



# DEPARTMENT OF CITY PLANNING

## RECOMMENDATION REPORT

### City Planning Commission

**Date:** October 12, 2023  
**Time:** After 8:30 a.m.\*  
**Place:** The meeting's telephone number and access code access number will be provided no later than 72 hours before the meeting on the meeting agenda published at <https://planning.lacity.org/about/commissions-boards-hearings> and/or by contacting [cpc@lacity.org](mailto:cpc@lacity.org)

**Public Hearing:** August 16, 2023  
**Appeal Status:** General Plan Amendment is not appealable. Vesting Zone Change and Height District Change are appealable by the Applicant to the City Council if disapproved in whole or in part. All other actions are appealable to City Council.  
**Expiration Date:** October 30, 2023

**Case No.:** CPC-2017-469-GPA-VZC-HD-MCUP-SPR  
**CEQA No.:** ENV-2017-470-EIR  
**Related Cases:** VTT-74745, VTT-74745-1A  
**Council No.:** 14 – de León  
**Plan Area:** Central City North  
**Plan Overlay:** River Improvement Overlay (RIO)  
**Certified NC:** Arts District Little Tokyo  
**Existing GPLU:** Heavy Industrial  
**Proposed GPLU:** Regional Commercial  
**Existing Zone:** M3-1-RIO  
**Proposed Zone:** (T)(Q)C2-2-RIO  
**Applicant:** Dilip Bhavani and Surjit Soni, LIG – 900, 910 and 926 East 4<sup>th</sup> Street, 405-411 South Hewitt Street, LLC  
**Representative:** Edgar Khalatian, Mayer Brown

**PROJECT LOCATION:** 900-926 East 4<sup>th</sup> Street; 406-414 South Colyton Street; 405-423 South Hewitt Street, Los Angeles, CA 90021

**PROPOSED PROJECT:** The 4th and Hewitt Project (Project) proposes the demolition of an existing office building, two storage/garage buildings, and surface parking lots, to allow for the construction of an 18-story office building (Office Building) comprised of 8,149 square feet of ground floor restaurant space, 308,527 square feet of office, 16,249 square feet of covered exterior employee common areas and a 3,500 square-foot ground floor courtyard accessible from Colyton Street and Hewitt Street. The Project would total 340,770 square feet of floor area, comprised of an existing 7,800 square-foot building (the bow-truss building) and the new 332,970 square-foot Office Building, on a 1.3-acre lot, for a Floor Area Ratio of 6:1, and a building height of 292 feet to the top of the parapet. Vehicle parking would be provided within three subterranean levels and four levels of above grade parking.

### REQUESTED ACTIONS:

#### ENV-2017-470-EIR

1. Pursuant to Section 21082.1(c)(3) of the California Public Resources Code (PRC), the consideration and certification of the **Environmental Impact Report (EIR)** for the above-referenced project and Adoption of the **Statement of Overriding Considerations** setting forth the reason and benefits of adopting the EIR with full knowledge that significant impacts may remain;
2. Pursuant to Section 21081.6 of the California PRC, the adoption of the proposed **Mitigation Measures and Mitigation Monitoring Program**; and

3. Pursuant to Section 21081 of the California PRC, the adoption of the required **Findings** for the certification of the EIR.

#### **CPC-2017-469-GPA-VZC-HD-MCUP-SPR**

4. Pursuant to the Los Angeles City Charter Section 555 and Los Angeles Municipal Code (LAMC) Section 11.5.6, a **General Plan Amendment** to the Central City North Community Plan to change the land use designation from Heavy Industrial to Regional Commercial;
5. Pursuant to LAMC Section 12.32 F and Q, a **Vesting Zone Change** and **Height District Change** from the M3-1-RIO Zone to the C2-2-RIO Zone;
6. Pursuant to LAMC Section 12.24 W.1, a **Main Conditional Use Permit** for the sale and/or dispensing of a full line of alcoholic beverages for on-site consumption for up to six establishments; and
7. Pursuant to LAMC Section 16.05, a **Site Plan Review** for a project resulting in an increase of more than 50,000 gross square feet of non-residential floor area.

#### **RECOMMENDED ACTIONS:**

#### **ENV-2017-470-EIR**

If the City Planning Commission denies the appeal of the Vesting Tentative Tract Map (VTT-74745-1A) and sustains the actions of the Advisory Agency:

1. **Find**, based on the independent judgment of the decision-maker, after consideration of the whole of the administrative record, the Project was assessed in the previously certified 4<sup>th</sup> and Hewitt Project EIR No. ENV-2017-470-EIR, SCH No. 2017091054, certified on October 12, 2023; and pursuant to CEQA Guidelines, Sections 15162 and 15164, no subsequent EIR, negative declaration, or addendum is required for approval of the project.

#### **CPC-2017-469-GPA-VZC-HD-MCUP-SPR**

1. **Recommend** that the Mayor and City Council Approve a **General Plan Amendment** to the Central City North Community Plan to change the land use designation from Heavy Industrial to Regional Commercial;
2. **Recommend** that the City Council Approve a **Vesting Zone Change** and **Height District Change** from the M3-1-RIO Zone to the (T)(Q)C2-2-RIO Zone;
3. **Approve** a **Main Conditional Use Permit** for the on-site sale and/or dispensing of a full-line of alcoholic beverages for up to six establishments;
4. **Approve** a **Site Plan Review** for a project resulting in an increase of more than 50,000 gross square feet of non-residential floor area;
5. **Adopt** the attached Conditions of Approval; and
6. **Adopt** the attached Findings.

VINCENT P. BERTONI, AICP  
Director of Planning



Milena Zasadzien, Principal City Planner



Kathleen King, City Planner



Mindy Nguyen, Senior City Planner



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**ADVICE TO PUBLIC:** \*The exact time this report will be considered during the meeting is uncertain since there may be several other items on the agenda. Written communications may be mailed to the Commission Secretariat, Room 272, City Hall, 200 North Spring Street, Los Angeles, CA 90012 (Phone No. 213-978-1300). While all written communications are given to the Commission for consideration, the initial packets are sent the week prior to the Commission's meeting date. If you challenge these agenda items in court, you may be limited to raising only those issues you or someone else raised at the public hearing agendaized herein, or in written correspondence on these matters delivered to this agency at or prior to the public hearing. As a covered entity under Title II of the Americans with Disabilities Act, the City of Los Angeles does not discriminate on the basis of disability, and upon request, will provide reasonable accommodation to ensure equal access to these programs, services and activities. Sign language interpreters may be provided upon request. To ensure availability of services, please make your request not later than three working days (72 hours) prior to the meeting by calling the Commission Secretariat at (213) 978-1300.

