR PER STRC. W/ TILE FINISH (12/A0.13)
- B1: WOOD JOIST PER STRC. W/ WOOD FINISH (13/A0.13)
- B2: WOOD JOIST PER STRC. W/ TILE FINISH (14/A0.13)
- B3: WOOD JOIST W/ WOOD DECK PER SPEC. (15/A0.13)
- B4: WOOD JOIST PER STRC. W/ METAL ROOFING PER SPEC. (16/A0.13)

FLOOR PLAN LEGEND

- FLOOR TYPE
- WINDOW TAG (A0.10 - SCHEDULE)
- DOOR TAG (A0.08, A0.09 - SCHEDULE)
- WALL TYPE
- X-X" ELEVATION MARKER
- PROPERTY LINE
- ACCESSIBLE ROUTE
- 1 HR
- 2 HR
- SD SMOKE DETECTOR
- CM CARBON MONOXIDE
- EXHAUST
- S NFPA - 14 CLASS - I STANDPIPE
- ILLUMINATED EXIT SIGN

MB	MASTER BEDROOM
BD	BEDROOM
BA	BATHROOM
LR	LIVING ROOM
KI	KITCHEN
DR	DINING ROOM
PWR	POWDER ROOM
CL	CLOSET
WIC	WALK IN CLOSET
LR	LAUNDRY ROOM
BC	BALCONY
EN	ENTRY



N

ROOF PLAN
3/16" = 1'-0"

1

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LICENSED ARCHITECT
RAMSEY DAHAM
No. C-34257
10/31/23
RENEWAL DATE
STATE OF CALIFORNIA

3676 & 3704 KELTON AVE.
LOS ANGELES, CA 90034

Revision Schedule

Revision Number	Revision Date

PROPOSED PLANS

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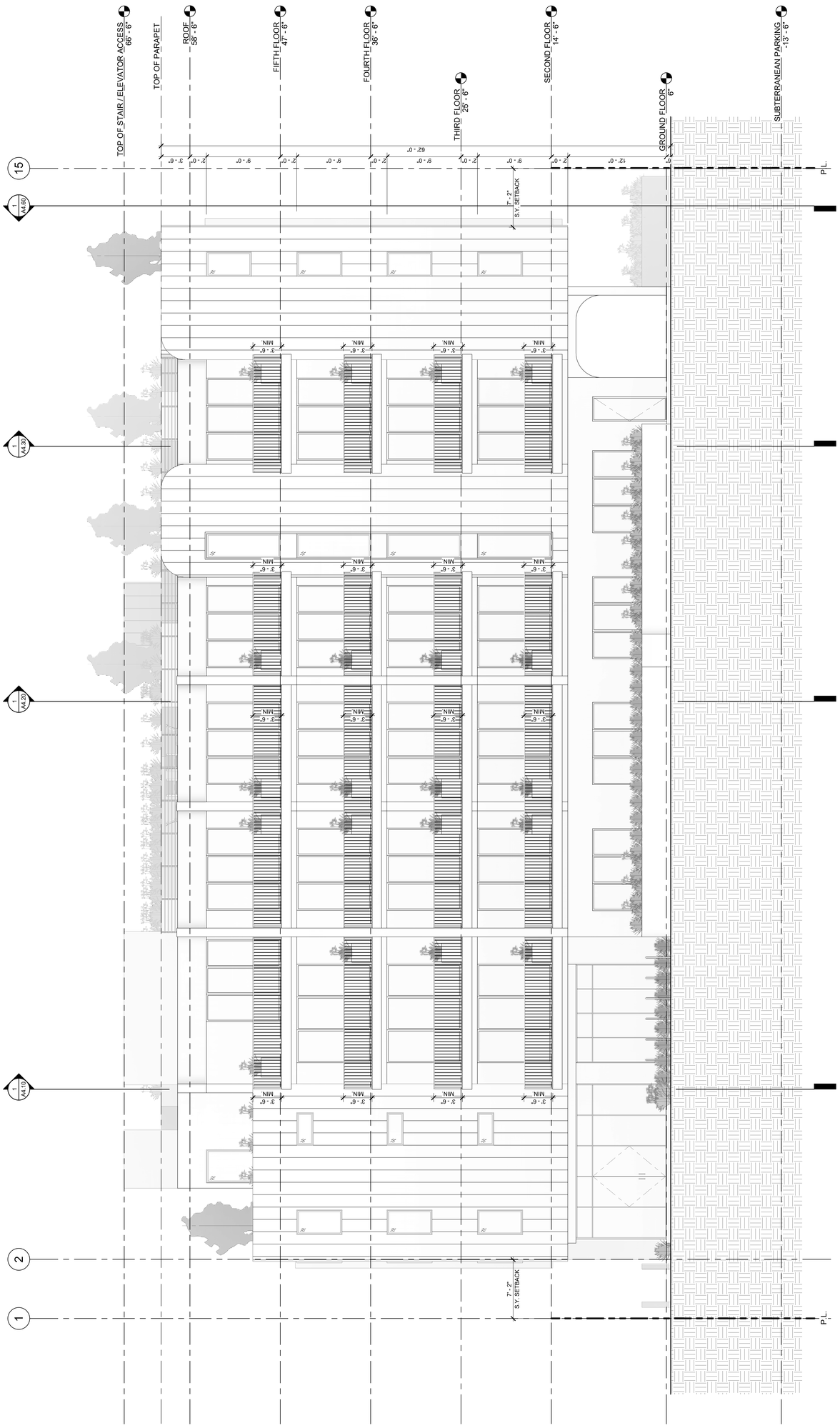
DATE	10/31/2022 1:19:11 PM
SCALE	As indicated
JOB #	21-A001

A2.60



SOUTH ELEVATION
3/16" = 1'-0"

1

A3.10

ELEVATIONS

Revision Schedule	
Revision Number	Revision Date

3676 & 3704 KELTON AVE.
LOS ANGELES, CA 90034

LICENSED ARCHITECT
RAMSEY OSKAM
No. C-34257
10/31/23
RENEWAL DATE
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DRAWN	CR
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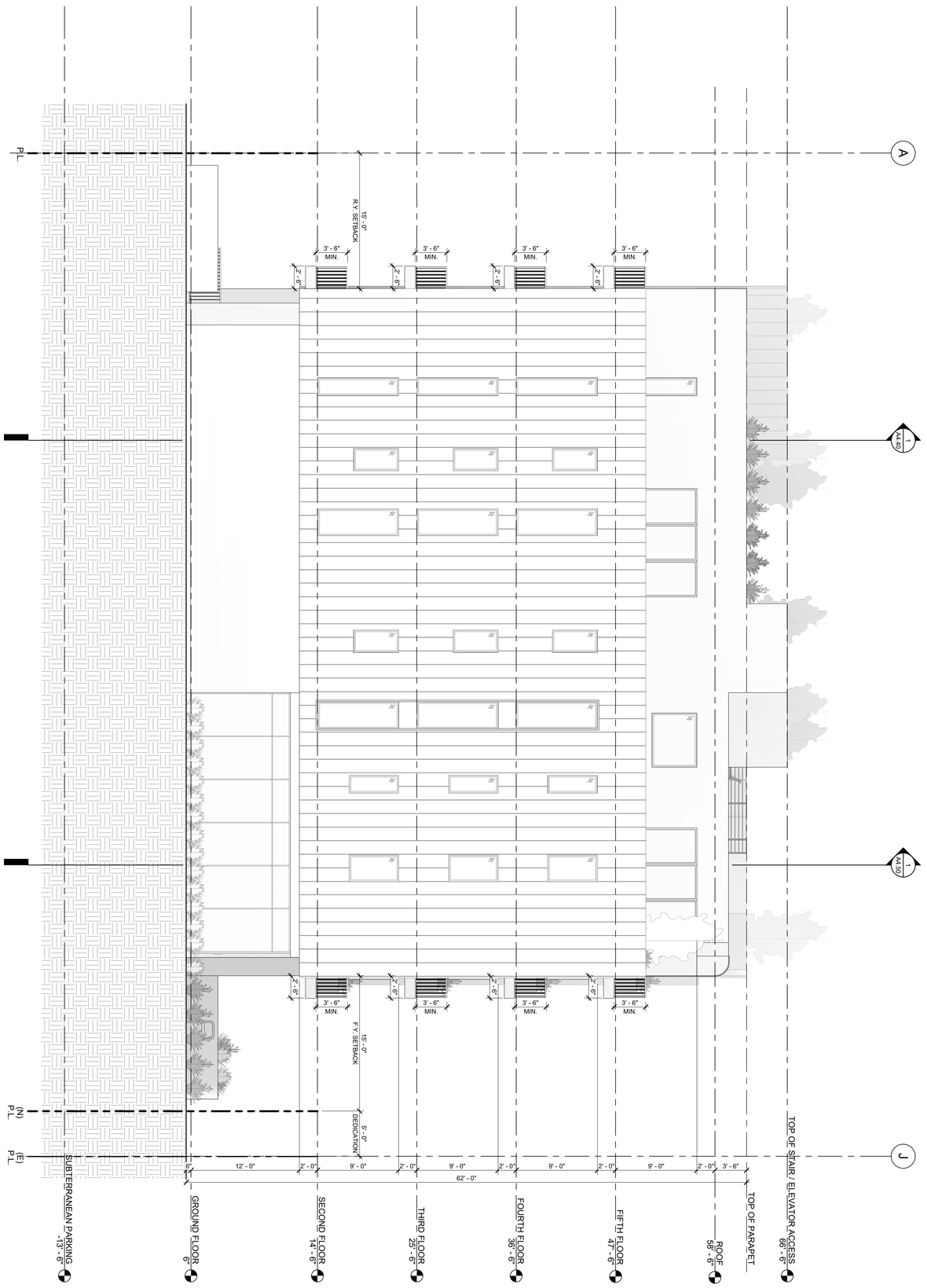
SCALE	3/16" = 1'-0"
JOB #	21-A001



WEST ELEVATION
3/16" = 1'-0"

1

A3.20	
DRAWN	CR
CHECKED	PNK
DATE	10/31/2022 1:20:53 PM
SCALE	3/16" = 1'-0"
JOB #	21-A001

ELEVATIONS

Revision Schedule	
Revision Number	Revision Date

3676 & 3704 KELTON AVE.
LOS ANGELES, CA 90034

LICENSED ARCHITECT
RAMSEY GHAMM
No. C-34231
10/31/23
RENEWAL DATE
STATE OF CALIFORNIA

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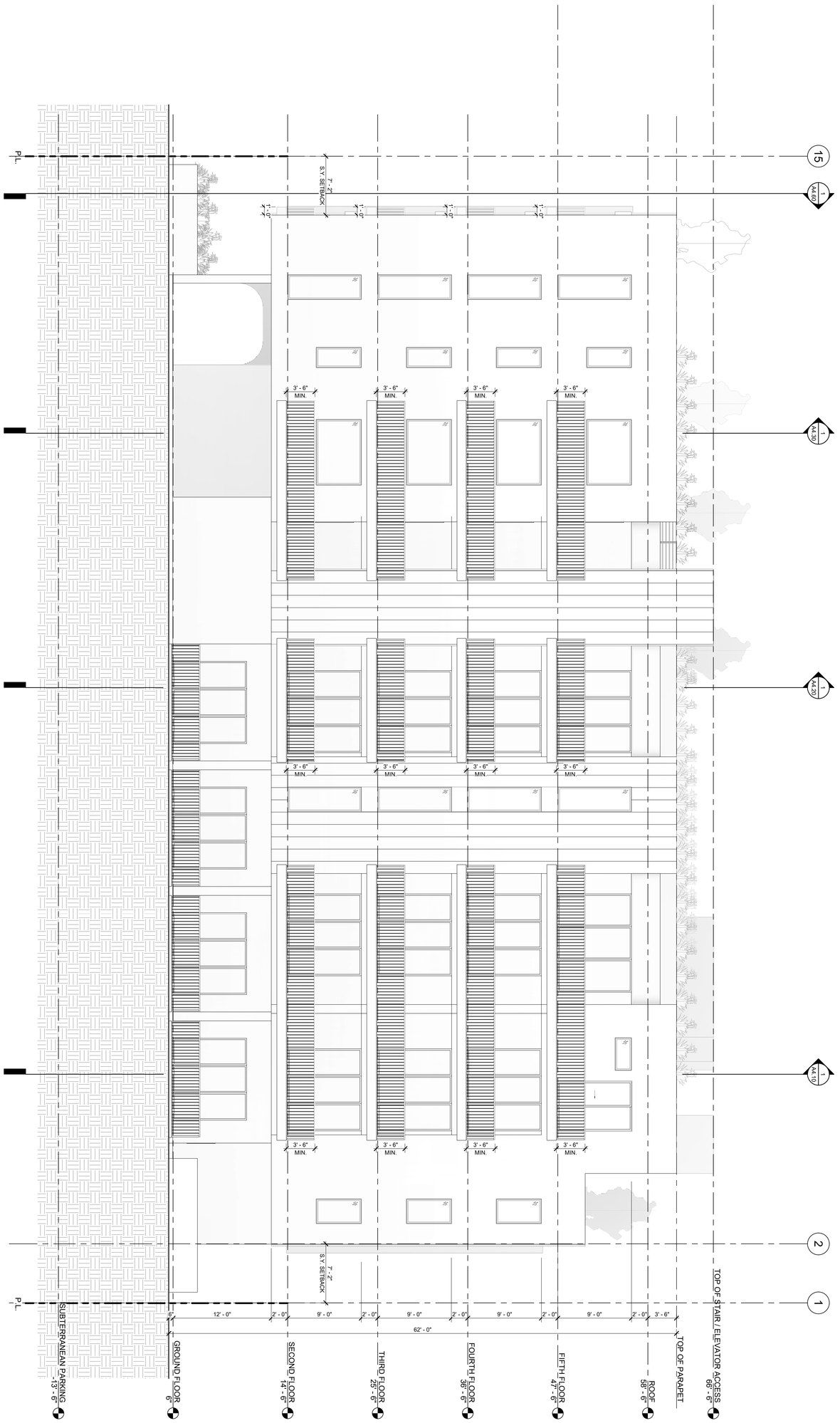

NORTH ELEVATION
3/16" = 1'-0"

1

A3.30

ELEVATIONS

DRAWN	CR
CHECKED	PNK
DATE	10/31/2022 1:21:47 PM
SCALE	3/16" = 1'-0"
JOB #	21-A001

Revision Schedule	
Revision Number	Revision Date

3676 & 3704 KELTON AVE.
LOS ANGELES, CA 90034

LICENSED ARCHITECT
RAMSEY EMAM
No. C-34301
10/31/23
STATE OF CALIFORNIA

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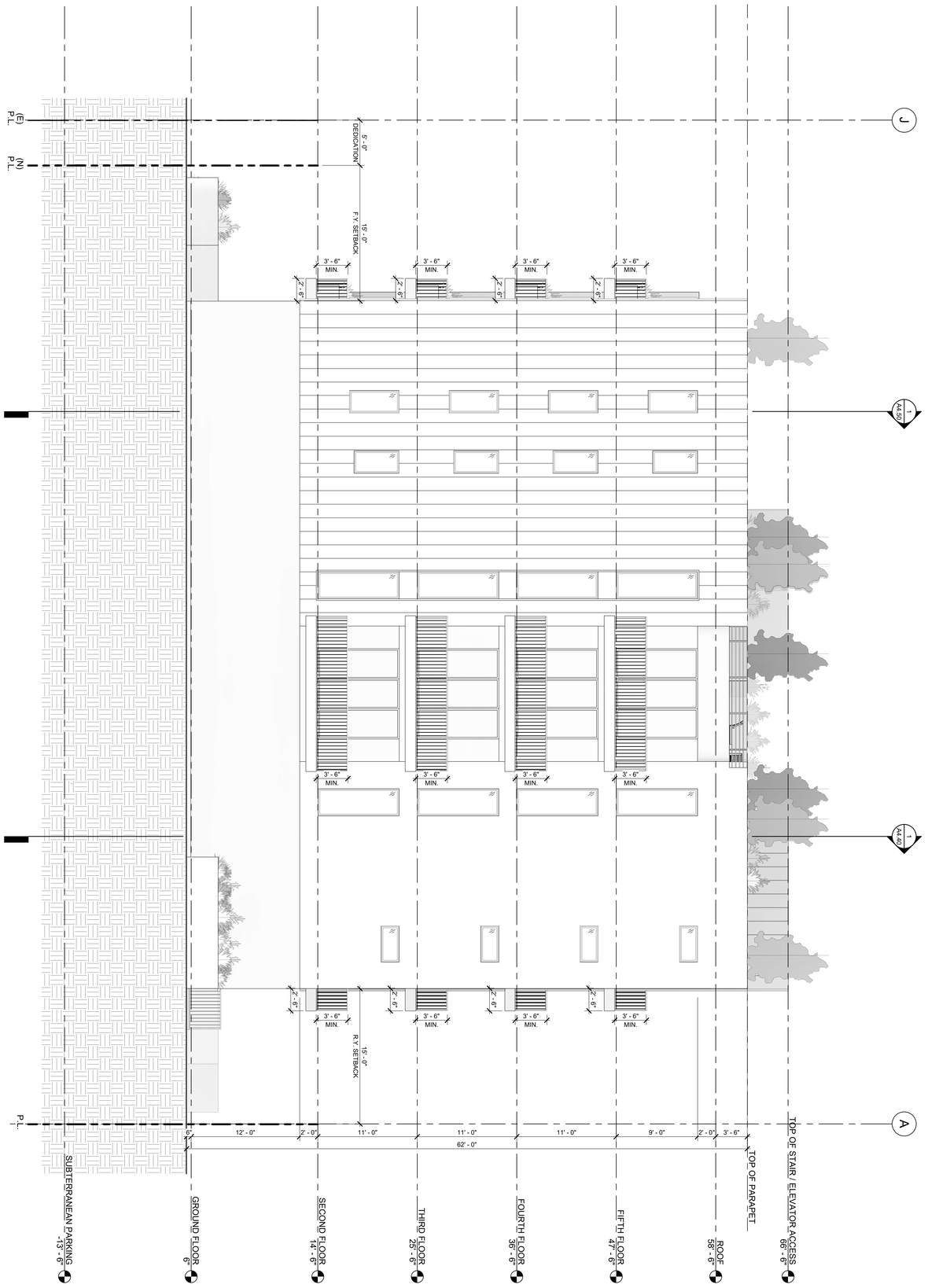
[o] 310.322.3700



EAST ELEVATION
3/16" = 1'-0"

1

A3.40

ELEVATIONS

DRAWN	CR
CHECKED	PNK
DATE	10/31/2022 1:22:43 PM
SCALE	3/16" = 1'-0"
JOB #	21-A001

Revision Schedule	
Revision Number	Revision Date

3676 & 3704 KELTON AVE.
LOS ANGELES, CA 90034

LICENSED ARCHITECT
MARIE T. CRAHAN
No. C-34297
10/31/23
RENEWAL DATE
STATE OF CALIFORNIA

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ASSEMBLY TYPES

WALLS

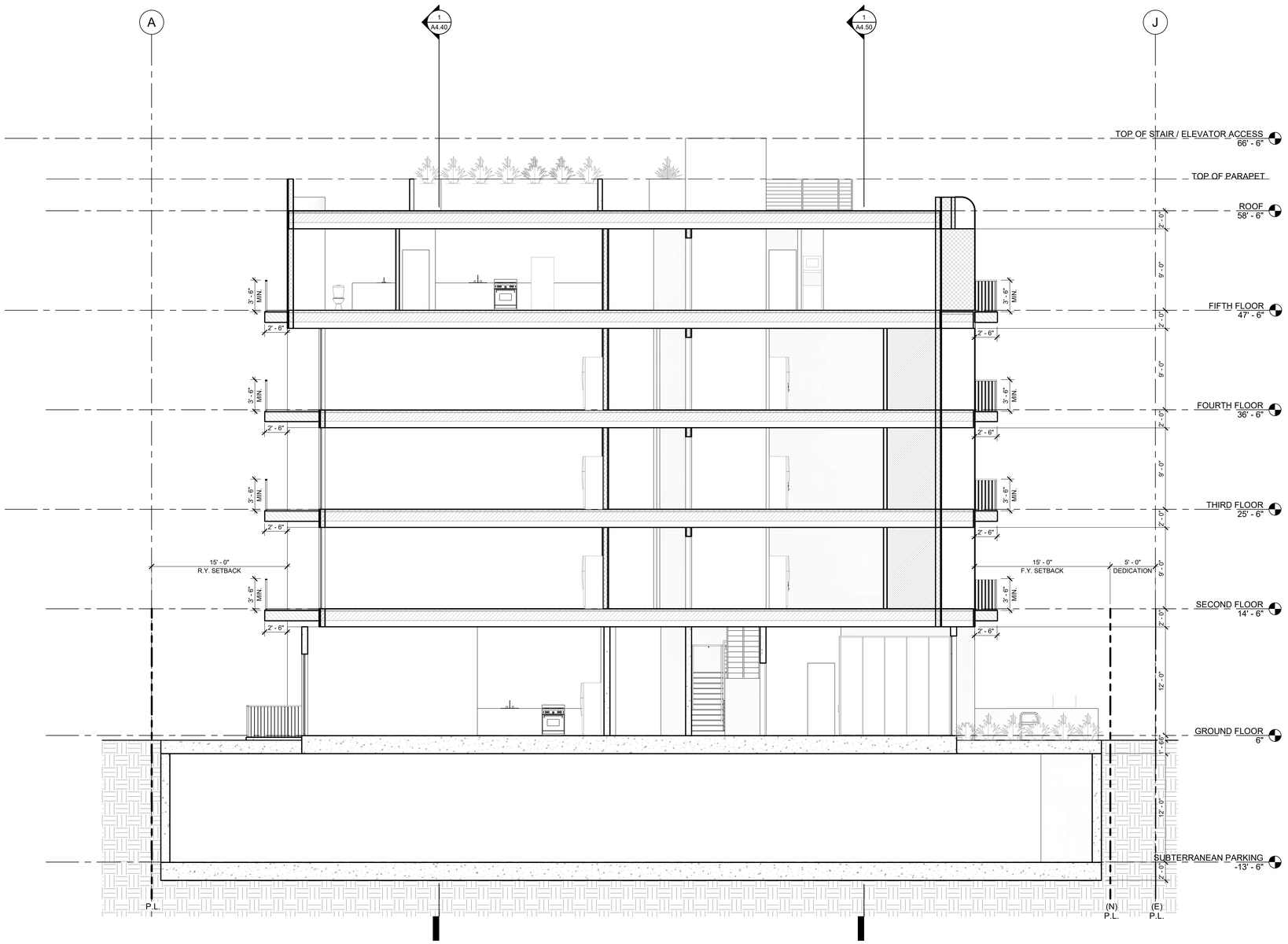
1. 2x4 & 2x6 INTERIOR WALL ASSM. (1/A0.13)
2. 2x6 PLUMBING WALL ASSM. (2/A0.13)
3. 1-HR 2x6 INTERIOR WALL ASSM. (3/A0.13)
4. 1-HR 2x6 EXTERIOR WALL ASSM. (4/A0.13)
5. 1-HR DOUBLE WALL ASSM. (5/A0.13)
6. CONC. WALL PER STRC. (6/A0.13)
7. CONC. RETAINING WALL PER STRC. (7/A0.13)
8. CMU WALL PER STRC. (8/A0.13)
9. 2-HR INTERIOR CORRIDOR WALL ASSM. (9/A0.13)

FLOORS

A-1. CONC FLOOR PER STRC. W/ POLISHED CONC. FINISH (10/A0.13)
A-2. CONC FLOOR PER STRC. W/ WOOD FINISH (11/A0.13)
A-3. CONC FLOOR PER STRC. W/ TILE FINISH (12/A0.13)
B-1. WOOD JOIST PER STRC. W/ WOOD FINISH (13/A0.13)
B-2. WOOD JOIST PER STRC. W/ TILE FINISH (14/A0.13)
B-3. WOOD JOIST W/ WOOD DECK PER SPEC. (15/A0.13)
B-4. WOOD JOIST PER STRC. W/ METAL ROOFING PER SPEC. (16/A0.13)

SECTION LEGEND

- 1 HR
- 2 HR
- X'-X" ELEVATION MARKER
- PROPERTY LINE
- EXISTING GRADE



PROPOSED SECTION A
3/16" = 1'-0"

1

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3676 & 3704 KELTON AVE.
LOS ANGELES, CA 90034

Revision Schedule

Revision Number	Revision Date

PROPOSED SECTIONS

DRAWN	CR
CHECKED	PNK
DATE	10/31/2022 1:22:54 PM
SCALE	As indicated
JOB #	21-A001

A4.10

ASSEMBLY TYPES

WALLS

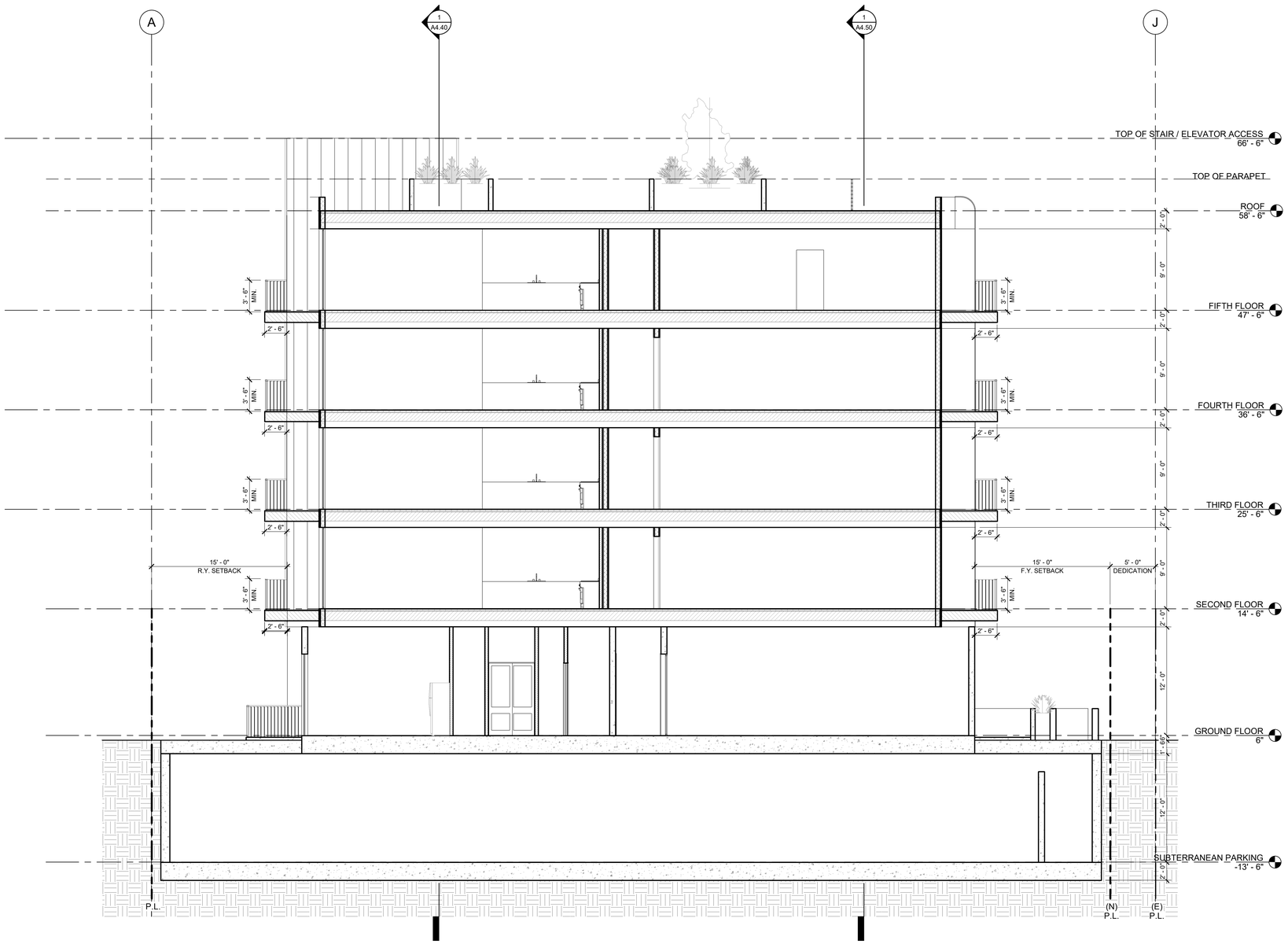
1. 2x4 & 2x6 INTERIOR WALL ASSM. (1/A0.13)
2. 2x6 PLUMBING WALL ASSM. (2/A0.13)
3. 1-HR 2x6 INTERIOR WALL ASSM. (3/A0.13)
4. 1-HR 2x6 EXTERIOR WALL ASSM. (4/A0.13)
5. 1-HR DOUBLE WALL ASSM. (5/A0.13)
6. CONC. WALL PER STRC. (6/A0.13)
7. CONC. RETAINING WALL PER STRC. (7/A0.13)
8. CMU WALL PER STRC. (8/A0.13)
9. 2-HR INTERIOR CORRIDOR WALL ASSM. (9/A0.13)

FLOORS

- A.1 CONC FLOOR PER STRC. W/ POLISHED CONC. FINISH (10/A0.13)
- A.2 CONC FLOOR PER STRC. W/ WOOD FINISH (11/A0.13)
- A.3 CONC FLOOR PER STRC. W/ TILE FINISH (12/A0.13)
- B.1 WOOD JOIST PER STRC. W/ WOOD FINISH (13/A0.13)
- B.2 WOOD JOIST PER STRC. W/ TILE FINISH (14/A0.13)
- B.3 WOOD JOIST W/ WOOD DECK PER SPEC. (15/A0.13)
- B.4 WOOD JOIST PER STRC. W/ METAL ROOFING PER SPEC. (16/A0.13)

SECTION LEGEND

- 1 HR
- 2 HR
- X'-X" ELEVATION MARKER
- PROPERTY LINE
- EXISTING GRADE



PROPOSED SECTION B
3/16" = 1'-0"

1

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LICENSED ARCHITECT
RAMSEY DAHAM
No. C-34257
10/31/23
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STATE OF CALIFORNIA

3676 & 3704 KELTON AVE.
LOS ANGELES, CA 90034

Revision Schedule

Revision Number	Revision Date

PROPOSED SECTIONS

DRAWN	CR
CHECKED	PNK
DATE	10/31/2022 1:23:02 PM
SCALE	As indicated
JOB #	21-A001

A4.20

ASSEMBLY TYPES

WALLS

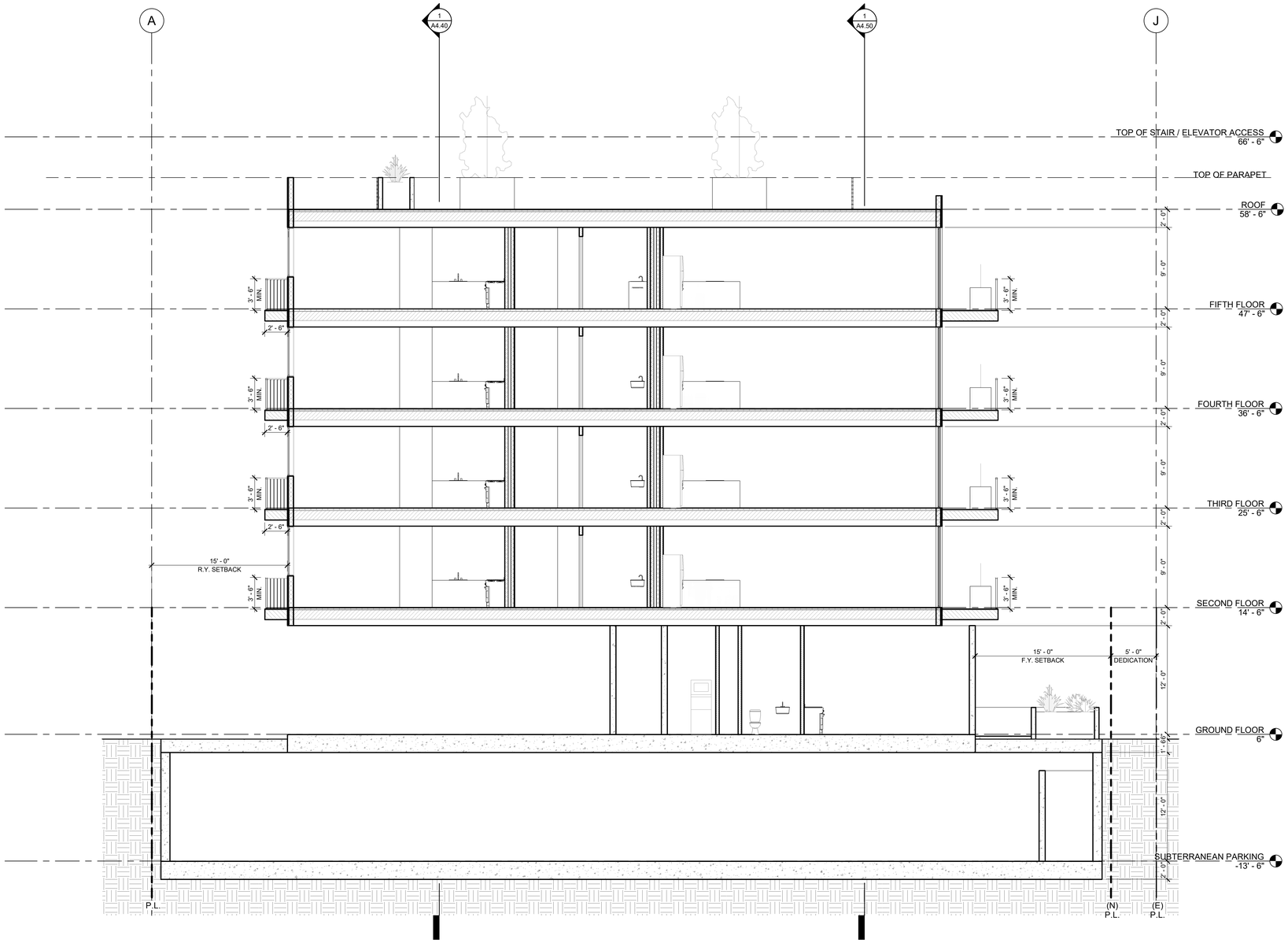
1. 2x4 & 2x6 INTERIOR WALL ASSM. (1/A0.13)
2. 2x6 PLUMBING WALL ASSM. (2/A0.13)
3. 1-HR 2x6 INTERIOR WALL ASSM. (3/A0.13)
4. 1-HR 2x6 EXTERIOR WALL ASSM. (4/A0.13)
5. 1-HR DOUBLE WALL ASSM. (5/A0.13)
6. CONC. WALL PER STRC. (6/A0.13)
7. CONC. RETAINING WALL PER STRC. (7/A0.13)
8. CMU WALL PER STRC. (8/A0.13)
9. 2-HR INTERIOR CORRIDOR WALL ASSM. (9/A0.13)

FLOORS

A.1. CONC FLOOR PER STRC. W/ POLISHED CONC. FINISH (10/A0.13)
A.2. CONC FLOOR PER STRC. W/ WOOD FINISH (11/A0.13)
A.3. CONC FLOOR PER STRC. W/ TILE FINISH (12/A0.13)
B.1. WOOD JOIST PER STRC. W/ WOOD FINISH (13/A0.13)
B.2. WOOD JOIST PER STRC. W/ TILE FINISH (14/A0.13)
B.3. WOOD JOIST W/ WOOD DECK PER SPEC. (15/A0.13)
B.4. WOOD JOIST PER STRC. W/ METAL ROOFING PER SPEC. (16/A0.13)

SECTION LEGEND

- 1 HR
- 2 HR
- X'-X" ELEVATION MARKER
- PROPERTY LINE
- EXISTING GRADE



PROPOSED SECTION C
3/16" = 1'-0"

1

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LOS ANGELES, CA 90034

Revision Schedule

Revision Number	Revision Date

PROPOSED SECTIONS

DRAWN	CR
CHECKED	PNK
DATE	10/31/2022 1:23:11 PM
SCALE	As indicated
JOB #	21-A001

A4.30

ASSEMBLY TYPES

WALLS

1. 2x4 & 2x6 INTERIOR WALL ASSM. (1/A0.13)
2. 2x6 PLUMBING WALL ASSM. (2/A0.13)
3. 1-HR 2x6 INTERIOR WALL ASSM. (3/A0.13)
4. 1-HR 2x6 EXTERIOR WALL ASSM. (4/A0.13)
5. 1-HR DOUBLE WALL ASSM. (5/A0.13)
6. CONC. WALL PER STRC. (6/A0.13)
7. CONC. RETAINING WALL PER STRC. (7/A0.13)
8. CMU WALL PER STRC. (8/A0.13)
9. 2-HR INTERIOR CORRIDOR WALL ASSM. (9/A0.13)

FLOORS

A1. CONC FLOOR PER STRC. W/ POLISHED CONC. FINISH (10/A0.13)
A2. CONC FLOOR PER STRC. W/ WOOD FINISH (11/A0.13)
A3. CONC FLOOR PER STRC. W/ TILE FINISH (12/A0.13)
B1. WOOD JOIST PER STRC. W/ WOOD FINISH (13/A0.13)
B2. WOOD JOIST PER STRC. W/ TILE FINISH (14/A0.13)
C1. WOOD JOIST W/ WOOD DECK PER SPEC. (15/A0.13)
C2. WOOD JOIST PER STRC. W/ METAL ROOFING PER SPEC. (16/A0.13)

SECTION LEGEND

- 1 HR
- 2 HR
- X'-X" ELEVATION MARKER
- PROPERTY LINE
- EXISTING GRADE

RAMP SECTION
3/16" = 1'-0"

1

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LICENSED ARCHITECT
RAMSEY DAHAM
No. C-34257
10/31/23
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Revision Schedule

Revision Number	Revision Date

PROPOSED SECTIONS

DRAWN	CR
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DATE	10/31/2022 1:24:14 PM
SCALE	As indicated
JOB #	21-A001

A4.60

ASSEMBLY TYPES

WALLS

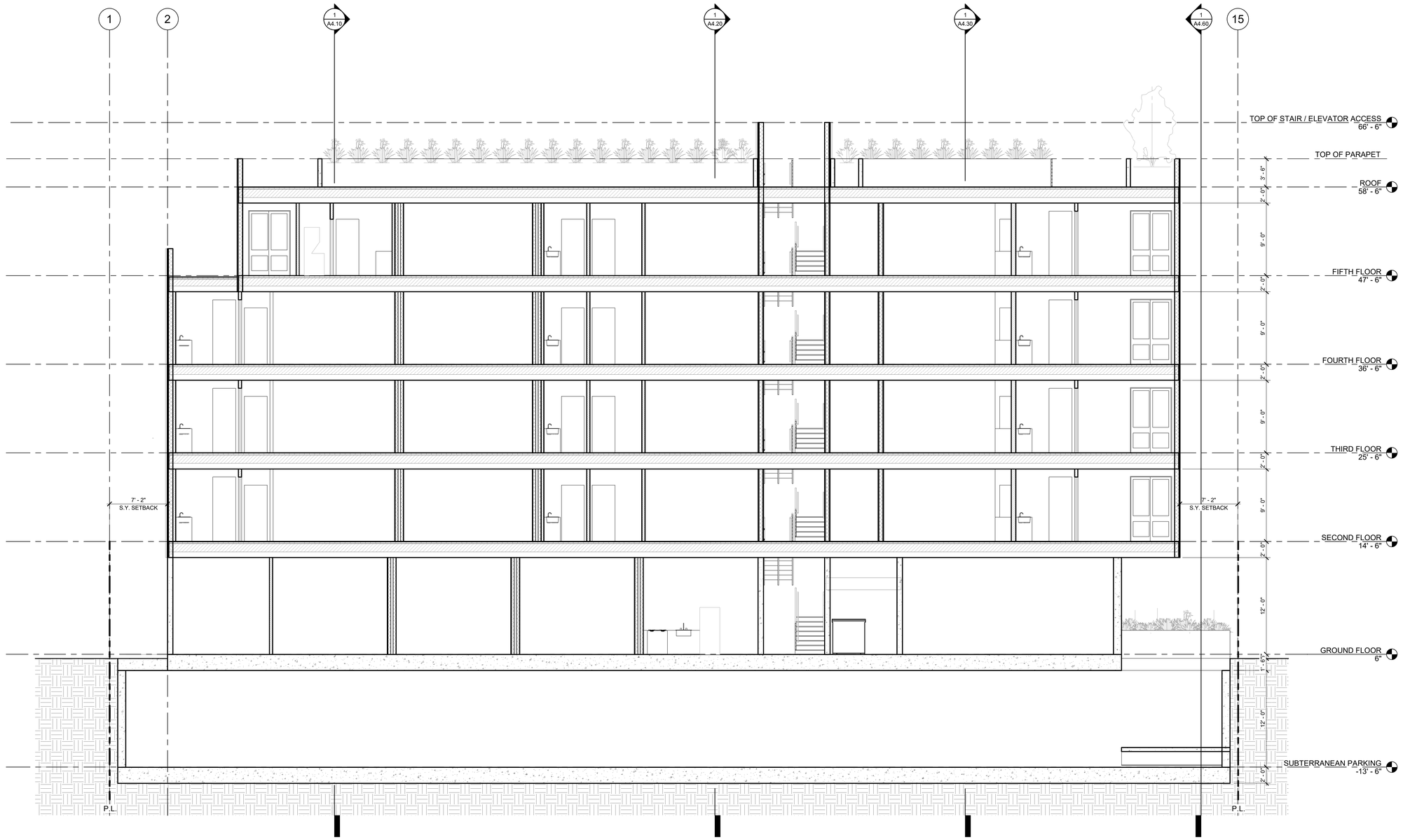
1. 2x4 & 2x6 INTERIOR WALL ASSM. (1/A0.13)
2. 2x6 PLUMBING WALL ASSM. (2/A0.13)
3. 1-HR 2x6 INTERIOR WALL ASSM. (3/A0.13)
4. 1-HR 2x6 EXTERIOR WALL ASSM. (4/A0.13)
5. 1-HR DOUBLE WALL ASSM. (5/A0.13)
6. CONC. WALL PER STRC. (6/A0.13)
7. CONC. RETAINING WALL PER STRC. (7/A0.13)
8. CMU WALL PER STRC. (8/A0.13)
9. 2-HR INTERIOR CORRIDOR WALL ASSM. (9/A0.13)