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Environmental Impact Report (EIR) links:

Draft EIR: <https://planning.lacity.org/development-services/eir/4th-and-hewitt-project-0>

Final EIR: <https://planning.lacity.org/development-services/eir/4th-and-hewitt-project-1>

## PROJECT ANALYSIS

### PROJECT SUMMARY

The 4th and Hewitt Project (Project) is a commercial development that would be located on a 1.3-acre site within the Arts District neighborhood of Downtown Los Angeles. The Project would include a total of 340,770 square feet of floor area, comprised of an existing 7,800 square-foot, vacant bow-truss building, and a new 332,970 square-foot, 18-story Office Building that would include 308,527 square feet of office space, 8,149 square feet of ground floor restaurant space, and 16,294 square feet of covered office exterior common areas. The Project Site is developed with an existing office building, the vacant bow-truss building, two storage/garage buildings, and surface parking lots. To accommodate the Project, the existing uses, excluding the vacant bow-truss building located at the southwest corner of Colyton Street and 4th Street, would be demolished.



*New Office Building Facing Hewitt Street*

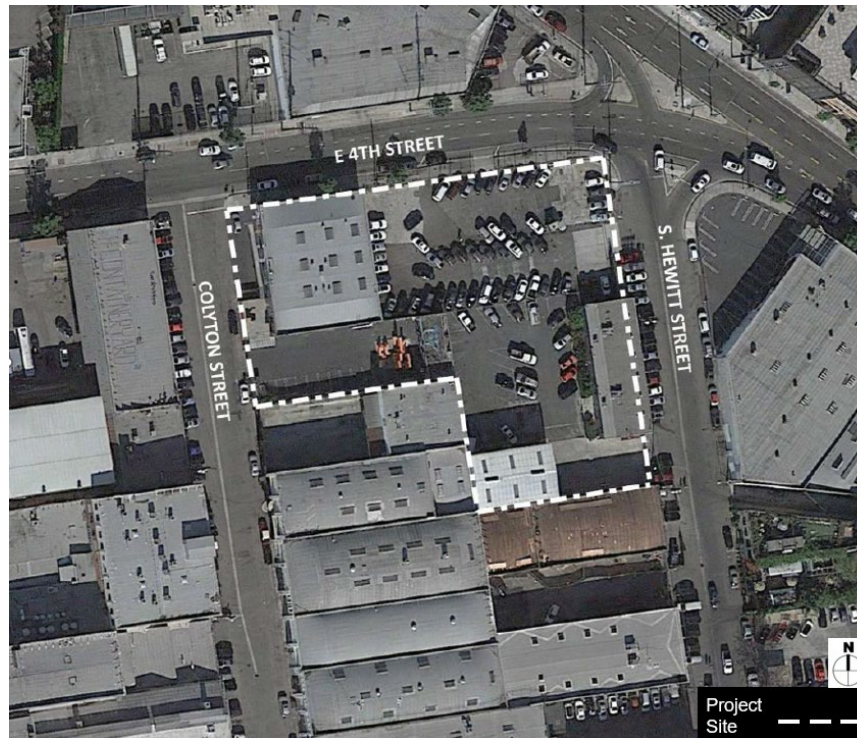
The Project would also include a 3,500 square-foot ground floor outdoor courtyard accessible that would connect to a publicly accessible ground floor pedestrian passageway and create a mid-block pedestrian cut-through from Colyton Street to Hewitt Street. Vehicle parking would be provided within three subterranean levels and four levels of above-grade parking contained in a parking podium. The Office Building would have a building height of 292 feet to the top of the parapet, (a maximum height of 297 feet to the top of the elevator overrun). The Project's Floor Area Ratio (FAR) would be 6:1.

### BACKGROUND

#### **Location and Setting**

The L-shaped, 1.3-acre Project Site is located within the Arts District neighborhood within the Central City North Community Plan area of the City of Los Angeles (City). The Project Site is

bounded by Colyton Street to the west, 4<sup>th</sup> Street to the north, Hewitt Street to the east, and parcels developed with commercial and manufacturing uses and a surface parking to the south.



*Aerial View of the Project Site*

### **Project Site Characteristics**

The topography of the Project Site is relatively flat and has approximately 280 feet of frontage along 4<sup>th</sup> Street, 150 feet of frontage along Colyton Street, 200 feet of frontage along Hewitt Street, and 288 feet along the southern property line.

The Project Site is located within a Methane Zone and would be required to comply with the LAMC methane seepage regulations for new projects to control methane intrusion emanating from geologic formations. The Site is not located within a Hillside Area, Very High Fire Hazard Severity Zone, Flood Zone, or the Alquist-Priolo Fault Zone. The Project Site is located within the East Los Angeles State Enterprise Zone, which permits a parking rate for commercial and office uses of two parking spaces per 1,000 square feet of gross floor area. However, the Project is also subject to Assembly Bill (AB) 2097, Government Code Section 65863.2, which prohibits the City from imposing a minimum automobile parking requirement on most development projects located within a half-mile radius of a major transit stop. No other special use districts are applicable to the Project Site.

There is one non-protected tree on the Project Site that would be removed as part of the Project. Three non-protected Brisbane Box (*Tristania conferta*) street trees are located in the public right-of-way along 4<sup>th</sup> Street and range between three and six inches in diameter that would be removed as part of the off-site improvements (e.g., utility work, sidewalk improvements, curb cuts, new driveways, etc.).

### **Existing Development to be Retained as Part of the Project**

As discussed above, the Project Site is currently improved with two single-story commercial buildings, two storage/garage buildings, and surface parking lots, totaling 54,581 square feet. All

existing development would be removed, except for the vacant bow-truss building, located at the southwest corner of 4<sup>th</sup> Street and Colyton Street. Constructed in 1952, this building is approximately 7,800 square feet, has a rectangular plan, and is constructed of brick with a bow-truss roof that has a series of skylights and is surrounded by brick parapets, and is the former site of the A+D Museum. The entrance to the building is accessed via a raised concrete ramp and metal balustrade. The building has been substantially altered over time and is not designated as a historic resource.



*Looking Southeast from 4<sup>th</sup> Street at the Bow-Truss Building*

### **Existing Land Use Designation and Zoning**

The Project Site is located in the Central City North Community Plan, which designates the Project Site for Heavy Industrial land uses, and is zoned M3-1-RIO (Heavy Industrial Zone, Height District 1, River Improvement Overlay). The Heavy Industrial land use designation allows for a wide range of industrial and commercial zones and the M3 Zone permits a variety of uses and intensities. Height District 1 does not impose a maximum height limit for parcels zoned M3, but restricts the Site's Floor Area Ratio (FAR) to 1.5:1. The M3 Zone does not require any setbacks. Further, the Project Site is subject to the Central City North Community Plan Footnote No. 6 which states, "For properties designated on zoning maps as Height District Nos. 1, 1L, 1VL, or 1XL (or their equivalent), development exceeding a floor area ratio of 1:5:1 up to 3:1 may be permitted through a zone change height district change procedure, including an environmental clearance." The RIO is a Supplemental Use District that is related to the Project's location in proximity to the Los Angeles River, the purpose of which is to support the goals of the Los Angeles River Revitalization Master Plan, which subjects the Project Site to specific development regulations related to landscaping, fencing, river access, and lighting.

### **Surrounding Uses**

The Project Site is located in an urbanized area and generally surrounded by low- to medium-rise industrial and commercial buildings, as well as a few residential buildings. The Arts District neighborhood continues to undergo changes to the development landscape, with new residential, live/work, restaurant, retail, and office uses being introduced within an area previously concentrated with commercial and industrial uses.

North: To the north of the Project Site, across 4th Street, is a one-story automotive repair shop and warehouse. Across 4th Place is a seven-story parking structure and a three-story office building. These parcels are designated for Heavy Industrial land uses and zoned M3-1-RIO.

East: To the east of the Project Site, across Hewitt Street, are one-story commercial and manufacturing uses as well as a surface parking lot. These parcels are designated for Heavy Industrial land uses and zoned M3-1-RIO.

South: To the south of the Project Site are one- and two-story commercial and manufacturing uses. These parcels are designated for Heavy Industrial land use and zoned M3-1-RIO.

West: To the west of the Project Site across Colyton Street are one-story manufacturing uses. These parcels are designated for Heavy Industrial land uses and zoned M3-1-RIO.

As shown in the figure below, similar to the Project's General Plan Amendment request to change the Project Site's land use designation from Heavy Industrial to Regional Commercial, several surrounding projects located in the Arts District neighborhood have requested or have been recently approved for the land use designation to be changed from Heavy Industrial to Regional Commercial or other non-industrial land use designations.



*Requested/Approved General Plan Amendments Near the Project Site*

### **Regional and Local Access**

Primary regional access is provided by State Route 110 (SR-110) and the Hollywood Freeway (US-101). Alameda Street is the major arterial providing regional access to the Project Site.



Vehicular access to the Site is provided via two new driveways on 4<sup>th</sup> Street and a loading dock accessible from Hewitt Street.

## **Streets and Circulation**

### *Bureau of Engineering (BOE) Improvements*

4th Street is designated by the 2035 Mobility Plan as an Avenue III and dedicated to a right-of-way width of 72 feet along the Project Site frontage. As required by BOE, a six-foot wide strip of land would be dedicated along a portion of 4th Street, to complete a 36-foot-wide half public right-of-way in accordance with the Avenue III Street standards. However, as the existing bow-truss building would remain, the six-foot-dedication would not be required adjacent to the existing building. A concrete curb, gutter, and a 13-foot full-width sidewalk with tree wells shall be provided along with a 23-foot minimum half roadway. Additionally, a 15-foot by 15-foot corner cut at the intersection of 4th Street and Hewitt Street shall be provided and limited to a height of 20 feet measured from the finished sidewalk surface.

Hewitt Street is designated by the 2035 Mobility Plan as a Collector and dedicated to a right-of-way width of 66 feet along the Project Site frontage. BOE is requesting that a two-foot to four-foot variable width strip of land be dedicated along Hewitt Street to complete a 34-foot-wide half public right-of-way in accordance with Collector Street standards. A concrete curb, gutter, and a 10-foot full-width sidewalk with tree wells shall be provided.

Colyton Street is designated by the 2035 Mobility Plan as a Collector and dedicated to a right-of-way width of 66 feet along the Project Site frontage. BOE is requesting that a four-foot-wide strip of land be dedicated along Colyton Street adjoining the Project Site to complete a 34-foot-wide half public right-of-way in accordance with Industrial Collector Street standards. Additionally, a 15-foot by 15-foot public street cut corner be dedicated at the intersection with 4th Street. A concrete curb, gutter, and a 10-foot full-width sidewalk with tree wells shall be provided along with a 23-foot minimum half roadway.

### *Living Streets Improvements*

On August 16, 2023, as part of the joint public hearing held by Deputy Advisory Agency (DAA) and Hearing Officer, and in collaboration with the Department of City Planning's Urban Design Studio and the Applicant, City Planning staff requested that the DAA consider the Living Streets typology, specifically that the Project have the flexibility to utilize and implement improvements consistent with this typology or to implement BOE's required improvements, as described above.