FLOORS

A-1. CONC FLOOR PER STRC. W/ POLISHED CONC. FINISH (10/A0.13)
A-2. CONC FLOOR PER STRC. W/ WOOD FINISH (11/A0.13)
A-3. CONC FLOOR PER STRC. W/ TILE FINISH (12/A0.13)
B-1. WOOD JOIST PER STRC. W/ WOOD FINISH (13/A0.13)
B-2. WOOD JOIST PER STRC. W/ TILE FINISH (14/A0.13)
B-3. WOOD JOIST W/ WOOD DECK PER SPEC. (15/A0.13)
B-4. WOOD JOIST PER STRC. W/ METAL ROOFING PER SPEC. (16/A0.13)

SECTION LEGEND

- 1 HR
- 2 HR
- X'-X" ELEVATION MARKER
- PROPERTY LINE
- EXISTING GRADE



PROPOSED SECTION D
3/16" = 1'-0"

1

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No. C-34257
10/31/23
RENEWAL DATE
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LOS ANGELES, CA 90034

Revision Schedule

Revision Number	Revision Date

PROPOSED SECTIONS

DRAWN	CR
CHECKED	PNK
DATE	10/31/2022 1:24:21 PM
SCALE	As indicated
JOB #	21-A001

A4.40

ASSEMBLY TYPES

WALLS

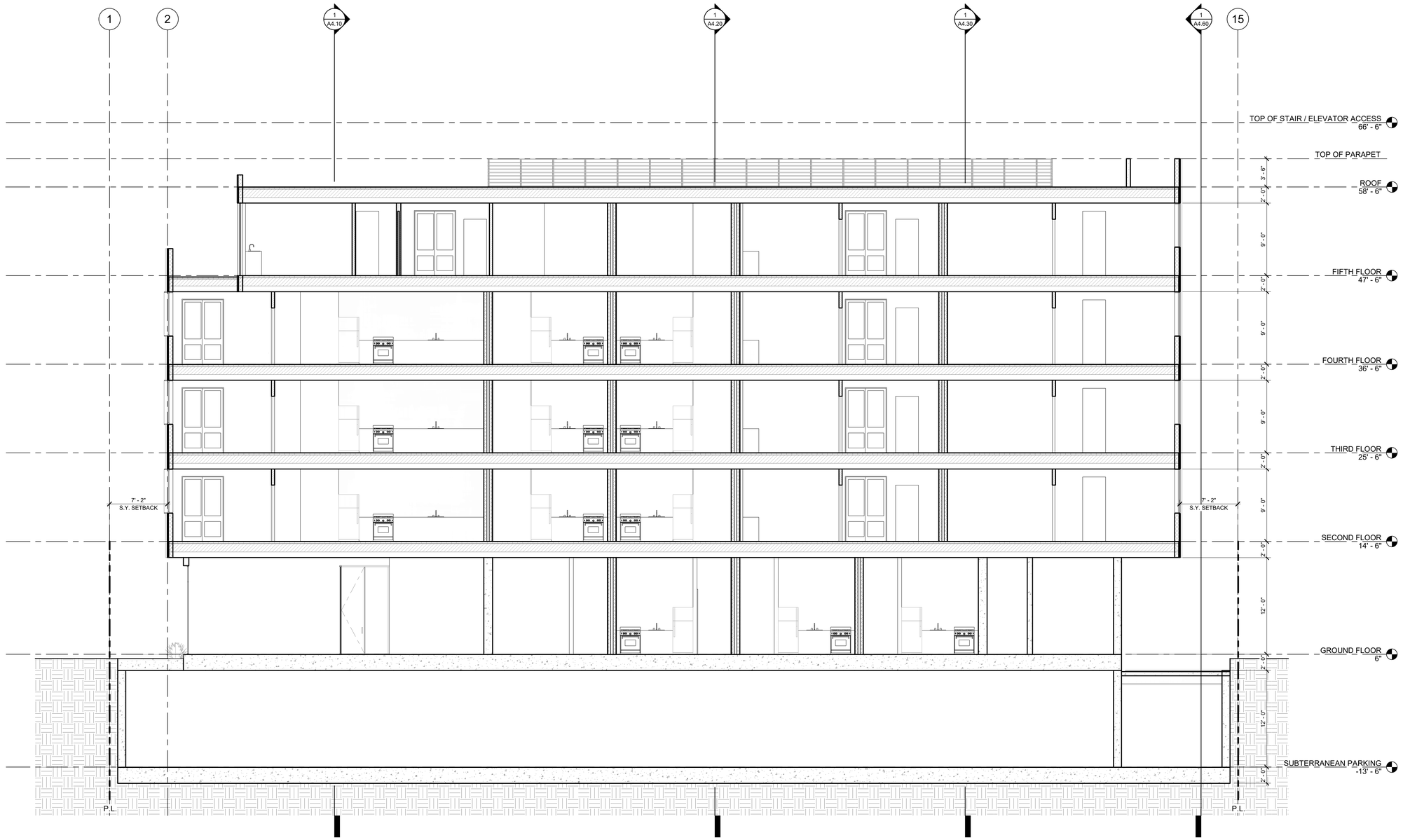
1. 2x4 & 2x6 INTERIOR WALL ASSM. (1/A0.13)
2. 2x6 PLUMBING WALL ASSM. (2/A0.13)
3. 1-HR 2x6 INTERIOR WALL ASSM. (3/A0.13)
4. 1-HR 2x6 EXTERIOR WALL ASSM. (4/A0.13)
5. 1-HR DOUBLE WALL ASSM. (5/A0.13)
6. CONC. WALL PER STRC. (6/A0.13)
7. CONC. RETAINING WALL PER STRC. (7/A0.13)
8. CMU WALL PER STRC. (8/A0.13)
9. 2-HR INTERIOR CORRIDOR WALL ASSM. (9/A0.13)

FLOORS

A-1. CONC FLOOR PER STRC. W/ POLISHED CONC. FINISH (10/A0.13)
A-2. CONC FLOOR PER STRC. W/ WOOD FINISH (11/A0.13)
A-3. CONC FLOOR PER STRC. W/ TILE FINISH (12/A0.13)
B-1. WOOD JOIST PER STRC. W/ WOOD FINISH (13/A0.13)
B-2. WOOD JOIST PER STRC. W/ TILE FINISH (14/A0.13)
B-3. WOOD JOIST W/ WOOD DECK PER SPEC. (15/A0.13)
B-4. WOOD JOIST PER STRC. W/ METAL ROOFING PER SPEC. (16/A0.13)

SECTION LEGEND

- 1 HR
- 2 HR
- X'-X" ELEVATION MARKER
- PROPERTY LINE
- EXISTING GRADE



PROPOSED SECTION E
3/16" = 1'-0"

1

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LICENSED ARCHITECT
RAMSEY DAHAM
No. C-34257
10/31/23
RENEWAL DATE
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LOS ANGELES, CA 90034

Revision Schedule

Revision Number	Revision Date

PROPOSED SECTIONS

DRAWN	CR
CHECKED	PNK
DATE	10/31/2022 1:24:38 PM
SCALE	As indicated
JOB #	21-A001

A4.50

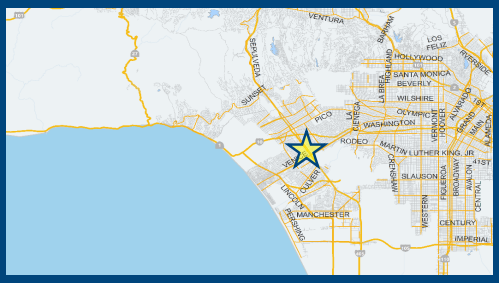
Attachment B

CITY OF LOS ANGELES VMT CALCULATOR Version 1.3

Project Screening Criteria: Is this project required to conduct a vehicle miles traveled analysis?

Project Information

Project: 3704 Kelton Ave
Scenario:
Address: 3704 KELTON AV, 90034



Is the project replacing an existing number of residential units with a smaller number of residential units AND is located within one-half mile of a fixed-rail or fixed-guideway transit station?

○ Yes ● No

Existing Land Use

Land Use Type	Value	Unit
Housing | Multi-Family	4	DU

☐ Click here to add a single custom land use type (will be included in the above list)

Proposed Project Land Use

Land Use Type	Value	Unit
Housing | Affordable Housing - Family	11	DU
Housing | Multi-Family	32	DU
Housing | Affordable Housing - Family	11	DU

☐ Click here to add a single custom land use type (will be included in the above list)

Project Screening Summary

Existing Land Use	Proposed Project
20 Daily Vehicle Trips	200 Daily Vehicle Trips
128 Daily VMT	1,261 Daily VMT

Tier 1 Screening Criteria

Project will have less residential units compared to existing residential units & is within one-half mile of a fixed-rail station. ☐

Tier 2 Screening Criteria

The net increase in daily trips < 250 trips	180 Net Daily Trips
The net increase in daily VMT ≤ 0	1,133 Net Daily VMT
The proposed project consists of only retail land uses ≤ 50,000 square feet total.	0.000 ksf

The proposed project is not required to perform VMT analysis.

Measuring the Miles

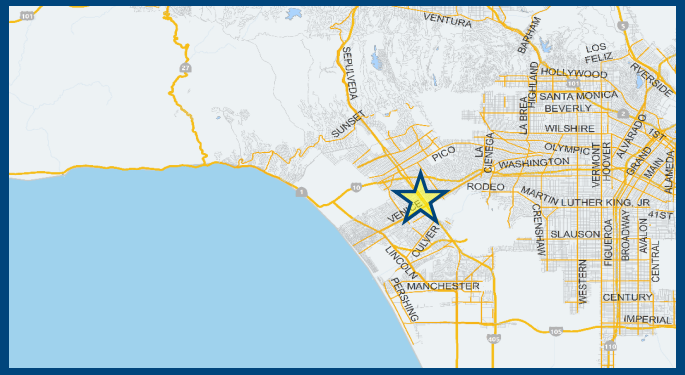
CITY OF LOS ANGELES VMT CALCULATOR Version 1.3

Project Information

Project: 3704 Kelton Ave

Scenario:

Address: 3704 KELTON AV, 90034



Proposed Project Land Use Type	Value	Unit
Housing | Multi-Family	32	DU
Housing | Affordable Housing - Family	11	DU

TDM Strategies

Select each section to show individual strategies

Use ☑ to denote if the TDM strategy is part of the proposed project or is a mitigation strategy

	Proposed Project	With Mitigation
Max Home Based TDM Achieved?	No	No
Max Work Based TDM Achieved?	No	No

A Parking

Reduce Parking Supply

☑ Proposed Prj ☐ Mitigation

65 city code parking provision for the project site

26 actual parking provision for the project site

Unbundle Parking

☑ Proposed Prj ☐ Mitigation

100 monthly parking cost (dollar) for the project site

Parking Cash-Out

☐ Proposed Prj ☐ Mitigation

50 percent of employees eligible

Price Workplace Parking

☐ Proposed Prj ☐ Mitigation

6.00 daily parking charge (dollar)

50 percent of employees subject to priced parking

Residential Area Parking Permits

☐ Proposed Prj ☐ Mitigation

200 cost (dollar) of annual permit

B Transit

C Education & Encouragement

D Commute Trip Reductions

E Shared Mobility

F Bicycle Infrastructure

G Neighborhood Enhancement

Analysis Results

Proposed Project	With Mitigation
162 Daily Vehicle Trips	**162** Daily Vehicle Trips
1,033 Daily VMT	**1,033** Daily VMT
N/A Household VMT per Capita	**N/A** Household VMT per Capita
N/A Work VMT per Employee	**N/A** Work VMT per Employee

Significant VMT Impact?

Proposed Project	With Mitigation
Household: N/A Threshold = 7.4 15% Below APC	**Household: N/A** Threshold = 7.4 15% Below APC
Work: N/A Threshold = 11.1 15% Below APC	**Work: N/A** Threshold = 11.1 15% Below APC

Measuring the Miles

Attachment C

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3704 Kelton Apartments - Los Angeles-South Coast County, Annual

EMFAC Off-Model Adjustment Factors for Gasoline Light Duty Vehicle to Account for the SAFE Vehicle Rule Not Applied

3704 Kelton Apartments

Los Angeles-South Coast County, Annual

1.0 Project Characteristics

1.1 Land Usage

Land Uses	Size	Metric	Lot Acreage	Floor Surface Area	Population
Apartments Mid Rise	43.00	Dwelling Unit	0.35	43,349.00	123

1.2 Other Project Characteristics

Urbanization	Urban	**Wind Speed (m/s)**	2.2	**Precipitation Freq (Days)**	33
Climate Zone	11			**Operational Year**	2025
Utility Company	Los Angeles Department of Water & Power				
CO2 Intensity (lb/MWhr)	691.98	**CH4 Intensity (lb/MWhr)**	0.033	**N2O Intensity (lb/MWhr)**	0.004

1.3 User Entered Comments & Non-Default Data

Project Characteristics -
Land Use - known lot acreage and building square footage
Demolition -
Grading -
Architectural Coating - known building exterior area
Woodstoves - no hearths or fireplaces in project
Area Coating - known building interior and exterior square footage
Land Use Change -
Sequestration -

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EMFAC Off-Model Adjustment Factors for Gasoline Light Duty Vehicle to Account for the SAFE Vehicle Rule Not Applied

Table Name	Column Name	Default Value	New Value
tblArchitecturalCoating	ConstArea_Residential_Exterior	29,261.00	17,177.00
tblArchitecturalCoating	ConstArea_Residential_Interior	87,782.00	43,349.00
tblAreaCoating	Area_Residential_Exterior	29261	17177
tblAreaCoating	Area_Residential_Interior	87782	43349
tblFireplaces	NumberGas	36.55	0.00
tblFireplaces	NumberNoFireplace	4.30	0.00
tblFireplaces	NumberWood	2.15	0.00
tblGrading	MaterialExported	0.00	6,941.00
tblLandUse	LandUseSquareFeet	43,000.00	43,349.00
tblLandUse	LotAcreage	1.13	0.35
tblSequestration	NumberOfNewTrees	0.00	12.00
tblWoodstoves	NumberCatalytic	2.15	0.00
tblWoodstoves	NumberNoncatalytic	2.15	0.00

2.0 Emissions Summary

2.1 Overall Construction

Unmitigated Construction

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Year	tons/yr										MT/yr					
2023	0.1147	0.4550	0.4975	1.1500e-003	0.0349	0.0193	0.0542	0.0103	0.0179	0.0281	0.0000	104.8056	104.8056	0.0204	5.1200e-003	106.8400

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Maximum	**0.1147**	**0.4550**	**0.4975**	**1.1500e-003**	**0.0349**	**0.0193**	**0.0542**	**0.0103**	**0.0179**	**0.0281**	**0.0000**	**104.8056**	**104.8056**	**0.0204**	**5.1200e-003**	**106.8400**

Mitigated Construction

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Year	tons/yr										MT/yr					
2023	0.1147	0.4550	0.4975	1.1500e-003	0.0349	0.0193	0.0542	0.0103	0.0179	0.0281	0.0000	104.8055	104.8055	0.0204	5.1200e-003	106.8400
Maximum	**0.1147**	**0.4550**	**0.4975**	**1.1500e-003**	**0.0349**	**0.0193**	**0.0542**	**0.0103**	**0.0179**	**0.0281**	**0.0000**	**104.8055**	**104.8055**	**0.0204**	**5.1200e-003**	**106.8400**

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio-CO2	Total CO2	CH4	N20	CO2e
Percent Reduction	**0.00**	**0.00**	**0.00**	**0.00**	**0.00**	**0.00**	**0.00**	**0.00**	**0.00**	**0.00**	**0.00**	**0.00**	**0.00**	**0.00**	**0.00**	**0.00**

Quarter	Start Date	End Date	Maximum Unmitigated ROG + NOX (tons/quarter)	Maximum Mitigated ROG + NOX (tons/quarter)
1	**6-1-2023**	**8-31-2023**	0.3273	0.3273
2	**9-1-2023**	**9-30-2023**	0.0795	0.0795
		Highest	0.3273	0.3273

2.2 Overall Operational

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Unmitigated Operational

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category	tons/yr										MT/yr					
Area	0.1770	5.1000e-003	0.4430	2.0000e-005		2.4600e-003	2.4600e-003		2.4600e-003	2.4600e-003	0.0000	0.7244	0.7244	6.9000e-004	0.0000	0.7417
Energy	2.0900e-003	0.0179	7.6000e-003	1.1000e-004		1.4400e-003	1.4400e-003		1.4400e-003	1.4400e-003	0.0000	72.3769	72.3769	2.8600e-003	6.8000e-004	72.6505
Mobile	0.1145	0.1290	1.1816	2.6100e-003	0.2855	1.9000e-003	0.2874	0.0762	1.7700e-003	0.0779	0.0000	241.5999	241.5999	0.0166	0.0105	245.1277
Waste						0.0000	0.0000		0.0000	0.0000	4.0152	0.0000	4.0152	0.2373	0.0000	9.9474
Water						0.0000	0.0000		0.0000	0.0000	0.8888	17.6094	18.4982	0.0921	2.2600e-003	21.4742
Total	**0.2935**	**0.1519**	**1.6322**	**2.7400e-003**	**0.2855**	**5.8000e-003**	**0.2913**	**0.0762**	**5.6700e-003**	**0.0818**	**4.9040**	**332.3106**	**337.2146**	**0.3496**	**0.0134**	**349.9415**

Mitigated Operational

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category	tons/yr										MT/yr					
Area	0.1770	5.1000e-003	0.4430	2.0000e-005		2.4600e-003	2.4600e-003		2.4600e-003	2.4600e-003	0.0000	0.7244	0.7244	6.9000e-004	0.0000	0.7417

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EMFAC Off-Model Adjustment Factors for Gasoline Light Duty Vehicle to Account for the SAFE Vehicle Rule Not Applied

Energy	2.0900e-003	0.0179	7.6000e-003	1.1000e-004		1.4400e-003	1.4400e-003		1.4400e-003	1.4400e-003	0.0000	72.3769	72.3769	2.8600e-003	6.8000e-004	72.6505
Mobile	0.1145	0.1290	1.1816	2.6100e-003	0.2855	1.9000e-003	0.2874	0.0762	1.7700e-003	0.0779	0.0000	241.5999	241.5999	0.0166	0.0105	245.1277
Waste						0.0000	0.0000		0.0000	0.0000	4.0152	0.0000	4.0152	0.2373	0.0000	9.9474
Water						0.0000	0.0000		0.0000	0.0000	0.8888	17.6094	18.4982	0.0921	2.2600e-003	21.4742
Total	**0.2935**	**0.1519**	**1.6322**	**2.7400e-003**	**0.2855**	**5.8000e-003**	**0.2913**	**0.0762**	**5.6700e-003**	**0.0818**	**4.9040**	**332.3106**	**337.2146**	**0.3496**	**0.0134**	**349.9415**

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio-CO2	Total CO2	CH4	N20	CO2e
Percent Reduction	**0.00**	**0.00**	**0.00**	**0.00**	**0.00**	**0.00**	**0.00**	**0.00**	**0.00**	**0.00**	**0.00**	**0.00**	**0.00**	**0.00**	**0.00**	**0.00**

2.3 Vegetation

Vegetation

	CO2e
Category	MT
New Trees	8.4960
Total	**8.4960**

3.0 Construction Detail

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EMFAC Off-Model Adjustment Factors for Gasoline Light Duty Vehicle to Account for the SAFE Vehicle Rule Not Applied

Construction Phase

Phase Number	Phase Name	Phase Type	Start Date	End Date	Num Days Week	Num Days	Phase Description
1	Demolition	Demolition	6/1/2023	6/14/2023	5	10	
2	Site Preparation	Site Preparation	6/15/2023	6/15/2023	5	1	
3	Grading	Grading	6/16/2023	6/19/2023	5	2	
4	Building Construction	Building Construction	6/20/2023	11/6/2023	5	100	
5	Paving	Paving	11/7/2023	11/13/2023	5	5	
6	Architectural Coating	Architectural Coating	11/14/2023	11/20/2023	5	5	

Acres of Grading (Site Preparation Phase): 0.5

Acres of Grading (Grading Phase): 1.5

Acres of Paving: 0

Residential Indoor: 43,349; Residential Outdoor: 17,177; Non-Residential Indoor: 0; Non-Residential Outdoor: 0; Striped Parking Area: 0 (Architectural

OffRoad Equipment

Phase Name	Offroad Equipment Type	Amount	Usage Hours	Horse Power	Load Factor
Demolition	Concrete/Industrial Saws	1	8.00	81	0.73
Demolition	Rubber Tired Dozers	1	1.00	247	0.40
Demolition	Tractors/Loaders/Backhoes	2	6.00	97	0.37
Site Preparation	Graders	1	8.00	187	0.41
Site Preparation	Tractors/Loaders/Backhoes	1	8.00	97	0.37
Grading	Graders	1	6.00	187	0.41
Grading	Rubber Tired Dozers	1	6.00	247	0.40
Grading	Tractors/Loaders/Backhoes	1	7.00	97	0.37
Building Construction	Cranes	1	4.00	231	0.29

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Building Construction	Forklifts	2	6.00	89	0.20
Building Construction	Tractors/Loaders/Backhoes	2	8.00	97	0.37
Paving	Cement and Mortar Mixers	4	6.00	9	0.56
Paving	Pavers	1	7.00	130	0.42
Paving	Rollers	1	7.00	80	0.38
Paving	Tractors/Loaders/Backhoes	1	7.00	97	0.37
Architectural Coating	Air Compressors	1	6.00	78	0.48

Trips and VMT

Phase Name	Offroad Equipment Count	Worker Trip Number	Vendor Trip Number	Hauling Trip Number	Worker Trip Length	Vendor Trip Length	Hauling Trip Length	Worker Vehicle Class	Vendor Vehicle Class	Hauling Vehicle Class
Demolition	4	10.00	0.00	14.00	14.70	6.90	20.00	LD_Mix	HDT_Mix	HHDT
Site Preparation	2	5.00	0.00	0.00	14.70	6.90	20.00	LD_Mix	HDT_Mix	HHDT
Grading	3	8.00	0.00	868.00	14.70	6.90	20.00	LD_Mix	HDT_Mix	HHDT
Building Construction	5	31.00	5.00	0.00	14.70	6.90	20.00	LD_Mix	HDT_Mix	HHDT
Paving	7	18.00	0.00	0.00	14.70	6.90	20.00	LD_Mix	HDT_Mix	HHDT
Architectural Coating	1	6.00	0.00	0.00	14.70	6.90	20.00	LD_Mix	HDT_Mix	HHDT

3.1 Mitigation Measures Construction

3.2 Demolition - 2023

Unmitigated Construction On-Site

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e

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Category	tons/yr										MT/yr					
Fugitive Dust					1.4800e-003	0.0000	1.4800e-003	2.2000e-004	0.0000	2.2000e-004	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Off-Road	3.2300e-003	0.0289	0.0370	6.0000e-005		1.4100e-003	1.4100e-003		1.3500e-003	1.3500e-003	0.0000	5.2091	5.2091	9.5000e-004	0.0000	5.2328
Total	**3.2300e-003**	**0.0289**	**0.0370**	**6.0000e-005**	**1.4800e-003**	**1.4100e-003**	**2.8900e-003**	**2.2000e-004**	**1.3500e-003**	**1.5700e-003**	**0.0000**	**5.2091**	**5.2091**	**9.5000e-004**	**0.0000**	**5.2328**

Unmitigated Construction Off-Site

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category	tons/yr										MT/yr					
Hauling	1.0000e-005	9.6000e-004	2.5000e-004	0.0000	1.2000e-004	1.0000e-005	1.3000e-004	3.0000e-005	1.0000e-005	4.0000e-005	0.0000	0.4083	0.4083	2.0000e-005	6.0000e-005	0.4282
Vendor	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Worker	1.6000e-004	1.3000e-004	1.7000e-003	0.0000	5.5000e-004	0.0000	5.5000e-004	1.5000e-004	0.0000	1.5000e-004	0.0000	0.4361	0.4361	1.0000e-005	1.0000e-005	0.4398
Total	**1.7000e-004**	**1.0900e-003**	**1.9500e-003**	**0.0000**	**6.7000e-004**	**1.0000e-005**	**6.8000e-004**	**1.8000e-004**	**1.0000e-005**	**1.9000e-004**	**0.0000**	**0.8444**	**0.8444**	**3.0000e-005**	**7.0000e-005**	**0.8680**

Mitigated Construction On-Site

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	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category	tons/yr										MT/yr					
Fugitive Dust					1.4800e-003	0.0000	1.4800e-003	2.2000e-004	0.0000	2.2000e-004	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Off-Road	3.2300e-003	0.0289	0.0370	6.0000e-005		1.4100e-003	1.4100e-003		1.3500e-003	1.3500e-003	0.0000	5.2091	5.2091	9.5000e-004	0.0000	5.2328
Total	**3.2300e-003**	**0.0289**	**0.0370**	**6.0000e-005**	**1.4800e-003**	**1.4100e-003**	**2.8900e-003**	**2.2000e-004**	**1.3500e-003**	**1.5700e-003**	**0.0000**	**5.2091**	**5.2091**	**9.5000e-004**	**0.0000**	**5.2328**

Mitigated Construction Off-Site

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category	tons/yr										MT/yr					
Hauling	1.0000e-005	9.6000e-004	2.5000e-004	0.0000	1.2000e-004	1.0000e-005	1.3000e-004	3.0000e-005	1.0000e-005	4.0000e-005	0.0000	0.4083	0.4083	2.0000e-005	6.0000e-005	0.4282
Vendor	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Worker	1.6000e-004	1.3000e-004	1.7000e-003	0.0000	5.5000e-004	0.0000	5.5000e-004	1.5000e-004	0.0000	1.5000e-004	0.0000	0.4361	0.4361	1.0000e-005	1.0000e-005	0.4398
Total	**1.7000e-004**	**1.0900e-003**	**1.9500e-003**	**0.0000**	**6.7000e-004**	**1.0000e-005**	**6.8000e-004**	**1.8000e-004**	**1.0000e-005**	**1.9000e-004**	**0.0000**	**0.8444**	**0.8444**	**3.0000e-005**	**7.0000e-005**	**0.8680**

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3.3 Site Preparation - 2023

Unmitigated Construction On-Site

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category	tons/yr										MT/yr					
Fugitive Dust					2.7000e-004	0.0000	2.7000e-004	3.0000e-005	0.0000	3.0000e-005	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Off-Road	2.7000e-004	3.0900e-003	1.9600e-003	0.0000		1.1000e-004	1.1000e-004		1.0000e-004	1.0000e-004	0.0000	0.4275	0.4275	1.4000e-004	0.0000	0.4309
Total	**2.7000e-004**	**3.0900e-003**	**1.9600e-003**	**0.0000**	**2.7000e-004**	**1.1000e-004**	**3.8000e-004**	**3.0000e-005**	**1.0000e-004**	**1.3000e-004**	**0.0000**	**0.4275**	**0.4275**	**1.4000e-004**	**0.0000**	**0.4309**

Unmitigated Construction Off-Site

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category	tons/yr										MT/yr					
Hauling	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Vendor	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Worker	1.0000e-005	1.0000e-005	9.0000e-005	0.0000	3.0000e-005	0.0000	3.0000e-005	1.0000e-005	0.0000	1.0000e-005	0.0000	0.0218	0.0218	0.0000	0.0000	0.0220

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Total	**1.0000e-005**	**1.0000e-005**	**9.0000e-005**	**0.0000**	**3.0000e-005**	**0.0000**	**3.0000e-005**	**1.0000e-005**	**0.0000**	**1.0000e-005**	**0.0000**	**0.0218**	**0.0218**	**0.0000**	**0.0000**	**0.0220**

Mitigated Construction On-Site

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category	tons/yr										MT/yr					
Fugitive Dust					2.7000e-004	0.0000	2.7000e-004	3.0000e-005	0.0000	3.0000e-005	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Off-Road	2.7000e-004	3.0900e-003	1.9600e-003	0.0000		1.1000e-004	1.1000e-004		1.0000e-004	1.0000e-004	0.0000	0.4275	0.4275	1.4000e-004	0.0000	0.4309
Total	**2.7000e-004**	**3.0900e-003**	**1.9600e-003**	**0.0000**	**2.7000e-004**	**1.1000e-004**	**3.8000e-004**	**3.0000e-005**	**1.0000e-004**	**1.3000e-004**	**0.0000**	**0.4275**	**0.4275**	**1.4000e-004**	**0.0000**	**0.4309**

Mitigated Construction Off-Site

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category	tons/yr										MT/yr					
Hauling	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000

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EMFAC Off-Model Adjustment Factors for Gasoline Light Duty Vehicle to Account for the SAFE Vehicle Rule Not Applied

Vendor	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Worker	1.0000e-005	1.0000e-005	9.0000e-005	0.0000	3.0000e-005	0.0000	3.0000e-005	1.0000e-005	0.0000	1.0000e-005	0.0000	0.0218	0.0218	0.0000	0.0000	0.0220
Total	**1.0000e-005**	**1.0000e-005**	**9.0000e-005**	**0.0000**	**3.0000e-005**	**0.0000**	**3.0000e-005**	**1.0000e-005**	**0.0000**	**1.0000e-005**	**0.0000**	**0.0218**	**0.0218**	**0.0000**	**0.0000**	**0.0220**

3.4 Grading - 2023

Unmitigated Construction On-Site

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category	tons/yr										MT/yr					
Fugitive Dust					5.7000e-003	0.0000	5.7000e-003	2.6300e-003	0.0000	2.6300e-003	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Off-Road	9.3000e-004	0.0102	5.5500e-003	1.0000e-005		4.2000e-004	4.2000e-004		3.9000e-004	3.9000e-004	0.0000	1.2381	1.2381	4.0000e-004	0.0000	1.2481
Total	**9.3000e-004**	**0.0102**	**5.5500e-003**	**1.0000e-005**	**5.7000e-003**	**4.2000e-004**	**6.1200e-003**	**2.6300e-003**	**3.9000e-004**	**3.0200e-003**	**0.0000**	**1.2381**	**1.2381**	**4.0000e-004**	**0.0000**	**1.2481**

Unmitigated Construction Off-Site

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e

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EMFAC Off-Model Adjustment Factors for Gasoline Light Duty Vehicle to Account for the SAFE Vehicle Rule Not Applied

Category	tons/yr										MT/yr					
Hauling	9.2000e-004	0.0597	0.0152	2.5000e-004	7.4700e-003	3.6000e-004	7.8200e-003	2.0500e-003	3.4000e-004	2.3900e-003	0.0000	25.3139	25.3139	1.3900e-003	4.0200e-003	26.5467
Vendor	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Worker	3.0000e-005	2.0000e-005	2.7000e-004	0.0000	9.0000e-005	0.0000	9.0000e-005	2.0000e-005	0.0000	2.0000e-005	0.0000	0.0698	0.0698	0.0000	0.0000	0.0704
Total	**9.5000e-004**	**0.0597**	**0.0155**	**2.5000e-004**	**7.5600e-003**	**3.6000e-004**	**7.9100e-003**	**2.0700e-003**	**3.4000e-004**	**2.4100e-003**	**0.0000**	**25.3837**	**25.3837**	**1.3900e-003**	**4.0200e-003**	**26.6170**

Mitigated Construction On-Site

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category	tons/yr										MT/yr					
Fugitive Dust					5.7000e-003	0.0000	5.7000e-003	2.6300e-003	0.0000	2.6300e-003	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Off-Road	9.3000e-004	0.0102	5.5500e-003	1.0000e-005		4.2000e-004	4.2000e-004		3.9000e-004	3.9000e-004	0.0000	1.2381	1.2381	4.0000e-004	0.0000	1.2481
Total	**9.3000e-004**	**0.0102**	**5.5500e-003**	**1.0000e-005**	**5.7000e-003**	**4.2000e-004**	**6.1200e-003**	**2.6300e-003**	**3.9000e-004**	**3.0200e-003**	**0.0000**	**1.2381**	**1.2381**	**4.0000e-004**	**0.0000**	**1.2481**

Mitigated Construction Off-Site

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	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category	tons/yr										MT/yr					
Hauling	9.2000e-004	0.0597	0.0152	2.5000e-004	7.4700e-003	3.6000e-004	7.8200e-003	2.0500e-003	3.4000e-004	2.3900e-003	0.0000	25.3139	25.3139	1.3900e-003	4.0200e-003	26.5467
Vendor	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Worker	3.0000e-005	2.0000e-005	2.7000e-004	0.0000	9.0000e-005	0.0000	9.0000e-005	2.0000e-005	0.0000	2.0000e-005	0.0000	0.0698	0.0698	0.0000	0.0000	0.0704
Total	**9.5000e-004**	**0.0597**	**0.0155**	**2.5000e-004**	**7.5600e-003**	**3.6000e-004**	**7.9100e-003**	**2.0700e-003**	**3.4000e-004**	**2.4100e-003**	**0.0000**	**25.3837**	**25.3837**	**1.3900e-003**	**4.0200e-003**	**26.6170**

3.5 Building Construction - 2023

Unmitigated Construction On-Site

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category	tons/yr										MT/yr					
Off-Road	0.0316	0.3209	0.3549	5.7000e-004		0.0160	0.0160		0.0147	0.0147	0.0000	50.1042	50.1042	0.0162	0.0000	50.5093
Total	**0.0316**	**0.3209**	**0.3549**	**5.7000e-004**		**0.0160**	**0.0160**		**0.0147**	**0.0147**	**0.0000**	**50.1042**	**50.1042**	**0.0162**	**0.0000**	**50.5093**

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Unmitigated Construction Off-Site

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category	tons/yr										MT/yr					
Hauling	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Vendor	2.8000e-004	0.0101	3.7700e-003	5.0000e-005	1.5800e-003	5.0000e-005	1.6200e-003	4.5000e-004	5.0000e-005	5.0000e-004	0.0000	4.5456	4.5456	1.5000e-004	6.5000e-004	4.7443
Worker	4.9200e-003	3.9000e-003	0.0528	1.5000e-004	0.0170	1.0000e-004	0.0171	4.5100e-003	1.0000e-004	4.6100e-003	0.0000	13.5198	13.5198	3.6000e-004	3.5000e-004	13.6336
Total	**5.2000e-003**	**0.0140**	**0.0566**	**2.0000e-004**	**0.0186**	**1.5000e-004**	**0.0187**	**4.9600e-003**	**1.5000e-004**	**5.1100e-003**	**0.0000**	**18.0653**	**18.0653**	**5.1000e-004**	**1.0000e-003**	**18.3779**

Mitigated Construction On-Site

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category	tons/yr										MT/yr					
Off-Road	0.0316	0.3209	0.3549	5.7000e-004		0.0160	0.0160		0.0147	0.0147	0.0000	50.1042	50.1042	0.0162	0.0000	50.5093
Total	**0.0316**	**0.3209**	**0.3549**	**5.7000e-004**		**0.0160**	**0.0160**		**0.0147**	**0.0147**	**0.0000**	**50.1042**	**50.1042**	**0.0162**	**0.0000**	**50.5093**

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EMFAC Off-Model Adjustment Factors for Gasoline Light Duty Vehicle to Account for the SAFE Vehicle Rule Not Applied

Mitigated Construction Off-Site

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category	tons/yr										MT/yr					
Hauling	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Vendor	2.8000e-004	0.0101	3.7700e-003	5.0000e-005	1.5800e-003	5.0000e-005	1.6200e-003	4.5000e-004	5.0000e-005	5.0000e-004	0.0000	4.5456	4.5456	1.5000e-004	6.5000e-004	4.7443
Worker	4.9200e-003	3.9000e-003	0.0528	1.5000e-004	0.0170	1.0000e-004	0.0171	4.5100e-003	1.0000e-004	4.6100e-003	0.0000	13.5198	13.5198	3.6000e-004	3.5000e-004	13.6336
Total	**5.2000e-003**	**0.0140**	**0.0566**	**2.0000e-004**	**0.0186**	**1.5000e-004**	**0.0187**	**4.9600e-003**	**1.5000e-004**	**5.1100e-003**	**0.0000**	**18.0653**	**18.0653**	**5.1000e-004**	**1.0000e-003**	**18.3779**

3.6 Paving - 2023

Unmitigated Construction On-Site

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category	tons/yr										MT/yr					
Off-Road	1.5300e-003	0.0138	0.0176	3.0000e-005		6.6000e-004	6.6000e-004		6.2000e-004	6.2000e-004	0.0000	2.3498	2.3498	6.8000e-004	0.0000	2.3669

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EMFAC Off-Model Adjustment Factors for Gasoline Light Duty Vehicle to Account for the SAFE Vehicle Rule Not Applied

Paving	0.0000					0.0000	0.0000		0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Total	**1.5300e-003**	**0.0138**	**0.0176**	**3.0000e-005**		**6.6000e-004**	**6.6000e-004**		**6.2000e-004**	**6.2000e-004**	**0.0000**	**2.3498**	**2.3498**	**6.8000e-004**	**0.0000**	**2.3669**

Unmitigated Construction Off-Site

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category	tons/yr										MT/yr					
Hauling	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Vendor	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Worker	1.4000e-004	1.1000e-004	1.5300e-003	0.0000	4.9000e-004	0.0000	5.0000e-004	1.3000e-004	0.0000	1.3000e-004	0.0000	0.3925	0.3925	1.0000e-005	1.0000e-005	0.3958
Total	**1.4000e-004**	**1.1000e-004**	**1.5300e-003**	**0.0000**	**4.9000e-004**	**0.0000**	**5.0000e-004**	**1.3000e-004**	**0.0000**	**1.3000e-004**	**0.0000**	**0.3925**	**0.3925**	**1.0000e-005**	**1.0000e-005**	**0.3958**

Mitigated Construction On-Site

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e

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Category	tons/yr										MT/yr					
Off-Road	1.5300e-003	0.0138	0.0176	3.0000e-005		6.6000e-004	6.6000e-004		6.2000e-004	6.2000e-004	0.0000	2.3498	2.3498	6.8000e-004	0.0000	2.3669
Paving	0.0000					0.0000	0.0000		0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Total	**1.5300e-003**	**0.0138**	**0.0176**	**3.0000e-005**		**6.6000e-004**	**6.6000e-004**		**6.2000e-004**	**6.2000e-004**	**0.0000**	**2.3498**	**2.3498**	**6.8000e-004**	**0.0000**	**2.3669**

Mitigated Construction Off-Site

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category	tons/yr										MT/yr					
Hauling	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Vendor	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Worker	1.4000e-004	1.1000e-004	1.5300e-003	0.0000	4.9000e-004	0.0000	5.0000e-004	1.3000e-004	0.0000	1.3000e-004	0.0000	0.3925	0.3925	1.0000e-005	1.0000e-005	0.3958
Total	**1.4000e-004**	**1.1000e-004**	**1.5300e-003**	**0.0000**	**4.9000e-004**	**0.0000**	**5.0000e-004**	**1.3000e-004**	**0.0000**	**1.3000e-004**	**0.0000**	**0.3925**	**0.3925**	**1.0000e-005**	**1.0000e-005**	**0.3958**

3.7 Architectural Coating - 2023

Unmitigated Construction On-Site

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EMFAC Off-Model Adjustment Factors for Gasoline Light Duty Vehicle to Account for the SAFE Vehicle Rule Not Applied

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category	tons/yr										MT/yr					
Archit. Coating	0.0701					0.0000	0.0000		0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Off-Road	4.8000e-004	3.2600e-003	4.5300e-003	1.0000e-005		1.8000e-004	1.8000e-004		1.8000e-004	1.8000e-004	0.0000	0.6383	0.6383	4.0000e-005	0.0000	0.6393
Total	**0.0706**	**3.2600e-003**	**4.5300e-003**	**1.0000e-005**		**1.8000e-004**	**1.8000e-004**		**1.8000e-004**	**1.8000e-004**	**0.0000**	**0.6383**	**0.6383**	**4.0000e-005**	**0.0000**	**0.6393**

Unmitigated Construction Off-Site

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category	tons/yr										MT/yr					
Hauling	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Vendor	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Worker	5.0000e-005	4.0000e-005	5.1000e-004	0.0000	1.6000e-004	0.0000	1.7000e-004	4.0000e-005	0.0000	4.0000e-005	0.0000	0.1308	0.1308	0.0000	0.0000	0.1319
Total	**5.0000e-005**	**4.0000e-005**	**5.1000e-004**	**0.0000**	**1.6000e-004**	**0.0000**	**1.7000e-004**	**4.0000e-005**	**0.0000**	**4.0000e-005**	**0.0000**	**0.1308**	**0.1308**	**0.0000**	**0.0000**	**0.1319**

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Mitigated Construction On-Site

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category	tons/yr										MT/yr					
Archit. Coating	0.0701					0.0000	0.0000		0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Off-Road	4.8000e-004	3.2600e-003	4.5300e-003	1.0000e-005		1.8000e-004	1.8000e-004		1.8000e-004	1.8000e-004	0.0000	0.6383	0.6383	4.0000e-005	0.0000	0.6393
Total	**0.0706**	**3.2600e-003**	**4.5300e-003**	**1.0000e-005**		**1.8000e-004**	**1.8000e-004**		**1.8000e-004**	**1.8000e-004**	**0.0000**	**0.6383**	**0.6383**	**4.0000e-005**	**0.0000**	**0.6393**

Mitigated Construction Off-Site

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category	tons/yr										MT/yr					
Hauling	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Vendor	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Worker	5.0000e-005	4.0000e-005	5.1000e-004	0.0000	1.6000e-004	0.0000	1.7000e-004	4.0000e-005	0.0000	4.0000e-005	0.0000	0.1308	0.1308	0.0000	0.0000	0.1319

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Total	**5.0000e-005**	**4.0000e-005**	**5.1000e-004**	**0.0000**	**1.6000e-004**	**0.0000**	**1.7000e-004**	**4.0000e-005**	**0.0000**	**4.0000e-005**	**0.0000**	**0.1308**	**0.1308**	**0.0000**	**0.0000**	**0.1319**

4.0 Operational Detail - Mobile

4.1 Mitigation Measures Mobile

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category	tons/yr										MT/yr					
Mitigated	0.1145	0.1290	1.1816	2.6100e-003	0.2855	1.9000e-003	0.2874	0.0762	1.7700e-003	0.0779	0.0000	241.5999	241.5999	0.0166	0.0105	245.1277
Unmitigated	0.1145	0.1290	1.1816	2.6100e-003	0.2855	1.9000e-003	0.2874	0.0762	1.7700e-003	0.0779	0.0000	241.5999	241.5999	0.0166	0.0105	245.1277

4.2 Trip Summary Information

	Average Daily Trip Rate			Unmitigated	Mitigated
Land Use	Weekday	Saturday	Sunday	Annual VMT	Annual VMT
Apartments Mid Rise	233.92	211.13	175.87	759,877	759,877
Total	233.92	211.13	175.87	759,877	759,877

4.3 Trip Type Information

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EMFAC Off-Model Adjustment Factors for Gasoline Light Duty Vehicle to Account for the SAFE Vehicle Rule Not Applied

	Miles			Trip %			Trip Purpose %		
Land Use	H-W or C-W	H-S or C-C	H-O or C-NW	H-W or C-W	H-S or C-C	H-O or C-NW	Primary	Diverted	Pass-by
Apartments Mid Rise	14.70	5.90	8.70	40.20	19.20	40.60	86	11	3

4.4 Fleet Mix

Land Use	LDA	LDT1	LDT2	MDV	LHD1	LHD2	MHD	HHD	OBUS	UBUS	MCY	SBUS	MH
Apartments Mid Rise	0.540171	0.064547	0.189075	0.126673	0.023412	0.006384	0.010926	0.008089	0.000929	0.000597	0.025155	0.000706	0.003335

5.0 Energy Detail

Historical Energy Use: N

5.1 Mitigation Measures Energy

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category	tons/yr										MT/yr					
Electricity Mitigated						0.0000	0.0000		0.0000	0.0000	0.0000	51.7003	51.7003	2.4700e-003	3.0000e-004	51.8510
Electricity Unmitigated						0.0000	0.0000		0.0000	0.0000	0.0000	51.7003	51.7003	2.4700e-003	3.0000e-004	51.8510
NaturalGas Mitigated	2.0900e-003	0.0179	7.6000e-003	1.1000e-004		1.4400e-003	1.4400e-003		1.4400e-003	1.4400e-003	0.0000	20.6766	20.6766	4.0000e-004	3.8000e-004	20.7995
NaturalGas Unmitigated	2.0900e-003	0.0179	7.6000e-003	1.1000e-004		1.4400e-003	1.4400e-003		1.4400e-003	1.4400e-003	0.0000	20.6766	20.6766	4.0000e-004	3.8000e-004	20.7995

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3704 Kelton Apartments - Los Angeles-South Coast County, Annual

EMFAC Off-Model Adjustment Factors for Gasoline Light Duty Vehicle to Account for the SAFE Vehicle Rule Not Applied

5.2 Energy by Land Use - NaturalGas

Unmitigated

	NaturalGas Use	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Land Use	kBTU/yr	tons/yr										MT/yr					
Apartments Mid Rise	387464	2.0900e-003	0.0179	7.6000e-003	1.1000e-004		1.4400e-003	1.4400e-003		1.4400e-003	1.4400e-003	0.0000	20.6766	20.6766	4.0000e-004	3.8000e-004	20.7995
Total		**2.0900e-003**	**0.0179**	**7.6000e-003**	**1.1000e-004**		**1.4400e-003**	**1.4400e-003**		**1.4400e-003**	**1.4400e-003**	**0.0000**	**20.6766**	**20.6766**	**4.0000e-004**	**3.8000e-004**	**20.7995**

Mitigated

	NaturalGas Use	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Land Use	kBTU/yr	tons/yr										MT/yr					
Apartments Mid Rise	387464	2.0900e-003	0.0179	7.6000e-003	1.1000e-004		1.4400e-003	1.4400e-003		1.4400e-003	1.4400e-003	0.0000	20.6766	20.6766	4.0000e-004	3.8000e-004	20.7995
Total		**2.0900e-003**	**0.0179**	**7.6000e-003**	**1.1000e-004**		**1.4400e-003**	**1.4400e-003**		**1.4400e-003**	**1.4400e-003**	**0.0000**	**20.6766**	**20.6766**	**4.0000e-004**	**3.8000e-004**	**20.7995**

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EMFAC Off-Model Adjustment Factors for Gasoline Light Duty Vehicle to Account for the SAFE Vehicle Rule Not Applied

5.3 Energy by Land Use - Electricity

Unmitigated

	Electricity Use	Total CO2	CH4	N2O	CO2e
Land Use	kWh/yr	MT/yr			
Apartments Mid Rise	164715	51.7003	2.4700e-003	3.0000e-004	51.8510
Total		**51.7003**	**2.4700e-003**	**3.0000e-004**	**51.8510**

Mitigated

	Electricity Use	Total CO2	CH4	N2O	CO2e
Land Use	kWh/yr	MT/yr			
Apartments Mid Rise	164715	51.7003	2.4700e-003	3.0000e-004	51.8510
Total		**51.7003**	**2.4700e-003**	**3.0000e-004**	**51.8510**

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EMFAC Off-Model Adjustment Factors for Gasoline Light Duty Vehicle to Account for the SAFE Vehicle Rule Not Applied

6.0 Area Detail

6.1 Mitigation Measures Area

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
Category	tons/yr										MT/yr					
Mitigated	0.1770	5.1000e-003	0.4430	2.0000e-005		2.4600e-003	2.4600e-003		2.4600e-003	2.4600e-003	0.0000	0.7244	0.7244	6.9000e-004	0.0000	0.7417
Unmitigated	0.1770	5.1000e-003	0.4430	2.0000e-005		2.4600e-003	2.4600e-003		2.4600e-003	2.4600e-003	0.0000	0.7244	0.7244	6.9000e-004	0.0000	0.7417

6.2 Area by SubCategory

Unmitigated

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
SubCategory	tons/yr										MT/yr					
Architectural Coating	7.0100e-003					0.0000	0.0000		0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Consumer Products	0.1566					0.0000	0.0000		0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000

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EMFAC Off-Model Adjustment Factors for Gasoline Light Duty Vehicle to Account for the SAFE Vehicle Rule Not Applied

Hearth	0.0000	0.0000	0.0000	0.0000		0.0000	0.0000		0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Landscaping	0.0133	5.1000e-003	0.4430	2.0000e-005		2.4600e-003	2.4600e-003		2.4600e-003	2.4600e-003	0.0000	0.7244	0.7244	6.9000e-004	0.0000	0.7417
Total	**0.1770**	**5.1000e-003**	**0.4430**	**2.0000e-005**		**2.4600e-003**	**2.4600e-003**		**2.4600e-003**	**2.4600e-003**	**0.0000**	**0.7244**	**0.7244**	**6.9000e-004**	**0.0000**	**0.7417**

Mitigated

	ROG	NOx	CO	SO2	Fugitive PM10	Exhaust PM10	PM10 Total	Fugitive PM2.5	Exhaust PM2.5	PM2.5 Total	Bio- CO2	NBio- CO2	Total CO2	CH4	N2O	CO2e
SubCategory	tons/yr										MT/yr					
Architectural Coating	7.0100e-003					0.0000	0.0000		0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Consumer Products	0.1566					0.0000	0.0000		0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Hearth	0.0000	0.0000	0.0000	0.0000		0.0000	0.0000		0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000	0.0000
Landscaping	0.0133	5.1000e-003	0.4430	2.0000e-005		2.4600e-003	2.4600e-003		2.4600e-003	2.4600e-003	0.0000	0.7244	0.7244	6.9000e-004	0.0000	0.7417
Total	**0.1770**	**5.1000e-003**	**0.4430**	**2.0000e-005**		**2.4600e-003**	**2.4600e-003**		**2.4600e-003**	**2.4600e-003**	**0.0000**	**0.7244**	**0.7244**	**6.9000e-004**	**0.0000**	**0.7417**

7.0 Water Detail

7.1 Mitigation Measures Water

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EMFAC Off-Model Adjustment Factors for Gasoline Light Duty Vehicle to Account for the SAFE Vehicle Rule Not Applied

	Total CO2	CH4	N2O	CO2e
Category	MT/yr			
Mitigated	18.4982	0.0921	2.2600e-003	21.4742
Unmitigated	18.4982	0.0921	2.2600e-003	21.4742

7.2 Water by Land Use

Unmitigated

	Indoor/Outdoor Use	Total CO2	CH4	N2O	CO2e
Land Use	Mgal	MT/yr			
Apartments Mid Rise	2.80162 / 1.76624	18.4982	0.0921	2.2600e-003	21.4742
Total		**18.4982**	**0.0921**	**2.2600e-003**	**21.4742**

3704 Kelton Apartments - Los Angeles-South Coast County, Annual

EMFAC Off-Model Adjustment Factors for Gasoline Light Duty Vehicle to Account for the SAFE Vehicle Rule Not Applied

Mitigated

	Indoor/Outdoor Use	Total CO2	CH4	N2O	CO2e
Land Use	Mgal	MT/yr			
Apartments Mid Rise	2.80162 / 1.76624	18.4982	0.0921	2.2600e-003	21.4742
Total		**18.4982**	**0.0921**	**2.2600e-003**	**21.4742**

8.0 Waste Detail

8.1 Mitigation Measures Waste

Category/Year

	Total CO2	CH4	N2O	CO2e
	MT/yr			
Mitigated	4.0152	0.2373	0.0000	9.9474
Unmitigated	4.0152	0.2373	0.0000	9.9474

3704 Kelton Apartments - Los Angeles-South Coast County, Annual

EMFAC Off-Model Adjustment Factors for Gasoline Light Duty Vehicle to Account for the SAFE Vehicle Rule Not Applied

8.2 Waste by Land Use

Unmitigated

	Waste Disposed	Total CO2	CH4	N2O	CO2e
Land Use	tons	MT/yr			
Apartments Mid Rise	19.78	4.0152	0.2373	0.0000	9.9474
Total		**4.0152**	**0.2373**	**0.0000**	**9.9474**

Mitigated

	Waste Disposed	Total CO2	CH4	N2O	CO2e
Land Use	tons	MT/yr			
Apartments Mid Rise	19.78	4.0152	0.2373	0.0000	9.9474
Total		**4.0152**	**0.2373**	**0.0000**	**9.9474**

3704 Kelton Apartments - Los Angeles-South Coast County, Annual

EMFAC Off-Model Adjustment Factors for Gasoline Light Duty Vehicle to Account for the SAFE Vehicle Rule Not Applied

9.0 Operational Offroad

Equipment Type	Number	Hours/Day	Days/Year	Horse Power	Load Factor	Fuel Type

10.0 Stationary Equipment

Fire Pumps and Emergency Generators

Equipment Type	Number	Hours/Day	Hours/Year	Horse Power	Load Factor	Fuel Type

Boilers

Equipment Type	Number	Heat Input/Day	Heat Input/Year	Boiler Rating	Fuel Type

User Defined Equipment

Equipment Type	Number

11.0 Vegetation

	Total CO2	CH4	N2O	CO2e
Category	MT			
Unmitigated	8.4960	0.0000	0.0000	8.4960

3704 Kelton Apartments - Los Angeles-South Coast County, Annual

EMFAC Off-Model Adjustment Factors for Gasoline Light Duty Vehicle to Account for the SAFE Vehicle Rule Not Applied

11.2 Net New Trees

Species Class

	Number of Trees	Total CO2	CH4	N2O	CO2e
		MT			
Miscellaneous	12	8.4960	0.0000	0.0000	8.4960
Total		**8.4960**	**0.0000**	**0.0000**	**8.4960**

TREE REPORT

PREPARED FOR

Kelton Ave. Investments LLC

3676 - 3704 Kelton Avenue

Los Angeles, CA 90034

PROPERTY

3676 - 3704 Kelton Avenue

Los Angeles, CA 90034

CONTACT

Michael Librush

3676 - 3704 Kelton Avenue

Los Angeles, CA 90034

June 20, 2023

PREPARED BY

LISA SMITH, **THE TREE RESOURCE ®**

REGISTERED CONSULTING ARBORIST #464

ISA BOARD CERTIFIED MASTER ARBORIST #WE3782B

ISA TREE RISK ASSESSOR QUALIFIED - INSTRUCTOR

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P.O. BOX 49314, LOS ANGELES, CA 90049

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TREE REPORT

3676 - 3704 Kelton Avenue
Los Angeles, CA 90034

SUMMARY

PROJECT OVERVIEW	
Site Address	**3676 - 3704 Kelton Avenue, Los Angeles, CA 90034**
Location and/or Specific Plan	**Los Angeles**
Project Description	**Multi Family Housing**
Number of Protected Trees on Site	**0**
Number of STREET PARKWAY TREES on site	**2**
Number of PARKWAY TREE removals	**2 (Due to BOE conditions to widen road at this narrow portion)**
Date of Site Inspection	**12/04/22**

This Tree Report was prepared at the request of the property owner, Kelton Ave. Investments LLC, who is preparing to build multi-family housing on this property. The subject property is 15,334.6 square feet and is located in Los Angeles.

It is currently developed with a 3,016 square foot multi family residence which the owner is preparing to demolish.

PROTECTED TREES, URBAN FORESTRY DIVISION

This property is under the jurisdiction of the City of Los Angeles and guided by the Native Tree Protection Ordinance No. 186873. **Protected Trees** are defined by this ordinance as oaks (*Quercus* sp.) indigenous to California but excluding the scrub oak (*Quercus dumosa*); Southern California black walnut (*Juglans californica var. californica*); Western sycamore (*Platanus racemosa*) and California bay laurel (*Umbellularia californica*) trees with a diameter at breast height (DBH) of four inches (4") or greater. **Protected Shrubs** are defined as Mexican elderberry (*Sambucus mexicana*); Toyon (*Heteromeles arbutifolia*) which measure four inches or more in cumulative diameter, four and one-half feet above the ground level at the base of the shrub.

There are NO trees or shrubs on this property that would be considered protected within the City of Los Angeles Native Tree Protection Ordinance.

NEIGHBOR TREES

I have also inspected the neighboring properties to confirm there are no protected tree species that are adjacent to the construction zone, or in areas of impact.

CITY OF LOS ANGELES STREET TREES, URBAN FORESTRY DIVISION

There are two (2) trees located in the parkway perimeter that are considered **City of Los Angeles Street Trees.** These two Ficus benjamina trees (Weeping Fig) appear to have been installed as garden amenity trees by the previous owner. These two trees are recommended for removal for the BOE condition of road widening.

NON-PROTECTED SIGNIFICANT TREES, DEPARTMENT OF CITY PLANNING

The Department of City Planning requires the identification of the location, size, type and condition of all existing trees on the site with a DBH of 8 inches (8") or greater. These trees will be identified as **Non-Protected Significant Trees.**

At this time, I observed nineteen (19) **Non-Protected Significant Trees** on the property. These trees will be impacted by construction and are recommended for removal and replacement to the satisfaction of the City of Los Angeles Department of City Planning.

ASSIGNMENT

The Assignment included:

- Field Observation and Inventory of Trees on Site
- Photographs of the subject trees are included in Appendix B
- Evaluation of potential construction impacts
- Matrix of proposed tree removals and trees to remain

LIMITS OF THE ASSIGNMENT

The field inspection was a visual, grade level tree assessment. No special tools or equipment were used. No tree risk assessments were performed. My site examination and the information in this report is limited to the date and time the inspection occurred. The information in this report is limited to the condition of the trees at the time of my inspection.

TREE CHARACTERISTICS AND SITE CONDITIONS

Detailed information with respect to size, condition, species and recommendations are included in the Summary of Field Inspections in Appendix C. The trees are numbered on the Tree Location Map in Appendix A.

IMPACT ANALYSIS AND SPECIFIC RECOMMENDATIONS

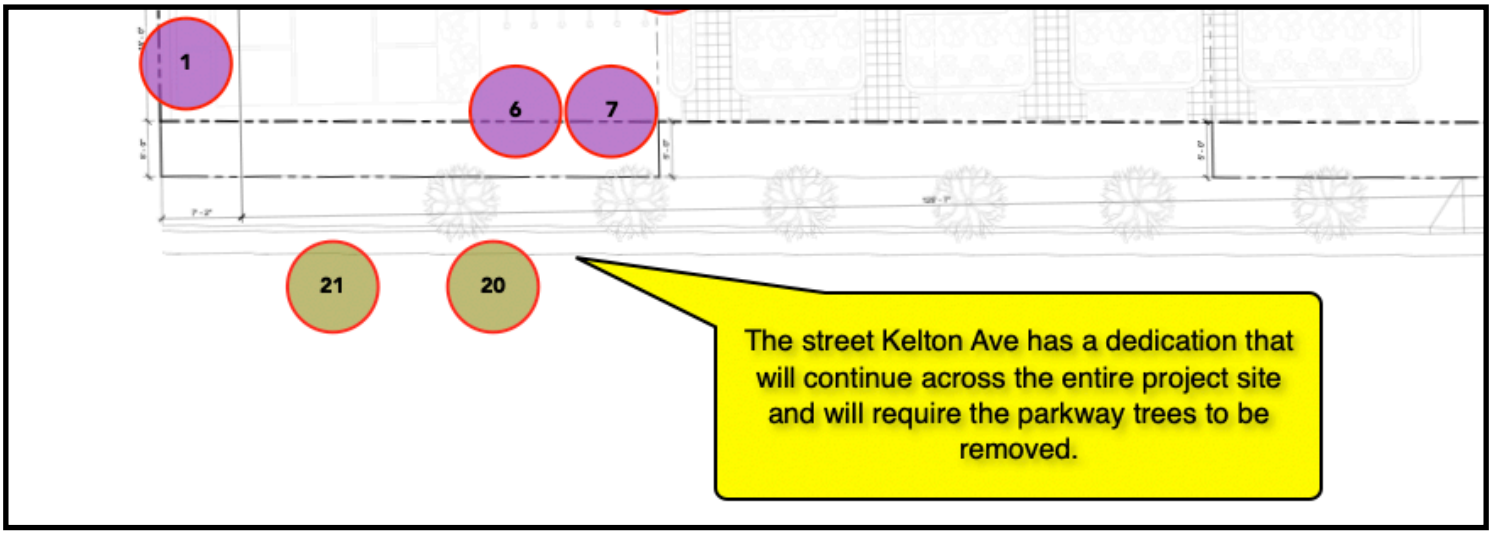
STREET TREES

Kelton Ave has a dedication that will continue across the entire project site and will require the removal of two (2) City of Los Angeles Ficus Street Trees #20 and #2. Replacement to the satisfaction of the Urban Forestry Department.

NON-PROTECTED TREES

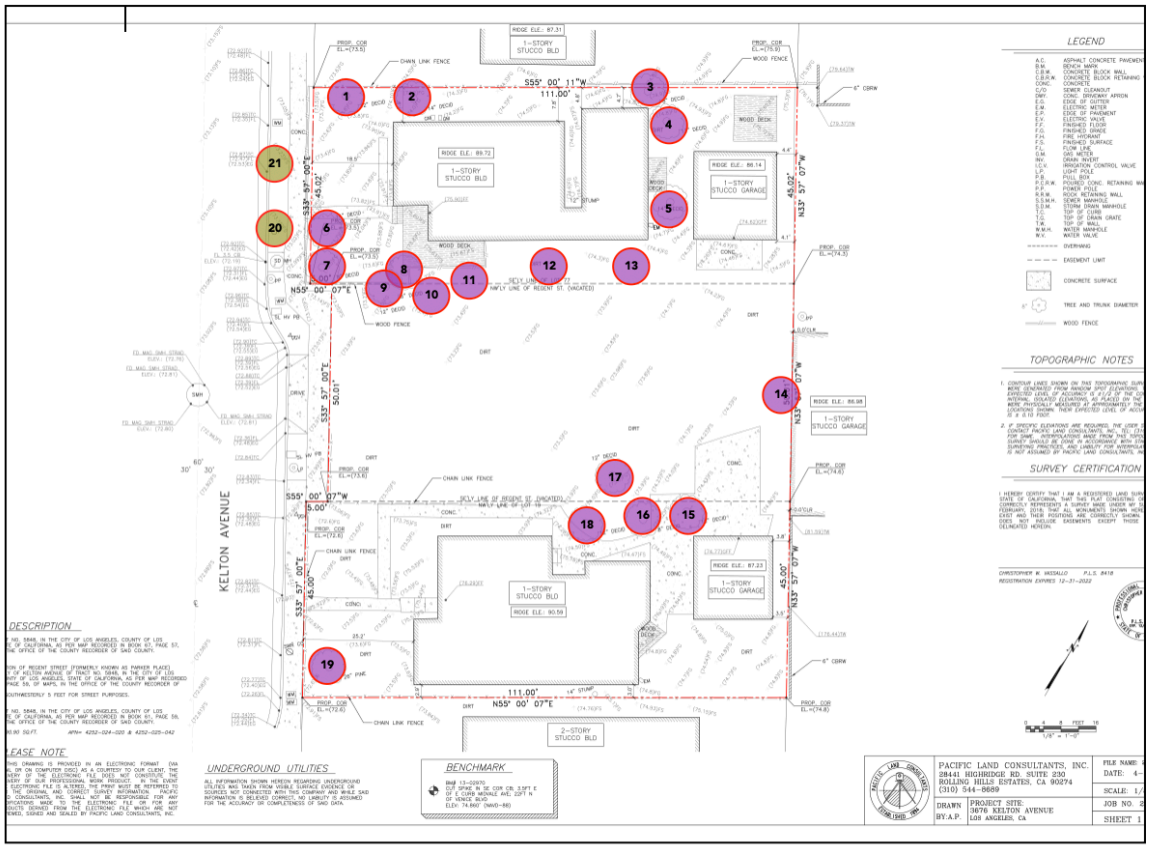
Nineteen (19) Non-Protected Significant Trees are in the direct footprint of the new construction and are recommended for removal.

SITE PLAN DETAILS



APPENDIX A.1 - TREE LOCATION MAP, REDUCED - Survey

Appendix A.1: Tree Locations on Project Survey



PROJECT SUMMARY	
Site Address	**3676 - 3704 Kelton Ave**
Location	**Los Angeles**
Project Description	**Multi Family Housing**

KEY

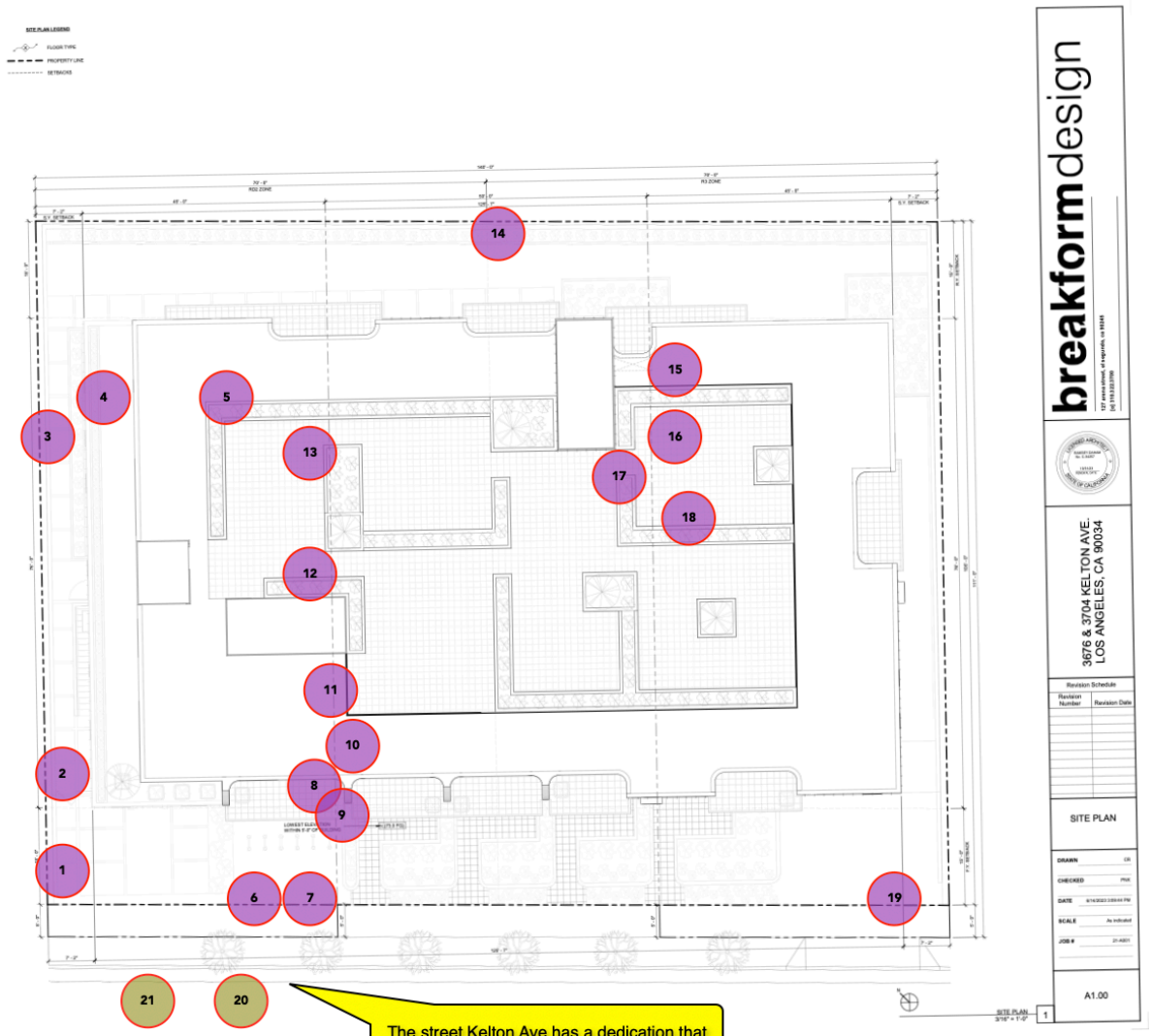
- Street Tree
- Non-Protected Significant Tree
- Tree recommended for removal

SUMMARY OF REPLACEMENT

STREET TREES REPLACED AT A 2:1 RATIO TO THE SATISFACTION OF THE URBAN FORESTRY DEPARTMENT

APPENDIX A.2 - TREE LOCATION MAP, REDUCED - Site Plan

Appendix A.2: Tree Locations on Project Site Plan



PROJECT SUMMARY	
Site Address	**3676 - 3704 Kelton Ave**
Location	**Los Angeles**
Project Description	**Multi Family Housing**

KEY

- Street Tree
- Non-Protected Significant Tree
- Tree recommended for removal

SUMMARY OF REPLACEMENT

STREET TREES REPLACED AT A 2:1 RATIO TO THE SATISFACTION OF THE URBAN FORESTRY DEPARTMENT

APPENDIX B - PHOTOGRAPHS

PHOTO 1 - Kelton Ave has a dedication that will continue across the entire project site and will require the removal of two (2) City of Los Angeles Ficus Street Trees #20 and #2. Replacement to the satisfaction of the Urban Forestry Department.

APPENDIX B - PHOTOGRAPHS

PHOTO 2 - Shows non-protected eucalyptus tree that is recommended for removal and replacement to the satisfaction of the City of Los Angeles.

APPENDIX B - PHOTOGRAPHS

PHOTO 3 - Shows non-protected Cedar tree that is recommended for removal and replacement to the satisfaction of the City of Los Angeles.

APPENDIX B - PHOTOGRAPHS

PHOTO 4 - Shows non-protected Brazilian pepper tree that is recommended for removal and replacement to the satisfaction of the City of Los Angeles.

APPENDIX B - PHOTOGRAPHS

PHOTO 5 - Shows non-protected Yucca that is recommended for removal and replacement to the satisfaction of the City of Los Angeles.

APPENDIX B - PHOTOGRAPHS

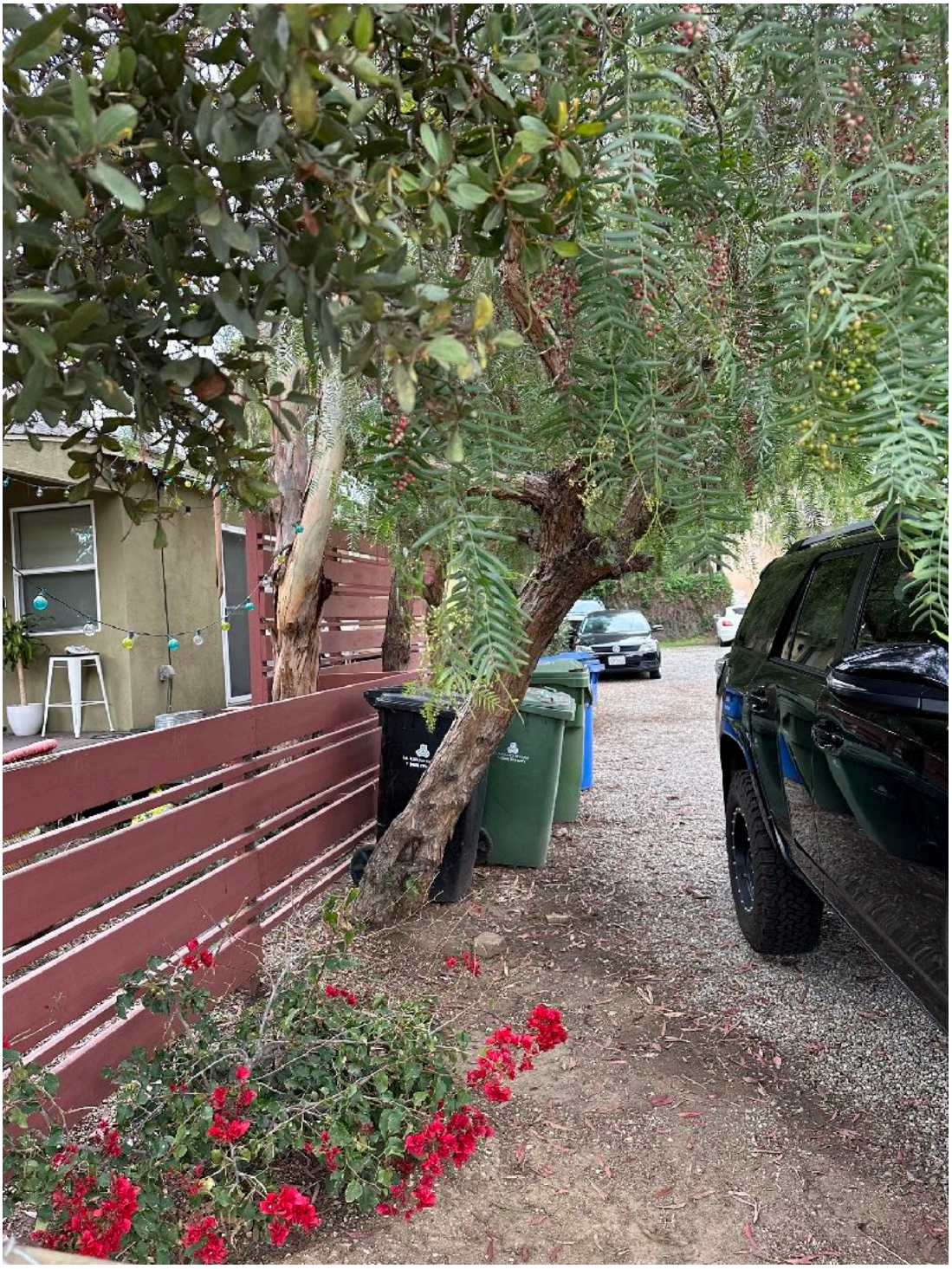


PHOTO 6 - Shows non-protected pepper tree that is recommended for removal and replacement to the satisfaction of the City of Los Angeles.

APPENDIX B - PHOTOGRAPHS

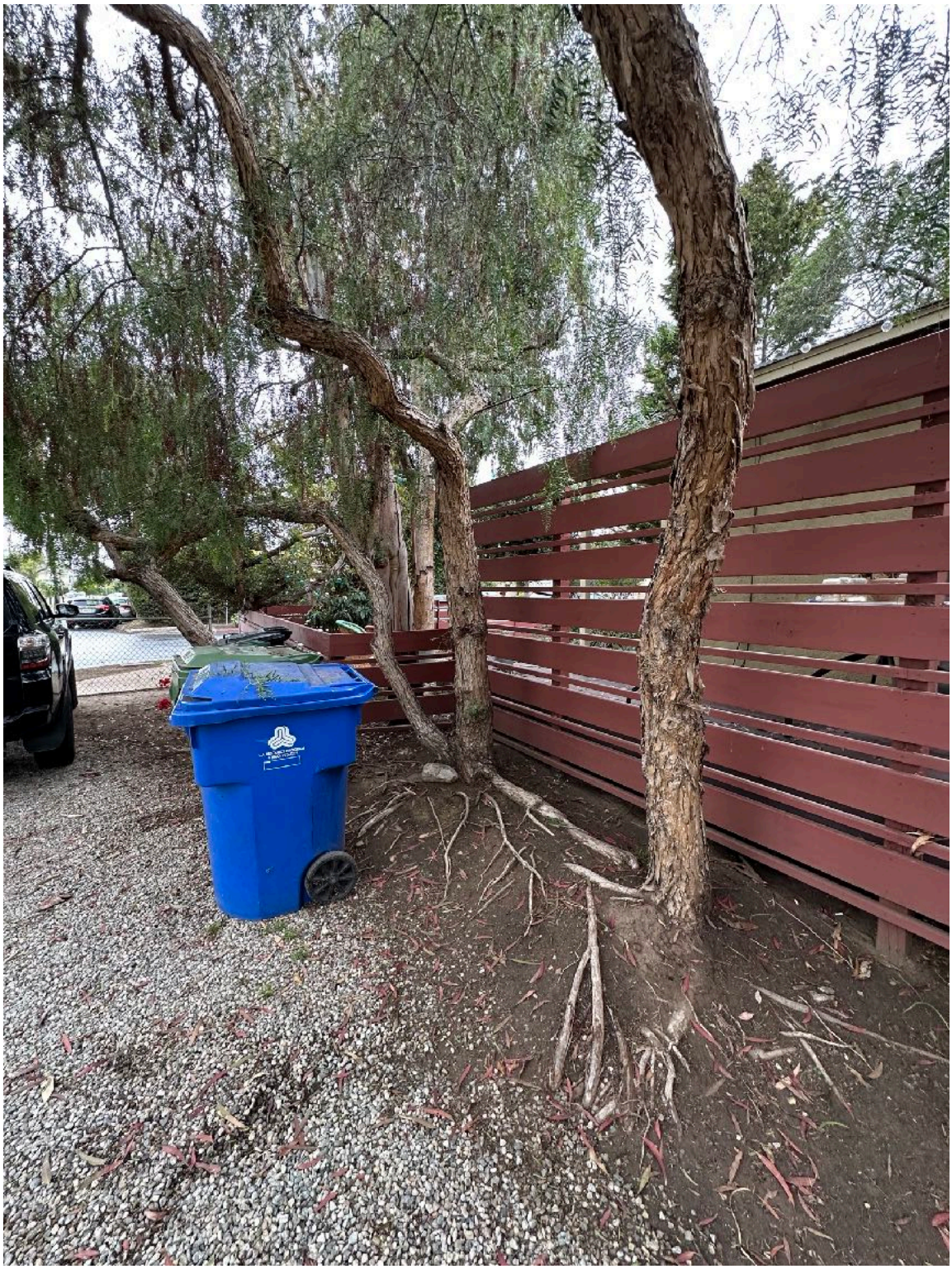


PHOTO 7 - Shows non-protected pepper trees that are recommended for removal and replacement to the satisfaction of the City of Los Angeles.

APPENDIX B - PHOTOGRAPHS

PHOTO 8 - Shows non-protected Acacia tree that is recommended for removal and replacement to the satisfaction of the City of Los Angeles.

APPENDIX B - PHOTOGRAPHS

PHOTO 9 - Shows non-protected Yucca that is recommended for removal and replacement to the satisfaction of the City of Los Angeles.

APPENDIX B - PHOTOGRAPHS

PHOTO 10 - Shows non-protected Yucca that is recommended for removal and replacement to the satisfaction of the City of Los Angeles.

APPENDIX B - PHOTOGRAPHS

PHOTO 11 - Shows non-protected King Palm tree that is recommended for removal and replacement to the satisfaction of the City of Los Angeles.

APPENDIX B - PHOTOGRAPHS

PHOTO 12 - Shows non-protected Aleppo pine tree that is recommended for removal and replacement to the satisfaction of the City of Los Angeles.

APPENDIX C - SUMMARY OF FIELD INSPECTION

Rating Code: A = Excellent, B = Good, C = Fair, D = Poor, E = Nearly Dead, F = Dead

Tree #	Species	Status	DBH (")	Height (')	Spread (')	Summary of Condition	Retain or Remove
1	Red Cedar Thuja	Non-Protected	14	45	25	C	Remove
2	Brazilian Pepper *Schinus terebinthifolius*	Non-Protected	15	35	25	C-D	Remove
3	Raywood Ash *Fraxinus raywoodii*	Non-Protected	7	15	8	C-D	Remove
4	Yucca *Yucca brevifolia*	Non-Protected	12	18	5	C	Remove
5	Blue Gum *Eucalyptus globulus*	Non-Protected	21.5	40	40	C	Remove
6	Silver Dollar Gum *Eucalyptus polyanthemos*	Non-Protected	17.5	45	25	C	Remove
7	Eucalyptus *Eucalyptus spp.*	Non-Protected	8	15	15	C-D	Remove
8	Silver Dollar Gum *Eucalyptus polyanthemos*	Non-Protected	12.5	35	20	C-D	Remove
9	California Pepper *Schinus molle*	Non-Protected	8	15	15	C-D	Remove
10	California Pepper *Schinus molle*	Non-Protected	9, 5	20	15	C-D	Remove
11	California Pepper *Schinus molle*	Non-Protected	8	15	12	C-D	Remove
12	Shoestring Acacia *Acacia stenophylla*	Non-Protected	12	25	12	B	Remove
13	Yucca *Yucca brevifolia*	Non-Protected	12	15	15	C	Remove
14	Brazilian Pepper *Schinus terebinthifolius*	Non-Protected	16, 16, 12, 10	40	40	D	Remove
15	Yucca *Yucca brevifolia*	Non-Protected	24	20	8	C	Remove
16	King Palm *Archontophoenix cunninghamiana*	Non-Protected	10, 7, 5	20	10	C	Remove
17	Fern Pine *Podocarpus gracilior*	Non-Protected	9, 8, 4	12	20	C	Remove
18	Cook Pine *Araucaria arcana*	Non-Protected	12	25	15	D	Remove
19	Aleppo Pine *Pinus halepensis*	Non-Protected	26	40	30	C	Remove
20	Weeping Fig *Ficus benjamina*	STREET PARKWAY TREE	7	15	12	C-D	Remove
21	Weeping Fig *Ficus benjamina*	STREET PARKWAY TREE	7.5	15	12	C-D	Remove

APPENDIX D - SUMMARY OF DATA

Table 2. Schedule of Proposed Removals

RECOMMENDATION

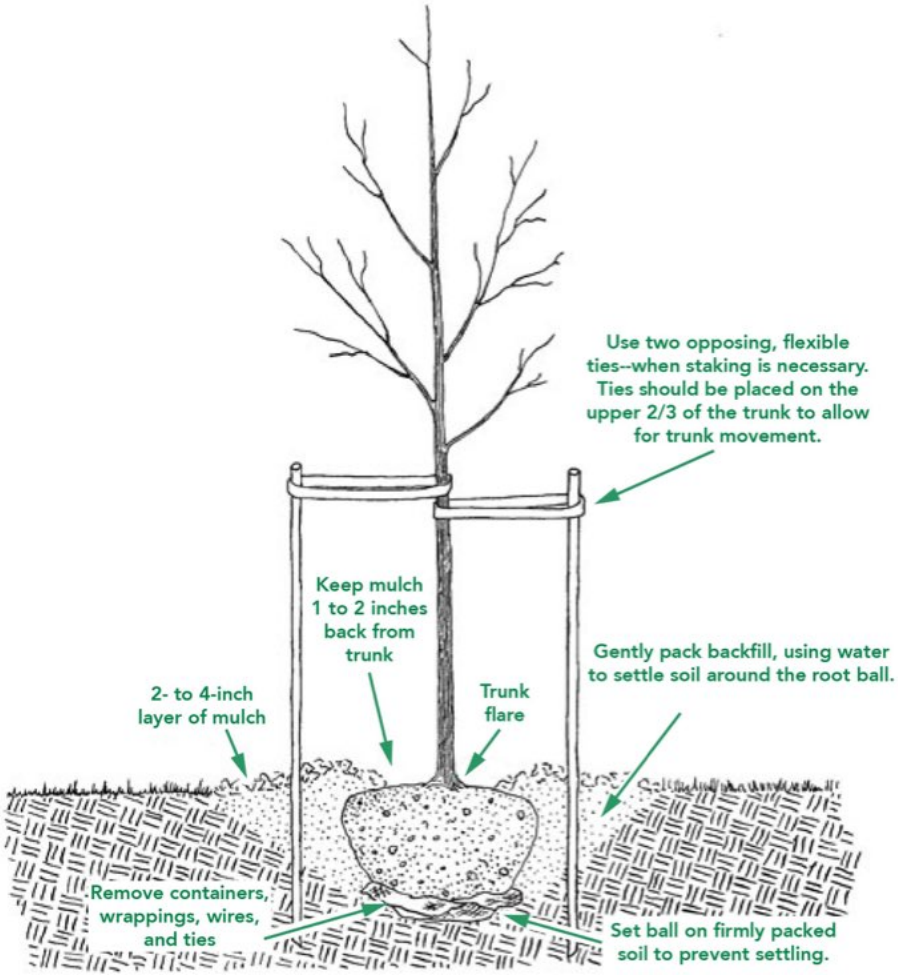
Tree #	Species	Status	Condition	Retain or Remove	Reason for Removal
1	Red Cedar *Thuja*	Non-Protected	C	Remove	Construction Impact
2	Brazilian Pepper *Schinus terebinthifolius*	Non-Protected	C-D	Remove	Construction Impact
3	Raywood Ash *Fraxinus raywoodii*	Non-Protected	C-D	Remove	Construction Impact
4	Yucca *Yucca brevifolia*	Non-Protected	C	Remove	Construction Impact
5	Blue Gum *Eucalyptus globulus*	Non-Protected	C	Remove	Construction Impact
6	Silver Dollar Gum *Eucalyptus polyanthemos*	Non-Protected	C	Remove	Construction Impact
7	Eucalyptus *Eucalyptus spp.*	Non-Protected	C-D	Remove	Construction Impact
8	Silver Dollar Gum *Eucalyptus polyanthemos*	Non-Protected	C-D	Remove	Construction Impact
9	California Pepper *Schinus molle*	Non-Protected	C-D	Remove	Construction Impact
10	California Pepper *Schinus molle*	Non-Protected	C-D	Remove	Construction Impact
11	California Pepper *Schinus molle*	Non-Protected	C-D	Remove	Construction Impact
12	Shoestring Acacia *Acacia stenophylla*	Non-Protected	C	Remove	Construction Impact
13	Yucca *Yucca brevifolia*	Non-Protected	C	Remove	Construction Impact
14	Brazilian Pepper *Schinus terebinthifolius*	Non-Protected	D	Remove	Construction Impact
15	Yucca *Yucca brevifolia*	Non-Protected	C	Remove	Construction Impact
16	King Palm *Archontophoenix cunninghamiana*	Non-Protected	C	Remove	Construction Impact
17	Fern Pine *Podocarpus gracilior*	Non-Protected	C	Remove	Construction Impact
18	Cook Pine *Araucaria arcana*	Non-Protected	D	Remove	Construction Impact
19	Aleppo Pine *Pinus halepensis*	Non-Protected	C	Remove	Construction Impact
20	Weeping Fig Ficus benjamina	Street	C	Remove	BOE Road Widening Condition
21	Weeping Fig Ficus benjamina	Street	C	Remove	BOE Road Widening Condition

APPENDIX D - SUMMARY OF DATA

Table 3. Summary of Replacement

	Existing Trees to Be Removed	Trees to be Planted in Replacement
NON-PROTECTED SIGNIFICANT TREES 8" + DBH Replaced 1:1	19	19
STREET TREES, REPLACED 2:1 to the satisfaction of the Urban Forestry Division.	2	4
TOTAL	**21**	**23**

NEW TREE PLANTING



The ideal time to plant trees and shrubs is during the dormant season, in the fall after leaf drop or early spring before budbreak. Weather conditions are cool and allow plants to establish roots in the new location before spring rains and summer heat stimulate new top growth. Before you begin planting your tree, be sure you have had all underground utilities located prior to digging.