The Living Streets typology is a developing street treatment to retain the original feel of the Arts District roadways instead of reconstructing the roadways with a traditional curb, gutter, and sidewalks. The DAA approved the Project, as requested. Under the Living Streets typology, the following dedications and improvements would be required:

4th Street would be improved with a concrete curb, a concrete gutter, and a 10-foot full-width concrete sidewalk with tree wells along the bow-truss building and a 13-foot full-width concrete sidewalk with tree wells along the remainder of the site. A 3-foot-wide strip of land be provided as a sidewalk easement, along 4th Street adjoining the tract except where existing structures to remain, to complete a 30-foot-wide half public right-of-way, as approved by the Advisory Agency, including a 15-foot by 15-foot property line cut corner at the intersection with Hewitt Street. The upper limit to the above cut corner shall be limited to the height of 20-feet measured from finished sidewalk surface.

Hewitt Street would be improved with a near-flush curb (1/4-inch vertical-height curb with 1:2 chamfer to provide a continuous detectable edge, as per CBC 11B-303.3) and 16-foot-wide concrete sidewalk with tree wells. Suitable surfacing to join the existing pavement and to complete a 14-foot half roadway from centerline would be required and a roadway surfacing shall allow for center draining condition and V-section gutters to remain, as per 'Shared Street' designation in S-470 standard plan. The existing partial 2-foot-wide sidewalk easement would be merged into the tract, and no dedication would be required along Hewitt Street adjoining the tract to maintain the existing 30-foot-wide half right-of-way.

Colyton Street would be improved with a near-flush curb (1/4-inch vertical-height curb with 1:2 chamfer to provide a continuous detectable edge, as per CBC 11B-303.3) and 16-foot-wide concrete sidewalk with tree wells. Suitable surfacing to join the existing pavement and to complete a 14-foot half roadway from centerline would be required and roadway surfacing shall allow for center draining condition and V-section gutters to remain, as per 'Shared Street' designation in S-470 standard plan. No dedication be required along Colyton Street adjoining the tract to maintain the existing 30-foot-wide half right-of-way. In addition, a minimum 15-foot by 15-foot building line cut corners be provided at the intersection with 4th Street.

### **Public Transit**

Public transit service in the vicinity of the Project Site is currently provided by multiple local lines and regional lines via stops along 1<sup>st</sup> Street, 3<sup>rd</sup> Street, 4<sup>th</sup> Street, 4<sup>th</sup> Place, 6<sup>th</sup> Street, 7<sup>th</sup> Street, Traction Avenue, Olympic Boulevard, Central Avenue, Alameda Street, Boyle Avenue, and Soto Street. Public transit service in the immediate Project vicinity is currently provided by the Los Angeles County Metropolitan Transportation Authority (Metro), LADOT Downtown Area Short Hop (DASH) and Montebello Bus Lines. The bus lines include Metro Local Lines 18, 30, 53, 60, 62, 66, 106, 251, Metro Shuttle 605, the LADOT DASH A bus, and Montebello bus lines M40 and M90. Additionally, the Project Site is 0.5 miles southeast of the Metro A and E (Formerly Gold) Line Little Tokyo/Arts District Station.

### **Bicycle Facilities**

The City's 2010 Bicycle Plan identifies the City's vision for a more integrated bicycle network throughout the City, including within the Project vicinity. Currently, there are no existing bicycle lanes, path, or routes adjacent to the Project Site. Dedicated Class II bicycle lanes are located along 3<sup>rd</sup> Street between 4<sup>th</sup> Place and Santa Fe Avenue, and become a Class III bicycle lane east of Garey Street. Dedicated Class III bicycle lanes are located along 2<sup>nd</sup> Street between Alameda Street and Santa Fe Avenue and Class II bicycle lanes along 1<sup>st</sup> Street from Beaudry Avenue to Mission Street in Boyle Heights.

### **Land Use Policies**

The Project Site is located within the Central City North Community Plan, the East Los Angeles State Enterprise Zone, Transit Priority Area, and the RIO; and is subject to AB 2097 (Government Code Section 65863.2).

#### *Central City North Community Plan and Downtown Community Plan Update*

The Central City North Community Plan Map designates the site as Heavy Industrial. According to the Community Plan, the Heavy Industrial land use designation allows for a corresponding zone of M3. The Heavy Industrial land use designation is subject to Footnote No. 6 which states that areas within Height District 1, 1L, 1VL, and 1XL may request a zone and height district change to increase allowable FAR from 1.5:1 to 3:1.

On May 3, 2023 the Los Angeles City Council (City Council) approved the Downtown Los Angeles Community Plan Update (DTLA 2040). Following the City Council approval, the implementing ordinances will be reviewed and finalized by the City Attorney. The purpose of the DTLA 2040 Plan is to develop and implement a future vision for Downtown Los Angeles that supports and sustains ongoing revitalization while thoughtfully accommodating projected future growth. As Downtown Los Angeles has been a rapidly changing setting within the City, it supports a collection of economic opportunities and entrepreneurship, people, culture, and distinct neighborhoods, and sits at the center of the regional transportation network.

DTLA 2040 identifies the Project Site with a land use designation of Hybrid Industrial, which is intended to “preserve productive activity and prioritize space for employment, including light industrial, new industry, commercial, and vertically-integrated businesses, with careful introduction of live/work uses.” This designation would allow a maximum FAR range of 3:1 to 6:1, with general uses that include creative office, live/work, manufacturing, and creative production.

#### *Los Angeles State Enterprise Zone*

On July 11, 2013, California Governor Edmund G. Brown Jr. signed legislation that resulted in the repeal of the Enterprise Zone Act and the dissolution of State Enterprise Zones. However, the City Council adopted an action on December 18, 2013 that approved the continuation of the reduced parking provision for former State Enterprise Zone areas.

#### *Transit Priority Area*

In September 2013, California Governor Jerry Brown signed Senate Bill 743 (SB 743), which made several changes to CEQA for projects located in areas served by transit. Among other things, SB 743 added California PRC Section 21099, which provides that “aesthetic and parking impacts of a residential, mixed-use residential, or employment center project on an infill site within a transit priority area shall not be considered significant impacts on the environment.” PRC Section 21064.3 defines “major transit stop” as “a site containing an existing rail transit station, a ferry terminal served by either a bus or rail transit service, or the intersection of two or more major bus routes with a frequency of service interval of 15 minutes or less during the morning and afternoon peak commute periods.” The Project is an employment center project and is located on an infill site. Metro runs multiple bus lines, including along 1<sup>st</sup> Street, 3<sup>rd</sup> Street, 4<sup>th</sup> Street, 6<sup>th</sup> Street, 7<sup>th</sup> Street, Alameda Street, and Central Avenue in the area. The Project is 0.5 miles away from Metro A and E Line’s Little Tokyo Stop and therefore qualifies as located within a Transit Priority Area.

#### *River Improvement Overlay (RIO) District*

In connection with the Los Angeles River Revitalization Master Plan, which focuses on the creation of parks, paths, and open spaces in the vicinity of the Los Angeles River, the RIO District proposes the establishment of a distinct sustainable environment in the surrounding neighborhoods to promote concepts developed in the Los Angeles River’s Master Plan. The RIO District establishes landscaping, design criteria, and administrative review procedures for projects within the RIO District. Although the Project is located within the boundaries of the RIO District, the Project Site is separated from the Los Angeles River by existing railroad tracks and is not immediately adjacent or accessible from the River. The Project has been reviewed in consideration of the relevant objectives of the Los Angeles River Design Guidelines, including employing high quality, attractive and distinguishable architecture and minimizing the quantity and appearance of parking and loading areas by locating all parking underground and placing loading areas within the building or away from public view.

*Assembly Bill 2097 (AB 2097)*

On September 22, 2022, Governor Newsom signed AB 2097 which added Government Code Section 65863.2. AB 2097 prohibits a public agency from imposing or enforcing any minimum automobile parking requirement on any residential, commercial, or other development project that is within one-half mile of a Major Transit Stop. The Project is subject to AB 2097 as it is 0.5 miles from the Little Tokyo/Arts District light rail station, a Major Transit Stop.

**Relevant Cases***Subject Property*

VTT-74745: On August 16, 2023, the Advisory Agency approved Vesting Tentative Tract Map No. 74745 for the merger and re-subdivision of a 1.3-acre site into one master ground lot and 12 airspace lots, and a Haul Route for the export of up to 84,300 cubic yards of soil. On September 11, 2023, this decision was appealed and is pending decision by the City Planning Commission concurrent with the subject case.

*Surrounding Properties (Within 1000-foot Radius)*

CPC-2016-3655-GPA-ZC-HD-CUB-ZAA-SPR: On September 14, 2017 the City Planning Commission recommended that the Mayor and City Council approve a General Plan Amendment, that the City Council approve a Zone and Height District Change from M3-1-RIO to (T)(Q)C2-2-RIO, approved a Conditional Use Permit for the sale and consumption of alcoholic beverages, a Zoning Administrator's Adjustment to permit a zero-foot side yard setback in lieu of a seven-foot setback, and a Site Plan Review for the conversion of an existing 44,600 square-foot manufacturing warehouse into a 66 guest-room hotel, including a restaurant, retail space, and screening room located at 400-416 South Alameda Street and 407-417 South Seaton Street. The City Council approved the Commission's recommendations on February 6, 2018, and Mayor Garcetti signed the ordinances on February 9, 2018.

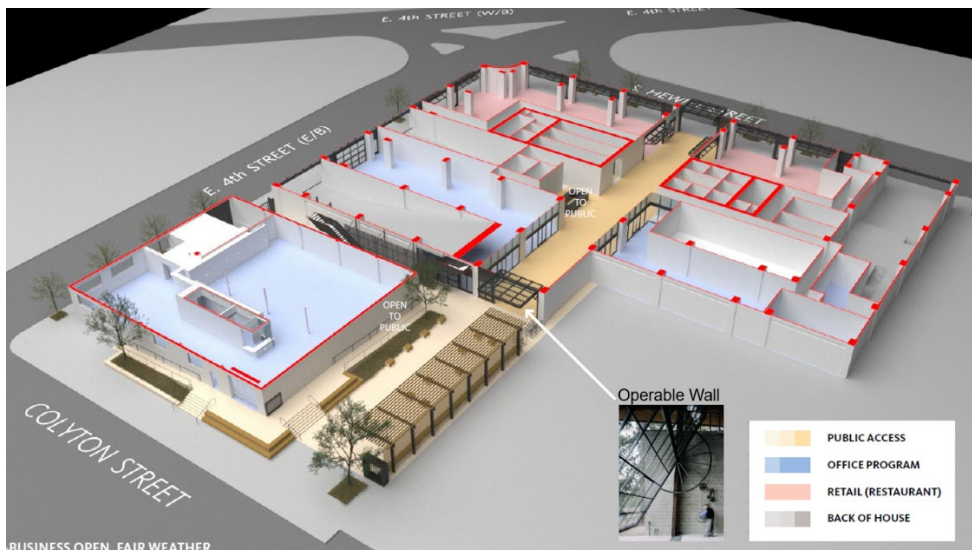
**PROJECT DETAILS**

The Project would include the construction of a new 18-story Office Building and the retention of the vacant 7,800 square-foot, bow-truss building. The Office Building would include 308,527 square feet of office space, 8,149 square feet of ground floor restaurant space, and 16,294 square feet of covered office exterior common areas, for a total FAR of 6:1.



*View from 4<sup>th</sup> Street looking East*

The Office Building would be comprised of three levels of subterranean parking, ground floor restaurant spaces accessible from 4<sup>th</sup> Street, Hewitt Street, the pedestrian passageway accessible from Colyton Street and Hewitt Street, four levels of above grade parking, and office uses on Levels 6-18. The figure below illustrates the ground floor areas of the Project Site that would be accessible to the public and/or programmed with restaurant or office uses.