If the tree you are planting is balled or bare root, it is important to understand that its root system has been reduced by 90 to 95 percent of its original size during transplanting. As a result of the trauma caused by the digging process, trees commonly exhibit what is known as transplant shock. Containerized trees may also experience transplant shock, particularly if they have circling roots that must be cut. Transplant shock is indicated by slow growth and reduced vigor following transplanting. Proper site preparation before and during planting coupled with good follow-up care reduces the amount of time the plant experiences transplant shock and allows the tree to quickly establish in its new location. Carefully follow nine simple steps, and you can significantly reduce the stress placed on the plant at the time of planting.

NEW TREE PLANTING, continued

1. Dig a shallow, broad planting hole. Make the hole wide, as much as three times the diameter of the root ball but only as deep as the root ball. It is important to make the hole wide because the roots on the newly establishing tree must push through surrounding soil in order to establish. On most planting sites in new developments, the existing soils have been compacted and are unsuitable for healthy root growth. Breaking up the soil in a large area around the tree provides the newly emerging roots room to expand into loose soil to hasten establishment.

2. Identify the trunk flare. The trunk flare is where the roots spread at the base of the tree. This point should be partially visible after the tree has been planted (see diagram). If the trunk flare is not partially visible, you may have to remove some soil from the top of the root ball. Find it so you can determine how deep the hole needs for proper planting.

3. Remove tree container for containerized trees. Carefully cutting down the sides of the container may make this easier. Inspect the root ball for circling roots and cut or remove them. Expose the trunk flare, if necessary.

4. Place the tree at the proper height. Before placing the tree in the hole, check to see that the hole has been dug to the proper depth and no more. The majority of the roots on the newly planted tree will develop in the top 12 inches of soil. If the tree is planted too deeply, new roots will have difficulty developing because of a lack of oxygen. It is better to plant the tree a little high, 1-2 inches above the base of the trunk flare, than to plant it at or below the original growing level. This planting level will allow for some settling.

5. Straighten the tree in the hole. Before you begin backfilling, have someone view the tree from several directions to confirm that the tree is straight. Once you begin backfilling, it is difficult to reposition the tree.

6. Fill the hole gently but firmly. Fill the hole about one-third full and gently but firmly pack the soil around the base of the root ball. Be careful not to damage the trunk or roots in the process. Fill the remainder of the hole, taking care to firmly pack soil to eliminate air pockets that may cause roots to dry out. To avoid this problem, add the soil a few inches at a time and settle with water. Continue this process until the hole is filled and the tree is firmly planted. It is not recommended to apply fertilizer at time of planting.

7. Stake the tree, if necessary. If the tree is grown properly at the nursery, staking for support will not be necessary in most home landscape situations. Studies have shown that trees establish more quickly and develop stronger trunk and root systems if they are not staked at the time of planting. However, protective staking may be required on sites where lawn mower damage, vandalism, or windy conditions are concerns. If staking is necessary for support, there are three methods to choose among: staking, guying, and ball stabilizing. One of the most common methods is staking. With this method, two stakes used in conjunction with a wide, flexible tie material on the lower half of the tree will hold the tree upright, provide flexibility, and minimize injury to the trunk (see diagram). Remove support staking and ties after the first year of growth.

8. Mulch the base of the tree. Mulch is simply organic matter applied to the area at the base of the tree. It acts as a blanket to hold moisture, it moderates soil temperature extremes, and it reduces competition from grass and weeds. A 2- to 3-inch layer is ideal. More than 3 inches may cause a problem with oxygen and moisture levels. When placing mulch, be sure that the actual trunk of the tree is not covered. Doing so may cause decay of the living bark at the base of the tree. A mulch-free area, 1 to 2 inches wide at the base of the tree, is sufficient to avoid moist bark conditions and prevent decay.

TREE MAINTENANCE AND PRUNING

Some trees do not generally require pruning. The occasional removal of dead twigs or wood is typical. Occasionally a tree has a defect or structural condition that would benefit from pruning. Any pruning activity should be performed under the guidance of a certified arborist or tree expert.

Because each cut has the potential to change the growth of the tree, no branch should be removed without a reason. Common reasons for pruning are to remove dead branches, to remove crowded or rubbing limbs, and to eliminate hazards. Trees may also be pruned to increase light and air penetration to the inside of the tree's crown or to the landscape below. In most cases, mature trees are pruned as a corrective or preventive measure.

Routine thinning does not necessarily improve the health of a tree. Trees produce a dense crown of leaves to manufacture the sugar used as energy for growth and development. Removal of foliage through pruning can reduce growth and stored energy reserves. Heavy pruning can be a significant health stress for the tree.

Yet if people and trees are to coexist in an urban or suburban environment, then we sometimes have to modify the trees. City environments do not mimic natural forest conditions. Safety is a major concern. Also, we want trees to complement other landscape plantings and lawns. Proper pruning, with an understanding of tree biology, can maintain good tree health and structure while enhancing the aesthetic and economic values of our landscapes.

Pruning Techniques – From the I.S.A. Guideline

Specific types of pruning may be necessary to maintain a mature tree in a healthy, safe, and attractive condition.

Cleaning is the removal of dead, dying, diseased, crowded, weakly attached, and low- vigor branches from the crown of a tree.

Thinning is the selective removal of branches to increase light penetration and air movement through the crown. Thinning opens the foliage of a tree, reduces weight on heavy limbs, and helps retain the tree's natural shape.

Raising removes the lower branches from a tree to provide clearance for buildings, vehicles, pedestrians, and vistas.

Reduction reduces the size of a tree, often for clearance for utility lines. Reducing the height or spread of a tree is best accomplished by pruning back the leaders and branch terminals to lateral branches that are large enough to assume the terminal roles (at least one-third the diameter of the cut stem). Compared to topping, reduction helps maintain the form and structural integrity of the tree.

TREE MAINTENANCE AND PRUNING, continued

How Much Should Be Pruned?

Mature trees should require little routine pruning. A widely accepted rule of thumb is never to remove more than one-quarter of a tree's leaf-bearing crown. In a mature tree, pruning even that much could have negative effects. Removing even a single, large- diameter limb can create a wound that the tree may not be able to close. The older and larger a tree becomes, the less energy it has in reserve to close wounds and defend against decay or insect attack. Pruning of mature trees is usually limited to removal of dead or potentially hazardous limbs.

Wound Dressings

Wound dressings were once thought to accelerate wound closure, protect against insects and diseases, and reduce decay. However, research has shown that dressings do not reduce decay or speed closure and rarely prevent insect or disease infestations. Most experts recommend that wound dressings not be used.

DISEASES AND INSECTS

Continual observation and monitoring of your tree can alert you to any abnormal changes. Some indicators are: excessive leaf drop, leaf discoloration, sap oozing from the trunk and bark with unusual cracks. Should you observe any changes, you should contact a Tree specialist or Certified Arborist to review the tree and provide specific recommendations. Trees are susceptible to hundreds of pests, many of which are typical and may not cause enough harm to warrant the use of chemicals. However, diseases and insects may be indication of further stress that should be identified by a professional.

GRADE CHANGES

The growing conditions and soil level of trees are subject to detrimental stress should they be changed during the course of construction. Raising the grade at the base of a tree trunk can have long-term negative consequences. This grade level should be maintained throughout the protected zone. This will also help in maintaining the drainage in which the tree has become accustomed.

INSPECTION

The property owner should establish an inspection calendar based on the recommendation provided by the tree specialist. This calendar of inspections can be determined based on several factors: the maturity of the tree, location of tree in proximity to high-use areas vs. low-use area, history of the tree, prior failures, external factors (such as construction activity) and the perceived value of the tree to the homeowner.

Assumptions and Limiting Conditions

No warranty is made, expressed or implied, that problems or deficiencies of the trees or the property will not occur in the future, from any cause. The Consultant shall not be responsible for damages or injuries caused by any tree defects, and assumes no responsibility for the correction of defects or tree related problems.

The owner of the trees may choose to accept or disregard the recommendations of the Consultant, or seek additional advice to determine if a tree meets the owner's risk abatement standards.

The Consulting Arborist has no past, present or future interest in the removal or retaining of any tree. Opinions contained herein are the independent and objective judgments of the consultant relating to circumstances and observations made on the subject site.

The recommendations contained in this report are the opinions of the Consulting Arborist at the time of inspection. These opinions are based on the knowledge, experience, and education of the Consultant. The field inspection was a visual, grade level tree assessment.

The Consulting Arborist shall not be required to give testimony, perform site monitoring, provide further documentation, be deposed, or to attend any meeting without subsequent contractual arrangements for this additional employment, including payment of additional fees for such services as described by the Consultant.

The Consultant assumes no responsibility for verification of ownership or locations of property lines, or for results of any actions or recommendations based on inaccurate information.

This Arborist report may not be reproduced without the express permission of the Consulting Arborist and the client to whom the report was issued. Any change or alteration to this report invalidates the entire report.

Should you have any further questions regarding this property, please contact me at (310) 663-2290.

Respectfully submitted,

Lisa Smith

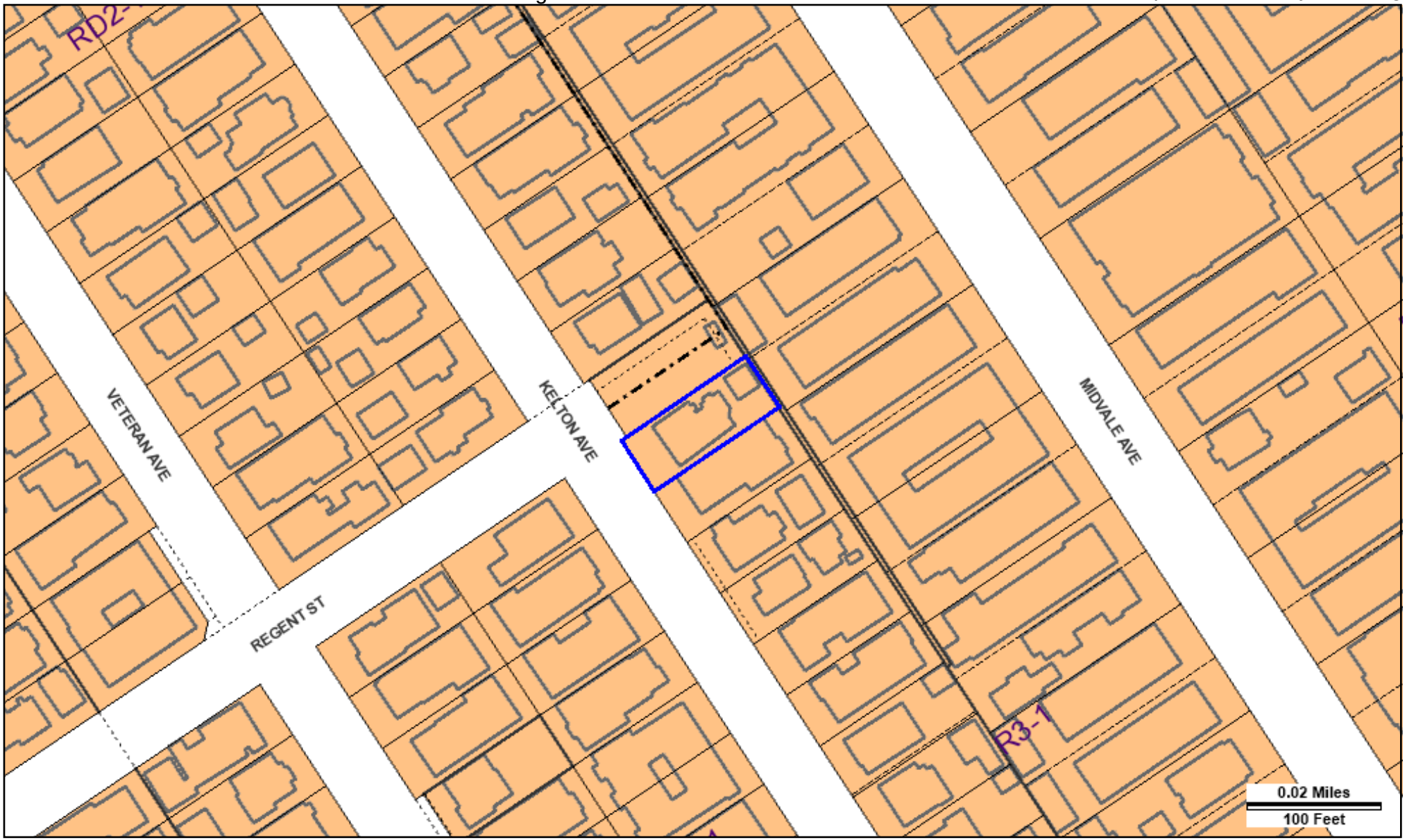
Registered Consulting Arborist #464
ISA Board Certified Master Arborist #WE3782B
ISA Tree Risk Assessor Qualified- Instructor
American Society of Consulting Arborists, Member

ZIMAS PUBLIC

Generalized Zoning

03/01/2023

City of Los Angeles
Department of City Planning



Address: 3704 1-2 S KELTON AVE
APN: 4252025042
PIN #: 117B161 516

Tract: TR 5848
Block: None
Lot: 19
Arb: None

Zoning: R3-1
General Plan: Medium Residential

N
W
E
S

3676-3704 S Kelton Ave

Photo Exhibit and Index Map

1

View of the southern portion of the project site from the west (Kelton Ave). The project site currently contains two duplexes spread across three lots.

2

View of the middle portion of the project site from the west (Kelton Ave). The project site currently contains two duplexes spread across three lots.

3

View of the northern portion of the project site from the west (Kelton Ave). The project site currently contains two duplexes spread across three lots.

4

View from in front of the project site facing southward on Kelton Ave.

5

View from in front of the project site facing northward on Kelton Ave.

6

View facing the project site from the intersection of Kelton Ave and Regent St

7

View of the project site from the property next door to the south which is currently improved with a two-story quadraplex.

8

View from southwest of the project site of the property two lots away which was recently developed with a four-story, 16-unit multifamily building.

9

West-facing view from the project site toward the intersection of Kelton Ave and Regent St.

10

View of Kelton Ave facing north from Venice Blvd.

11

Commercial development along Venice Blvd just south of the project site.

Massage Snobs Mobile Spa & Massage
Taj Limo
Regent St
Veteran Ave
Happy Kids Garden WeeCare
Regent St
Greenfield Ave
Durant Stephenson
Veteran Ave
Dynamic Power Group
H&A Limousine Services
Greenfield Ave
Painted Rock Trading Post
Venice Family Dentistry
Kelton Ave
Midvale Ave
Big Keto Energy
Health food store
LA Superbikes - Mobile Mechanic & Drop Off
Motorcycle repair shop
Tara's Himalaya
A&S Market
A A Smogs
Rozina's Beauty Spa
Fanaticos
3801 Girard
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Renata Ooms <renata.ooms@lacity.org>

Regarding 3676/3704 Kenton Ave

Adly Hanna <ahanna0220@msn.com> Wed, May 24, 2023 at 11:26 AM
To: "renata.ooms@lacity.org" <renata.ooms@lacity.org>

Dear Madam As a homeowner in this area I do not want to have a big building to make this nice homey area to be too much and make it to jammed with cars specially we don't have that much area to park also makes too much traffic and it is Narrow Street it will create more dangerous driving conditions. Our area of palms is not suited for a huge development. Thank you for you understanding.

Sent from my iPhone

Renata Ooms <renata.ooms@lacity.org>

Proposed five-story tower at 3676-3704 Kelton Avenue

Alberto Cioni <acioni61@hotmail.com> Fri, Jun 30, 2023 at 9:38 AM

Dear Dept. of City Planning, Elected Representatives and Palms Neighborhood Council

We are resident and owner of Palms opposed to the waivers of development standards requested for the proposed five-story tower at 3676-3704 Kelton Avenue.

I urge you to formally oppose the project and ensure the developer's excessive extra requests for waivers of development standards are denied. The waivers requested by the developer (unreasonable increases in height and floor area) would exacerbate existing issues that directly impact public health and safety in our neighborhood.

Simply put, if development standards are ignored and the new building is constructed as currently proposed, the already strained infrastructure of Kelton Avenue and the surrounding neighborhood will become overwhelmed.

The parking situation is already terrible and there is always a traffic jam on Kelton to get in and out the property.

The street is also very narrow.

Please STOP this project.

Best Regards,

Alberto Cioni and Maina Cioni
3652 Kelton Ave
Los Angeles CA 90034
310-780-7970

Renata Ooms <renata.ooms@lacity.org>

Contact Information

Aletta McDonald-Walker <aletta@passantinoandersen.com> Thu, Jun 29, 2023 at 9:32 AM
To: renata.ooms@lacity.org

Good morning Renata,

Would you be able to help me with contact information for the applicant for the property at 3676-3704 Kelton Avenue? I am with a communications firm and would like to ask the applicant some questions about the proposed project.

Thank you in advance.

Kind Regards,

Aletta M. Walker
904-607-5430
Regional Director
www.PassantinoAndersen.com

Renata Ooms <renata.ooms@lacity.org>

3676-3704 Kelton Ave at Regent Street

Allesondra Helwig <allesondrahelwig@gmail.com> Sat, May 27, 2023 at 7:44 PM
To: councilmember.varoslavsky@lacity.org, "dylan.sittig@lacity.org" <dylan.sittig@lacity.org>, "mayor.helpdesk@lacity.org" <mayor.helpdesk@lacity.org>, "randell.erving@palmsnc.la" <randell.erving@palmsnc.la>, "renata.ooms@lacity.org" <renata.ooms@lacity.org>, "vanessa.saldana@lacity.org" <vanessa.saldana@lacity.org>

Hello,

My name is Allesondra Helwig and I live at 10910 Regent Street, Los Angeles CA 90034, right across the street from the proposed new 5-story 43 unit building at 3676-3704 Kelton.

I am writing to ask that you please decline the request for this new apartment building in our neighborhood.
Our neighborhood is a truly a community and a new development is going to negatively impact us.

Our street parking is already scarce, traffic has increased due to other new developments on Overland, and there are already too many accidents and hit and runs due to the narrow roads.
A new building this size is going to significantly affect the safety of our neighborhood roads.
The construction noise and dust is going to affect our quality of life. And personally, I've had to move 5 times in 2 years to finally find a neighborhood where I can breathe comfortably and have a good quality of life. I am asking to please keep our neighborhood as the peaceful community it has been.

This part of Palms is not well suited for a huge development. This new development is not going to benefit the community, it's only serving the developer. Our neighborhood needs a SMALLER building that can fit into our neighborhood and is affordable for all it's tenants. Please do not approve a large building to change our neighborhood: we want to enhance our community, not change it.

Thank you so much for your consideration.

-Allesondra Helwig
718-269-8434

--
All the Best, Allesondra Helwig www.allesondrahelwig.com

Renata Ooms <renata.ooms@lacity.org>

The Importance of Implementing Reasonable Limits on Development Projects

spamnation@me.com <spamnation@me.com> Sun, Jul 2, 2023 at 3:02 PM
To: Kay Hartman <kay.hartman@palmsnc.la>, tanya.leal@palmsnc.la, colin.jaehnke@palmsnc.la, ana.garcia.lima@palmsnc.la, jacey.hutchinson@palmsnc.la, natasia.gascon@palmsnc.la, josh.nadel@palmsnc.la, charles.miller@palmsnc.la, steven.stanton@palmsnc.la, farshid.ghazavi@palmsnc.la, Katrina.Kaiser@palmsnc.la, info@palmsnc.la
Cc: Renata Ooms <renata.ooms@lacity.org>, Councilmember.Yaroslavsky@lacity.org, gary.gero@lacity.org, dylan.sittig@lacity.org, vanessa.saldana@lacity.org, jarrett.thompson@lacity.org, vanessa.serrano@lacity.org, freddy.cupen-ames@lacity.org, ray.regalado@lacity.org

The Importance of Implementing

Reasonable Limits on Development Projects

As residents of the Kelton/Regent Palms Residential Area “A” we realize our residents have perhaps been referred to in the pejorative as NIMBY’s, a term often used to label citizen activists such as ourselves who voice any opposition to development projects in our community.

However, it is important to consider that we individuals have valid concerns and motivations. Referring to us solely as "Nimbys" dismisses our legitimate reasons to oppose a development.

It's more productive to engage in an open dialogue to understand our perspectives rather than dismissing them with a derogatory label.

Addressing our concerns and finding common ground, makes it possible to work towards sustainable and inclusive development that benefits everyone involved.

We fully recognize the need for progress and growth; however, we also believe it is crucial to establish reasonable limits to ensure that the size and nature of development projects align with the best interests of our neighborhood and community.

Our concern stems from witnessing the exponential growth of large-scale development projects that do not seem to consider the impact on neighborhood infrastructure, environment, and quality of life. While we recognize the economic benefits that such projects can bring, we firmly believe that growth must be managed thoughtfully to maintain the equilibrium between progress and the preservation of our community's core values.

By some estimates the proposed Kelton & Regent Apartment complex now under consideration is some 205% larger than what would be normally allowed by LA City zoning regulations for the property parcels involved.

To emphasize the need for reasonable limits on development projects, we would like to highlight the following points:

Our neighborhood’s existing infrastructure, such as roads, utilities, sewers, storm drains, and public services, have limitations that should be considered when planning any new development. Uncontrolled expansion without corresponding upgrades places undue strain on these systems, leading to congestion, inadequate resources, and a decline in the quality of essential services. We have seen no sign that any of these infrastructure deficits are being taken into consideration, studied, addressed, or rectified.

Development projects must be assessed for their ecological footprint. Rapid and unrestricted construction can disrupt natural habitats, increase water consumption, and contribute to pollution. By implementing reasonable limits, we can strike a balance that protects our environment, preserves green spaces, and maintains the ecological health of our community.

By imposing reasonable limits, we ensure that new developments integrate harmoniously with the existing structures, preserving the visual identity that draws residents and visitors to our community.

Excessive development without careful planning can lead to increased traffic congestion, compromising the safety and convenience of residents. Implementing reasonable limits ensures that traffic management strategies are considered - Such as proper road design, stop signs, speed bumps, public transportation enhancements, pedestrian-friendly infrastructure, not to mention adequate on-street parking, to minimize the adverse effects on our daily lives.

By imposing reasonable limits, we can maintain a stable community fabric, fostering long-term relationships for both property owners & tenants alike while preserving the spirit of community that makes our neighborhood what it is.

We firmly believe that by establishing reasonable limits on the size and nature of development projects, we can safeguard our community's well-being while embracing progress.

We urge you, as influential representatives of the Palms Neighborhood Council as well as members of our community, to champion this cause and engage in a dialogue that considers the concerns and aspirations of all residents. By doing so, we can create a future that upholds our values while facilitating responsible and sustainable growth.

_____

That all being said there are several questions you might be able to assist with by helping us to understand the priorities and complexities that need to be dealt with in the face of ever-increasing urbanization coming not only to Palms but citywide as well.

We are aware that there is some support by Palms residents for more development of the Kelton/Regent complex as proposed - However in the 3-block radius of Kelton & Regent we have amassed 300+ supporters on the two petitions circulating who are voicing their objections to this “as proposed” development project.

• Can you give us an idea, or estimate as to the number of supporters there are for this housing development? — 5, 10, 50, or more?

• Are these supporters residents of “Residential Area A” or do they reside in other areas not necessarily affected by the proposed Kelton/Regent complex now under consideration?

• We have been told there are a substantial number of “Palms Stakeholders” in favor of more development whose opinions and interests need consideration as well.

• In the interest of full disclosure and clarity - Who would you consider to be the Top Ten Stakeholders, be they a person, group, organization with a vested interest, or stake, in the decision-making and activities of a business, organization, project, or community council?

Lastly…

Thank you for your consideration of these matters and for your (often thankless) service as members of the Palms NC.

We would hope to have your support in advocating for reasonable limits on development projects and ensuring the long-term prosperity of our community.

Renata Ooms <renata.ooms@lacity.org>

Opposition to 43-Unit Tower at Kelton & Regent in Palms

1 message

Tonie Pimienta <tonie.pimienta@gmail.com> Mon, Apr 17, 2023 at 9:42 PM
To: renata.ooms@lacity.org, info@palmsnc.la, councilmember.yaroslavsky@lacity.org

Dear Dept. of City Planning and Elected Representatives,

I am a resident of the Palms neighborhood of Los Angeles, urging you to stop the proposed five-story, 43-unit tower at Kelton Ave. & Regent St. I have signed the petition at https://www.change.org/Kelton-and-Regent Please protect our neighborhood!

Best,
Antonieta P. Lefebvre

Renata Ooms <renata.ooms@lacity.org>

Kelton & Regent Development

bruce hirayama <bhirayamala@gmail.com> Fri, Jun 30, 2023 at 10:24 AM
To: renata.ooms@lacity.org

this is an inappropriate project given the narrow streets, inadequate parking, and size of the structure and number of units. it should not be approved

Renata Ooms <renata.ooms@lacity.org>

Kelton and Regent Street proposed development

Christine Kinney Work <christinekwork@gmail.com> Sat, Jul 1, 2023 at 12:28 PM
To: "renata.ooms@lacity.org" <renata.ooms@lacity.org>, "info@palmsnc.la" <info@palmsnc.la>, councilmember.yaroslavsky@lacity.org, "dylan.sittig@lacity.org" <dylan.sittig@lacity.org>

Dear All,

My husband and myself are long-time Palms residents who live just around the corner from the proposed development at 3676-3704 Kelton Avenue. We are opposed to the waivers of development standards requested for this project. We urge you to formally oppose the project and ensure the developer's excessive extra requests for waivers of development standards are denied. These waivers (unreasonable increases in height and floor area) would exacerbate existing issues that directly impact public health and safety in our neighborhood. These include already difficult parking issues and narrow streets for which you have to pull over when another vehicle is approaching. In addition, it will increase already congested traffic on Veteran Avenue (where we reside), which many take as a through street to Venice Blvd. Sometimes there is so much traffic during rush hour that it becomes difficult to pull out of our driveway.

Another issue is the construction noise and parking of construction vehicles and equipment for an extended period of time. Right now, this is a quiet neighborhood where many walk their dogs, and children are always present. Please consider the many negative aspects of this development and the impact it would have on us.

Thank you,

Christine and Jay Work

Renata Ooms <renata.ooms@lacity.org>

Kelton Ave proposal development

Daniel Hairapetian <DanielHairap@mail.com> Wed, Jul 12, 2023 at 9:16 PM
To: Renata.ooms@lacity.org

Dear Dept. of City Planning and Elected Representatives—I am a resident of Palms opposed to the waivers of development standards requested for the proposed five-story tower at 3676-3704 Kelton Avenue. I urge you to formally oppose the project and ensure the developer's excessive extra requests for waivers of development standards are denied. The waivers requested by the developer (unreasonable increases in height and floor area) would exacerbate existing issues that directly impact public health and safety in our neighborhood. Simply put, if development standards are ignored and the new building is constructed as currently proposed, the already strained infrastructure of Kelton Avenue and the surrounding neighborhood will become overwhelmed.

Renata Ooms <renata.ooms@lacity.org>

Development at 3704 Kelton Avenue

David Lefebvre <dlefebvre@gmail.com> Fri, Apr 7, 2023 at 1:34 PM
To: renata.ooms@lacity.org

Hi Ms. Ooms,

My name is David Lefebvre -- I'm a Palms resident, and am reaching out to you in connection with the hearing regarding the proposed development at 3704 Kelton Avenue (described in the attached flyer we received), three doors down from my home. Do you have time to connect this week so that I can understand more about your role as a City Planner and how you might (or might not) be able to assist? I can be flexible with your schedule, including evenings.

Thank you!

Best,
David Lefebvre
Homeowner, 3658 Kelton Ave.

Renata Ooms <renata.ooms@lacity.org>

Petition to Stop the five-story, 43-unit tower at Kelton & Regent (Palms)

David Lefebvre <dlefebvre@gmail.com> Sun, Apr 16, 2023 at 8:57 PM
To: Renata Ooms <renata.ooms@lacity.org>, info@palmsnc.la, councilmember.yaroslavsky@lacity.org, Natasia Gascon <natasia.gascon@palmsnc.la>, Nicholas Smith <nicholas.smith@palmsnc.la>, allen.zipper@lacity.org, kay.hartman@palmsnc.la

Dear Ms. Ooms, Members of the Palms Neighborhood Council, and Councilwoman Yaroslavsky,

My name is David Lefebvre -- I'm a Palms resident, and am writing to formally express my strong opposition to the proposed new five-story, 43-unit tower at 3676-3704 Kelton Avenue (Case Number CPC-2023-582-CU-DB-HCA-PHP; Environmental Case Number ENV-2023-583-CE). The site is located three doors down from my home.

The principal grounds for my opposition are set forth in this online petition: https://www.change.org/Kelton-and-Regent. I started the petition five days ago and sent it to only four of my neighbors -- and since then, it has been shared throughout the community and signed by over 100 people. This is a true grassroots movement to protect our neighborhood.

My neighbors and I look forward to providing further feedback to the Department of City Planning at this Tuesday's virtual public hearing. Thank you for your attention to this matter.

Sincerely,
David Lefebvre
Homeowner, 3658 Kelton Ave.

Renata Ooms <renata.ooms@lacity.org>

Waivers of Development Standards for 3676-3704 Kelton Avenue

David Lefebvre <dlefebvre@gmail.com> Wed, Jun 28, 2023 at 11:19 PM
To: info@palmsnc.la
Bcc: renata.ooms@lacity.org

To: Members of the Palms Neighborhood Council

- Kay Hartman - President (kay.hartman@palmsnc.la; president@palmsnc.la)
- Tanya Leal - Vice President (tanya.leal@palmsnc.la)
- Colin Jaehnke - Secretary (colin.jaehnke@palmsnc.la)
- Ana Garcia Lima - Treasurer (ana.garcia.lima@palmsnc.la)
- Jacey Hutchinson - Community-Based Organization Representative (jacey.hutchinson@palmsnc.la)
- Natasia Gascon - Business Representative (natasia.gascon@palmsnc.la)
- Joshua Nadel - Business Representative (josh.nadel@palmsnc.la)
- Charles Miller - Residential Representative Area B (charles.miller@palmsnc.la)
- Steven J. Stanton - Residential Representative Area C (steven.stanton@palmsnc.la)
- Farshid Ghazavi - Residential Representative Area D (farshid.ghazavi@palmsnc.la)
- Katrina M. Kaiser - Residential Representative Area E (Katrina.Kaiser@palmsnc.la)
- info@palmsnc.la

Subject: Opposition to Requested Waivers of Development Standards for Proposed Development at 3676-3704 Kelton Avenue (Case Number CPC-2023-582-CU-DB-HCA-PHP; Environmental Case Number ENV-2023-583-CE)

We, the concerned residents of the Palms neighborhood, are writing to express our opposition to the waivers of Los Angeles development standards requested for the proposed five-story tower at 3676-3704 Kelton Avenue. The waivers requested by the developer (substantial increases in height and floor area) would exacerbate existing issues that directly impact public health and safety in our neighborhood. We therefore urge the Palms Neighborhood Council to formally oppose the project and to urge the Los Angeles Department of City Planning to reject the developer's requested waivers.

We acknowledge the urgent need for housing options in our city, and support the construction of larger apartment buildings on major corridors such as Venice Boulevard or Overland Avenue, which have good infrastructure and can handle large developments. We also understand that California's Density Bonus Law allows the developer to increase density on any property in exchange for a certain number of new affordable dwelling units. However, our opposition to this specific proposal stems from legitimate concerns that are unique to the characteristics of our area of Palms, and pertains to the developer's additional, discretionary, "off-menu" requests for waivers of development standards—which, if approved, would lead to a shocking 205% density increase. Simply put, if development standards are ignored and the new building is constructed as currently proposed, the already strained infrastructure of Kelton Avenue and the surrounding neighborhood will become overwhelmed.

Kelton Avenue is burdened by a lack of effective storm drains and sewage pipes, which regularly overflow into the street. Adding such a disproportionately large-scale development on Kelton Avenue will place even greater stress on the already struggling stormwater and sewage infrastructure, which presents significant health hazards to our community and threatens the quality of life for both new and existing residents.

Moreover, we are deeply concerned about the inevitable safety risks stemming from increased vehicular traffic. Each of Kelton Avenue and Regent Street is a one-lane, non-signalized street on which accidents and near-accidents occur daily, as vehicles play "chicken" while trying to squeeze through the narrow road. Further, there are no stop signs at the Kelton & Regent intersection, and Regent Street lacks sidewalks completely. The area's physical constraints pose safety risks to pedestrians, cyclists, and motorists alike. The community already struggles with limited parking availability, and the new building's insufficient parking would exacerbate congestion, increase the number of vehicles circling for parking, and impede emergency vehicle access. Adding a significant number of new residents will further exacerbate these issues and make the street more dangerous for all users. This is particularly concerning in light of the existing apartment building for individuals with developmental disabilities on Kelton Avenue, and multiple nearby licensed childcare facilities, each located within 650 feet of the project address.

While we acknowledge the developer's right to take advantage of California's Density Bonus program, the developer is not automatically entitled to excessive extra waivers of Los Angeles development standards. The developer has not demonstrated that the application of normal development standards would physically prevent the project's development:

in fact, since filing his initial request with the LA Department of City Planning, the developer has reduced his previously requested waiver of Los Angeles floor area (FAR) standards, and altogether abandoned his previously requested waiver of Los Angeles open space standards—all without reducing the number or percentage of units reserved for low-income households. Those changes reveal that the prior waivers were never actually required for the project to "pencil out" and raise serious questions about whether any of the off-menu waivers still being requested are truly "necessary" to build the same level of affordable housing.

We acknowledge that the Neighborhood Council's role is not to grant or deny zoning incentives or concessions; that responsibility lies with the City Planning Department. Rather, the Council's stated mission is "to empower stakeholders by helping them to improve the quality of life in the community, giving the community a voice in the City government, improving the delivery of services to the neighborhood, and holding City officials and Departments accountable to community needs." We urge you to fulfill that mission by listening to our concerns and conveying them to the City. We implore you, as representatives of our community, to advocate for the interests of the people you represent—not the interests of outside developers.

The community is overwhelmingly opposed to the proposed development in its current form. Nearly 300 people have signed a grassroots Change.org petition expressing their opposition (https://www.change.org/Kelton-and-Regent). Moreover, dozens of concerned residents attended the recent Palms Neighborhood Council PLUM Committee meeting in person on June 14 to voice their concerns; by contrast, not a single member of the public attended in support of the project. Your colleague Kay Hartman, the current President of the Palms Neighborhood Council and a resident of Area A where the development is planned, voted against the project at the PLUM Committee meeting. But the larger PLUM committee utterly failed in its responsibility to represent the community—and effectively painted a bullseye on our neighborhood for future predatory developers—by voting to approve the project over the forceful and unanimous objections of Palms residents. We hope the larger Council will make a better choice.

Lastly, we bring your attention to the events following the June 14 PLUM Committee meeting, which raise significant questions about the fairness of this project. It has come to our attention that the full PLUM Committee engaged in social activities with the developer's representative after the meeting, with a majority of discussion centered on the Kelton Avenue project. Such actions cast doubt on the integrity of the decision-making process and potentially violate the Brown Act. We request a thorough investigation into these allegations to ensure fairness and transparency.

Given these compelling concerns, we urgently request the Palms Neighborhood Council to formally oppose the various waivers of development standards requested by the developer for 3676-3704 Kelton Avenue. We sincerely appreciate your attention to this matter and trust that you will give careful consideration to our opposition, recognizing the specific adverse impact on public health and safety that granting these waivers would entail.

Sincerely,
Residents of Palms

cc:

Renata Ooms - City Planner, Los Angeles City Planning (renata.ooms@lacity.org)
Los Angeles City Councilwoman Katy Young Yaroslavsky, Fifth District (Councilmember.Yaroslavsky@lacity.org)
Gary Gero - Chief of Staff, Councilwoman Katy Yaroslavsky, Fifth District (gary.gero@lacity.org)
Dylan Sittig - Senior Planning Deputy, Councilwoman Katy Yaroslavsky, Fifth District (dylan.sittig@lacity.org)
Vanessa Saldana - Field Deputy, Palms & Westside, Councilwoman Katy Yaroslavsky, Fifth District (vanessa.saldana@lacity.org)
Jarrett Thompson - Transportation Deputy / Senior Field Deputy, Councilwoman Katy Yaroslavsky, Fifth District (jarrett.thompson@lacity.org)
Vanessa Serrano - Interim General Manager, EmpowerLA - Department of Neighborhood Empowerment (vanessa.serrano@lacity.org)
Freddy Cupen-Ames - Project Coordinator, EmpowerLA - Department of Neighborhood Empowerment (freddy.cupen-ames@lacity.org)
Ray Regalado - President, Board of Neighborhood Commissioners (ray.regalado@lacity.org)

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Petition details Comments Updates

Stop the Five-Story Development @ Kelton & Regent

202 have signed. Let's get to 500!

At 500 signatures, this petition is more likely to be **featured in recommendations!**

laura wede signed this petition

A K signed this petition

Sign this petition

First name

Last name

Email

North Hollywood, 91606
United States

Display my name and comment on this petition

Sign this petition

By signing, you accept Change.org's Terms of Service and Privacy Policy, and agree to receive occasional emails about campaigns on Change.org. You can unsubscribe at any time.

David Lefebvre started this petition to City of Los Angeles, Department of City Planning Renata Ooms

The hearing will be conducted entirely remotely by Zoom at the following URL: https://planning-lacity-org.zoom.us/j/89484629718

Enter Meeting ID: 89484629718 and Passcode: 476365

City of Los Angeles, Department of City Planning
Attn: Renata Ooms, City Planner

Dear Ms. Ooms,

We, the residents of Kelton Avenue (and surrounding areas) in Palms, write to express our strong opposition to the proposed new five-story development at 3676-3704 Kelton Avenue (Case Number CPC-2023-582-CU-DB-HCA-PHP; Environmental Case Number ENV-2023-583-CE). The new tower would have a significant, quantifiable, direct, and unavoidable impact upon the physical environment of our neighborhood, and would result in significant effects relating to traffic, noise, and our quality of life. We therefore urge the City to reject the numerous undue exemptions, waivers, bonuses, and other actions requested by the developer in connection with this project.

First, the proposed 43-unit tower would be excessively large and tall, and cause a jarring lack of transition in our neighborhood's scale, density, and character. Our area of Palms is characterized by single-family homes and apartment buildings no taller than two stories, which would be dwarfed by the five-story, three-parcel-wide structure being proposed. The tower would not only be a blight on our street, but would also cast a literal shadow over the entire neighborhood. The nearest buildings remotely resembling the proposed development are all the way on Overland Avenue—a busy, five-lane thoroughfare that is more appropriately suited to the type of structure proposed.

Screenshot taken on 5/4/2023 by Renata Ooms, Department of City Planning

Second, the proposed development would strain our neighborhood's already inadequate parking situation—as well as dangerously increase traffic on our quiet, narrow, non-signalized streets, which cannot handle more cars. As it is, street parking is very difficult to come by in our neighborhood, and residents' driveways are frequently blocked due to overcrowding. The addition of a densely populated new tower would only exacerbate this problem: inevitably, visitors and tenants without guaranteed spaces will proliferate through the surrounding streets in search of parking, further straining a limited resource and worsening gridlock. Unsafe streets due to congestion are particularly concerning in light of the multiple licensed child care facilities located within 650 feet of the project address.

Third, if the tower were approved, the process of construction would cause significant disruption to the neighborhood, including noise, dust, street closures, increased traffic, and workers looking for parking, which would negatively impact our quality of life. Kelton Avenue is simply not equipped to handle the large trucks and equipment that would be needed for such a project; as it is, LA sanitation trucks can barely make it down the street to handle our weekly garbage collection.

Fourth, we are concerned that this development conflicts with the fundamental purpose of zoning laws, and would open the floodgates to further unchecked development in our neighborhood. As you know, zoning regulations are necessary to "encourage the most appropriate use of land; to conserve and stabilize the value of property; to provide adequate open spaces for light and air ... ; to prevent undue concentration of population; to lessen congestion on streets; ... and to promote health, safety, and the general welfare" (LAMC 12.02). Giving the developer carte blanche to ignore development standards—over the community's objections, no less—would undermine each of these goals and set a dangerous precedent.

Fifth, to date, the developer has provided little to no community engagement, dialogue, or transparency regarding this project. Indeed, most of us were completely unaware of the planned building until we received the Department of City Planning's notice of the April 18, 2023 hearing in the mail. If the project moves forward, many residents are eager to know what both the developer and the City's plans will be with regard to traffic mitigation, pedestrian safety, parking and code enforcement, and environmental impact, among other issues—but to date, no such information has been shared with the community.

In conclusion, the proposed tower would have a significant, quantifiable, direct, and unavoidable impact upon the physical environment of our neighborhood, and would result in significant effects relating to traffic, noise, and quality of life. We therefore urge you to take our concerns seriously, and to reject the exemptions, waivers, bonuses, and other actions requested by the developer in connection with this project. Thank you for your attention to this matter.

Sincerely,

The Residents of Kelton Avenue (and surrounding areas) in Palms

Screenshot taken on 5/4/2023 by Renata Ooms, Department of City Planning

Renata Ooms <renata.ooms@lacity.org>

3704 kelton ave new unpleasant construction

dipendra parikh <sonawala90034@yahoo.com>
To: "mayor.helpdesk@lacity.org" <mayor.helpdesk@lacity.org>

dipendra parikh <sonawala90034@yahoo.com>
To:mayor.helpdesk@lacity.org
Cc:councilmember.yaroslavsky@lacity.org,dylan.sittig@lacity.org,renata.ooms@lacity.org,vanessa.seldana@lacity.org,randell.erving@palmsnc.la

Wed, May 24 at 9:45 PM

hi mayor my name is, Dipendra Parikh, i have lived here since 1982.at 3648 kelton ave no 3 los angeles ca.90034.i live with my wife and son. my phone no 310504 6442 .proposed 5 story 62 at 3676-3704 kelton ave,at regent street with only 11 units for very low income house holds and only 24 parking spaces! this is a huge out of space would negatively impact our neighborhood of houses and small apartment buildings and our quality of life! keep developments that only benefit builders out of area.

1 -traffic has already greatly increased

2-street parking is already scarce

3-intersection is so small that it lacks a 4 way stop sign

4-narrow streets -2 cars cannot pass whencars are parked

5-size would block out sun and light and inhibit air flow

6-width of street at site is less than 22 feet

7-would create even more dangerous driving conditions

8-only 11 units are low rents;we need lower rents for all.

9-construction noise & dustwould impact air &life quality

10-huge development on overland already increased traffic

11-this only benefitsdevelopers &people in 11 units not32

12-this area of palms not suited for 11 families.

13 we need smaller affordable units that fit into our neighborhoods,not large developments that change it.

i recommend everybody concerned should visit our neighborhood and see to verify what will be the future for residence of kelton ave n neighboring residents.

Renata Ooms <renata.ooms@lacity.org>

stop the proposed 5 story developmentin our nbeighbor hood

dipendra parikh <sonawala90034@yahoo.com> Wed, May 24, 2023 at 9:45 PM
To: "mayor.helpdesk@lacity.org" <mayor.helpdesk@lacity.org>
Cc: "councilmember.yaroslavsky@lacity.org" <councilmember.yaroslavsky@lacity.org>, "dylan.sittig@lacity.org" <dylan.sittig@lacity.org>, "renata.ooms@lacity.org" <renata.ooms@lacity.org>, "vanessa.seldana@lacity.org" <vanessa.seldana@lacity.org>, "randell.erving@palmsnc.la" <randell.erving@palmsnc.la>

hi mayor my name is, Dipendra Parikh, i have lived here since 1982.at 3648 kelton ave no 3 los angeles ca.90034.i live with my wife and son. my phone no 310504 6442 .proposed 5 story 62feet high,43 unit bld.

at 3676-3704 kelton ave,at regent street with only 11 units for very low income house holds and only 24 parking spaces! this is a huge out of space would negatively impact our neighborhood

of houses and small apartment buildings and our quality of life! keep developments that only benefit builders out of area.

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13 we need smaller affordable units that fit into our neighborhoods,not large developments that change it.

i recommend everybody concerned should visit our neighborhood and see to verify what will be the future for residence of kelton ave n neighboring residents.

Renata Ooms <renata.ooms@lacity.org>

Concerns Regarding Proposed Development at 3676-3704 Kelton Avenue

1 message

Dylan Tarason <dylantarason@yahoo.com> Thu, Jul 6, 2023 at 10:30 AM
To: "Councilmember.Yaroslavsky@lacity.org" <councilmember.yaroslavsky@lacity.org>, "renata.ooms@lacity.org" <renata.ooms@lacity.org>

Subject: Concerns Regarding Proposed Development at 3676-3704 Kelton Avenue

Dear Department of City Planning and Councilwoman Katy Young Yaroslavsky,

As a long-time resident of Palms, I feel compelled to express my strong opposition to the requested waivers of development standards for the proposed five-story tower at 3676-3704 Kelton Avenue. The Palms Neighborhood Council shares these concerns and stands united in urging the City to reject the developer's requests.

While we understand the need for housing options in our city, our opposition to this specific development arises from the unique characteristics of Kelton Avenue that cannot be overlooked the proposed tower would overwhelm the infrastructure of Kelton Avenue and the surrounding neighborhood.

One major concern is the strain it would place on our stormwater and sewage infrastructure. Currently, Kelton Avenue suffers from a lack of effective storm drains and sewage pipes, leading to regular overflows onto the street. Adding another large-scale development would only exacerbate this issue, posing significant health hazards and compromising the quality of life for both existing and future residents.

Additionally, the expected increase in vehicular traffic is deeply concerning. Kelton Avenue is a narrow, one-lane street with no signals, resulting in frequent accidents and near-misses. Pedestrians, cyclists, and motorists already face safety risks due to the street's physical constraints. Moreover, parking availability in our community is already limited, and the proposed building's insufficient parking would worsen congestion, increase circling for parking spaces, and impede emergency vehicle access. The potential dangers are amplified considering the presence of an apartment building for individuals with developmental disabilities on Kelton Avenue, as well as multiple licensed childcare facilities in close proximity.

Given these compelling concerns, I urgently request the Department of City Planning to reject the waivers of development standards requested for 3676-3704 Kelton Avenue. Our community's well-being and the adverse impact on public health and safety should be paramount in this decision-making process. We appreciate your attention to this matter and trust that you will carefully consider our opposition. We remain committed to finding solutions that strike a balance between responsible growth and the betterment of our community.

Sincerely,

Dylan Tarason

Long-time Palms Resident

Renata Ooms <renata.ooms@lacity.org>

Concerns with 3676 Kelton Development (CASE# CPC-2023-582-CU-DB-HCA-PHP)

Eloisa Julio <soyhoolio@gmail.com> Tue, Apr 18, 2023 at 10:12 PM
To: "renata.ooms@lacity.org" <renata.ooms@lacity.org>

Hi Renata,

I am a resident on Kelton Ave and wanted to voice my OPPOSITION to the development at 3676 S KELTON AVE.

I live 4 parcels south of this proposed development, and have been renting in this location for 12 years. I consider this neighborhood my long term home.

I was able to attend a portion of the hearing on 4/18, and agree with many of those who spoke in opposition to the meeting.

Here are my personal opinions on this development:

- **Too large, dense for this street:** this 5 story building is more suitable on larger streets, like the new developments on Overland and across Venice, where they can support the additional people and traffic that this development would bring. Most buildings on this street are small 2-3 story apartment buildings, or single family homes. This street is already very narrow - due to all the cars parked on the curb, only one car can move in either direction at a time. This building would bring even more traffic to an already packed street.

- **Lack of parking spaces:** while I wish this area had public transportation that was good enough to not need a car, it is not the reality of this neighborhood. I expect most people who purchase/rent a unit in this development will have at least one car, or visitors who will bring more cars to the street. Where will they park? It is already incredibly difficult to find street parking on this block.

- **Lack of community outreach**: I don't feel the proposers of this project adequately informed the community about this. Until the notice for the public hearing which I received in the mail, I have never received any communications that this project was going to occur this year. I work from home and am pretty much available throughout the week here, so had the luxury to attend the hearings. That said, I imagine many others on this street may have opinions about this project that weren't able to be voiced because they do not have the same availability to attend the hearings, nor the knowledge this project was happening in the first place. I recognize Jesi Harris and team - they had another 5-story proposed development ~5 parcels to the south on this street, where they also failed to adequately engage the community with. (CASE# CPC-2021-6888-CU-DB-HCA) I still haven't heard much from them since THAT hearing either, which is very disheartening. Did that get approved? I am worried with both of these 5-story buildings popping up, the character of this neighborhood will disappear and rental prices will go up, pushing us long term residents out of the neighborhood.

- **Other comments:** I will paste David Lefebvre's petition for the sake of time, as he has outlined many of the other concerns I have: https://www.change.org/p/stop-the-five-story-development-kelton-regent

I am hoping that these two massive buildings will not be built on Kelton. I would like to be added to the interested parties list, as well as get the "shadow survey/report" that was mentioned on the call.

Thanks,
Eloisa

Renata Ooms <renata.ooms@lacity.org>

Fw: Kelton Ave. Proposed Apartment Building

1 message

Tonyhaydee moreracampos <tonyhaydee@hotmail.com> Tue, Jul 4, 2023 at 8:36 AM
To: Renata Ooms <Renata.ooms@lacity.org>, "councilmember.yaroslavsky@lacity.org" <councilmember.yaroslavsky@lacity.org>, Dylan Sittig <dylan.sittig@lacity.org>

See below....

From: Tonyhaydee moreracampos
Sent: Sunday, July 2, 2023 2:43 PM
To: dylan.sittig@lacity.org <dylan.sittig@lacity.org>; councilmember.yaroslavsky@lacity.org <councilmember.yaroslavsky@lacity.org>
Subject: Kelton Ave. Proposed Apartment Building

Hello,
I am a resident of the Palms area of Los Angeles. I have lived in the area since 1962 along with my multi-generational family. We own the home we have lived in for over 50 years.

I attended a community meeting on June 14 before the Palms Land Use Managment committee regarding a planned building at 3676-3704 Kelton Ave. 90034.

There were more than 60 residents there, all of whom stated their opposition to the project as presented. Many are longtime residents of Palms as I am. We were all in opposition to granting the developer excessive variances to the current zoning and building codes. One of the arguments presented by the developer was that they need such a high density building to make a profit. The developer knew what the zoning restrictions were before they purchased the lots. Their poor financial decision of not assessing the zoning codes when purchasing the properties should not then negatively impact our community. We are not in opposition to the project; we are in opposition to the size and scale of the proposed building. The building as proposed with the waivers requested by the developer would mean a **205%** increase above allowable base density. That is not acceptable given the current zoning laws for the area.

We also voiced our concerns about parking. Parking is at a premium in our neighborhood. Kelton Ave. is a very narrow street. The proposed 43-unit building will only have 30 parking spaces. Apparently, as stated by the developer the rent would be discounted for tenants without cars? So, in other words, pay less rent if you don't park in the building? What will stop tenants from not paying the additional rent and then parking their vehicles on the street?

I feel that Palms has done its fair share for the housing crisis. There are multiple large apartment buildings currently finished or in construction on Overland Ave. between Venice Blvd. & Palms Blvd.

The Roy
The Venue
The Jagger
T Loft Apartments

There are other multiple buildings under construction, including a very large apartment complex on the west/north corner of Overland Ave. and Charnock Rd. These very high density buildings are already negatively impacting our community. Traffic on Overland Ave. has increased tremendously. Also, the current safety concerns on LA public transportation have impacted traffic in our area.

I respectfully request that the requested multiple zoning and building code variances not be granted on this proposed project.

Thank you,
Haydee Campos

Renata Ooms <renata.ooms@lacity.org>

Proposed 5 Story Building 3676-3704 S. Kelton Ave. & 10845 W. Regent, Los Angeles, 90034

1 message

Tonyhaydee moreracampos <tonyhaydee@hotmail.com> Tue, May 9, 2023 at 10:52 AM
To: "Renata.ooms@lacity.org" <Renata.ooms@lacity.org>, "plum@palmsnc.la" <plum@palmsnc.la>, "councilmember.yaroslavsky@lacity" <councilmember.yaroslavsky@lacity>

Subject: Proposed 5 Story Building 3676-3704 S. Kelton Ave. & 10845 W. Regent, Los Angeles, 90034

To the Los Angeles Planning Department and City Councilwoman Yaroslavsky:

We submit this outline of reasons in opposition to the proposed development as a family of nearly 3 generations that have lived at 3611 Midvale Ave. for nearly 60 years.
The project we are directing our grievance to is to be located at **3676-3704 Kelton Ave. and 10845 Regent Ave. Los Angeles, 90034.**

Although we are apparently not within the designated 500-foot radius apparently required (my understanding was that for zoning variance it 1000 ft.?) we are not much more than 700 ft. away on Midvale Ave which is the next street east of Kelton.

Although we are not able to attend the meeting scheduled for Wednesday May 10th , we did not want to miss the opportunity to be heard.
The following is our list of factors, which as residents and taxpayers in the area for nearly 60 years we request you strongly consider:

1. It is incomprehensible that the City of Los Angeles would not have any regard for the rules and regulations that were written, adopted by ordinance and paid for by tax payers so many years ago.
2. What has allowed this outrageously disproportionate project to the neighborhood, **requesting a variance to essentially all the standing planning and zoning requirements,** to proceed to this level of acceptance by the City Planning Department? What justifies this developers desire to build to this extent in an RD3 Zone??
3. The impact of this development will be negative not only to the neighboring single family and 2-to-4-unit apartments near by, but it will undoubtedly change the social fabric of this last remaining low scale neighborhood in the Palms area.
4. This project essentially condemns the remaining single family to the north to sell their properties at the mercy of developer offers once this size and density of structure goes up. Why? Because as we know from our long residence on Midvale Ave once you have a multiunit and multi-level apartment (s) in close vicinity, the property value is no longer competitive to where all single-family residences are located.
5. Case and point of #4 above being: Our own property, before the 1960's all single-family Bungalows, and now nearly entirely built to the subsequent R3 Zone of the 1960's has been reduced in value to about 66% of the neighboring streets where R1 Zoning has been maintained. Allowing this overdevelopment on Kelton will do the same and worse for all those that bought their properties based on single-family or small-scale apartments zoning laws.
6. The proposed development will undoubtedly, substantially increase traffic to an already precariously narrow residential street. Also, the proposed project will increase street parking which is already at a premium for the neighborhood.
7. This density of this proposed development results in livable unit sizes that are nothing more than a hotel room, with a much higher level of resident turn over and the detachment that comes with transitory and short stay rental situations.
8. The apartment unit size that this outrageous density proposes, will no longer accommodate lower income immigrant families for which the larger apartment footprints afforded by the appropriate density in this area has allowed since our family came in the 1960's. This is an undisputable fact fully backed by our experience of over 50 years.
9. This density is sure to place a burden on the utilities of the area, especially water consumption. Is there environmental study backing that the sewer system for this area can take on such an increase in allowed density??
10. Finally, and over all these stated reservations, I/we at 3611 Midvale again would highly question the premise of our opening paragraph above: Why, why, should this developer be allowed to proceed with such an outrageous deviation from long ago established and paid for planning. The only answer is money and personal profit, and the City Planning Department and City government seem to have no quarrel in taking on this new direction versus responsible, socially coherent city development.

Thank you for your time in considering the above, we hope from the bottom of our hearts that the issues we have noted are considered in this matter. We strongly object to this project as is currently planned.

Sincerely,

Tony Morera, Haydee and Ada Campos

Renata Ooms <renata.ooms@lacity.org>

Kelton Ave skyscraper project

1 message

Henry Hai <henryhai9999@gmail.com> Sun, Apr 16, 2023 at 9:15 PM
To: renata.ooms@lacity.org

Dear Dept. of City Planning and Elected Representatives—I am a resident of Palms, urging you to stop the proposed five-story, 43-unit tower at Kelton Ave. & Regent St. I have signed the petition at https://www.change.org/Kelton-and-Regent Please protect our neighborhood.

Renata Ooms <renata.ooms@lacity.org>

Stop Kelton Ave construction project

Henry Hai <henryhai9999@gmail.com> Mon, May 1, 2023 at 12:23 AM
To: renata.ooms@lacity.org

Stop the 5 story tower project of 43 units apartment development with little parking.

My 93 year old dad barely gets out of bed and needs a wheelchair and has to use a walker to get to the restroom with help. You expect him to ride a bike or take a bus??? He needs help to get in my car on Kelton Ave. No way is he riding a bike or taking a bus!!!

It is an outrage to say he cannot use a vehicle for street parking!!! It is an undue hardship. He barely walks with help!!!

My 83 year old mom has never ridden a bike. She has bad hips. You expect overweight elderly person to learn how to ride a bike in old age??? Who is going to take her to the bus stop and pick her up from the bus, take her to her destination and back again? You?

She needs a car and street parking!!! This is Age and Disability Discrimination!!!

I am 62 years old. Do you expect me to ride a bike 50 miles one way trip to get around in Los Angeles? You expect me to transfer to 10 different buses??? I would arrive sweaty and stinky. People would wonder from bad odors if I ever showered!!! That is if I survived and did not have a heart attack or stroke on the trip. I have irregular heartbeat and see cardiologist.

Most people on Kelton Ave are 50 years old or older. They are not going give up their cars and ride bikes or take buses. There are no trains on Kelton Ave in this area of Palms. It is narrow one lane street.

The skyscraper tower project will need 2 or 3 vehicle parking on Kelton Ave. They will need 100 parking spots in the apartment skyscraper. The development is not providing even one spot for some of its units.

This is a greedy developer who is looking to cut as many costs as possible and max out profits over people.

Help us stop the 5 story tower and refuse any construction on narrow Kelton Ave. Let him build his tower on main Blvd. like all the reasonable developers.

Renata Ooms <renata.ooms@lacity.org>

Stop Kelton Ave skyscraper tower Development project

1 message

Henry Hai <henryhai9999@gmail.com> Wed, Jun 7, 2023 at 11:42 PM
To: renata.ooms@lacity.org

Stop the 5 story tower project of 43 units apartment development with little parking.

My 93 year old dad barely gets out of bed and needs a wheelchair and has to use a walker to get to the restroom with help. You expect him to ride a bike or take a bus??? He needs help to get in my car on Kelton Ave. No way is he riding a bike or taking a bus!!!

It is an outrage to say he cannot use a vehicle for street parking!!! It is an undue hardship. He barely walks with help!!!

My 83 year old mom has never ridden a bike. She has bad hips. You expect overweight elderly person to learn how to ride a bike in old age??? Who is going to take her to the bus stop and pick her up from the bus, take her to her destination and back again? You?

She needs a car and street parking!!! This is Age and Disability Discrimination!!!

I am 62 years old. Do you expect me to ride a bike 50 miles one way trip to get around in Los Angeles? You expect me to transfer to 10 different buses??? I would arrive sweaty and stinky. People would wonder from bad odors if I ever showered!!! That is if I survived and did not have a heart attack or stroke on the trip. I have irregular heartbeat and see cardiologist.

Most people on Kelton Ave are 50 years old or older. They are not going give up their cars and ride bikes or take buses. There are no trains on Kelton Ave in this area of Palms. It is narrow one lane street.

The skyscraper tower project will need 2 or 3 vehicle parking on Kelton Ave. They will need 100 parking spots in the apartment skyscraper. The development is not providing even one spot for some of its units.

This is a greedy developer who is looking to cut as many costs as possible and max out profits over people.

Help us stop the 5 story tower and refuse any construction on narrow Kelton Ave. Let him build his tower on main Blvd. like all the reasonable developers.

Renata Ooms <renata.ooms@lacity.org>

Kelton Avenue project

Henry Hai <henryhai9999@gmail.com> Sat, Jul 1, 2023 at 7:00 PM
To: renata.ooms@lacity.org

Dear Dept. of City Planning and Elected Representatives—I am a resident of Palms opposed to the waivers of development standards requested for the proposed five-story tower at 3676-3704 Kelton Avenue. I urge you to formally oppose the project and ensure the developer's excessive extra requests for waivers of development standards are denied. The waivers requested by the developer (unreasonable increases in height and floor area) would exacerbate existing issues that directly impact public health and safety in our neighborhood. Simply put, if development standards are ignored and the new building is constructed as currently proposed, the already strained infrastructure of Kelton Avenue and the surrounding neighborhood will become overwhelmed.

Renata Ooms <renata.ooms@lacity.org>

Kelton Ave tower construction approved???

2 messages

Henry Hai <henryhai9999@gmail.com> Mon, Jun 19, 2023 at 6:17 PM
To: renata.ooms@lacity.org

I recently heard that there is another development on Kelton Ave near Venice Blvd (half block away from another tower project) and that it has already been approved by the city. Is that correct?

It is 27 units apartment tower on Kelton Ave. That will cause parking nightmare from 5 Floor structure on narrow one lane street. I heard that there was no public notice and no public hearing. Is that true?

Renata Ooms <renata.ooms@lacity.org> Mon, Jul 3, 2023 at 9:40 AM
To: Henry Hai <henryhai9999@gmail.com>

Hello Henry,
Yes, please see attached.
- Renata

Renata Ooms
Pronouns: She, Hers, Her
City Planner
Los Angeles City Planning
200 N. Spring St., Room 763
Los Angeles, CA 90012
T: (213) 978-1222 | Planning4LA.org
E-NEWS

[Quoted text hidden]

2 attachments

3730-3736 Kelton - CPC-2021-6888-CUB-DB - Plans.pdf
656K

3730-3736 Kelton - CPC-2021-6888-CUB-DB - LOD.pdf
665K

Renata Ooms <renata.ooms@lacity.org>

Kelton avenue project

1 message

Henry Hai <henryhai9999@gmail.com> Wed, Jul 5, 2023 at 8:35 PM
To: renata.ooms@lacity.org

It is my understanding that one of the two houses to be torned down and build the 27 units apartment 5 story tower with the sever lack of parking spots at 3730 - 3736 South Kelton Ave has disabled facility. It houses the blind and deaf.

Why has the LA city approved making people who are not yet homeless, tossed out on the street? Why attack the most vulnerable among its LA citizens who cannot fight back? Do you want to add to the homeless population?

Is city to send the message that unscrupulous and greedy developers are welcome to seek out and hunt the unfortunate and disabled people.

It is my understanding that the blind and deaf do not know where to go and what to do. Was that your intention? Was it your intention to take away their cheap rent controlled disabled house?

P.S. 3730 South Kelton Ave does not exist. I walked around and could not find it.

> I recently heard that there is another development on Kelton Ave near Venice Blvd (half block away from another tower project) and that it has already been approved by the city.

Renata Ooms <renata.ooms@lacity.org>

Kelton Avenue Tower project

Mary Hai <maryhai@mail.com> Thu, Jun 8, 2023 at 9:51 PM
To: Renata.ooms@lacity.org

I object to Kelton Ave project development. You insist that we ride bikes, instead of driving a car, so we will not need a vehicle for street parking. This is unrealistic and insult to our intelligence. You rarely see anyone ride a bike.

We cannot allow 43 units apartment construction to take place on our narrow street taking up all of our parking. We need all the street parking for our house residents.

I protest and reject any new construction. Parking is difficult already now in evenings and weekends, so I do not drive out during that time, otherwise I will not find any parking when I return.

It would be a nightmare, if this monster tower is allowed to get built on our narrow avenue. Right now, the empty dirt lot on Regent is used as parking lot by the 2 duplexes and they do not use street parking on Kelton Ave. Under the proposal, the parking lot would be converted and 2 duplexes would be torn down for construction for highrise tower. The parking lot would not exist.

Stop the 5 story tower and save our neighborhood!!!

Renata Ooms <renata.ooms@lacity.org>

Kelton Avenue tower project

Mary Hai <maryhai@mail.com> Fri, Apr 21, 2023 at 9:34 PM
To: renata.ooms@lacity.org

I object to Kelton Ave project development. You insist that we ride bikes, instead of driving a car, so we will not need a vehicle for street parking. This is unrealistic and insult to our intelligence. You rarely see anyone ride a bike.

We cannot allow 43 units apartment construction to take place on our narrow street taking up all of our parking. We need all the street parking for our house residents.

I protest and reject any new construction. Parking is difficult already now in evenings and weekends, so I do not drive out during that time, otherwise, I will not find any parking when I return.

It would be a nightmare, if this monster tower is allowed to get built on our narrow avenue. Right now, the empty dirt lot on Regent is used as parking lot by the 2 duplexes and they do not use street parking on Kelton Ave. Under the proposal, the parking lot would be converted and 2 duplexes would be torn down for construction for highrise tower. The parking lot would not exist.

Stop the 5 story tower and save our neighborhood!!!

Renata Ooms <renata.ooms@lacity.org>

Kelton Ave project

Henry Zzzzzian <Henryzzzzzian@mail.com> Thu, Apr 6, 2023 at 6:27 PM
To: Renata.ooms@lacity.org

I cannot attend the public hearing meeting please keep in your records.

Kelton Avenue Project

I am upset that on a narrow avenue you want to build a 5 floor high rise skyscraper with 43 units and 11 units for very low income people.

There will be plumbing problems, bacteria, germs and diseases. They will need electricity, gas, and water.

Plus, more pollution and traffic on narrow residential street and lack of parking for 43 units on Kelton Ave. Distrub peace and quiet of our neighborhood.

Low income housing will cause çrime to rise, such as, drug addicts, gangs and drive by shootings and devalue house prices make it bad investments. Let them build a project like this in rich, wealthy neighborhoods.

Renata Ooms <renata.ooms@lacity.org>

Kelton Avenue Construction project

Henry Zzzzzian <Henryzzzzzian@mail.com> Thu, Jun 8, 2023 at 8:56 PM
To: Renata.ooms@lacity.org

> > > > > I am upset that on a narrow avenue you want to build a 5 floor high rise skyscraper with 43 units and 11 units for very low income people.
> > > > >
> > > > > There will be plumbing problems, bacteria, germs and diseases. They will need electricity, gas, and water.
> > > > >
> > > > > Plus, more pollution and traffic on narrow residential street and lack of parking for 43 units on Kelton Ave. Disturb peace and quiet of our neighborhood.
> > > > >
> > > > > Low income housing will cause çrime to rise, such as, drug addicts, gangs and drive by shootings and devalue house prices make it bad investments. Let them build a project like this in rich, wealthy neighborhoods.

Renata Ooms <renata.ooms@lacity.org>

Support for 3704 Kelton

James <jsyme.bus@gmail.com> Fri, Apr 21, 2023 at 11:42 AM
To: renata.ooms@lacity.org

Greetings,

I'm a resident at 3632 Kelton Ave (just down the street from the proposed development). I, unfortunately, was unable to attend the public hearing on Tuesday, but I wanted to write to try to express support for the proposed project at 3704 Kelton Ave.

Los Angeles--and California--has been in the midst of an unprecedented housing shortage, with prices rising rapidly, contributing to a declining middle class presence in Los Angeles and California and rapidly increasing homeless populations--one area where California is unfortunately a national leader. One of the vital policy levers we have at our disposal to combat this problem is densification and transit-oriented development. This project serves both of those goals well, providing around 50 new units, at least 10 of which are reserved for low-income renters.

While I understand the concerns of my neighbors regarding parking shortages, I disagree with the premise that stopping this development will "save the neighborhood". The best way that we can preserve the key characteristics of Palms--a diverse, middle class neighborhood in West-Los Angeles, increasingly an anomaly--is to facilitate development that will allow the neighborhood to house people from a range of socioeconomic backgrounds. In the absence of new, dense developments, we will continue to see what we've seen on this street--and in West LA writ large--for the past several years: houses being flipped for gaudy prices (two houses next door to us are selling for nearly $2 million) and redevelopment of SFH zoning, which will preserve the *housing* characteristic of the neighborhood while displacing the people that have lived here for so long. Unfortunately, without densification, many residents will be forced out of Palms over time, and the neighborhood's rich demographic mix will be lost. Perhaps one solution to the parking problem could be to introduce permitted parking for residents, which would reduce the ability of non-residents to park on the streets.

The new development coincides with new public transportation initiatives on Venice Blvd (dedicated bus lanes w/protected bike lanes) that should hopefully encourage new residents to use public transportation and reduce demands for on-street parking. Of course, apartments are more energy efficient per capita than single family homes, requiring less energy and water per resident. Furthermore, additional housing will help combat the ever-growing homelessness crisis. It is in these veins that the new development should help advance city-wide climate and equity goals. I implore the city to avoid stagnation in climate and equitable development and seek to build a better Los Angeles for all, one that allows people housing and better serves advancement toward critical climate goals.

Thank you,

James Syme

3632 Kelton Ave, Los Angeles, CA 90034
(505) 730-9842
jsyme.bus@gmail.com

Renata Ooms <renata.ooms@lacity.org>

re development project comments

1 message

Janet Schwartz <zschwartzer@gmail.com> Thu, May 25, 2023 at 9:19 AM
To: renata.ooms@lacity.org

Renata,
Thank you for responding to my clarification of the "tow-away" signs and for correcting my comment to be a question and not a suggestion.

Thank you for moderating the zoom session. If the project is approved, hopefully the construction will not be too disruptive. Again, I appreciate the opportunity to express concerns.

Sincerely,

Janet

Renata Ooms <renata.ooms@lacity.org>

Proposed building on Kelton Avenue

7 messages

Janet Schwartz <zschwartzer@gmail.com> Wed, May 24, 2023 at 11:51 AM
To: renata.ooms@lacity.org

To Renata Ooms:

I listened to the hearing on 4/18/2023 about the proposed project to construct a 5-story apartment building at 3676-3704 Kelton Avenue. The apartment building would be located where Regent Street ends at Kelton Avenue.

At the "zoom" meeting in April, around 25 people expressed opinions about the project. Four people were enthusiastic about the project, but their comments seemed to be scripted and prepared. It was unclear whether they even lived near the proposed project. The remaining people eloquently expressed their opposition to the project.

I currently live in a four-plex apartment building on Kelton and concur with the comments of my fellow neighbors.

-- Driving on Kelton:
Kelton Avenue between Venice Boulevard and Charnock Road is narrow and has become a one-lane street due to parked cars on both sides of the street, except during hours reserved for street cleaning. Judging from the disruption caused by current large construction projects on Overland and Charnock, I surmise that this project will cause difficulties for both drivers and pedestrians. The issues caused by the construction sites at Overland and Charnock have existed for over a year, and currently pedestrians are hindered from crossing at the signal at Overland Avenue and Charnock Road.

-- General comments about the proposed building:
With only a couple of exceptions, the existing apartment buildings on Kelton are two or three stories. The few very large buildings on the street cover a few parcels but have two or three stories of apartments with a large parking area below the apartments. The proposed building is going to be a tall, black behemoth which will dominate the surrounding area. It will require months to construct the building, and the noise and the pollution from the construction site will be overwhelming. If there are financial issues, it will take years to complete the project. Who is actually funding this project and are they financially secure?

-- Parking on Kelton:
Parking is already an issue on Kelton and nearby streets. The smaller apartment buildings have one or two parking spaces per unit, but new construction project does not even offer one parking space per unit. Admittedly, this area features good public transportation from three transit systems, but it takes time to travel by public transportation. The Los Angeles area was developed for the automobile, and the traffic situation has become worse over the years.

-- Affordable housing:
To garner support for the project and perhaps for financial incentives, the developers have proposed 11 units for affordable low income housing. There was no indication of the rent for these units, and 11 units are not sufficient to solve the issue of affordability

for housing in West Los Angeles. I assume that the rent will be considerable for the remaining units. As I was looking for an apartment in West Los Angeles, I discovered that large rental corporations require a steady monthly income of three times the rent in addition to a considerable upfront fee. That causes difficulty for people with intermittent income and for those living on retirement income and savings to qualify for an apartment in a larger more modern building. Fortunately, landlords in smaller buildings are willing to consider savings when evaluating a prospective tenant.

General questions:
-- Have the developers done due diligence in researching the stability of the ground in the event of an earthquake?
-- Have the developers researched the sewer system and connections to the main sewer line? Are they planning to tear up the street for the sewer system?
-- Because parking spaces are currently limited, where will the construction workers park?
-- Are the developers planning to set up "tow-away" zones near the construction site?
-- What are the proposed working hours for the project?

Personal information:
I was born in Culver City and grew up in West Los Angeles. I attended the local public schools including nearby Palms Jr. High (now Palms Middle School). I learned how to drive in the 1960s when traffic was not so horrendous, and free or inexpensive parking was readily available. I lived elsewhere for decades, but visited Los Angeles frequently. I relocated to Los Angeles in 2012 to assist my family.

Thank you for considering and reviewing my comments.

Janet Schwartz
3715 Kelton Avenue, #4
Los Angeles, CA 90034-7118

Renata Ooms <renata.ooms@lacity.org> Wed, May 24, 2023 at 12:37 PM
To: Jesi Harris <HarrisLandUse@gmail.com>, Brian Silveira <silveira.brian@gmail.com>

LOS ANGELES CITY PLANNING

Renata Ooms
Pronouns: She, Hers, Her
City Planner
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200 N. Spring St., Room 763
Los Angeles, CA 90012
T: (213) 978-1222 | Planning4LA.org
E-NEWS

[Quoted text hidden]

Renata Ooms <renata.ooms@lacity.org> Wed, May 24, 2023 at 12:47 PM
To: Janet Schwartz <zschwartzer@gmail.com>, Jesi Harris <HarrisLandUse@gmail.com>, Brian Silveira <silveira.brian@gmail.com>

Hello Janet,

Your comment has been received. It will be included in the record.

I am cc'ing the project's representative Jesi Harris. Responses to your questions are below:

General questions:
-- Have the developers done due diligence in researching the stability of the ground in the event of an earthquake? All projects in the City go through a building and safety evaluation before they can pull building permits. There are regulations in place for how to build safely in various soil conditions.
-- Have the developers researched the sewer system and connections to the main sewer line? Are they planning to tear up the street for the sewer system? All projects in the city go through DWP review and permitting before they can receive building permits. There are regulations in place for sewer connections.
-- Because parking spaces are currently limited, where will the construction workers park? This suggestion for limitations on construction parking is noted.
-- Are the developers planning to set up "tow-away" zones near the construction site? This is noted as a suggestion.
-- What are the proposed working hours for the project? There are construction hour limits for all projects in the city of Los Angeles. Example is below:

SEC. 112.04. POWERED EQUIPMENT INTENDED FOR REPETITIVE USE IN RESIDENTIAL AREAS AND OTHER MACHINERY, EQUIPMENT, AND DEVICES.
(Title and Section Amended by Ord. No. 161,574, Eff 9/8/86.)

(a) Between the hours of 10:00 p.m and. 7:00 a.m. of the following day, no person shall operate any lawn mower, backpack blower, lawn edger, riding tractor, or any other machinery, equipment, or other mechanical or electrical device, or any hand tool which creates a loud, raucous or impulsive sound, within any residential zone or within 500 feet of a residence.

(b) Except as to the equipment and operations specifically mentioned and related elsewhere in this Chapter or for emergency work as that term is defined in Section 111.01(d), and except as to aircraft, tow tractors, aircraft auxiliary power units, trains and motor vehicles in their respective operations governed by State or federal regulations, no person shall operate or cause to be operated any machinery, equipment, tools, or other mechanical or electrical device, or engage in any other activity in such manner as to create any noise which would cause the noise level on the premises of any other occupied property, or, if a condominium, apartment house, duplex, or attached business, within any adjoining unit, to exceed the ambient noise level by more than five (5) decibels.

(c) Notwithstanding the provisions of Subsection (a) above, no gas powered blower shall be used within 500 feet of a residence at anytime. Both the user of such a blower as well as the individual who contracted for the services of the user, if any, shall be subject to the requirements of and penalty provisions for this ordinance. Violation of the provisions of this subsection shall be punishable as an infraction in an amount not to exceed One Hundred Dollars ($100.00), notwithstanding the graduated fines set forth in LAMC § 11.00(m). ***(Amended by Ord. No. 171,890, Eff. 2/13/98.)***

SEC. 112.05. MAXIMUM NOISE LEVEL OF POWERED EQUIPMENT OR POWERED HAND TOOLS.
(Amended by Ord. No. 161,574, Eff. 9/8/86.)

Between the hours of 7:00 a.m. and 10:00 p.m., in any residential zone of the City or within 500 feet thereof, no person shall operate or cause to be operated any powered equipment or powered hand tool that produces a maximum noise level exceeding the following noise limits at a distance of 50 feet therefrom:

(a) 75dB(A) for construction, industrial, and agricultural machinery including crawler-tractors, dozers, rotary drills and augers, loaders, power shovels, cranes, derricks, motor graders, paving

machines, off-highway trucks, ditchers, trenchers, compactors, scrapers, wagons, pavement breakers, compressors and pneumatic or other powered equipment;

(b) 75dB(A) for powered equipment of 20 HP or less intended for infrequent use in residential areas, including chain saws, log chippers and powered hand tools;

(c) 65dB(A) for powered equipment intended for repetitive use in residential areas, including lawn mowers, backpack blowers, small lawn and garden tools and riding tractors;

The noise limits for particular equipment listed above in (a), (b) and (c) shall be deemed to be superseded and replaced by noise limits for such equipment from and after their establishment by final regulations adopted by the Federal Environmental Protection Agency and published in the Federal Register.

Said noise limitations shall not apply where compliance therewith is technically infeasible The burden of proving that compliance is technically infeasible shall be upon the person or persons charged with a violation of this section. Technical infeasibility shall mean that said noise limitations cannot be complied with despite the use of mufflers, shields, sound barriers and/or other noise reduction device or techniques during the operation of the equipment.

LOS ANGELES CITY PLANNING

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Brian Silveira <silveira.brian@gmail.com> Wed, May 24, 2023 at 12:50 PM
To: Renata Ooms <renata.ooms@lacity.org>

Thank you, Renata.
[Quoted text hidden]

Janet Schwartz <zschwartzer@gmail.com> Wed, May 24, 2023 at 8:40 PM
To: Renata Ooms <renata.ooms@lacity.org>
Cc: Jesi Harris <HarrisLandUse@gmail.com>, Brian Silveira <silveira.brian@gmail.com>

My question about a tow-away zone was merely a question and not a suggestion. Please do not indicate that I suggested it. Tow-away zones would merely exacerbate the existing parking problems.

By the way, in the general comments section I also asked who is funding the project and whether they are financially secure.

Thank you for considering my comments and answering my questions.

Janet

On Wed, May 24, 2023 at 12:47 PM Renata Ooms <renata.ooms@lacity.org> wrote:
[Quoted text hidden]

Renata Ooms <renata.ooms@lacity.org> Thu, May 25, 2023 at 8:00 AM
To: Janet Schwartz <zschwartzer@gmail.com>
Cc: Brian Silveira <silveira.brian@gmail.com>, Jesi Harris <HarrisLandUse@gmail.com>

Hi Janet,

I see, I miss interpreted the tow-away question. Thank you for the clarification.

The City is not privy to the information you request about the finances of a private developer. I have connected you with the developer's team - perhaps they can address how the project is bing financed.

Sincerely,
Renata
[Quoted text hidden]
--
[Quoted text hidden]

Janet Schwartz <zschwartzer@gmail.com> Thu, May 25, 2023 at 9:13 AM
To: Renata Ooms <renata.ooms@lacity.org>
Cc: Brian Silveira <silveira.brian@gmail.com>, Jesi Harris <HarrisLandUse@gmail.com>

I would like to clarify my concerns about financing. I am not interested in learning the name of the company, but rather in their stability in view of recent banking issues and possible future financial problems for the entire country if the U.S. government defaults. An issue in financing could halt an unfinished project.

In the past while riding on buses I noticed signs indicating that First Republic bank and Pacific Western bank were financing projects. I am not sure what the situation is now after the recent issues with those two financial institutions. Also, the buildings I noticed were businesses and not residential buildings.

I appreciate being able to express my concerns and thank you for considering them. I realize that the Los Angeles area has grown exponentially over the years and that there will continue to be development projects.

Janet
[Quoted text hidden]

Renata Ooms <renata.ooms@lacity.org>

Petition for Kelton & Regent 5-Story Apartment

1 message

Jessica Foster <jessymfoster@gmail.com> Mon, Apr 17, 2023 at 6:31 PM
To: renata.ooms@lacity.org, info@palmsnc.la, councilmember.yaroslavsky@lacity.org

Dear Dept. of City Planning and Elected Representatives,

My boyfriend and I are new homeowners in Palms, just right down the street from Kelton Ave and Regent Street where they want to build a five-story 43 unit tower. We loved this neighborhood because of the community and families that live around us. The house that we ended up buying was actually being poached by other apartment developers and the previous home owners chose us because they wanted to allow new homeowners to start a family, and not be taken over by apartment complexes. Not only that but parking is a huge concern. With the increased cars, it's going to require females in our area to have to park farther away from their homes in the dark and be in more danger.

We strongly urge that you please stop this from happening. We have all signed the petition at https://www.change.org/Kelton-and-Regent. Please do the right thing and protect our neighborhood!

Thanks,
Jessica

--
Jessica Foster
jessymfoster@gmail.com | (925) 858-5114

Renata Ooms <renata.ooms@lacity.org>

Please stop the Five-Story Development @ Kelton & Regent

Jessica Tiao <jesstiao@gmail.com> Mon, Apr 17, 2023 at 1:41 PM
To: renata.ooms@lacity.org, info@palmsnc.la, councilmember.yaroslavsky@lacity.org

Dear Dept. of City Planning and Elected Representatives—

I am a resident of Palms, urging you to stop the proposed five-story, 43-unit tower at Kelton Ave. & Regent St. I have signed the petition at https://www.change.org/Kelton-and-Regent

I have lived in West LA for 34 years. I love Los Angeles, and I urge you to hear our concerns:

The proposed development would strain our neighborhood's already inadequate parking situation—as well as dangerously increase traffic on our quiet, narrow, non-signalized streets, which cannot handle more cars. As it is, street parking is very difficult to come by in our neighborhood, and residents' driveways are frequently blocked due to overcrowding. The addition of a densely populated new tower would only exacerbate this problem: inevitably, visitors and tenants without guaranteed spaces will proliferate through the surrounding streets in search of parking, further straining a limited resource and worsening gridlock. **Unsafe streets due to congestion and a lack of parking are particularly concerning in light of the multiple licensed child care facilities located within 650 feet of the project address.**

I urge you to listen to our concerns and address them as our city elected representatives. If you do not listen to our voice, I will consider voting in elected representatives who represent our interests in a meaningful way.

Resident of Palms,
Jessica Tiao

Renata Ooms <renata.ooms@lacity.org>

Written Concerns as to why I object to the Five-Story Development @ Kelton & Regent

1 message

Jessica Tiao <jesstiao@gmail.com> Tue, Apr 18, 2023 at 1:20 PM
To: Renata Ooms <renata.ooms@lacity.org>, councilmember.yaroslavsky@lacity.org
Cc: info@palmsnc.la

Hi Renata (and cc' Councilwoman Katy Yaroslavsky,)

Thank you SO much Renata for your service to the West Los Angeles community in your tireless work.

After learning about state laws, it sounds like you / government representatives may have your hands tied due to state laws. I plan to attend today's virtual Department of City Planning meeting.