*4th and Hewitt Project Ground Floor Programming*

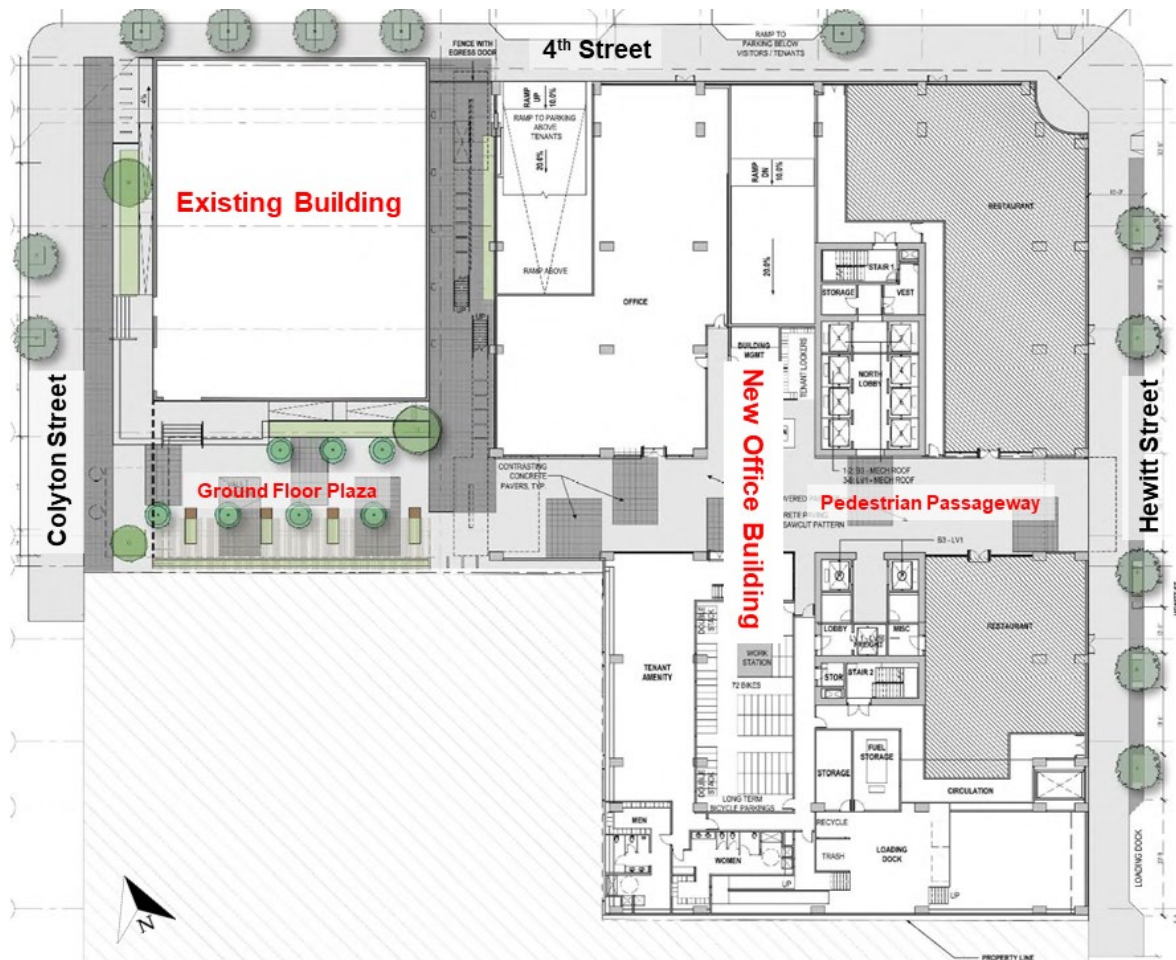
A 3,500 square-foot ground floor outdoor courtyard accessible from Colyton Street would connect to a publicly accessible ground floor pedestrian passageway and would create a mid-block pedestrian cut-through from Colyton Street to Hewitt Street. As conditioned, these areas would be publicly accessible during restaurant and office business hours, after which, the western gates and eastern doorways would be closed and locked. After-hours access would be provided to tenants via keycard. Operable walls would be installed at the west and east entrances to the pedestrian passageway.



*After-Hours Access*

The Office Building would have a building height of 292 feet to the top of the parapet (a maximum height of 297 feet to the top of the elevator overrun).

Open and uncovered employee amenities include a 3,500 square-foot outdoor courtyard on the ground floor and an outdoor amenity deck that wraps around the building on Level 6, and Levels 16 through 18. Exterior, covered areas are located on Levels 7 through 15. The ground floor outdoor courtyard, pedestrian passageway, the respective locations of the existing bow-truss building and new Office Building are shown in the Project Site Plan below.



Site Plan

### Project Design and Architecture

The Project's overall design would pay homage to the Arts District neighborhood's utilitarian style, while also constructing a modern office building. The Office Building ground floor and parking podium would be designed to resemble the surrounding warehouses' minimal but functional design and would include iron-framed windows and large bi-fold doors set into unfinished concrete.



*Rendering of the West Façade*

The parking levels visible from 4<sup>th</sup> Street and Hewitt Street façades, would be designed with non-operable, industrial steel frame opaque windows set in exposed concrete walls. The parking level along the southern façade would be fully enclosed with articulated unfinished concrete and would be accented with murals, similar to other murals found throughout the Arts District neighborhood. As seen in the rendering above, the parking podium's western façade, adjacent to the bow-truss building and outdoor courtyard, would include non-operable opaque windows set in concrete similar to the north and east facades. Additionally, the west façade would include black mesh screens to screen the parking ramps, as they are right up against the west façade. The mesh screens would become a background element for a mural. Further, the western façade includes two exit stairs that scale the building. Finally, the southern portion of the west façade's podium adjacent to the neighboring property would be fully enclosed in concrete decorated with a mural.





*View of Ground Floor Uses and Above Grade Parking Levels along Hewitt Street*

The Office Building's transition from a utilitarian building to a modern day office building would begin on Level 6, where the Project would include a substantial setback as it transitions from the podium levels to the office levels. The western façade would feature a maximum setback of approximately 45 feet, the northern façade would be set back approximately 9 feet, and the eastern and southern facades would be set back approximately 7.5 feet. From Level 6 upwards, the exterior of the Office Building would be encased in a glass curtain wall with concrete accents to continue the industrial base design throughout the entire building. Sliding glass panels would lead to outdoor balconies on each office level providing an indoor/outdoor feel. As mentioned, an employee outdoor amenity deck on Level 6 would wrap around the entire Office Building. Metal screen railings on the balconies would tie into the parking podium's industrial steel frame windows.

The project proposes a chamfered edge at the northeast corner of the building's podium. The chamfer would be inset with a mural, which would match the character of the Arts District neighborhood. Further up the northeast corner, a geometric design element composed of exposed concrete would wrap the outdoor terraces on levels 13 through 18.



*View Looking East from Colyton Street*



*View Looking Southwest From 4<sup>th</sup> Street*

## Open Space and Landscaping

The Project would not be required to provide open space (as defined in the LAMC), as only commercial uses are proposed. However the Project would provide a publicly accessible ground floor courtyard and tenant amenity spaces. As mentioned, a 3,500 square-foot courtyard would be located adjacent to the bow-truss building, accessible from Colyton Street (or from Hewitt Street via the pedestrian passageway), would include a pergola and be landscaped with potted plants and trees. As a Los Angeles Department of Water and Power (LADWP) vault would be located beneath the courtyard, landscaping cannot be planted in the ground.



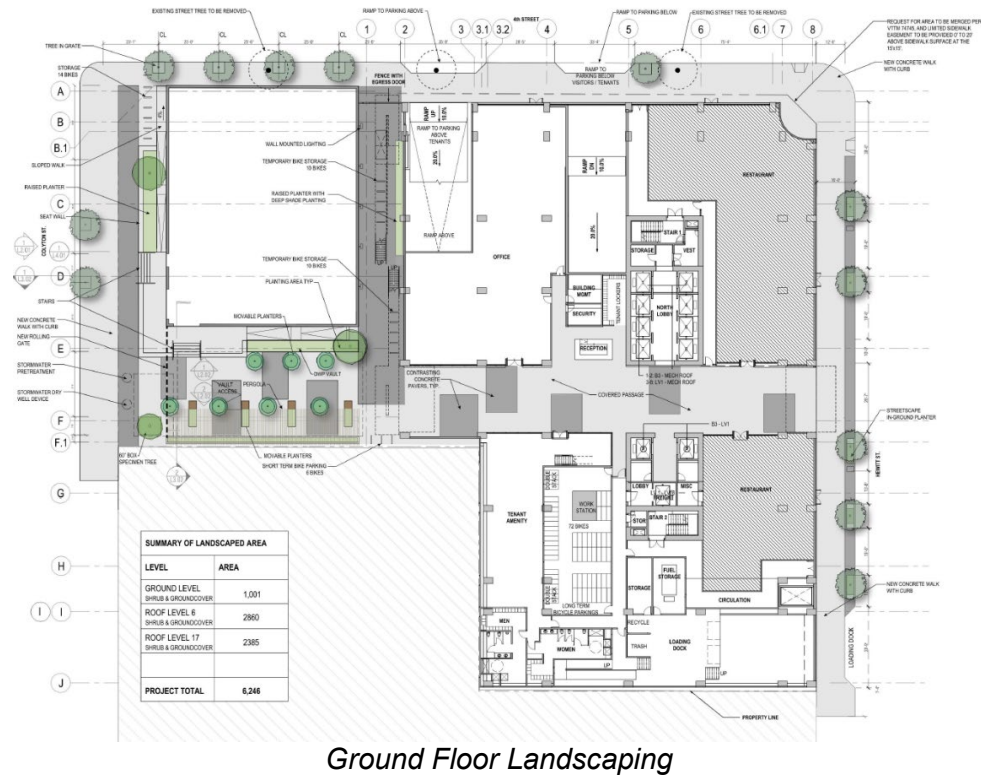
*View of the Courtyard Looking East*



*Looking East from Colyton Street with the Pedestrian Passageway in the Background*

Approximately 1,000 square feet of the ground floor would be landscaped with shrubs and groundcover. The three existing non-protected street trees (Brisbane box trees) along 4<sup>th</sup> Street would be removed and replaced with five street trees on 4<sup>th</sup> Street, five street trees on Hewitt Street, and two trees on Colyton Street (see the Ground Floor Landscape Figure below). Three

additional trees would be planted on-site: two trees would be planted adjacent to the Colyton Street frontage and one potted tree would be located in the courtyard area. Tree removals and replacements are subject to the approval of the City of Los Angeles Board of Public Works Urban Forestry Division.



Ground Floor Landscaping

Additional office tenant open space areas would be located on Levels 6 through 18, ranging in size from 524 square feet to 10,839 square feet.

**Parking**

*Vehicle Parking*

As discussed above, the City cannot require the Project to provide vehicle parking for the proposed commercial uses, as the Site meets the requirements of Government Code Section 65863.2 (AB 2097). However, the Applicant has chosen to voluntarily provide vehicle parking for the proposed commercial uses at rates consistent with the East Los Angeles State Enterprise Zone and the bicycle replacement provisions of LAMC 12.21 A.4, which sets a commercial and office parking ratio of two parking spaces per 1,000 square feet of floor area, with bicycle parking reductions to replace 28 vehicular spaces. After the bicycle replacement provisions and continuing to provide parking for the bow-truss building to be retained, a total of 660 vehicular parking spaces would be provided within three subterranean levels and four above-grade parking levels contained within a parking podium.

*Bicycle Parking*

Pursuant to LAMC Section 12.21.A.4 and A.16, the Project would be required to provide a total of 107 long- and short-term bicycle parking spaces. As shown in the table below, the Project would provide a total of 112 total bicycle parking spaces, five more spaces than what would be required, including 40 short-term bicycle parking spaces which would be located at the northwest

corner of the Site adjacent to the bow-truss building, throughout the corridor between the existing building and the proposed Office Building, and in the courtyard and passageway that connect to the Office Building lobby; and 72 long-term bicycle parking spaces would be located on the ground floor inside the Office Building, accessible from the pedestrian passageway.