I would like to request that the following concerns are to the records **so that the City Council can review what my objections are and why** -

The developer's requested exemptions:

1. A CEQA exemption pursuant to CEQA Guidelines, Section, 15332, Class 32 (Urban Infill), and that there is no substantial evidence demonstrating that an exception to a categorical exemption pursuant to CEQA Guidelines, Section 15300.2 applies; and

2. Pursuant to Los Angeles Municipal Code (LAMC) Section 12.22-A,25(g) a Density Bonus for a housing development project with 43 dwelling units, including 11 set aside for Very Low Income Households, with reduced vehicular parking pursuant to AB 2097, and requesting the following Incentives and Waivers of Development Standards:

a. An on-menu incentive to permit a 10% decrease in required northerly side yard setback to allow a 7-foot 2-inch side yard in lieu of the 8 feet required by the RD2 and R3 zones pursuant to LAMC 12.09.1-B,2(a) and LAMC 12.10-C,3, respectively; and

b. An on-menu incentive to permit averaging over the project site located on three continuous parcels zoned RD2-1 and R3-1; and

c. A waiver of development standards to permit 17-foot increase in height to a maximum of 62 feet with no step-back after 45 feet of height in lieu of the maximum 45 feet allowed in the RD2-1 and R3-1 zones pursuant to LAMC 12.21.1; and

d. A waiver of development standards to permit a 34% increase in allowable floor area to allow 42,222 square feet of floor area (FAR 4:1) in lieu of the 31,590 square feet (FAR 3:1) permitted pursuant to LAMC 12.21.1-A, 1; and

e. A waiver of development standards to permit a 38% reduction in open space to allow 2,950 square feet of usable open space in lieu of the 4,725 square feet required by LAMC 12.21.-G,2; and

3. Pursuant to LAMC Section 12.24-U,26, a Conditional Use Permit to allow a Density Bonus for a housing development project in which the density increase is greater than otherwise permitted by LAMC Section 12.22-A,25.

Thank you again for your consideration,
Jessica Tiao

Renata Ooms <renata.ooms@lacity.org>

Opposition to 43 unit, 5 story tower @ Kelton Ave & Regent Street

John Siscel <johnsiscel04@gmail.com> Tue, Apr 18, 2023 at 5:13 AM
To: renata.ooms@lacity.org, councilmember.yaroslavsky@lacity.org, info@palmsnc.la

Dear Dept. of City Planning and Elected Representatives,

I am a long time resident of Palms, having lived here since 2008. I live just up the street from the proposed 43 unit tower before the Department of City Planning.

I am urging you to stop the proposed five-story, 43-unit tower at Kelton Ave. & Regent St. This is a single family residential neighborhood, and adding a 43 unit building will exacerbate existing parking and traffic issue, and adding dozens of cars to dangerously narrow streets will make our neighborhood less safe to walk and drive. Furthermore, a giant apartment tower in our single family street should not be permitted for zoning purposes.

I have signed the petition at https://www.change.org/Kelton-and-Regent.

I strongly urge you to deny the project's permits. Please do not allow it to be built. Please protect our neighborhood!

Best,

John Siscel
3623 Kelton Avenue
Los Angeles, CA 90034

Renata Ooms <renata.ooms@lacity.org>

OPPOSITION TO THE PROPOSED FIVE-STORY DEVELOPMENT AT 3676-3704 KELTON AVENUE 90034

Joyce McClure <jjc612@yahoo.com> Wed, May 3, 2023 at 8:10 PM
To: "councilmember.yaroslavsky@lacity.org" <councilmember.yaroslavsky@lacity.org>
Cc: "dylan.sittig@lacity.org" <dylan.sittig@lacity.org>, "renata.ooms@lacity.org" <renata.ooms@lacity.org>, Vanessa Saldana <vanessa.saldana@lacity.org>, "randell.erving@palmsnc.la" <randell.erving@palmsnc.la>

RE:Case Number CPC-2023-582-CU-DB-HCA-PHP;
Environmental Case Number ENV-2023-583-CE.
PROPOSED FIVE-STORY DEVELOPMENT AT 3676-3704 KELTON AVENUE, LA 90034

May 3, 2023

Dear Councilmember Yaroslavsky,

It has come to my attention that you have not come out against the above referenced project. I cannot express how disappointed I am that you are not concerned about the wellbeing of this neighborhood, where the majority of the people living here voted for you. We hoped that protecting the quality of life for your constituents would take precedence over supporting a huge development that benefits only a very few. I can think of no valid reason for supporting a 43 unit development, with 11 parking spaces in the middle of an already congested residential area, where traffic has already been negatively impacted by the plethora of apartments that have been (and are still being) built on nearby Overland Avenue. Commuter traffic has spilled over onto Charnock Road, as well as on the narrow north-south residential streets in this area, creating difficulty for residents. I am also greatly concerned about the noise, dust and further traffic hinderances the building of this development would create.

If you have visited the site of the proposed development, you are aware that Kelton Avenue, along with most of the neighboring streets, is not even wide enough, in some parts, for two opposing vehicles to pass each other without one of them pulling over to the side of the street. Currently, there is only a two-way STOP sign at the intersection of Kelton and Regent (the proposed building site), further attesting to the residential quality of this neighborhood which would be destroyed by this huge development. If that is not enough, please explain where we are to find street parking, with the addition of a 43 unit building, offering only 11 parking spaces in an area already greatly impacted by too few places to park on the street. If a project of this magnitude goes through, residents of Kelton Avenue **AND** the adjoining streets will be forced to park blocks away on street cleaning days. Do you have any remedy included in the building plan for this?

I question why ALL people who would be impacted by such a huge development were not notified of the April 18, 2023 meeting. **The magnitude of this development encompasses an area much larger than a 500 foot radius!** Traffic in the area and reduced parking will have a domino affect for half a mile, at the least. It will alter traffic patterns and create even more congestion on all neighboring streets. Five hundred feet may be the legal requirement, but one expects elected and appointed officials to consider input from everyone who will be impacted when making major decisions. Why not inform everyone in Palms and neighboring Culver City who will feel the impact of this development? This project has certainly NOT been publicized and elected officials DO have voters' addresses where information and planning meeting times can, and should, be sent.

I have lived in my home for almost thirty-seven years. Moving away is not an option for me or most people who live in this area. I have seen the population density increase and the traffic and parking on my street become a huge issue, particularly during the evening commute. I no longer expect to find a place to park on my street and have become used to waiting for traffic to clear to cross Charnock Road. I have been a victim of the uptick in crime and have also had my vehicles hit due to the lack of clearance for two vehicles to pass each other on the street, losing two sideview mirrors and leaving scratches and dents on two separate vehicles during the past year. I have also witnessed several fender benders on my street in the last few years. Purposefully creating any more traffic in this area would create an even more dangerous situation.

As a long-time supporter of the Yaroslavsky political family (I recently found an old "Zev" potholder in my linen closet!), I am saddened to see that big business may be more important to you than the comfort and wellbeing of your constituents. I hope you will choose to put the quality of life of the many residents of this area ahead of the profits of a few and fairly represent us in this matter. I truly do not believe that this community can support a
43-unit building without even adequate parking for its residents. I respectfully propose scaling down this project to a two-story building, with enough parking spaces to serve all of its residents, despite whatever the state has approved. Adding any more traffic to the already narrow and crowded streets is dangerous and will serve to further diminish the quality of life for everyone in the neighborhood--far past a five hundred foot radius. Please consider this and please take a stand in favor of supporting your constituents in this community and preserving what is left of our quality of life

Sincerely,

Joyce McClure - Homeowner
3644 Veteran Avenue
Los Angeles, CA 90034
310-488-6702

Renata Ooms <renata.ooms@lacity.org>

OPPOSITION TO THE PROPOSED FIVE-STORY DEVELOPMENT AT 3676-3704 KELTON AVENUE 90034

Joyce McClure <jjc612@yahoo.com> Thu, May 4, 2023 at 9:20 AM
To: Renata Ooms <renata.ooms@lacity.org>

Dear Renata,

Thank you so much for your quick response and for correcting the misinformation I had regarding parking spaces. Although I am strongly in favor of low income housing, I cannot support 43 units on such a narrow street in an already overcrowded neighborhood where parking is already scarce and traffic is becoming worse every day.

As stated, my issue is not with very low income housing in my neighborhood, which I wholeheartedly support, but rather with creating a huge out-of-character development in an already overcrowded neighborhood. My suggestion would be to scale the development back to 22 units, offering half to very low income residents, and increasing the parking spaces to 44, since most households have more than one vehicle. This would serve to mitigate some of the project's negative impact on the community. Perhaps the developers could be given other incentives to provide low income housing, which, I assume is why this project is even being considered for approval in this area.

Today, I am forced to remain in my house or leave without a car. It is street cleaning day on the opposite side of the street so I can't move my car from in front of my house because I won't find another place to park on my street. I also cannot move my car, because I need the space it's occupying to put out my trashcans later today, since tomorrow is trash collection day. This is a challenge those of us who live in single family houses, or small apartment buildings, with City trash services, have to face each week. If we don't "save" a place in front of our homes to place our trashcans, we are forced to keep our trash for another week or block our driveway with our trashcans, necessitating searching for a parking space that may be blocks away. Anything that adds to this situation is unacceptable.

Again, thank you for your response and correction of information. Please inform me of any upcoming meetings regarding this proposed development. If there is anyone else I need to contact regarding my opposition to approval of this proposed development, I would greatly appreciate their contact information.

Thank you,

Joyce

Joyce McClure - Homeowner
3644 Veteran Avenue
Los Angeles, CA 90034
310-488-6702

Sent from my iPhone

> On May 4, 2023, at 8:00 AM, Renata Ooms <renata.ooms@lacity.org> wrote:
>
> [Quoted text hidden]

Renata Ooms <renata.ooms@lacity.org>

OPPOSITION TO THE PROPOSED FIVE-STORY DEVELOPMENT AT 3676-3704 KELTON AVENUE 90034

Joyce McClure <jjc612@yahoo.com> Tue, May 9, 2023 at 9:56 AM
To: Jesi Harris <harrislanduse@gmail.com>
Cc: "councilmember.yaroslavsky@lacity.org" <councilmember.yaroslavsky@lacity.org>, Dylan Sittig <dylan.sittig@lacity.org>, Renata Ooms <renata.ooms@lacity.org>, Vanessa Saldana <vanessa.saldana@lacity.org>, Randell Erving <randell.erving@palmsnc.la>, Brian Silveira <silveira.brian@gmail.com>

Hello, Jesi,

Thank you for your response to my strong opposition to the proposed 5-story, 62' high, development at 3676-3704 Kelton. I am glad to hear that the parking spaces have been increased. However, realistically, unless there will always be one person/1 car in 33 of the units and no vehicles, whatsoever, in 10 of the units, this still creates a negative, domino effect on parking throughout the neighborhood. Where will the estimated 10 – 20 additional vehicles park? A friend, who visited last Thursday evening, at 7 P.M., needed to park 3 blocks from my house, due to the scarcity of parking. On my street, alone, I am aware of one family that has 2 tandem parking spaces and at least 5 cars, taking up a minimum of 3 spaces on the street. Another building has residents who take up 4 street parking spaces. It is a constant struggle to find street parking, particularly on street cleaning and trash pick-up days and there certainly fewer people living on my street than in the area of Kelton where you propose to build. I know that people are disgusted by the constant effort to park near their homes. Personally, I am tired of searching for blocks to find a spot and on some days don't even leave home because I know I won't find a place to park near my house. I cannot imagine parking intentionally being made any worse.

I, too would love to see more people using public transportation, but this is not realistically going to happen in the next few years—or until this area provides a system where people do not have to walk or changes buses or trains and spend a considerable amount of extra time to get to and from work. The bus system does not adequately service the Westside. If one wants to go downtown or into the Valley by bus, one must change buses and spend time waiting, plus additional transportation is almost always necessary when one reaches the vicinity of one's destination. I do not find that this area provides great transportation links, since for every job I have had since living here for the past 36+ years, there was no way I could reasonably take public transportation to any of my jobs. For my job less than 3 miles away, public transportation would have quadrupled the amount of time it took me to get there by car, plus I would have had to walk and wait in areas where I would not have felt safe. I do not understand why or by whom this area was characterized as being well-served by transportation links. Personally, I find them severely lacking.

No one was more excited than I to see the Metrolink Rail System start servicing the Westside. I used the train on the very first day it opened to the public and used it frequently when it first opened in my area, even though it required that I drive to a station and park in order to take it, making a car still necessity. Sadly, after several incidents, I no longer felt comfortable or even safe on the Metro. I do not know anyone who regularly rides it any longer, other than for special occasions and with a group of people. I do not believe it will be a chosen option for daily work transportation until people can be feel they will be safe while riding it and will not have to find additional transportation when they reach their station. L.A. simply does not have the type of transportation system that New York and other major cities have. We lack the number of stations needed for people to be able to walk directly to their destination after disembarking from the Metro. Until transportation greatly improves, the West Los Angeles area will be vehicle dependent.

From your response, it appears that you have not visited or driven through Palms. There is a plethora of large multifamily apartment buildings, everywhere. Palms is already densely populated! On Overland, between Palms Boulevard and Venice Boulevard, there are currently six huge, multistory developments in the process of being built or having just been completed. However, Overland is a main street, with 4 lanes of traffic, and not a two-lane street where two cars can barely squeeze by each other when cars are parked along the street and where and one car often has to pull over to allow the opposing car to pass, as is Kelton Avenue and its neighboring streets to the west. Certainly, some of these units along the main boulevard can, and should, be used to house very low-income residents. Again, I welcome having very low-income residents in my neighborhood. I'd like to see them included in existing buildings and would be pleased to see you build two, two-story buildings of six units each, with adequate parking, exclusively for ultra-low-income residents. My opposition is to a 62' high building that is out of place and unsafe in this area, where residents are dependent upon street parking and whose vehicles would clog the already congested, narrow streets, adding to already unsafe conditions.

You appear to support that what's left of the single-family housing, mixed with duplexes and small apartment buildings, in Palms should be eradicated so 11 very low-income apartment units can be built and you can still make a profit from the other 32 units of your proposed complex, when, Midvale, the next street going east from Kelton (between Palms and Venice) is lined with large apartment buildings, as are the next two streets to the east, Westwood and Glendon. Additionally, the four streets between Overland Avenue and Motor Avenue also contain large apartment buildings, suited for your plans, as well as the six streets east of Motor Avenue. Certainly, these areas of Palms are a better solution for the huge complex that you desire to erect. Why not build in one of these areas of Palms? You are asking that I, and all other single-family home and smaller apartment building dwellers, give up our quality of life for 11 very-low-income units that could be placed in any one of the building on the streets in Palms that are already completely devoted to large apartment buildings--or a smaller building to house only these residents could be build at your proposed site.

I understand there are tradeoffs for builders to get permits, but no community should ever suffer so any person or company can make a profit. To insist on placing your development on Kelton seems absurd when there are clearly these other areas of Palms that are more suitable for your project that will not alter the nature of the neighborhood or create or further existing unsafe traffic and parking conditions. Will you next request that the single-family homes, between Charnock and Palms and Palms and National be rezoned and converted into multifamily dwellings to further increase the density of this residential area that is already bursting at its seams from the amount of traffic on narrow streets with an already severe lack of parking for its current residents? I cannot believe that the purpose of any law or plan is to destroy one neighborhood for the benefit (and profit) of a few. This makes no sense to me. I see it as bad business and bad government.

My plan is to NOT build any large developments on the narrow streets in Palms and NOT destroy the quality of life for current residents. I, and the residents of this area, implore you to move your 62' high development to a more suitable place that is already filled with large complexes and has streets wide enough to safely to support this type of development—and where it would fit in with the existing structures. Those of us who moved here want to live in a mixed area, containing single-family homes, duplexes and small apartments. We do not want to pushed out by incongruously large and out-of-place developments that create unsafe conditions, further parking issues and, literally, block out our sunlight. Many of us do not have the option to move.

I am not being rude or flippant, but how can you possibly justify ruining the quality of life and increasing dangerous driving conditions in a community just to make a profit? Why don't you choose somewhere else to build that is suitable for what you would like to build? Many of us question how high the rents will need to be in the majority of your proposed units to make up for your 11 low-income units. Rents are already skyrocketing all over the Westside. We need affordable housing for everyone, not just 11 families.

I learned long ago that legal and moral are often mutually exclusive. I remain adamantly opposed to a project of this size being built on a narrow, crowded street. Just because you possibly can build a huge building, if you include very low-income housing, this does not mean that it is the right thing to do. Certainly, negatively impacting the lives of everyone else in this community should be the best argument against allowing your company to come here to make a profit. Personally, I don't feel that it is right to take something from someone else to make a profit. I respectfully ask that you consider the effect your development would have on all of the current residents of this community and move it elsewhere—or, at the least, scaled it down to a maximum total of a two-story, 12-unit, low-income development, that includes adequate parking, required or not, that would fit in with the community, minimally increase traffic and parking and not block out the light. In the end, people should always come before profit. I am sure you can go somewhere else and make a profit where it would not harm others. I cannot support your proposed building project and remain resolutely opposed to any large development at 3676-3704 Kelton Avenue.

Thank you,

Joyce McClure
3644 Veteran Avenue
Los Angeles, CA 90034
310-488-6702

On Thursday, May 4, 2023 at 01:22:39 PM PDT, Jesi Harris <harrislanduse@gmail.com> wrote:

Hi, Joyce,

I hope you're well.

My name is Jesi and I'm part of the project team representing the proposed development at 3676-3704 Kelton Ave. I'm responding to an email you sent to the assigned City Planner Renata Ooms on Wednesday, May 3rd. I wanted to clear up some things about the project details and goals as well as make myself available for you to ask any questions you have.

The proposed apartment building includes 43 units and 33 parking spaces (the number of parking spaces has increased in response to some concerns expressed by the community). Of the 43 apartment units, 11 will be reserved for Very Low Income households. As I'm sure you know, Los Angeles (and California cities in general) are suffering from a massive housing shortage that is driving up rents and pushing Angelenos into homelessness. I, and the rest of the project team, are doing the best we can to help meet the growing need for both affordable and market rate housing.

To that end, the Density Bonus policy is a state-based program created in the 1970s (and since enhanced with policy updates) to incentivize developers to include affordable units in their developments. The program seeks to make the private development of affordable units financially feasible.

One of the causes of our city's deep housing shortage is low zoning in urbanized areas, like Palms. Cities that do a good job of providing adequate housing for their population include a mix of housing typologies in the city center where residents can access local economic, educational, recreational, and transportation resources. The Palms neighborhood is strong in all of these regards and, therefore, an ideal location for the kind of dense, transit-oriented development that we need to meet (née, exceed) our housing obligation. In fact, this area is so well-served by transportation alternatives that a recent state bill (AB 2097) prohibited the city from requiring any new parking in new developments in neighborhoods like Palms although, as stated above, we are offering 33.

The truth is, parking costs a lot to build (while encouraging car ownership. It's infeasible for developers to build a lot of parking *and* affordable units. (I found this article very illuminating, I recommend giving it a listen.) The development encourages the use of other modes based on its proximity to quality transit and local amenities, as well as its inclusion of 76 bicycle parking spaces. Discouraging the use of single-occupant vehicles is part of the City's greenhouse gas emissions and traffic violence reduction goals.

We are actively working with neighbors in your community to support traffic-calming improvements along Kelton Ave. As an advocate for housing and mobility justice, I encourage you to join us as we work to increase the housing supply while making sure the streets are safe for everyone. I'm confident that, if we work together, we can make an inclusive, safe community for everyone.

Please feel free to reach out by email or phone (number in signature) at any time if you have questions or comments about the project.

Best regards,
Jesi Harris

--
Jesi Harris
Planning Project Manager, Brian Silveira & Associates
704.277.7332

Renata Ooms <renata.ooms@lacity.org>

PROPOSED FIVE-STORY DEVELOPMENT AT 3676-3704 KELTON AVENUE, LA 90034

Joyce McClure <jjc612@yahoo.com> Mon, May 22, 2023 at 4:47 PM
To: Renata Ooms <renata.ooms@lacity.org>

RE:Case Number CPC-2023-582-CU-DB-HCA-PHP;
Environmental Case Number ENV-2023-583-CE.
PROPOSED FIVE-STORY DEVELOPMENT AT 3676-3704 KELTON AVENUE, LA 90034

Dear Ms. Ooms,

Please add me to the list to be informed about the above proposed development. I thought I had already requested this, but I have not heard anything, other than the initial letter informing me of the proposed development.

Please note that I am adamantly opposed to a project of this size, on a narrow street, in an already crowded area of Palms, where parking is so difficult that residents must park streets away from their homes and apartments and walk through a neighborhood that is no longer safe. As you are surely aware, this area is not well served by public transportation and, at this time, residents do need vehicles to commute.

Please do not allow this developer to destroy this neighborhood by allowing him/her to get this project through **simply because he/she is adding 11 very low income units**. We all assume that the rest of the rents, and the lack of space devoted to parking, **will allow this developer to make huge profits on rents, while destroying the quality of life for everyone else in the neighborhood**. Is it really worth hurting the rest of the neighborhood for 11 units?? There are many places where very low/low income rents can be built that are not in huge developments **in the middle of quiet, residential neighborhoods**. **PLEASE C0NSIDER THE PEOPLE WHO CURRENTLY LIVE IN THIS AREA.**

When I asked my neighbors what they thought about this, most had not even heard about the development. It appears that **ONLY those who LEGALLY HAD TO BE INFORMED, WERE INFORMED.** Not a single neighbor was in favor of this when they learned what was proposed. **IS IT TOO LATE FOR THEM TO VOICE THEIR OPINIONS? Keeping people who will be affected by this project UNINFORMED OF THIS PROPOSED DEVELOPMENT WAS NOT IN THE SPIRIT OF BETTERMENT OF OUR COMMUNITY! Other than 11 units, ONLY THE DEVELOPER WILL BENEFIT. DO YOU CONSIDER THIS ETHICAL AND FAIR?? THIS IS NOT ABOUT PROVIDING LOW COST HOUSING. IT IS ANOTHER CASE WHERE BIG BUSINESS COMES IN AND (LITERALLY) BULLDOZES THE RESIDENTS. THE DEVELOPERS CAN GET WHAT THEY WANT WITH THE 11 UNITS. WHAT ABOUT THE REST OF US?? How many people do you think will move, rather than live in close proximity to a huge building? Where will they find rents they can afford??**

PLEASE DO NOT ALLOW THIS TO BE ANOTHER EXAMPLE WHERE BIG BUSINESS/DEVELOPERS ARE ALLOW TO PROFIT ON THE BACKS OF THE CURRENT RESIDENTS. PLEASE DO NOT RUIN OUR QUALITY OF LIFE!

I am so disappointed in my elected officials, and other, for not standing up for our community.

Sincerely,

Joyce McClure, Owner
3644 Veteran Avenue
L.A., CA 90034
310-488-6702

Renata Ooms <renata.ooms@lacity.org>

MORE QUESTIONS REGARDING THE PROPOSED FIVE-STORY DEVELOPMENT AT 3676-3704 KELTON AVENUE, LA 90034

Joyce McClure <jjc612@yahoo.com> Mon, May 22, 2023 at 8:54 PM
To: "mayor.helpdesk@lacity.org" <mayor.helpdesk@lacity.org>, "councilmember.yaroslavsky@lacity.org" <councilmember.yaroslavsky@lacity.org>, "dylan.sittig@lacity.org" <dylan.sittig@lacity.org>, "renata.ooms@lacity.org" <renata.ooms@lacity.org>, "vanessa.saldana@lacity.org" <vanessa.saldana@lacity.org>, "randell.erving@palmsnc.la" <randell.erving@palmsnc.la>
Cc: Me <jjc612@yahoo.com>

RE:Case Number CPC-2023-582-CU-DB-HCA-PHP;
Environmental Case Number ENV-2023-583-CE.
PROPOSED FIVE-STORY DEVELOPMENT AT 3676-3704 KELTON AVENUE, LA 90034

May 22, 2023

Hello, all concerned with this project:

IT HAS COME TO MY ATTENTION THAT ALMOST NO ONE IN MY NEIGHBORHOOD HAS ANY IDEA ABOUT THIS PROPOSED PROJECT! After speaking with so many people in my neighborhood who had no ideas this project was being considered, I realize that I (and my neighbors) have other questions we would like answered regarding why this development is being considered for approval:

1. WHY WAS EVERYTHING KEPT SECRET? WHY WERE NEIGHBORS NOT INFORMED OF SOMETHING THAT AFFECTS THEIR LIVES SIMPLY BECAUSE THEY LIVE OUTSIDE A 500' RADIUS?? EVERYONE I'VE SPOKEN WITH FEELS THIS WAS NOT PROPERLY PUBLICIZED. NO ONE EVER EXPECTED SOMETHING LIKE THIS TO EVEN BE CONSIDERED! WE ALL FEEL BLINDSIDED!!

2. When I moved to my current address, in July of 1986, my property was designated as R-3. We left for the summer and came back to the two Craftsman cottages across the street being converted into a 2-story apartment building, covering both lots--with, at least, adequate underground parking for all apartments.

When we were undecided as to whether we wanted to live with this large apartment building across from us, we inquired into building an apartment on our property, selling and moving to another area. First, we were told there was a moratorium on building. Later, we were told that our property had been downzoned from R-3 to
RD-2 and that no apartments could be built on our side of the street, even if we were able to acquire the house next door and own two lots. **WE WERE TOLD THAT THE AREA WAS TOO DENSE AND THE STREETS WERE TOO NARROW TO ACCOMMODATE ANOTHER APARTMENT BUILDING AND THE ADDITIONAL TRAFFIC IT WOULD BRING INTO THE NEIGHBORHOOD!** THIS WAS HOW IT WAS, EVEN THOUGH MY HOUSE IS **NEXT DOOR** TO AN APARTMENT BUILDING, **HAS APARTMENTS ACROSS THE STREET FROM IT** AND **AN APARTMENT BUILDING BEHIND IT**! I ACCEPTED THIS AND THE LOSS OF POTENTIAL PROFIT THAT OTHERS IN THE AREA HAD ENJOYED FROM BUILDING RENTAL PROPERTY AND SETTLED INTO A LIFE IN A NICE NEIGHBORHOOD THAT I THOUGHT WAS STABLE, **BUT NOW IS BEING THREATENED.**

CAN SOMEONE PLEASE EXPLAIN WHY THE STREETS ARE NO LONGER TOO NARROW AND THE AREA CAN, ALL-OF-A-SUDDEN, ACCOMMODATE EVEN MORE TRAFFIC THAN THE INCREASED TRAFFIC WE HAVE ALREADY BEEN EXPERIENCING DUE TO THE, AT LEAST, 6 HUGE BUILDINGS THAT HAVE BEEN OR ARE BEING ERECTED ON OVERLAND AVENUE, WHICH IS, AT LEAST, WIDE ENOUGH FOR TWO CARS TO PASS WITHOUT ONE PULLING OVER??? WHAT KIND OF POLITIC IS THIS?? THIS IS NOT ABOUT 11 UNITS OF LOW-INCOME HOUSING. IT IS ABOUT DEVELOPERS COMING INTO OUR NEIGHBORHOOD AND CHANGING IT WITH A HUGE DEVELOPMENT. IT AFFECTS EVERY FAMILY FROM VENICE TO PALMS AND OVERLAND TO SEPULVEDA, AS FAR AS A DOMINO EFFECT OF INCREASED TRAFFIC AND ALREADY SCARCE PARKING. IT WOULD ABSOLUTELY DIMINISH OUR QUALITY OF LIFE.
FOR EXAMPLE, I WITNESSED A SCREAMING MATCH OVER A PARKING SPACE ON MY STREET ABOUT TWO WEEKS AGO. PARKING HAS BECOME A TRULY HUGE ISSUE. WILL THIS BECOME A DAILY OCCURRENCE, WITH ANOTHER 20-40 VEHICLES ENTERING INTO THE PARKING FIASCO?

WAS I NOT ABLE TO BUILD BACK THEN, WHEN WE CONSIDERED IT AFTER THE APARTMENT WENT UP, BECAUSE I DID NOT HAVE THE POLITICAL CONNECTIONS?? NOW, I WONDER. I ALSO WONDER WHAT THIS HUGE DEVELOPMENT WILL DO TO MY PROPERTY VALUE. AS A RETIRED, HANDICAPPED PERSON, I DO NOT HAVE THE OPTION OF PICKING UP AND MOVING ANYWHERE IN THIS AREA IF MY PROPERTY GOES DOWN IN VALUE FROM THE INCREASE IN DENSITY, LACK OF PARKING AND INCREASE IN TRAFFIC FROM THIS DEVELOPMENT. WILL I NEED TO LEAVE MY HOME OF 36+ YEARS AND MOVE FAR AWAY FROM MY FAMILY AND FRIENDS SO THIS DEVELOPER CAN MAKE A PROFIT--OR, SIMPLY, SUFFER THROUGH MORE TRAFFIC AND ANGRY DRIVERS SQUEEZING DOWN UNSAFE STREETS, ALONG WITH CONSTRUCTION NOISE AND DUST CLOGGING THE AIR?? I DON'T SEE ANY GOOD CHOICE, FOR ME.

I THINK THIS WHOLE SITUATION NEEDS TO BE INVESTIGATED, AS DO MY NEIGHBORS. WE FEEL THAT BECAUSE THERE ARE SO MANY RENTERS, THE DEVELOPER CHOSE THIS AREA TO AVOID A HUGE OPPOSITION TO THIS PROJECT BECAUSE PEOPLE WERE NOT PROPERLY INFORMED AND DID NOT UNDERSTAND WHAT IS GOING ON. WE SINCERELY DOUBT THAT THIS WOULD HAVE SLIPPED BY UNNOTICED IN AN AREA WITH MOSTLY OWNER-OCCUPIED HOMES. WE ARE TRULY CONCERNED THAT THIS WHOLE PROJECT WAS HUSHED UP AND THAT A DEVELOPER COULD BE ALLOWED TO PUT IN 11 UNITS OF LOW-COST HOUSING AND BE ENTITLED TO BUILD A 5-STORY BUILDING,THAT WILL STICK OUT IN THIS AREA AND THAT DOES NOT HAVE ADEQUATE PARKING FOR ITS TENANTS IN AN AREA WHERE PUBLIC TRANSPORTATION IS NOT A VIABLE OPTION FOR MOST PEOPLE. HAVING THE METRO A MILE AWAY IS NOT CLOSE ENOUGH FOR MOST PEOPLE WITH JOBS TO WALK TO AND TAKE PUBLIC TRANSPORTATION ON A DAILY BASIS. THIS AREA IS CAR-DEPENDENT, AT THIS TIME, AND WILL BE UNTIL ADEQUATE PUBLIC TRANSPORTATION IS CLOSE BY. WHY NOT WAIT UNTIL THEN TO PUT IN THIS TYPE OF BUILDING? TAKING THE METRO IS SIMPLY NOT A SAFE OR CONVENIENT ALTERNATIVE TO OWNING A CAR AT THIS TIME, IN THIS AREA, NOR IS THE METRO SOMETHING ONE WOULD CHOOSE TO TAKE AT NIGHT.

BASICALLY, WE WOULD LIKE TO KNOW WHY THIS WOULD EVER EVEN BE CONSIDERED ON SUCH A NARROW STREET. MOST PEOPLE DO NOT HAVE ANY IDEA WHERE TO LOOK TO FIND OUT WHAT IS UNDER CONSIDERATION FOR BUILDING IN THEIR AREA. WE EXPECT OUR REPRESENTATIVES TO INFORM EVERYONE IN THE NEIGHBORHOOD OF ANY POTENTIAL DEVELOPMENT THAT WOULD CHANGE OUR QUALITY OF LIFE. IF WE ARE CLOSE ENOUGH TO BE AFFECTED BY A DEVELOPMENT, WE SHOULD BE INFORMED THAT IT IS BEING CONSIDERED TO BE BUILT!!! WE EXPECT OUR REPRESENTATIVES TO

CONSIDER OUR INTERESTS AND NOT THOSE OF A DEVELOPER. WE WILL BE VOTING BASED ON HOW OUR REPRESENTATIVES SHOW THEIR CONCERN FOR OUR NEEDS.

3. WHAT DO MY NEIGHBORS AND I WANT RIGHT NOW? WE WANT TO KNOW WHY NO ONE WE SPOKE WITH SEEMS TO HAVE HEARD ANYTHING ABOUT THIS PROPOSED PROJECT??????
WE WANT YOU TO PUT UP POSTERS, MAIL FLYERS AND HAVE SEVERAL MORE MEETINGS TO PUBLICIZE WHAT IS PROPOSED SO THAT EVERYONE IN THIS NEIGHBORHOOD (PALMS TO VENICE & SEPULVEDA TO OVERLAND) UNDERSTANDS WHAT THIS DEVELOPER WANTS TO BUILD IN OUR NEIGHBORHOOD. WE WANT OUR VOICES HEARD!!!

These are not rhetorical questions. My neighbors and I really do want answers. After speaking with my neighbors and learning that they, too, are infuriated about not being previously informed and DO NOT know what to do to FIGHT THIS PROJECT, I have further strengthened my opposition to this negatively life-altering project. I AM APPALLED by the manner in which this project has tried to sneak past people who will be impacted from it, so that 11 units and ONE developer will have better lives. Because the developer paid a large sum to acquire the land and must provide low cost housing as a requirement for having his project even considered, does this mean the rest of us need to have our lives changed so the developer can make money? Do the rest of us not matter at all??

Sincerely,

Joyce McClure, Homeowner
3644 Veteran Avenue
Los Angeles, CA 90034
310-488-6702

Renata Ooms <renata.ooms@lacity.org>

CPC-2023-582-CU-DB-HCA-PHP

1 message

Kathryn Reesman <krbruin@sbcglobal.net> Thu, Jun 15, 2023 at 12:05 PM
To: renata.ooms@lacity.org

Hello Ms Ooms,

This is regarding the proposed development to be located at 3676-3704 S. Kelton Ave. in Los Angeles 90034 (Palms)

Do the plans for this proposed structure show there will be at least one elevator?
For the density, size and expected population, would even two elevators be recommended or required?

Has any consideration or recommendation been given to having one (or more) unit meet the ADA requirements?
With this new build, accommodations for a wheelchair user or otherly-disabled tenant could be included more easily than trying to adapt an older, existing structure.
With elevator(s), would at least one unit contained here meet the 'No stairs' requirement? a ground-floor unit?
Also designed to accommodate a handicap-accessible bathroom and kitchen, with continuous level flooring throughout?
Accommodation for a larger, van-type vehicle in the garage may also be taken into consideration.
It is Very difficult to find handicap-accessible housing anywhere in Los Angeles. For a wheelchair user, or a family with a disabled child. This seems an option worthy of consideration, in this case.

Thank you,
Kathryn Reesman

Renata Ooms <renata.ooms@lacity.org>

STRONGLY OPPOSE the construction of 43 unit apartment building on 3676-3704 South Kelton Avenue; 19845 West Regent Street

Kelly <kelly.lally@gmail.com> Tue, Apr 11, 2023 at 7:51 PM
To: councilmember.yaroslavsky@lacity.org, "renata.ooms@lacity.org" <renata.ooms@lacity.org>

Dear Ms. Yaroslavsky & Ms. Ooms -

I am writing to express my **strong opposition** to the permit application (Case Number: CPC-2023-582-CU-DB-HCA-PHP) for the 43-unit apartment building in our neighborhood. As a long-time member of this community (who lives directly across from the proposed housing project), I strongly believe that this development would have a detrimental impact on the character of the neighborhood and the quality of life of its residents.

Our neighborhood is a peaceful, family-friendly area that has always been characterized by single-family homes, low-density housing with many work-from-home residents. A building of this size would significantly alter the landscape of our community, leading to increased traffic, noise, and congestion in the area - making it nearly impossible to be able to work from home.

The proposed development also creates insurmountable problems, such as:

- **Safety hazards -** particularly during peak hours - as the roads are very narrow and residents are already playing a game of “chicken" as it is.
- **Lack of Parking** - our neighborhood simply cannot support a project of this size. The planned underground parking structure only accommodates 26 spaces, whereas 2 people per 43 units = 86 spaces.
- **Clear-cutting of 15-20 mature trees** (many of which are over 50 years old) taking with it the biodiversity of wildlife (rabbits, cats, squirrels, birds) that call it home.
- **Upward pressure on rent prices**, as landlords and property owners raise rents to capitalize on the new apartment buildings (vs. older “dingbat” style apartments that make up the neighborhood currently).
- **Noise Pollution** - during construction as well as from the additional residents to the neighborhood and in such a small space. This puts my job and family at risk of financial hardship, since I work from home.

I beg you not to sell our neighborhood to investors and developers who have no interest in the vitality and long-term health of our community. **Please deny this permit application.**

Thank you for your attention to this matter and I look forward to your response.

Best,
Kelly Lally
3707 Kelton Avenue
Los Angeles, CA
Phone: (650) 580-6096

Renata Ooms <renata.ooms@lacity.org>

Re: 43 unit apartment building on 3676-3704 South Kelton Avenue

5 messages

Kelly <kelly.lally@gmail.com> Mon, Apr 17, 2023 at 4:17 PM
To: Jesi Harris <harrislanduse@gmail.com>
Cc: councilmember.yaroslavsky@lacity.org, Brian Silveira <silveira.brian@gmail.com>, "renata.ooms@lacity.org" <renata.ooms@lacity.org>, plum@palmsnc.la

Hi Jesi -

I don't believe I authorized the sharing of my information - so would love to know how you obtained my contact information. Let's start there.

My responses are provided in blue below for your reference and I have added the Palms Neighborhood Council.

TL; DR:

- **Unchecked development in our city if this project is approved - it's scary how many permit exemptions are being asked for when these exemptions exist to protect the community**
- **Our neighborhood is simply too small and the roads too narrow to support a development of this size**
- **Business Profits are coming at the expense of the quality of life of residents, the health of our community and environmental diversity (this needs to END NOW!)**
- **Noise Levels will make it impossible to work from home - causing loss of income for families**
- **Unsafe conditions with construction noise, dust, street closures, traffic and additional crime brought in from Venice & Palms Blvd**

[Jesi] I empathize with the traffic and parking pressures inherent to added housing supply. Because of both the severe housing shortage and environmental crises in which we find ourselves, leaders at the state and local levels are prioritizing the production of housing units, especially affordable housing, over accommodating vehicular movement and storage. Especially in areas - like the Palms Neighborhood - which are rich in economic and transportation resources. Because of recently implemented legislation (AB 2097), we do not have a minimum parking requirement here. That said, we are adding additional parking to the subterranean garage to provide a total of 31 parking spaces onsite. I hope this helps ease some of your concerns.

[Kelly's response] Quite frankly, a project of this size needs to be built on a road like Palms Blvd or Venice Blvd, not a small, quiet neighborhood of single family homes like ours. You are changing the character of our neighborhood, damaging the quality of life for residents and making it impossible to live here. Believe me, no one wants this monstrosity in the neighborhood affecting property values. By adding luxury apartments (which no one will be able to afford in this economy), you are not solving anything but your bottomline.

Quite frankly, a building of this size simply cannot even be supported on our street, which is very narrow with already limited parking. You should see how dangerous it is with two cars trying to pass each other on the road. No one can even pull over because there is no space. You wouldn't even be able to get your construction trucks down it. This begs the question:

- Are you going to be closing off the street during the several years of construction?
- Where will your construction equipment be stored or parked?
- Your building is 43 units (2 people each to be conservative) which means we'll need parking for 86 people. How do you plan to address this given that you've only allotted for 31 underground spaces? The math isn't "mathing".

[Jesi] We acknowledge that development comes with trade-offs, including the removal of trees and other vegetation. Believe me, I would like to preserve all existing trees. However, we have to build housing to meet our incredible housing needs. By concentrating dense development in the City center, we can better preserve open space and reduce sprawl which will ultimately lead to more tree (and wildland) preservation in the long run. We are making every effort to preserve the two existing parkway trees while adding four drought-resistant trees onsite. Furthermore, a tree expert conducted a tree study at this site and found that no trees or shrubs on the property are considered protected within the LA Native Tree Protection Ordinance.

[Kelly's response] This is not nearly a solution to the city's housing needs and cutting down 20 mature trees is jeopardizing the health of our community, our children and (not to mention) our planet which is in a global warming crisis. How do you think we get the air that we breathe? What do you think protects us from the hot LA summer sun? What do you think buffers the noise from Palms and Venice Blvd? What do you think is going to protect against all of the debris and dust generated from this project? At some point **WE HAVE TO PUT QUALITY OF LIFE OF RESIDENTS ABOVE BUSINESS PROFITS** of a developer who is going to come in, leave and wash their hands of it.

[Jesi] The proposed project includes 43 apartment units with 11 of those reserved for Very Low Income households. The 11 Very Low Income units will not be permitted to exceed a pre-established rent level for the next 55 years, keeping them available *only* to qualified households during that time period. The market rate units will add to the City's apartment supply, determined to be woefully deficient in the last Regional Housing Needs Assessment. Adding to the Housing supply is the only way to reduce the current market rent rates.

[Kelly's response] Adding to the housing supply with luxury apartments that people will struggle to afford is not the solution. More foot traffic and more crime is going to start coming in from Venice and Palms Blvd. How are you going to ensure that our families are safe from the crime that will come with that?

This project qualifies for a Class 32 Categorical Exemption from further CEQA (California Environmental Quality Act) review. As part of its exemption application, a noise study was completed that determined that neither the construction nor operational activities would surpass the level of acceptable noise impacts as determined by LA municipal code.

[Kelly's response] Not very reassuring that this project is being exempt from CA Environmental Quality Review. Yikes! Also, what is the appropriate level of noise? Kind of laughable when I am the primary wage earner of my family, I live directly across the street, and my job is remote. Are you going to pay my income when my employer terminates my employment for not being able to participate in Zoom meetings with all the construction noise in the background? As of now, I can hear the slightest conversation on the sidewalk. Jackhammers, construction trucks, machinery, crews yelling is going to make it impossible to work let alone enjoy living in this neighborhood. I beg you to understand that **THIS IS HOW I SUPPORT MY FAMILY.** Long term - the increase in residents is going to be a nuisance.

[Jesi] For what it's worth, I can relate to the temporary inconvenience of the construction noise - there are several projects happening in my neighborhood as well and I also work from home. However, given our City's growing housing crisis, I hope you understand the need to build more housing and the patience this will require from all of us. We apologize in advance for any inconvenience this causes you or your family.

This is beyond a temporary inconvenience. Let's talk about what this is really about. DEVELOPERS PROFITING at the expense of the residents who live here. It's DEVELOPERS PROFITING off of our environment which we need to be protecting. It's DEVELOPERS PROFITING over the health of our families. You have to ask yourself, Jesi, when you wake up in the morning, what is this all for? When are you going to realize that your actions are HURTING PEOPLE?

On Wed, Apr 12, 2023 at 5:41 PM Jesi Harris <harrislanduse@gmail.com> wrote:

> Hi, Kelly,
>
> Thank you for reaching out about the proposed mixed-income apartment development at 3676-3704 Kelton Ave. Brian Silveira and I are representatives of the proposed project and we appreciate the opportunity to connect directly with you about your concerns.
>
> I'm responding to the email you sent to City Planner Renata Ooms and Councilmember Yaroslavsky on Tuesday, April 11th.
>
> I empathize with the traffic and parking pressures inherent to added housing supply. Because of both the severe housing shortage and environmental crises in which we find ourselves, leaders at the state and local levels are prioritizing the production of housing units, especially affordable housing, over accommodating vehicular movement and storage. Especially in areas - like the Palms Neighborhood - which are rich in economic and transportation resources.
> Because of recently implemented legislation (AB 2097), we do not have a minimum parking requirement here. That said, we are adding additional parking to the subterranean garage to provide a total of 31 parking spaces onsite. I hope this helps ease some of your concerns.
>
> We acknowledge that development comes with trade-offs, including the removal of trees and other vegetation. Believe me, I would like to preserve all existing trees. However, we have to build housing to meet our incredible housing needs. By concentrating dense development in the City center, we can better preserve open space and reduce sprawl which will ultimately lead to more tree (and wildland) preservation in the long run. We are making every effort to preserve the two existing parkway trees while adding four drought-resistant trees onsite. Furthermore, a tree expert conducted a tree

study at this site and found that no trees or shrubs on the property are considered protected within the LA Native Tree Protection Ordinance.

The proposed project includes 43 apartment units with 11 of those reserved for Very Low Income households. The 11 Very Low Income units will not be permitted to exceed a pre-established rent level for the next 55 years, keeping them available *only* to qualified households during that time period. The market rate units will add to the City's apartment supply, determined to be woefully deficient in the last Regional Housing Needs Assessment. Adding to the Housing supply is the only way to reduce the current market rent rates.

This project qualifies for a Class 32 Categorical Exemption from further CEQA (California Environmental Quality Act) review. As part of its exemption application, a noise study was completed that determined that neither the construction nor operational activities would surpass the level of acceptable noise impacts as determined by LA municipal code. For what it's worth, I can relate to the temporary inconvenience of the construction noise - there are several projects happening in my neighborhood as well and I also work from home. However, given our City's growing housing crisis, I hope you understand the need to build more housing and the patience this will require from all of us. We apologize in advance for any inconvenience this causes you or your family.

Thank you, again, for voicing your concerns. We will be presenting this project to the Palms Neighborhood Council Planning and Land Use Management Committee on Thursday, May 10th. It will be another opportunity to voice your concerns and collaborate with the project team on possible solutions. Please feel free to reach out by phone or email should you have any additional comments or questions.

Best,
Jesi

--
Jesi Harris
Planning Project Manager, Brian Silveira & Associates
704.277.7332

Renata Ooms <renata.ooms@lacity.org> Tue, Apr 18, 2023 at 9:28 AM
To: Kelly <kelly.lally@gmail.com>
Cc: Jesi Harris <harrislanduse@gmail.com>, councilmember.yaroslavsky@lacity.org, Brian Silveira <silveira.brian@gmail.com>, plum@palmsnc.la, Heather Bleemers <heather.bleemers@lacity.org>

Hello Kelly,

I am sorry that you felt uncomfortable when the applicant's representative emailed you. I see how that could feel surprising. All comments (emailed or mailed) are part of the public record and may be requested by any member of the public. Jesi Harris represents the applicant for this project. It is customary for the applicant's representative to receive copies of all comments received on a project. The applicant needs to know what the community concerns are. Part of the outreach the applicant is doing in this case is responding to and conversing with residents to better understand concerns.

I will add this second email to the record for this project.

Renata Ooms
Pronouns: She, Hers, Her
City Planner
Los Angeles City Planning
200 N. Spring St., Room 763
Los Angeles, CA 90012
T: (213) 978-1222 | Planning4LA.org
E-NEWS

[Quoted text hidden]

Jesi Harris <harrislanduse@gmail.com> Tue, Apr 18, 2023 at 10:08 AM
To: Kelly <kelly.lally@gmail.com>
Cc: councilmember.yaroslavsky@lacity.org, Brian Silveira <silveira.brian@gmail.com>, "renata.ooms@lacity.org" <renata.ooms@lacity.org>, plum@palmsnc.la

Hi, Kelly,

I appreciate your response. I do feel that this conversation may be easier for us to conduct over the phone or on Zoom so I want to offer that - you can call me on my cell phone number any time (listed in my signature) or we can schedule something at your convenience.

- **Unchecked development in our city if this project is approved - it's scary how many permit exemptions are being asked for when these exemptions exist to protect the community**
  - **Los Angeles is in the midst of a housing crisis because we've been developing single-family-style residential development in our central urban areas for the last century. Unfortunately, it's impossible to support a metropolitan population of 10 million people in low density housing. It's led to the housing crisis and intense traffic we're experiencing now, of which homelessness and gridlock are the very visible - but not isolated - results. The Density Bonus program is meant to encourage dense, mixed-income multifamily development to address years of low-density urban housing by granting incentives and waivers that make the project financially and physically feasible.**
- **Our neighborhood is simply too small and the roads too narrow to support a development of this size**
  - **The project is dedicating 5 feet of lot frontage to complete the 30-foot half-right-of-way (18-foot half-roadway) required for standard local streets in the city. An over-dependence on single-occupant motor vehicles is the underlying cause of parking and traffic issues (not to mention the number one contributor to our greenhouse gas emissions). Underbuilding our much-needed housing is not a practical response to the transportation impacts of appropriate urban housing development.**
- **Business Profits are coming at the expense of the quality of life of residents, the health of our community and environmental diversity (this needs to END NOW!)**
  - **Adequate housing development will enhance the lives of LA residents, especially those who do not have homes and those who are overpaying in rent because of the housing shortage. Responsible urban development includes housing typologies for all segments of the community and dense multifamily housing in urban centers is the best way to preserve our valuable environmental resources. The proposed development also integrates many sustainability elements such as native, drought-resistant landscaping, a greywater reuse system, ample bike parking, and Energy Star rated appliances.**
- **Noise Levels will make it impossible to work from home - causing loss of income for families**
  - **Areas across Los Angeles need to see more dense housing development which requires new construction. The noise study contained within the CEQA Categorical Exemption report demonstrates that the noise levels from the construction equipment will be below the 75 dBA from 50 feet of distance that is permitted for temporary construction noise. For reference, this is about the noise level of a vacuum cleaner. I know this is a nuisance and I apologize for that but there's no way around it if we're going to meet our housing needs. I live in an apartment building and I know that when it was built, it also caused noise impacts to surrounding residents.**
- **Unsafe conditions with construction noise, dust, street closures, traffic and additional crime brought in from Venice & Palms Blvd**
  - **As with my response above, I want to acknowledge that your concerns about construction impacts are valid but they are necessary to meet our housing need which, based on the last Regional Housing Needs Assessment, is about half a million units short. All construction sites are required to add water periodically to reduce dust expelled from the site. An Air Quality study was conducted as part of the CEQA Exemption report and found that the projected impacts were not significant based on standards set by the South Coast Air Quality Management District.**
  - **I'm not sure I understand your point about crime - providing housing, especially affordable housing, helps to meet people's basic needs reducing economic desperation and crime. LA's current low-density development style has led to a drastic housing shortage that is impacting all of us.**

- Are you going to be closing off the street during the several years of construction?
  - I will discuss the construction plan with the General Contractor and get back to you with more details about the impacts. All residents will have access to and from their homes during construction.
- Where will your construction equipment be stored or parked?
  - For much of the development process, the staging area can be on the project site. Once building construction occupies the majority of the lot, a staging plan for the construction equipment will be developed and implemented in a way that is least disruptive to the surrounding residents.

- Your building is 43 units (2 people each to be conservative) which means we'll need parking for 86 people. How do you plan to address this given that you've only allotted for 31 underground spaces? The math isn't "mathing".
  - Over-dependence on single-occupant motor vehicles is not a practical reason to underdevelop our needed housing, especially in the middle of a severe housing crisis. The parking will be provided as part of an "unbundled parking" strategy meaning that residents will rent their apartment units separately from a parking space. This discourages individual car ownership and encourages them to choose other modes as it divorces the cost of parking from the cost of the residential unit.

Thanks again for staying in touch with your questions and concerns, Kelly. Please feel free to reach out by phone or we can continue chatting via email if that's your preference.

Best,
Jesi

On Mon, Apr 17, 2023 at 4:18 PM Kelly <kelly.lally@gmail.com> wrote:
[Quoted text hidden]
[Quoted text hidden]

Kelly <kelly.lally@gmail.com> Tue, Apr 18, 2023 at 1:34 PM
To: Renata Ooms <renata.ooms@lacity.org>
Cc: Jesi Harris <harrislanduse@gmail.com>, councilmember.yaroslavsky@lacity.org, Brian Silveira <silveira.brian@gmail.com>, plum@palmsnc.la, Heather Bleemers <heather.bleemers@lacity.org>

Thank you for your thoughtful response. I was told that this project was months in the making and already approved by the Palms Neighborhood Council. Do you know why residents in our neighborhood were just looped in a few weeks ago and the Public Notice on the fence went up last week? Is this all just an illusion to think we're involved in the process?

Any project details / approvals / timeline would be appreciated. I'm all for affordable housing - but this isn't a block / neighborhood that can accommodate it. It's simply too small.
[Quoted text hidden]

Kay Hartman <kay.hartman@palmsnc.la> Tue, Apr 18, 2023 at 2:13 PM
To: Kelly <kelly.lally@gmail.com>
Cc: Renata Ooms <renata.ooms@lacity.org>, Jesi Harris <harrislanduse@gmail.com>, councilmember.yaroslavsky@lacity.org, Brian Silveira <silveira.brian@gmail.com>, plum@palmsnc.la, Heather Bleemers <heather.bleemers@lacity.org>

To be clear, the Palms NC did not vote on this project. The PLUM committee had a preview of the project in November. When the development team was ready, they were going to return to the PLUM committee for a hearing and a vote. Then the state of emergency for Covid was declared to be over and the NC and its committees were not allowed to meet on zoom. Then we were given a lot of paperwork that we had to do to confirm our ability to continue using the meeting locations we've always used. We are in that paperwork process and are unable to meet. The project is moving forward without the Neighborhood Council being able to hold its hearing and make our recommendation. I just want to make sure we have the facts straight.
[Quoted text hidden]
--
Kay Hartman
Palms Neighborhood Council Treasurer
palmsnc.la

Renata Ooms <renata.ooms@lacity.org>

URGENT: STOP THE PROPOSED 5-STORY DEVELOPMENT ON KELTON (3676-3704 KELTON AVE)

1 message

Kelly <kelly.lally@gmail.com> Tue, May 16, 2023 at 5:22 PM
To: "mayor.helpdesk@lacity.org" <mayor.helpdesk@lacity.org>, councilmember.varoslavsky@lacity.org, "dylan.sittig@lacity.org" <dylan.sittig@lacity.org>, "renata.ooms@lacity.org" <renata.ooms@lacity.org>, "vanessa.saldana@lacity.org" <vanessa.saldana@lacity.org>, "randell.erving@palmsnc.la" <randell.erving@palmsnc.la>

Hello City & Neighborhood Leadership -

I am writing to you today as a concerned resident who lives directly across the street from the proposed development on 3676-3704 Kelton Avenue. My partner and I want to express our **STRONG OPPOSITION** to this project and urge you to reconsider its approval. I kindly request your attention to the following critical points, which highlight the detrimental impact this proposed development would have on our community:

🌳 **Unsuitability for our Neighborhood:** The scale of this proposed development is far too large for our small neighborhood. Typically, projects of this magnitude are seen on main boulevards like Overland and Sepulveda, where noise and parking concerns are less prominent, and the infrastructure is designed to support such developments.

🚘 **Hazardous Driving Conditions:** Kelton Avenue is currently a one-lane street (with cars parked on both sides), which leads to dangerous driving conditions. Best case, you are waiting for 5 mins while the Amazon driver comes back to his van. Worse case (as is more common) cars try to squeeze past each other, often resulting in side-swiping parked cars. Expanding the development would exacerbate these issues, posing a significant threat to the safety of both pedestrians and motorists alike. Street closures, increased traffic, and workers looking for parking will only exacerbate the current issue.

🚶 **Lack of Sidewalks:** The absence of sidewalks on Kelton and Regent severely limits safe pedestrian mobility. It forces residents, including myself, to walk in the middle of the street when accessing essential amenities like Palms and Venice, where bus and metro lines are located.

💩 ****MAJOR SEWER ISSUES:** The existing sewer infrastructure is already strained and cannot adequately accommodate an additional 43 units, resulting in approximately 100 new residents. Recent incidents, such as the Sanitation Department's visit to the main house on 3704 Kelton due to a backed-up septic line, highlight the infeasibility of this project. Furthermore, heavy rainstorms lead to sewage overflow on Kelton, causing backups into driveways.

🌳 **Irreversible Loss of Old Growth Trees:** The proposed development entails the clear-cutting of over 20 old growth trees that contribute to the health and quality of life for our families and the preschools operating on Kelton & Veteran Ave (respectively). Preserving these trees is not only essential for our well-being but also for the overall sustainability and environmental balance of our community. You can simply not cut down 20+ mature trees and replace them with 6 saplings.

🏠 **Displacement of Long-time Residents:** The existing multi-family dwellings on this parcel contain long-time residents, who are being forced out of their homes. It also jeopardizes the rest of the community, the majority of whom work from home and will be forced to leave due to excessive noise spanning at least 2 years and upward pressure on rent prices. We need smaller affordable housing for all, not just 11 families.

❤ **Neglecting the Needs of Disabled Residents:** Palms Manor, located at 3740 Kelton Ave, houses disabled and elderly residents who were not told about the projects being developed. Excessive noise and pollution along with the safety concerns this project brings will greatly impact them.

🏙 **Unchecked development in Palms and throughout the city,** given that the developer is requesting NUMEROUS exceptions. As you know, zoning regulations are necessary to “encourage the most appropriate use of land; to conserve and stabilize the value of property; to provide adequate open spaces for light and air ... ; to prevent undue concentration of population; to lessen congestion on streets; ... and to promote health, safety, and the general welfare” (LAMC 12.02). Giving the developer carte blanche to ignore development standards—over the community’s objections, no less—would undermine each of these goals and set a dangerous precedent.

I invite each of you to visit our street and witness firsthand the pressing concerns I have raised. Your visit will shed light on the urgency of protecting the existing residents, prioritizing our interests and well-being over the profits of developers.

Thank you for considering my concerns, and I eagerly anticipate your response. Together, we can work towards a resolution that preserves the livability and health of our community.

Best,
Kelly Lally
John Bevins
3707 Kelton Ave, Los Angeles, CA 90034
Phone: (650) 580-6096

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******URGENT!!! STOP THE PROPOSED 5-STORY****** DEVELOPMENT IN OUR NEIGHBORHOOD!!!

PROPOSED 5-STORY, 62' HIGH, 43 UNIT BUILDING

at 3676 - 3704 Kelton Ave (at Regent St.) with ONLY 11 units for very low income households and ONLY 24 PARKING SPACES! Voice your concerns to everyone listed below about how this HUGE, OUT OF PLACE, development would NEGATIVELY IMPACT our neighborhood of houses & small apartment buildings & our QUALITY OF LIFE! KEEP DEVELOPMENTS THAT ONLY BENEFIT BUILDERS OUT OF OUR AREA!

***TRAFFIC HAS ALREADY GREATLY INCREASED**
***STREET PARKING IS ALREADY SCARCE**
***INTERSECTION IS SO SMALL THAT IT LACKS A 4-WAY STOP SIGN**
***NARROW STREETS – 2 CARS CAN'T PASS WHEN CARS ARE PARKED**
***SIZE WOULD BLOCK OUT SUN & LIGHT & INHIBIT AIR FLOW**
***WIDTH OF STREET AT SITE IS LESS THAN 22 FEET**
***WOULD CREATE EVEN MORE DANGEROUS DRIVING CONDITIONS**
***ONLY 11 UNITS ARE LOW RENT; WE NEED LOWER RENTS FOR ALL!**
***CONSTRUCTION NOISE & DUST WOULD IMPACT AIR & LIFE QUALITY**
***HUGE DEVELOPMENTS ON OVERLAND ALREADY INCREASED TRAFFIC**
***THIS ONLY BENEFITS DEVELOPERS AND PEOPLE IN 11 UNITS, NOT 32**
***THIS AREA OF PALMS IS NOT SUITED FOR A HUGE DEVELOPMENT**

BUILD AFFORDABLE HOUSING FOR ALL, NOT JUST FOR 11 FAMILIES!
WE NEED SMALLER, AFFORDABLE UNITS THAT FIT INTO OUR NEIGHBORHOOD, NOT LARGE DEVELOPMENTS THAT CHANGE IT!

CONTACT EMAILS TO COPY & PASTE: mayor.helpdesk@lacity.org, councilmember.yaroslavsky@lacity.org, dylan.sittig@lacity.org, renata.ooms@lacity.org, vanessa.saldana@lacity.org, randell.erving@palmsnc.la

Mayor Karen Bass - mayor.helpdesk@lacity.org
Katy Yaroslavsky - councilmember.yaroslavsky@lacity.org - Councilwoman Katy Yaroslavsky, Fifth District
200 North Spring Street, Room 440, Los Angeles, CA 90012 **213-473-7005** Los Angeles City Hall Office
Dylan Sittig - dylan.sittig@lacity.org - **Senior Planning Deputy** - Office of Councilwoman Katy Yaroslavsky
Renata Ooms - **renata.ooms@lacity.org** – City Planner – 213-978-1222
Vanessa Saldana – vanessa.saldana@lacity.org - **Field Deputy** - Office of Councilwoman Katy Yaroslavsky
Randell Erving, Jr. - randell.erving@palmsnc.la - **President Palms Neighborhood Council**
REPRESENTING THE DEVELOPER = harrislanduse@gmail.com - Jesi Harris represents Brian Silveira & Associates, a company registered with the ETHICS COMMISSION as a LOBBYING FIRM 704-277-7332

WRITE/EMAIL YOUR OWN LETTER OR SIGN & DATE THIS FLYER, ADD YOUR COMMENTS, AND EMAIL TO ALL THE ABOVE PEOPLE, WITH YOUR NAME AND ADDRESS. CALL ALL PEOPLE INVOLVED AND LET THEM KNOW HOW YOU FEEL ABOUT THIS ATTACK ON OUR COMMUNITY!

Renata Ooms <renata.ooms@lacity.org>

📝 🌳 PETITION TO STOP THE FIVE-STORY DEVELOPMENT ON KELTON & REGENT 🌳 (PALMS NEIGHBORHOOD)

Kelly <kelly.lally@gmail.com> Tue, Apr 18, 2023 at 1:32 PM
To: renata.ooms@lacity.org, info@palmsnc.la, plum@palmsnc.la, councilmember.yaroslavsky@lacity.org
Cc: mail@kcrw.org

Dear Ms. Ooms, Councilwoman Yaroslavsky, and Palms Neighborhood Council -

I am writing to you on behalf of a group of concerned residents of Palms who have signed **a petition to stop the proposed five-story development on Kelton & Regent:**

- **List of signatures (note: I'm the organizer listed as San Francisco - which is an old address. I now reside at 3707 Kelton Ave, Los Angeles, CA 90034.)**
- **Public Comments**
- **(🔗 change.org petition link). Given the inconspicuous notice of the Public Hearing - neighbors are adding their names still**

***PLEASE NOTE THAT NEIGHBORS ARE STILL LEARNING ABOUT THIS DEVELOPMENT GIVEN THE INCONSPICUOUS HEARING NOTICE THAT WAS PUBLISHED TO US. OUR PETITION SERVES *IN ADDITION* TO THE ONE ORGANIZED BY DAVID LEFEBVRE : https://chng.it/8x8PDXqgtT**

Our community is deeply worried about the negative impacts that this development will have on the surrounding area, including:

- **Unchecked development** in Palms and West LA - this development requires numerous exemptions that were all put in place to PROTECT RESIDENTS
- **Noise Levels**, impacting the health of residents and our ability to earn a living
- **Unsafe conditions** via increased traffic on very narrow roads with scarce parking as it is
- **Harmful impact on health** of families due to dust and debris from construction - exacerbated by the clear-cutting of 20 mature trees (being replaced with 4 young trees)
- **Insufficient parking** on top of an already aggravated situation
- **Driving up rent prices** as landlords look to capitalize on the new luxury accommodations

As residents of this area, we also feel that this development is not in line with the character and scale of our neighborhood. The proposed project would tower over the existing buildings and negatively impact the quality of life of the neighborhood that drives our property values.

We understand that there is a need for affordable housing development in the area, but we strongly believe that it should be done in a way that respects the scale and character of our community. **We urge you to consider the concerns of the residents and reject this proposal.**

Thank you for your time and attention to this matter.

Best,
Kelly Lally
3707 Kelton Avenue
Los Angeles, CA 90034

Name	City	State	Postal Code	Country	Signed On
Kelly Lally	Los Angeles	CA	90034	US	2023-04-13
Geoffrey Briggs	Los Angeles	CA	90009	US	2023-04-14
David Lefebvre	Los Angeles	CA	90034	US	2023-04-14
Henry Hairapetian	Los Angeles	CA	90034	US	2023-04-14
Todd Bartholomew	Los Angeles	CA	90009	US	2023-04-14
Neissa Diabate	Los Angeles	CA	90034	US	2023-04-15
Ruth Hairapetian	Los Angeles	CA	90034	US	2023-04-16
hei li lin	Los Angeles	CA	90034	US	2023-04-16
Brett Alphin	Los Angeles	CA	90006	US	2023-04-16
Johnny Peguero	Los Angeles	MA	90034	US	2023-04-17
Zachariah Lawson	Greensboro		27406	US	2023-04-17
Tim Feinman	Mount Laurel		8054	US	2023-04-17
Samantha Ibarra	Taft		93268	US	2023-04-17
GABRIEL MCCOY	Yukon		73099	US	2023-04-17
Michelle Carrillo	Minneapolis		55423	US	2023-04-17
Melody Horney	Phoenix		85053	US	2023-04-17
Derek Moore	Dundee		60118	US	2023-04-17
Sang Kim	Fremont		94539	US	2023-04-17
Roman Leckey	Bridgewater		22812	US	2023-04-17
abdale arab	Seattle			US	2023-04-17
Antonieta Lefebvre	Los Angeles	CA	90034	US	2023-04-17
Huanlie Kwey	Los Angeles	CA	90034	US	2023-04-17
Laura Baca	Los Angeles	CA	90034	US	2023-04-17
Reed Hutchinson	Los Angeles	CA	90034	US	2023-04-18
Jean Hutchinson	Los Angeles	CA	90034	US	2023-04-18
Ryan Goto	Los Angeles	CA	90013	US	2023-04-18

Name	City	State	Postal Code	Country	Commented Date	Comment
David Lefebvre	Los Angeles	CA	90034	US	2023-04-14	"Kelly — great minds think alike! I separately started a similar petition the other day: https://chng.it/Rn7rRkjNmSYour printed flier looks amazing. Would be great to coordinate efforts. You'll see my email address since I signed your petition; please also sign mine and let's get in touch directly over email! Thanks, David"
David Lefebvre	Los Angeles	CA	90034	US	2023-04-14	"Hi Kelly — great minds think alike! I separately started a similar petition the other day: https://chng.it/Rn7rRkjNmS. Please sign that one too!Your printed flier looks amazing -- I wish there was a good way to coordinate efforts! Thanks for everything you're doing.- David"
Reed Hutchinson	Los Angeles	CA	90034	US	2023-04-18	"I live on Kelton and this has an affect on the whole neighborhood"

Renata Ooms <renata.ooms@lacity.org>

OPPOSE THE REQUESTED WAIVERS OF DEVELOPMENT STANDARDS FOR 3676-3704 KELTON AVE

Kelly <kelly.lally@gmail.com> Sun, Jul 2, 2023 at 9:13 AM
To: renata.ooms@lacity.org, councilmember.yaroslavsky@lacity.org, dylan.sittig@lacity.org, info@palmsnc.la

Dear Dept. of City Planning and Elected Representatives -

As a concerned resident of Palms who lives directly across the street from the proposed development, I am writing to express my **strong opposition** to the proposed five-story tower at 3676-3704 Kelton Avenue, as currently designed. The developer is asking for a **205% increase above allowable base density in an area zoned for duplexes and fourplexes.** I want to be clear that I do not oppose a building in our neighborhood, but it needs to fit the existing zoning and character of the neighborhood because that's what our infrastructure and small street can support. A project of this massive size and scale belongs on a major corridor such as Venice, Overland, or Sepulveda, where you won't have infrastructure issues (see my personal examples below). Therefore, I kindly request that you take a stance against this project and ensure that the **developer's excessive requests for waivers of development standards are DENIED.**

The waivers sought by the developer, particularly the unreasonable increases in height and floor area, will directly undermine public health and safety and exacerbate existing issues that we are already grappling with. One of my primary concerns is the strain that such a development would place on the already burdened infrastructure of Kelton Avenue and its surrounding areas. The current proposal disregards crucial development standards that protect the community, and if approved, would overwhelm our neighborhood's infrastructure. ***The resulting impact on public health and safety would be devastating and harmful for BOTH EXISTING AND NEW RESIDENTS on the street.***

Existing problems include:

1. **Lack of a four-way stop sign** at the corner of Kelton and Regent currently causes accidents and near-misses due to poor visibility. For example, two months ago, I witnessed a car turning onto Kelton (from Regent) that was struck by oncoming traffic.
2. **Multiple (and recent) sewer backups** at my residence in January 2023, and more recently, they have been reported in May by the owners of 3704 Kelton (where the proposed development is planned to be). If you visit the neighborhood, you'll see areas where Public Works has cut out sections of the street to work on the sewer line. Note: this has always been an issue along the entire length of Kelton.
3. **Lack of sidewalks on Kelton and Regent** makes it difficult to walk to stores and public transportation. For example, last year, a jogger was hit by a car on Kelton because there is no sidewalk. With cars parked on both sides of the street, pedestrians have to walk/run in the middle of the road causing unsafe conditions when compared with speed cars cutting through the neighborhood to get from Palms to Venice Blvd.
4. **Lack of speed bumps on Kelton** is a major safety concern, as aggressive drivers tend to speed down the road while trying to get from Palms to Venice (and vice versa). For example, three weeks ago, an Amazon truck was double parked (which is often the case), and because it obstructed traffic, I almost got struck by a car coming from the opposite direction while riding my bike.
5. **The destruction of 20 old growth trees** (which would absorb a lot of the noise, dust, and debris in the air) will only exacerbate environmental health and air quality issues. This will make it impossible for those living in the area, especially individuals like me with asthma, to cope with the declining air quality. The developers' response to replace these with 6 small trees is simply a joke.

As elected officials, I believe ***it is your responsibility to represent the voice and best interests of the public and the communities you serve (over helping developers "pencil out").*** I implore you to stand firm in adhering to your mandate and oppose the proposed project, ensuring that the waivers requested by the developer are denied.

Thank you for your attention to this matter, and I trust that you will give due consideration to the concerns raised by the 300+ members of our community who have signed the change.org petition. Your support in opposing this project is greatly appreciated.

Best,
Kelly Lally
3707 Kelton Ave
Los Angeles, CA 90034

Renata Ooms <renata.ooms@lacity.org>

New petition to you: Protect Our Community: Say "No" to Proposed Project on Kelton & Regent

Kelly L via Change.org <change@t.change.org>
Reply-To: "Change.org" <reply.en-US@reply.e.change.org>
To: renata.ooms@lacity.org

Thu, Apr 20, 2023 at 8:24 AM

change.org

Renata Ooms: you've been listed as a decision maker

Kelly L started a petition on Change.org and listed you as a decision maker. Learn more about Kelly L's petition and how you can respond:

Kelly L is petitioning Renata Ooms (LA City Planner)

Protect Our Community: Say "No" to Proposed Project on Kelton & Regent

Dear Councilwoman, City Planner & Palms Neighborhood Council - Imagine a small family-friendly neighborhood where you know your neighbor by their first name, walk your dog to the enjoyment of birds and wildlife, and have a quiet place to decompress...

View the petition

WHAT YOU CAN DO

1. View the petition: Learn about the petition and its supporters. You will receive updates as new supporters sign the petition so you can see who is signing and why.

2. Respond to the petition: Post a response to let the petition supporters know you're listening, say whether you agree with their call to action, or ask them for more information.

3. Continue the dialogue: Read the comments posted by petition supporters and continue the dialogue so that others can see you're an engaged leader who is willing to participate in open discussion.

CHANGE.ORG FOR DECISION MAKERS

On Change.org, decision makers like you connect directly with people around the world to resolve issues. Learn more.

This notification was sent to renata.ooms@lacity.org, the address listed as a decision maker.

This is a one-time notification to the email address listed above. You will not receive any further notifications regarding this petition from us.

Privacy policy

We'd love to hear from you! Contact us through our help center.

Change.org · 548 Market St #29993, San Francisco, CA 94104-5401, USA

change.org Start a petition My petitions Browse Membership Log in

Petition details Comments Updates

Protect Our Community: Say "No" to Proposed Project on Kelton & Regent

34 have signed. Let's get to 50!

At 50 signatures, this petition is more likely to be **featured in recommendations!**

Mary Hairapetian signed this petition

Cameron Lampert signed this petition

Sign this petition

First name

Last name

Email

North Hollywood, 91606
United States

Display my name and comment on this petition

Sign this petition

By signing, you accept Change.org's Terms of Service and Privacy Policy, and agree to receive occasional emails about campaigns on Change.org. You can unsubscribe at any time.

Kelly L started this petition to Councilwoman District 5 Katy Yaroslavsky and 2 others

Dear Councilwoman, City Planner & Palms Neighborhood Council -

Imagine a small family-friendly neighborhood where you know your neighbor by their first name, walk your dog to the enjoyment of birds and wildlife, and have a quiet place to decompress after a long day at work.

All of this stands to be destroyed with the proposed construction of a six-story apartment complex that spans 42,222 sq ft on 3676-3704 South Kelton Avenue & 10845 Regent Street. The project would consist of 43 units, 11 reserved for very low-income residents. At two residents per unit, that's 86 people (and cars) are added to our street!

We are signing this petition to stop this luxury project which is simply unsustainable for our community and is not an appropriate solution to solve the housing issues of Los Angeles, harming us in the following ways:

- **Upward pressure on rent prices**, as landlords and property owners raise rents to capitalize on the new apartment buildings (vs. older dingbat style apartments that make up the neighborhood currently).
- **Safety hazards** - particularly during peak hours, as the roads are very narrow and residents are already playing a game of chicken.
- **Parking Limitations** - our neighborhood simply cannot support a building of this size. The planned underground parking structure is only for 31 cars, forcing existing residents to compete for parking blocks away.
- **Clear-cutting of at least 20 mature trees** (one of which is over 50 years old) takes with it the biodiversity of wildlife (cats, squirrels, birds) that call it home.
- **Increased Noise Levels** - coming from not only the construction but the increase in foot traffic and cars in the neighborhood - impacting the livelihood of our work-from-home residents.
- **Crime** - coming into the neighborhood from Venice and Palms with the addition of luxury apartments - 11 units of which are for very low-income residents

Denying this permit application will protect the vitality and longevity of our beautiful community.

Thank you,

Screenshot taken on 5/4/2023 by Renata Ooms, Department of City Planning

Renata Ooms <renata.ooms@lacity.org>

Kelton Avenue Project

Kim Carter <crash0burn@hotmail.com> Wed, Apr 5, 2023 at 7:43 PM
To: "Renata.ooms@lacity.org" <Renata.ooms@lacity.org>

I am upset that on a narrow avenue you want to build a 5 floor high rise skyscraper with 43 units and 11 units for very low income people.

There will be plumbing problems, bacteria, germs and diseases. They will need electricity, gas, and water.

Plus, more pollution and traffic on narrow residential street and lack of parking for 43 units on Kelton Ave. Distrub peace and quiet of our neighborhood.

Low income housing will cause çrime to rise, such as, drug addicts, gangs and drive by shootings and devalue house prices make it bad investments. Let them build a project like this in rich, wealthy neighborhoods.

Renata Ooms <renata.ooms@lacity.org>

Opposition to 5-Story Tower at Kelton & Regent St

1 message

Lily Poulter <lilyvpoulter96@gmail.com> Thu, Jul 6, 2023 at 8:43 AM
To: "renata.ooms@lacity.org" <renata.ooms@lacity.org>, "info@palmsnc.la" <info@palmsnc.la>, councilmember.yaroslavsky@lacity.org, "dylan.sittig@lacity.org" <dylan.sittig@lacity.org>

Dear Department of City Planning and Elected Representatives,

I am a resident of Palms opposed to the waivers of development standards requested for the proposed five-story tower at 3676-3704 Kelton Avenue. I urge you to formally oppose the project and ensure the developer's excessive extra request for waivers of development standards are denied. The waivers requested by the developer (unreasonable increase in height and floor area) would exacerbate existing issues that directly impact public health and safety in our neighborhood.
Simply put, if development standards are ignored and the new building is constructed as currently proposed, the already strained infrastructure of Kelton Avenue and the surrounding neighborhood will become overwhelmed.

I strongly urge you to reconsider this proposed housing development. While I recognize the need for affordable housing, I believe that this project is simply not the right fit for our neighborhood. Thank you for your attention to this matter.

Sincerely,

Lily Poulter

Renata Ooms <renata.ooms@lacity.org>

3676-3704 Kelton development

Manzar Foroohar <manzarforoohar@gmail.com> Fri, Jun 30, 2023 at 10:33 PM
To: renata.ooms@lacity.org

Dear Ms. Ooms,
I am a resident of Palms opposed to the waivers of development standards requested for the proposed five-story tower at 3676-3704 Kelton Avenue. I urge you to formally oppose the project and ensure the developer's excessive extra requests for waivers of development standards are denied. Overwhelming majority of Kelton Ave. residents oppose waivers and discretionary allowances requested by the developer because the development goes much beyond the infrastructure capacity of the neighborhood. We already have severe sewer line backups, storm drainage problems and traffic safety issues on Kelton. Simply put, if development standards are ignored and the new building is constructed as currently proposed, the already strained infrastructure of Kelton Avenue and the surrounding neighborhood will become overwhelmed.
Thank you in advance for addressing our concerns for the future of our neighborhood.
Regards,
Manzar Foroohar
History Professor Emerita
3675 Kelton Ave.

Renata Ooms <renata.ooms@lacity.org>

proposed five-story, 43-unit tower at Kelton Ave. & Regent St.

1 message

Manzar Foroohar <manzarforoohar@gmail.com> Mon, Apr 17, 2023 at 2:49 PM
To: renata.ooms@lacity.org

Dear Dept. of City Planning and Elected Representatives—I am a resident of Palms, urging you to stop the proposed five-story, 43-unit tower at Kelton Ave. & Regent St. I have signed the petition at https://www.change.org/Kelton-and-Regent Please protect our neighborhood!"

Manzar Foroohar
3675 Kelton Ave.
Los Angeles, 90034

Renata Ooms <renata.ooms@lacity.org>

DENY proposed 5 story project on Kelton Ave

1 message

Neissa Diabate <neissadiabate@gmail.com> Thu, Jul 6, 2023 at 11:48 AM
To: renata.ooms@lacity.org

Dear Dept. of City Planning and Elected Representatives,

I live right next door to the proposed five story tower at 3676-3704 Kelton Ave. Please, I beg you to deny the waivers of development standards requested for the proposed five-story tower at 3676-3704 Kelton Avenue. I urge you to formally oppose the project and ensure the developer's excessive extra requests for waivers of development standards are denied.

The waivers requested by the developer (unreasonable increases in height and floor area) would exacerbate existing issues that directly impact public health and safety in our neighborhood such as sewage back up, narrow streets, and limited sidewalks.

Simply put, if development standards are ignored and the new building is constructed as currently proposed, the already strained infrastructure of Kelton Avenue and the surrounding neighborhood will become overwhelmed.

I beg you to look deeply into this case and hear the people of Palms. We need real representation. We want our voice heard and we want our voice to matter. Yes, we understand the need for more housing. Yes we understand that something will be built on that lot. We just ask that the building resemble something more in line with the already existing community. Please hear us out and do your part to support us.

Thank you!!
Neissa Diabate
3708 Kelton Ave

Renata Ooms <renata.ooms@lacity.org>

Protect Our Neighborhood

1 message

Neissa Diabate <neissadiabate@gmail.com> Mon, Apr 17, 2023 at 8:09 PM
To: renata.ooms@lacity.org

Dear Dept. of City Planning and Elected Representatives—

I am a resident of Palms, urging you to stop the proposed five-story, 43-unit tower at Kelton Ave. & Regent St. I have signed the petition at https://www.change.org/Kelton-and-Regent Please protect our neighborhood!

Thank you!!
Neissa Diabate

April 5, 2023

Renata Ooms, City Planner
R200 North Spring Street, Rm 763
Los Angeles, CA 90012

Re: case CPC-2023-582-CU-DB-HCA-PHP
3676-3704 South Kelton Ave, 10845 West Regent St.

Greetings, with sincere hope that you will give consideration to the effect of this proposed development (43 units) on our existing neighborhood.

No one living in this area wants more people, more traffic, more noise, less light or less green space. Those are some of the consequences of permitting the proposed exceptions. Changes in the building code have already negatively impacted the quality of our lives.

Increased noise affects our health, both physical and mental. (L.A. Times 12/12/2023) The American Public Health (10/2021) linked noise pollution with chronic conditions such as heart disease, cognitive impairment, depression and anxiety. Our health and well being will be undermined by such a gigantic complex. The small number of low income units is less than reassuring in terms of the massive homeless population in our area.

The issue of increased density leads one to ask "Who benefits?" Clearly, it is not the current residents of our neighborhood. Crime here has risen dramatically as it usually does with increasing density. Parking is an issue and responses of services such as police and fire, code enforcement, and others are inadequate for the needs of the current population. How will increasing numbers help anyone except the developers? If they pay more property taxes do we get to pay less based on the failure of the city to meet our basic needs for health and safety? The proposal clearly will reduce the quality of our lives and perhaps the value of our own property.

The greatest loss may be the impact on the environment. We need more land for pocket parks or community gardens and ways to increase our connection to the natural world. It may be shortsighted to issue permits for massive construction when changes in climate have not been adequately addressed. Future impacts are also important to many of us. Please do not approve the proposed development in this neighborhood.

Respectfully,
Norma Brackeen
Norma Brackeen
isobelgo@yahoo.com
3712 ½ Greenfield Ave
Los Angeles, CA 90034-7004

Renata Ooms <renata.ooms@lacity.org>

Fw: URGENT!!! Stop The Proposed 5-Story Development In Our Neighborhood!!!

Paul <hhai_999@yahoo.com> Thu, Jun 8, 2023 at 10:04 PM
To: Renata Ooms <renata.ooms@lacity.org>

I am located at 3651 Kelton Ave and I am in opposition of Kelton Ave development. See attached attachment file below.

----- Forwarded Message -----
From: Ruth Hairapetian <rhairap@gmail.com>
To: "hhai_999@yahoo.com" <hhai_999@yahoo.com>
Sent: Friday, May 19, 2023 at 08:56:41 AM PDT
Subject: URGENT!!! Stop The Proposed 5-Story Development In Our Neighborhood!!!

3394_001.pdf
118K

******URGENT!!! STOP THE PROPOSED 5-STORY******
DEVELOPMENT IN OUR NEIGHBORHOOD!!!

PROPOSED 5-STORY, 62' HIGH, 43 UNIT BUILDING

at 3676 - 3704 Kelton Ave (at Regent St.) with ONLY 11 units for very low income households and ONLY 24 PARKING SPACES! Voice your concerns to everyone listed below about how this HUGE, OUT OF PLACE, development would NEGATIVELY IMPACT our neighborhood of houses & small apartment buildings & our QUALITY OF LIFE! KEEP DEVELOPMENTS THAT ONLY BENEFIT BUILDERS OUT OF OUR AREA!

***TRAFFIC HAS ALREADY GREATLY INCREASED**
***STREET PARKING IS ALREADY SCARCE**
***INTERSECTION IS SO SMALL THAT IT LACKS A 4-WAY STOP SIGN**
***NARROW STREETS – 2 CARS CAN'T PASS WHEN CARS ARE PARKED**
***SIZE WOULD BLOCK OUT SUN & LIGHT & INHIBIT AIR FLOW**
***WIDTH OF STREET AT SITE IS LESS THAN 22 FEET**
***WOULD CREATE EVEN MORE DANGEROUS DRIVING CONDITIONS**
***ONLY 11 UNITS ARE LOW RENT; WE NEED LOWER RENTS FOR ALL!**
***CONSTRUCTION NOISE & DUST WOULD IMPACT AIR & LIFE QUALITY**
***HUGE DEVELOPMENTS ON OVERLAND ALREADY INCREASED TRAFFIC**
***THIS ONLY BENEFITS DEVELOPERS AND PEOPLE IN 11 UNITS, NOT 32**
***THIS AREA OF PALMS IS NOT SUITED FOR A HUGE DEVELOPMENT**

BUILD AFFORDABLE HOUSING FOR ALL, NOT JUST FOR 11 FAMILIES!
WE NEED SMALLER, AFFORDABLE UNITS THAT FIT INTO OUR NEIGHBORHOOD, NOT LARGE DEVELOPMENTS THAT CHANGE IT!

CONTACT EMAILS TO COPY & PASTE: mayor.helpdesk@lacity.org, councilmember.yaroslavsky@lacity.org, dylan.sittig@lacity.org, renata.ooms@lacity.org, vanessa.saldana@lacity.org, randell.erving@palmsnc.la

Mayor Karen Bass - mayor.helpdesk@lacity.org
Katy Yaroslavsky - councilmember.yaroslavsky@lacity.org - Councilwoman Katy Yaroslavsky, Fifth District
200 North Spring Street, Room 440, Los Angeles, CA 90012 **213-473-7005** Los Angeles City Hall Office
Dylan Sittig - dylan.sittig@lacity.org - **Senior Planning Deputy** - Office of Councilwoman Katy Yaroslavsky
Renata Ooms - **renata.ooms@lacity.org** – **City Planner – 213-978-1222**
Vanessa Saldana – vanessa.saldana@lacity.org - **Field Deputy** - Office of Councilwoman Katy Yaroslavsky
Randell Erving, Jr. - randell.erving@palmsnc.la - **President Palms Neighborhood Council**
REPRESENTING THE DEVELOPER = harrislanduse@gmail.com - Jesi Harris represents Brian Silveira & Associates, a company registered with the ETHICS COMMISSION as a LOBBYING FIRM 704-277-7332

WRITE/EMAIL YOUR OWN LETTER OR SIGN & DATE THIS FLYER, ADD YOUR COMMENTS, AND EMAIL TO ALL THE ABOVE PEOPLE, WITH YOUR NAME AND ADDRESS. CALL ALL PEOPLE INVOLVED AND LET THEM KNOW HOW YOU FEEL ABOUT THIS ATTACK ON OUR COMMUNITY!

Renata Ooms <renata.ooms@lacity.org>

Kelton Avenue project 43 apartments units

2 messages

Paul <hhai_999@yahoo.com> Fri, Mar 31, 2023 at 7:59 PM
To: "renata.ooms@lacity.org" <renata.ooms@lacity.org>

I protest the Kelton project due to noise pollution and smog pollution.

I reject the horrible traffic mess with this project.

I deny the lack of awful inadequate parking on narrow street Kelton Ave in present and current condition.

There is no street parking for 43 new dwelling units. There is no parking for their visitors.

The proposed construction is a gross massive perversion of residential area.

We are registered voters. I am the spoke person for the community, but I am too busy to attend the public hearing.

Renata Ooms <renata.ooms@lacity.org> Mon, Apr 3, 2023 at 11:20 AM
To: Paul <hhai_999@yahoo.com>

Received, thank you. Your comment will be included in the record.

LOS ANGELES CITY PLANNING

Renata Ooms
Pronouns: She, Hers, Her
City Planner
Los Angeles City Planning
200 N. Spring St., Room 763
Los Angeles, CA 90012
T: (213) 978-1222 | Planning4LA.org
E-NEWS

[Quoted text hidden]

Renata Ooms <renata.ooms@lacity.org>

Opposition to 5 Story Development on Kelton and Regent

1 message

Rachel Bensimon <rachelgbensimon@gmail.com> Sat, Jul 1, 2023 at 10:32 AM
To: renata.ooms@lacity.org, info@palmsnc.la, councilmember.yaroslavsky@lacity.org, dylan.sittig@lacity.org

Dear Dept. of City Planning and Elected Representatives,

I am a resident of Palms opposed to the waivers of development standards requested for the proposed five-story tower at 3676-3704 Kelton Avenue. I urge you to formally oppose the project and ensure the developer's excessive extra requests for waivers of development standards are denied. The waivers requested by the developer (unreasonable increases in height and floor area) would exacerbate existing issues that directly impact public health and safety in our neighborhood. Simply put, if development standards are ignored and the new building is constructed as currently proposed, the already strained infrastructure of Kelton Avenue and the surrounding neighborhood will become overwhelmed.

I am an owner of a home on Veteran less than a block away and the sheer density of the neighborhood is already an issue without this project. Parking is difficult any day and near impossible on street cleaning days. Our streets are small and residential, and while I generally support increasing the amount of housing available in the city, our neighborhood specifically is not built to support something like this. Our streets are barely wide enough as it is to accommodate trash trucks and public services without interruption.

Please consider the existing owners and residents well being. A larger street would be much better suited to support something like this.

Rachel Bensimon
310-621-0040

Sent from my iPhone

Renata Ooms <renata.ooms@lacity.org>

Kelton Ave. & Regent St. Project - A Fait Accomplí

1 message

REED HUTCHINSON <calfoto@icloud.com> Tue, Apr 18, 2023 at 10:21 AM
To: renata.ooms@lacity.org, councilmember.yaroslavsky@lacity.org
Cc: info@palmsnc.la, David Lefebvre <dlefebvre@gmail.com>

Dear Renata & Council Member Yaroslavsky,

Good Morning,

Re:
"I am a resident of Palms, urging you to stop the proposed five-story, 43-unit tower at Kelton Ave. & Regent St. I have signed the petition at https://www.change.org/Kelton-and-Regent Please protect our neighborhood!"

By now you most likely have heard about the petition to stop development of a 5 story apartment building at the intersection of Kelton Ave. & Regent street in the Palms neighborhood of Los Angeles.

Through differing Change.Org petitions that have been started close to 200 residents have now voiced their objections to scope and size of this project.

What seems to be most disheartening is that the Palms Neighborhood Council and/or the Los Angeles City Planning Commission have done little in the way of good faith efforts to make the residents in the area aware of the proposed project in a timely manner in order to raise any effective objections to the project.

Much of the planning and approvals for the project seem to taken place long before the people who will be most affected were notified, and only if they were within 500 feet of the project (Which my home is).

We all realize there is a need for more housing in Los Angeles, but this project is so completely out of proportion to the neighborhood in which it will be built is one main reason for our alarm.

From Sepulveda to Overland Ave. along Venice Blvd. there are no Apartment buildings anywhere close to this size. Neither are there any from Palms to Venice Bl. that are remotely close to in size.

Along Overland Ave. there are now many Multi-Story Apartments being built – But Overland is a major thoroughfare with 4 lanes to accommodate the traffic flow and parking for construction workers.

Kelton Ave. by comparison is a relatively narrow street made narrower with public parking on both sides of the street. The neighborhood already has trouble with adequate parking, this new project certainly will exacerbate and not ameliorate the situation. Not to mention to fight for spaces on the street sweeping days and for trash pickup.

Since this project by all appearances seems to be a "Fait Accompli" the residents in this area want to know how or if the city and/or county will be able to provide any solutions and remedies for the problems that will soon arise in the course of it's impending construction?

- *Speed Bumps to lower the average speed of cars racing to Venice Blvd.*
- *Permit parking as recently added to Charnock Ave. near Overland 2 blocks away.*

• Increased Sewer line capacity – The grease trap directly in front of this site is a notorious sore spot for the area as DWP records would indicate.

• Better storm drainage on the SW side of Kelton & Charnock (there is none) – Underground parking in buildings along Kelton have been flooded with 4+ feet of water in the past.

I could go on with a laundry list of potential problems but these chickens, when they come home to roost, will most eventually land upon your doorsteps.

Thanks for your consideration,

Reed & Jean Hutchinson (Home owners)

3661 Kelton Ave. LA 90034

PS – If they can build 44 Units on 3 lots just 100 ft. away, can I build 14 on my lot?

Renata Ooms <renata.ooms@lacity.org>

RE: 3676-3704 Kelton Avenue (Case Number CPC-2023-582-CU-DB-HCA-PHP; Environmental Case Number ENV-2023-583-CE)

1 message

Jean Hutchinson <jhutchinson@amciglobal.com> Thu, Jul 6, 2023 at 12:20 PM
To: Renata Ooms <renata.ooms@lacity.org>

Hi Renata,

I am a long-time resident of Kelton Ave. As a concerned member of our community, I am just puzzled that a proposed Five story structure has been given an opportunity to be built on our street. Our street is not a major street like Overland or Venice it is less than 30 feet wide in an R2 & R3 zone.

The first time I became aware of the build was with a flyer Notice of Public Hearing. We were unaware the properties were even for sale. When we spoke with the representatives for the developers, they were unwilling to compromise on the on 43 units and only added 5 parking spaces bringing it to 33. They let us know that they will not waiver because they did not need to. They also let us know as homeowners that we too could build an 11 unit building on our property, which I thought was in poor taste in my mind. The representatives spoke as if they went door to door to let us know. I work at home and no one I know every spoke with them. They are also building a second unit down the street in which they were completely taken aback by when I asked if they were aware of the Disabled adult community living next door to that, as well as less they 500 feet of this one. Their faces on the zoom call spoke volumes. This is a community that have some with sensory issues, 3 that lack vocal capabilities and all that need assistance with mobility. Nothing is being offered or done for them and who will speak for them? This is a Hud subsidized housing that have been in our community for over 20 years and this how they are to be treated.

We have since attended several meetings with the counsel in which the first only plum meeting not cancelled took place just a few weeks ago. We had a large turnout and everyone spoke very articulately, but at the end of the day the majority of committee sided with the developers. Their reasoning given was that we must make assure the developers make money and that they will make our lives worse if they don't get their way. The developers paid over value of the properties 2 million for one lot and over 3 for the other and why am I now concerned they make a profit? Immediately after the meeting both Council and the developer's representative enjoyed cocktails at the local Lobster and Beer restaurant down the street. I am sorry the optics are a bit clouded in my view.

As I mentioned before I have lived here for many years, we were told that we were at capacity then that there were not to have any large builds due to having a strain on infrastructure. Our street is the low end so to speak. When the water from the streets above as far up as Sepulveda travel down Charnock to Kelton. If you look at our street a few of the driveways on the West side have bumps so the water won't crest into their property. We have a pump we use to get water out from underneath our home. We also have sewer issues as well. Only a couple of months ago Pierre the homeowner at one of the properties being built on called to complain about the sewer and the city routinely comes out. We personally have the sewage back up in our driveway and others have it worse. On the 3700 block the smell was so bad that the city made an opening along the street to ventilate the smell, I guess to spread out the odor so to speak. What do you think these 2 new developments are going to do.

This is also located at a T intersection with no stop signs or speed bumps in the road. Regent doesn't even have sidewalks which you have to walk in the street which will make safety an issue.

While growth and development are essential for progress, it is important that we strike a balance to preserve the charm, character, and sustainability of our Palms neighborhood. The significant increase in the number of residents will undoubtedly exert pressure on natural environment the full growth trees on the properties will be lost and small saplings will be placed on the roof.

The existing infrastructure, including roads, utilities and public services, are not equipped to handle the increased demands from a large-scale development. This will lead to traffic congestion, inadequate parking, and potential strains on emergency services.

I appreciate all you can do on our behalf.

Jean Hutchinson

CELL: 310-633-1923

jhutchinson@amciglobal.com

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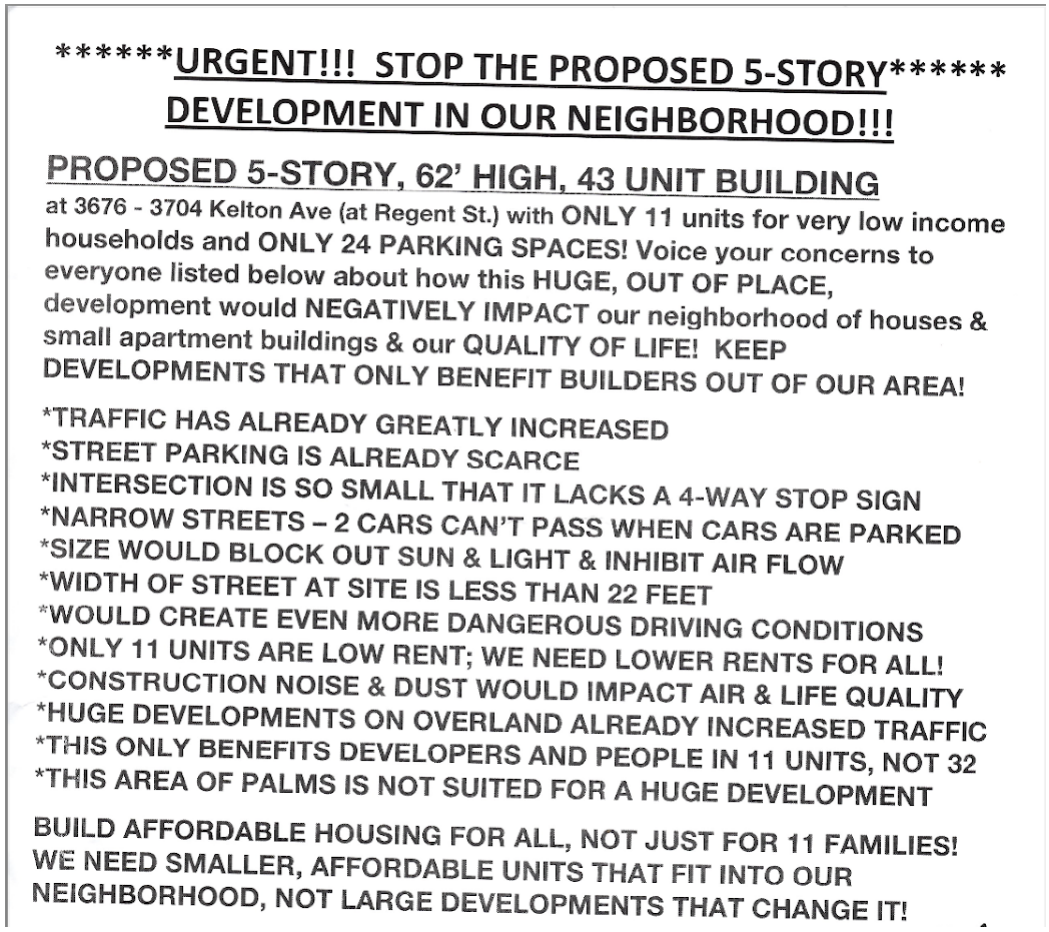
Renata Ooms <renata.ooms@lacity.org>

Proposed 5-story Development in Palms

Reilly <johnreillybarratt@gmail.com> Tue, Jun 6, 2023 at 12:55 PM
To: "mayor.helpdesk@lacity.org" <mayor.helpdesk@lacity.org>
Bcc: renata.ooms@lacity.org

This development would represent a terrible change for this little neighborhood. Please do not make the irresponsible and horrifying mistake of letting this developer cash grab go through. It would be a negative mark on the rest of your political career and serve as a monument to your failure to serve the community. Please do the right thing, thank you for your time and consideration. Peace and love be with you.



******URGENT!!! STOP THE PROPOSED 5-STORY******
DEVELOPMENT IN OUR NEIGHBORHOOD!!!

PROPOSED 5-STORY, 62' HIGH, 43 UNIT BUILDING
at 3676 - 3704 Kelton Ave (at Regent St.) with ONLY 11 units for very low income households and ONLY 24 PARKING SPACES! Voice your concerns to everyone listed below about how this HUGE, OUT OF PLACE, development would NEGATIVELY IMPACT our neighborhood of houses & small apartment buildings & our QUALITY OF LIFE! KEEP DEVELOPMENTS THAT ONLY BENEFIT BUILDERS OUT OF OUR AREA!

*TRAFFIC HAS ALREADY GREATLY INCREASED
*STREET PARKING IS ALREADY SCARCE
*INTERSECTION IS SO SMALL THAT IT LACKS A 4-WAY STOP SIGN
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BUILD AFFORDABLE HOUSING FOR ALL, NOT JUST FOR 11 FAMILIES!
WE NEED SMALLER, AFFORDABLE UNITS THAT FIT INTO OUR NEIGHBORHOOD, NOT LARGE DEVELOPMENTS THAT CHANGE IT!

Reilly Barratt
johnreillybarratt@gmail.com
302-897-4861

Renata Ooms <renata.ooms@lacity.org>

Kelton Avenue construction project

1 message

Ruth Hairapetian <RuthHairap@mail.com> Wed, Jul 12, 2023 at 9:25 PM
To: renata.ooms@lacity.org

Dear Dept. of City Planning and Elected Representatives—I am a resident of Palms opposed to the waivers of development standards requested for the proposed five-story tower at 3676-3704 Kelton Avenue. I urge you to formally oppose the project and ensure the developer's excessive extra requests for waivers of development standards are denied. The waivers requested by the developer (unreasonable increases in height and floor area) would exacerbate existing issues that directly impact public health and safety in our neighborhood. Simply put, if development standards are ignored and the new building is constructed as currently proposed, the already strained infrastructure of Kelton Avenue and the surrounding neighborhood will become overwhelmed.

Renata Ooms <renata.ooms@lacity.org>

Please Stop the Proposed Five-Story, 43-Unit Tower at Kelton Ave & Regent St

1 message

Ruth Hairapetian <rhairap@gmail.com> Fri, Apr 21, 2023 at 2:08 PM
To: renata.ooms@lacity.org

Dear Dept. of City Planning and Elected Representatives—I am a resident of Palms, urging you to stop the proposed five-story, 43-unit tower at Kelton Ave. & Regent St. I have signed the petition at https://www.change.org/Kelton-and-Regent Please protect our neighborhood!"

Renata Ooms <renata.ooms@lacity.org>

3676-3704 South Kelton Avenue

1 message

Sky Bartsch <sjveteran@gmail.com> Mon, Apr 10, 2023 at 11:48 AM
To: "renata.ooms@lacity.org" <renata.ooms@lacity.org>

Hello Renata,

Hope you are doing well! I received a notice about the project that is being built at 3676-3704 South Kelton Ave. It said for additional information on the project we could reach out to you. If you could please send over that information you have available that would be great. There is a upcoming hearing and we have some concerns about this project but wanted to understand it a little bit better.

Thank you,
Jessica

Renata Ooms <renata.ooms@lacity.org>

3676-3704 South Kelton Avenue

Sky Bartsch <sjveteran@gmail.com> Tue, Apr 18, 2023 at 4:20 PM
To: Renata Ooms <renata.ooms@lacity.org>

Hello Renata,

Thank you so much for sending all this over! Couple comments below.

- Design guidelines mention "the site will attempt to preserve existing site trees." The plans don't call out any of the existing trees to remain so very confusing line on if that is actually happening?

- I mentioned parking off site. Jesi's comment about off site parking being explored with parking on Venice or another street like another project did is not off site parking. The intention of off site parking is a designated rented parking facility or private land and not just push the problem down the road. This is common practice for high density areas or areas where neighbors have safety concerns. Ignoring these could create a massive issue and shouldn't take an accident of human life to be implemented. That is why Beverly Hills and Los Angeles have haul routes with trucking and project parking restrictions. Creating safe clear paths and guidelines for construction is not ideal for a builder but important for looking out the interest of all parties.

Thank you,
Sky
[Quoted text hidden]

Renata Ooms <renata.ooms@lacity.org>

3676 - 3704 KELTON AVE / 5 Story, 43 Unit Building with 24 parking spaces.

Steven Hochman <stevehochman@gmail.com> Wed, May 17, 2023 at 8:22 PM
To: randell.eerving@palmsnc.la, MAYOR.helpdesk@lacity.org, councilmember.varoslavsky@lacity.org, dylan.sittig@lacity.org, renata.ooms@lacity.org, vanessa.saldana@lacity.org

To whom it may concern -

I have been living in this area for 10 years (3732 Veteran Ave, LA 90034) and this project is OUTRAGEOUS as it is currently proposed.

(1) WAY TOO BIG

Not only will this proposed 5 story building be an absolute MONSTROSITY compared to all the other buildings in our region, but

(2) NO PARKING AVAILABLE

Parking is Horrendous in this area and it is INSANE that there will not be enough parking spaces for all tenants if this building is ever approved.

Whatever size building that is proposed should absolutely include parking for ALL tenants, that does not require ANY parking on the streets.

Feel free to contact me if needed -
Steve Hochman
818-402-4166

Renata Ooms <renata.ooms@lacity.org>

In Opposition to the tower project on Kelton Avenue

Timothy Staton-Davis <timothysd@hotmail.com> Thu, Jul 6, 2023 at 11:22 AM
To: "renata.ooms@lacity.org" <renata.ooms@lacity.org>, "info@palmsnc.la" <info@palmsnc.la>, "councilmember.yaroslavsky@lacity.org" <councilmember.yaroslavsky@lacity.org>, "dylan.sittig@lacity.org" <dylan.sittig@lacity.org>

Dear Department of City Planning and Elected Representatives,
I am Timothy Staton-Davis, a resident of Palms (at 3720 Veteran Ave) opposed to the waivers of development standards requested for the proposed five-story tower at 3676-3704 Kelton Avenue.
I urge you to formally oppose the project and ensure the developer's excessive extra requests for waivers of development standards are denied.
The waivers requested by the developer would exacerbate existing issues that directly impact public health and safety in our neighborhood.
As I see it, if development standards are ignored and the new building is constructed as currently proposed, the already strained infrastructure of Kelton Avenue and the surrounding neighborhood will become overwhelmed.

This neighborhood is filled with homes that are safe spaces for families and individuals to live and enjoy.
There are already a few multi-floor apartments spread around in different parts of the neighborhood and it's apparent how the current layout of the area feels like the "limit".
As my other neighbors have likely already mentioned, there are major issues that continued development of these large-scale "towers" will have on the neighborhood and community.
Strained infrastructure, negatively affected the environment in our area, continuing to erode of community's sense of place.

Please take the community's input into HEAVY consideration.

Community over profit.
Local business over corporations.

Thank You,
Timothy Staton-Davis

Renata Ooms <renata.ooms@lacity.org>

Development on 3676-3704 Kelton ave Palms

Yana <st.yana@yahoo.com> Mon, Jun 12, 2023 at 2:10 PM
To: mayor.helpdesk@lacity.org, councilmember.yaroslavsky@lacity.org, dylan.sittig@lacity.org, renata.ooms@lacity.org, vanessa.saldana@lacity.org, randell.erving@palmsnc.la, harrislanduse@gmail.com

Hello,

I'm writing you to OPPOSE the proposed huge development on 3676-3704 Kelton ave at Palms
Please absolutely reconsider NOT to build a huge complex - not only the street is extremely small to support something like that, the amount of traffic would be terrible, and such a huge development is not consistent with the neighborhood which has mostly houses and small apt buildings. Let alone dealing with the noise and pollution.
We already have so many huge developments at overland and charnock - let's not go out of control here please. People in Culver City are already dealing with a lot of nuisance created by the actions (or non action) of the local government.

Thank you
Yana S

Renata Ooms <renata.ooms@lacity.org>

PLEASE STOP proposed assault on our neighborhood.

1 message

Yvette Lodge <yvettelodge@hotmail.com> Fri, May 19, 2023 at 6:20 PM
To: "renata.ooms@lacity.org" <renata.ooms@lacity.org>
Cc: "vanessa.saldana@lacity.org" <vanessa.saldana@lacity.org>

Date: 5.19.2023 **URGENT**

******URGENT!!! STOP THE PROPOSED 5-STORY****** DEVELOPMENT IN OUR NEIGHBORHOOD!!!

PROPOSED 5-STORY, 62' HIGH, 43 UNIT BUILDING

at 3676 - 3704 Kelton Ave (at Regent St.) with ONLY 11 units for very low income households and ONLY 24 PARKING SPACES! Voice your concerns to everyone listed below about how this HUGE, OUT OF PLACE, development would NEGATIVELY IMPACT our neighborhood of houses & small apartment buildings & our QUALITY OF LIFE! KEEP DEVELOPMENTS THAT ONLY BENEFIT BUILDERS OUT OF OUR AREA!

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BUILD AFFORDABLE HOUSING FOR ALL, NOT JUST FOR 11 FAMILIES! WE NEED SMALLER, AFFORDABLE UNITS THAT FIT INTO OUR NEIGHBORHOOD, NOT LARGE DEVELOPMENTS THAT CHANGE IT!

CONTACT EMAILS TO COPY & PASTE: mayor.helpdesk@lacity.org, councilmember.yaroslavsky@lacity.org, dylan.sittig@lacity.org, renata.ooms@lacity.org, vanessa.saldana@lacity.org, randell.erving@palmsnc.la

Mayor Karen Bass - mayor.helpdesk@lacity.org
Katy Yaroslavsky - councilmember.yaroslavsky@lacity.org - Councilwoman Katy Yaroslavsky, Fifth District
200 North Spring Street, Room 440, Los Angeles, CA 90012 **213-473-7005** Los Angeles City Hall Office
Dylan Sittig - dylan.sittig@lacity.org - **Senior Planning Deputy** - Office of Councilwoman Katy Yaroslavsky
Renata Ooms - renata.ooms@lacity.org – City Planner – 213-978-1222
Vanessa Saldana – vanessa.saldana@lacity.org - **Field Deputy** - Office of Councilwoman Katy Yaroslavsky
Randell Erving, Jr. - randell.erving@palmsnc.la - **President Palms Neighborhood Council**
REPRESENTING THE DEVELOPER = harrislanduse@gmail.com - Jesi Harris represents Brian Silveira & Associates, a company registered with the ETHICS COMMISSION as a LOBBYING FIRM 704-277-7332

WRITE/EMAIL YOUR OWN LETTER OR SIGN & DATE THIS FLYER, ADD YOUR COMMENTS, AND EMAIL TO ALL THE ABOVE PEOPLE, WITH YOUR NAME AND ADDRESS. CALL ALL PEOPLE INVOLVED AND LET THEM KNOW HOW YOU FEEL ABOUT THIS ATTACK ON OUR COMMUNITY!

From: Yvette Lodge
3622 Kelton Avenue, Los Angeles, CA 90034

Development on Kelton Avenue 90034.jpeg
1604K

FORM GEN. 160 (Rev. 8-12)

CITY OF LOS ANGELES

INTER-DEPARTMENTAL CORRESPONDENCE

DATE: April 12, 2023

TO: Heather Bleemers, Senior City Planner
Department of City Planning

FROM: Bryan Ramirez, Street Tree Superintendent I
Bureau of Street Services, Urban Forestry Division

SUBJECT: **CPC-2023-582-DB-HCA-PHP – 3676 S. KELTON AVE .**

In regard to your request for review of this case regarding Urban Forestry requirements, it is our recommendation that:

1. **STREET TREES**

a. Project shall preserve all healthy mature street trees whenever possible. All feasible alternatives in project design should be considered and implemented to retain healthy mature street trees. A permit is required for the removal of any street tree and shall be replaced 2:1 as approved by the Board of Public Works and Urban Forestry Division.

b. Plant street trees at all feasible planting locations within dedicated streets as directed and required by the Bureau of Street Services, Urban Forestry Division. All tree plantings shall be installed to current tree planting standards when the City has previously been paid for tree plantings. The sub divider or contractor shall notify the Urban Forestry Division at: (213) 847-3077 upon completion of construction for tree planting direction and instructions.

Note: Removal of street trees requires approval from the Board of Public Works. All projects must have environmental (CEQA) documents that appropriately address any removal and replacement of street trees. Contact Urban Forestry Division at: (213) 847-3077 for tree removal permit information.

BR:djm

FORM GEN. 160 (Rev. 6-80)

CITY OF LOS ANGELES
INTER-DEPARTMENTAL CORRESPONDENCE

Date: 2/24/2023

To: Charlie Rausch, Senior City Planner
Department of City Planning
200 N. Spring St., 6th Floor MS-395

From: Gil De La Cruz, P.E.
Case Management Supervisor
Private Development Division
Bureau of Street Lighting

SUBJECT: STREET LIGHTING REQUIREMENTS FOR DISCRETIONARY ACTIONS

CITY PLANNING CASE No.: CPC 2023-582 CU DB HCA
3676 S Kelton Ave

The Bureau of Street Lighting's recommended condition of approval for the subject city planning case is as follows: (Improvement condition added to S-3 (c) where applicable.)

IMPROVEMENT CONDITION: No street lighting improvements if no street widening per BOE improvement conditions. Otherwise, relocate and upgrade street lights: one (1) on Kelton Ave.

NOTES:

The quantity of street lights identified may be modified slightly during the plan check process based on illumination calculations and equipment selection.

Conditions set: 1) in compliance with a Specific Plan, 2) by LADOT, or 3) by other legal instrument excluding the Bureau of Engineering conditions, requiring an improvement that will change the geometrics of the public roadway or driveway apron may require additional or the reconstruction of street lighting improvements as part of that condition.

CC: Land Development Group MS 901
Engineering District Office: WLA

+FORM. GEN. 160 (Rev. 6-80)

CITY OF LOS ANGELES

INTER-DEPARTMENTAL CORRESPONDENCE

February 16, 2023

TO: Vincent Bertoni, AICP, Director of Planning
Department of City Planning
Attention: planning.expedited@lacity.org

FROM: Los Angeles Fire Department

SUBJECT: **CPC-2023-582.: 3676 Kelton**

Submit plot plans for Fire Department approval and review prior to recordation of City Planning Case.

RECOMMENDATIONS:

Access for Fire Department apparatus and personnel to and into all structures shall be required.

Address identification. New and existing buildings shall have approved building identification placed in a position that is plainly legible and visible from the street or road fronting the property.

One or more Knox Boxes will be required to be installed for LAFD access to project. Location and number to be determined by LAFD Field Inspector. (Refer to FPB Req # 75).

The entrance or exit of all ground dwelling units shall not be more than 150 feet from the edge of a roadway of an improved street, access road, or designated fire lane.

No building or portion of a building shall be constructed more than 150 feet from the edge of a roadway of an improved street, access road, or designated fire lane.

Fire Lane Requirements:

1) Fire lane width shall not be less than 20 feet. When a fire lane must accommodate the operation of Fire Department aerial ladder apparatus or where fire hydrants are installed, those portions shall not be less than 28 feet in width.
2) The width of private roadways for general access use and fire lanes shall not be less than 20 feet, and the fire lane must be clear to the sky.
3) Fire lanes, where required and dead ending streets shall terminate in a cul-de-sac or other approved turning area. No dead ending street or fire lane shall be greater than 700 feet in length or secondary access shall be required.
4) Submit plot plans indicating access road and turning area for Fire Department approval.
5) All parking restrictions for fire lanes shall be posted and/or painted prior to any Temporary Certificate of Occupancy being issued.
6) Plans showing areas to be posted and/or painted, “FIRE LANE NO PARKING” shall be submitted and approved by the Fire Department prior to building permit application sign-off.
7) Electric Gates approved by the Fire Department shall be tested by the Fire Department prior to Building and Safety granting a Certificate of Occupancy.
8) All public street and fire lane cul-de-sacs shall have the curbs painted red and/or be posted “No Parking at Any Time” prior to the issuance of a Certificate of Occupancy or Temporary Certificate of Occupancy for any structures adjacent to the cul-de-sac.
9) No framing shall be allowed until the roadway is installed to the satisfaction of the Fire Department.

Construction of public or private roadway in the proposed development shall not exceed 10 percent in grade.

On small lot subdivisions, any lots used for access purposes shall be recorded on the final map as a “Fire Lane”.

Private development shall conform to the standard street dimensions shown on Department of Public Works Standard Plan S-470-0.

Standard cut-corners will be used on all turns.

The Fire Department may require additional vehicular access where buildings exceed 28 feet in height.

Smoke Vents may be required where roof access is not possible; location and number of vents to be determined at Plan Review.

Where above ground floors are used for residential purposes, the access requirement shall be interpreted as being the horizontal travel distance from the street, driveway, alley, or designated fire lane to the main entrance of individual units.

The following recommendations of the Fire Department relative to fire safety shall be incorporated into the building plans, which includes the submittal of a plot plan for approval by the Fire Department either prior to the recordation of a final map or the approval of a building permit. The plot plan shall include the following minimum design features: fire lanes, where required, shall be a minimum of 20 feet in width; all structures must be within 300 feet of an approved fire hydrant, and entrances to any dwelling unit or guest room shall not be more than 150 feet in distance in horizontal travel from the edge of the roadway of an improved street or approved fire lane.

2014 CITY OF LOS ANGELES FIRE CODE, SECTION 503.1.4 (EXCEPTION)

a. When this exception is applied to a fully fire sprinklered residential building equipped with a wet standpipe outlet inside an exit stairway with at least a 2 hour rating the distance from the wet standpipe outlet in the stairway to the entry door of any dwelling unit or guest room shall not exceed 150 feet of horizontal travel AND the distance from the edge of the roadway of an improved street or approved fire lane to the door into the same exit stairway directly from outside the building shall not exceed 150 feet of horizontal travel.

b. It is the intent of this policy that in no case will the maximum travel distance exceed 150 feet inside the structure and 150 feet outside the structure. The term “horizontal travel” refers to the actual path of travel to be taken by a person responding to an emergency in the building.

c. This policy does not apply to single-family dwellings or to non-residential buildings.

Site plans shall include all overhead utility lines adjacent to the site.

Where access for a given development requires accommodation of Fire Department apparatus, overhead clearance shall not be less than 14 feet.

No proposed development utilizing cluster, group, or condominium design of one or two family dwellings shall be more than 150 feet from the edge of the roadway of an improved street, access road, or designated fire lane.

FPB #105
5101.1 Emergency responder radio coverage in new buildings. All new buildings shall have approved radio coverage for emergency responders within the building based upon the existing coverage levels of the public safety communication systems of the jurisdiction at the exterior of the building. This section shall not require improvement of the existing public safety communication systems.

That in order to provide assurance that the proposed common fire lane and fire protection facilities, for the project, not maintained by the City, are properly and adequately maintained, the sub-divider shall record with the County Recorder, prior to the recordation of the final map, a covenant and agreement (Planning Department General Form CP-6770) to assure the following:

A. The establishment of a property owners association, which shall cause a yearly inspection to be, made by a registered civil engineer of all common fire lanes and fire protection facilities. The association will undertake any necessary maintenance and corrective measures. Each future property owner shall automatically become a member of the association or organization required above and is automatically subject to a proportionate share of the cost.

B. The future owners of affected lots with common fire lanes and fire protection facilities shall be informed or their responsibility for the maintenance of the devices on their lots. The future owner and all successors will be presented with a copy of the maintenance program for their lot. Any amendment or modification that would defeat the obligation of said association as the Advisory Agency must approve required hereinabove in writing after consultation with the Fire Department.

C. In the event that the property owners association fails to maintain the common property and easements as required by the CC and R's, the individual property owners shall be responsible for their proportional share of the maintenance.

D. Prior to any building permits being issued, the applicant shall improve, to the satisfaction of the Fire Department, all common fire lanes and install all private fire hydrants to be required.

E. That the Common Fire Lanes and Fire Protection facilities be shown on the Final Map.

The plot plans shall be approved by the Fire Department showing fire hydrants and access for each phase of the project prior to the recording of the final map for that phase. Each phase shall comply independently with code requirements.

Any roof elevation changes in excess of 3 feet may require the installation of ships ladders.

Provide Fire Department pathway front to rear with access to each roof deck via gate or pony wall

less than 36 inches.

Building designs for multi-storied residential buildings shall incorporate at least one access stairwell off the main lobby of the building; But, in no case greater than 150ft horizontal travel distance from the edge of the public street, Private Street or Fire Lane. This stairwell shall extend onto the roof.

Entrance to the main lobby shall be located off the address side of the building.

Any required Fire Annunciator panel or Fire Control Room shall be located within 20ft visual line of site of the main entrance stairwell or to the satisfaction of the Fire Department.

Where rescue window access is required, provide conditions and improvements necessary to meet accessibility standards as determined by the Los Angeles Fire Department.

Adequate off-site public and on-site private fire hydrants may be required. Their number and location to be determined after the Fire Department's review of the plot plan.

Any required fire hydrants to be installed shall be fully operational and accepted by the Fire Department prior to any building construction.

The applicant is further advised that all subsequent contact regarding these conditions must be with the Hydrant and Access Unit. This would include clarification, verification of condition compliance and plans or building permit applications, etc., and shall be accomplished **BY APPOINTMENT ONLY**, in order to assure that you receive service with a minimum amount of waiting please call **(213) 482-6543**. You should advise any consultant representing you of this requirement as well.

Kristin M. Crowley
Fire Chief

Orin Saunders, Fire Marshal
Bureau of Fire Prevention and Public Safety

OS:MRC:mrc

CPC-2023-582.: 3676 Kelton

Neighborhood Council Planning and Land Use Management Committee

Neal Anderberg, Co-chair
Kay Hartman, Co-chair
Natasia Gascón, Member
Alex Ogle, Member
Jamie Tully, Member

CITY OF LOS ANGELES
CALIFORNIA

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PALMS NEIGHBORHOOD COUNCIL

Planning and Land Use Management Committee Meeting Agenda
Wednesday, June 14, 2023 – 7:00 p.m. to 9:00 p.m.

The IMAN Center
3376 Motor Ave, Los Angeles, CA 900347 (onsite parking available)

The Neighborhood Council system enables meaningful civic participation for all Angelenos and serves as a voice for improving government responsiveness to local communities and their needs. We are an advisory body to the City of Los Angeles, comprised of stakeholder volunteers who are devoted to the mission of improving our communities.

If you are compensated to monitor, attend, or speak at this meeting, City law may require you to register as a lobbyist and report your activity. See Los Angeles Municipal Code Section 48.01 et seq. More information is available at ethics.lacity.org/lobbying. For assistance, please contact the Ethics Commission at (213) 978-1960 or ethics.commission@lacity.org.

I. CALL TO ORDER & ROLL CALL

a. Call to Order
   - 7:11

b. Roll Call
   - Kay, Neal, Jamie

II. INTRODUCTIONS

a. Introductions and welcome
   - Committee introduced ourselves

III. GENERAL PUBLIC COMMENT

a. Limited to non-agenda items – subjects related to planning and land use management issues
   - Somone expressed concern about how their comments would be delivered to City Hall. Kay pointed out that

Venessa Saldana, field deputy for Councilmember Yaroslavsky, was present.

IV. COMMITTEE BUSINESS

a. Developer presentations this month

i. Jesi Harris will present 3704 Kelton. CPC-2023-582-CU-DB-HCA-PHP

- Jesi presented what's happened to the project since the last time he presented to the committee and since he received community feedback.
- The project will include trees planted in the ground.
- The number of parking spaces is increased by 7
- Made changes on the north side of the project to ease impact on neighbor
- LADOT will install a 4-way stop sign at the intersection
- Sewer issues should have been fixed years ago. LASAN is willing to provide fixes. Developer is willing to pay for repairs in the intersection.
- Project will rent a lot nearby for employee parking and will shuttle people into and out of the development site.
- Added landscaping to rear yard
- Public comments were many. All were opposed to the project. Main complaints included the building is out of scope for the neighborhood, the project will create a parking burden for the current residents, the sewer backs up and adding more density won't help, it's too much traffic for such a small street, the street is too narrow
- Kay said the City wants these affordable units and the purpose of the waivers is to allow the financials of the project to "pencil out." There's nothing that can happen to stop this project.
- Neal applauds the members of the community who came to passionately express their concerns with the project. Palms has experienced rapid turnover over the years from single family homes to duplexes to apartments. This developer in specific is open to community concerns and has already made changes in response to the community. This developer is unusual. Most developers won't make changes in response to the community.
- Neal moves to approve the project. Alex seconds.
- Alex yes, Neal yes, Jamie yes, Kay no. Motion passes 3-1.
- Kay read into the record a letter written for the Committee by some community members. The letter assumed the project would be rejected. Since it was

approved, the letter would not be voted on, but Kay read it into the record.

b. Committee member updates
    - Did not get to this.
c. Update from Dylan Sittig from Councilmember Katy Yaroslavsky's office
    - Did not get to this.
d. Motor Avenue Improvement Association update
    - Did not get to this.
e. MOTION to approve a CIS on Council File 23-0479 about the mass Ellis Act evictions at Barrington Plaza. https://drive.google.com/file/d/1RSd4HnnYmfzTbZQyCFnN7HpSUoTYh598/view?usp=drive_link
    - Did not get to this.
f. MOTION to approve a letter to Councilmember Yaroslavsky about street closures during business peak hours. https://drive.google.com/file/d/15iJMQTXLj5fVhJ_Yc499-xXNm0jLG81Q/view?usp=drive_link
    - Did not get to this.
g. DISCUSSION AND POSSIBLE MOTION on the Westside Community Planning Advisory Group. https://drive.google.com/drive/folders/1J4MOBCvTBE_hnOZIArhPZpkbHNmyr16H?usp=drive_link
    - Did not get to this.
h. WRAC MOTION to approve a letter about night skies. https://drive.google.com/file/d/1vXsrGnokGmrSD11eaN5CNP8-9sOHuSY8/view?usp=drive_link
    - Did not get to this.
i. DISCUSSION AND POSSIBLE CIS OR LETTER. Deep dive on 22-1476 about spot street widening for new development projects. Jamie is presenting in favor. Neal is presenting opposed.
    - Did not get to this.
j. DISCUSSION AND POSSIBLE MOTION. Does the Palms NC PLUM Committee want the Palms Neighborhood Council to sign on to the Sepulveda Corridor justice letter?
    - Did not get to this.
k. Review and update Guidelines for Neighborhood Appropriate Development. This is working copy.
    - Did not get to this.
l. DECISION AND POSSIBLE CIS OR LETTER. Do we want to write a CIS for any of these motions this month or next? Which motion do we select for our next deep dive?
    i. AB-42 that would prohibit a local government from requiring fire sprinklers in tiny homes less than 500 square feet

- Did not get to this.

V. Adjournment:

- 9:03

b. Next meeting is scheduled for July 12, 2023 at 7:00 p.m. at the IMAN Center.

Time allocations for agenda items are approximate and may be shortened or lengthened at the discretion of the President/Committee Chairs. Public comment will be taken for each motion as well as for any item in the consent agenda prior to Board action. Comments from the public on Agenda items will be heard only when the respective item is being considered. Comments from the public on other matters not appearing on the Agenda that is within the Assembly's subject matter jurisdiction will be heard during the Public Comment period. Public comment is limited to 1 to 2 minutes per speaker, at the discretion or unless waived by the Assembly. In the interest of addressing all items on the agenda, time limits for individual comments and discussion may be set at the discretion of the President/Committee Chair. All items on the consent agenda will be determined by a single Committee vote and without Committee discussion. Committee members may request that any item be removed from the consent agenda and considered individually at any time prior to that vote.

Per Board of Neighborhood Commissioners Policy #2014-01, agendas are posted for public review at: 1) Woodbine Park Kiosk, 3409 S. Vinton Ave.; 2) Palms Neighborhood Council website, www.palmsnc.la

In compliance with Government Code section 54957.5, non-exempt writings that are distributed to a majority or all of the board in advance of a meeting may be viewed at Woodbine Park Kiosk, 3409 S. Vinton Ave, at our website: www.palmsnc.la or at the scheduled meeting. In addition, if you would like a copy of any record related to an item on the agenda, please contact the Secretary at secretary@palmsnc.la. You can also receive our agendas via email by subscribing to LA CIty's Early Notification System (ENS)

Palms NC Board and Committee members abide by a code of civility (http://empowerla.org/code-of-conduct/). Any person who interferes with the conduct of a Neighborhood Council meeting by willfully interrupting and/or disrupting the meeting is subject to removal. A peace officer may be requested to assist with the removal should any person fail to comply with an order of removal by the Neighborhood Council. Any person who resists removal by a peace officer is subject to arrest and prosecution pursuant to California Penal Code Section 403.

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EVENTO. SI NECESITA ASISTENCIA CON ESTA NOTIFICACION, POR FAVOR LLAME A NUESTRA OFICINA AL secretary@palmsnc.la