<b><i>Bicycle Parking</i></b>							
<b>Use</b>	<b>Square Feet</b>	<b>Spaces Required</b>			<b>Spaces Provided</b>		
		<b>Short-Term</b>	<b>Long-Term</b>	<b>Total</b>	<b>Short-Term</b>	<b>Long-Term</b>	<b>Total</b>
Office Building	327,976	33 (1/10,000 sf)	66 (1/5,000 sf)	<b>99</b>	34	67	<b>101</b>
Restaurant	8,149	4 (1/2,000 sf)	4 (1/2000 sf)	<b>8</b>	6	5	<b>11</b>
Bow-Truss Building	7,800	0 (Existing use to remain)			0	0	<b>0</b>
<b>Total</b>		<b>37</b>	<b>70</b>	<b>107</b>	<b>40</b>	<b>72</b>	<b>112</b>

### **Access and Circulation**

Vehicular access to the Project Site would be provided from two driveways on 4<sup>th</sup> Street, including an ingress/egress driveway that would provide access to the subterranean parking garage and ingress/egress driveway that would provide access to the parking podium levels. As 4<sup>th</sup> Street is a one-way eastbound street, both driveways would be restricted to right-turn in and out movements. An at-grade loading dock would be accessible from Hewitt Street. All driveways and access would be designed in accordance with the City of Los Angeles Department of Transportation (LADOT) standards.

Pedestrian access to the bow-truss building would continue to be provided from Colyton Street, while access to the ground floor restaurant uses would be from 4<sup>th</sup> Street, Hewitt Street, and/or the ground floor courtyard. The main entrance to the office uses would be from the passageway accessible from either Colyton or Hewitt Streets with a secondary entrance from 4<sup>th</sup> Street.

In addition, as described above, the Project's pedestrian passageway would provide through access between Hewitt Street and Colyton Street. The various access points to the Site are shown in the figure below.



*Ground Floor Circulation Diagram*

## Sustainability Features

The Project would incorporate environmentally sustainable building features and comply with construction protocols required by the Los Angeles Green Building Code and CALGreen, which would reduce energy and water usage and would include, but not be limited to, the following features: 30 percent of provided parking to be Electric Vehicle (EV)-ready (198 spaces) and EV charging stations (EVSE) for 10 percent of the total parking spaces (66 spaces), a “cool roof”, low-flow plumbing fixtures, 30 percent of the useable roof to be solar-ready, and energy-efficient mechanical systems, glazing and window frames, and lighting. Additionally, the Project would be designed to meet the Leadership in Energy and Environmental Design (LEED) Silver standards.

## Project Design Analysis

### *Citywide Design Guidelines*

The Citywide Design Guidelines (Guidelines), adopted by the City Planning Commission on June 9, 2011, and last updated and adopted on October 24, 2019, establish a baseline for urban design expectations and present overarching design themes and best practices for residential, commercial, and industrial projects. Projects should either substantially comply with the Guidelines or through alternative methods to achieve the same objectives, and the Guidelines may be used as a basis to condition a project. The Guidelines focus on three main design approaches: Pedestrian-First Design, 360 Degree Design, and Climate-Adaptive Design.

### Pedestrian-First Design

The Project would encourage pedestrian activity along 4<sup>th</sup> Street and Hewitt Steet and create a seamless transition from the public right-of-way to the restaurant space with the introduction of new restaurant uses on the ground floor that are designed with large bi-fold doors. Pedestrian access along Colyton Street would be provided through the introduction of the publicly accessible

ground floor courtyard which would connect to a pedestrian passageway, providing access to Hewitt Street. Potted plants, a pergola, and potted shade trees would soften the courtyard area and enhance the pedestrian experience. The pedestrian passageway would not only improve connectivity through the Site but would serve as the primary pedestrian access point to the Office Building lobby, short- and long-term bicycle parking, bicycle lockers and showers. Pedestrian sidewalks would be constructed along Colyton and Hewitt Streets where none currently exist.



*Ground Floor Courtyard with Potted Plants and Trees and Pergola*

Vehicular parking onsite would be provided in three subterranean levels and four above-grade levels contained within a parking podium. The Project would screen the parking podium from the street with non-operable, industrial steel frame windows, set in exposed concrete walls along 4<sup>th</sup> and Hewitt Streets and a portion of Colyton Street. The parking podium's southern façade and a portion of the façade that faces Colyton Street would be shielded behind exposed concrete with a mural overlay. The parking podium was purposely designed to blend seamlessly into the design of the entire building and to make for a more pedestrian-experience on the ground level.

### 360 Degree Design

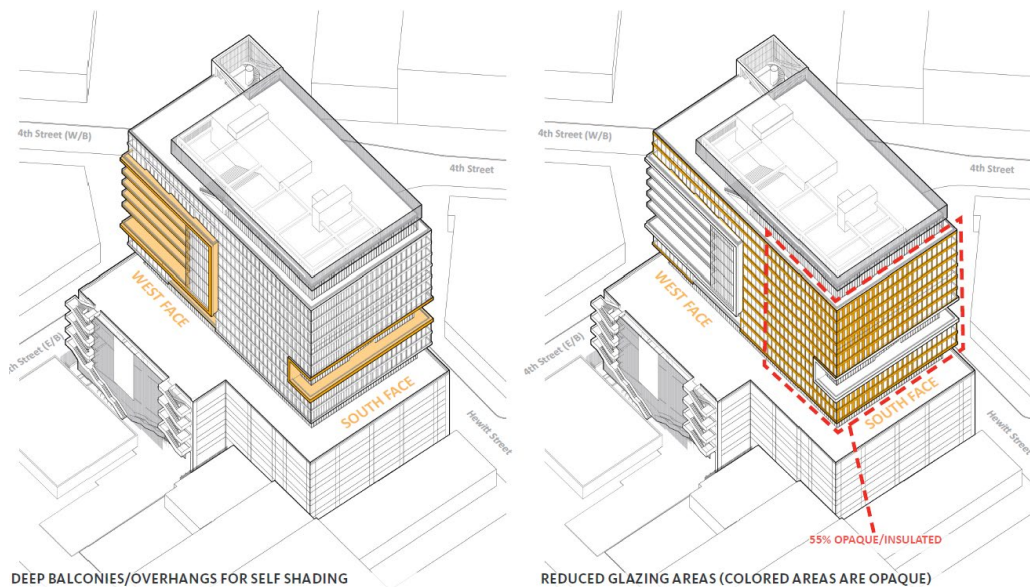
The Project's orientation, the design of the commercial uses, and the ground floor courtyard and pedestrian passageway ensure that all of the Project's frontages, including internal frontages throughout the Site, would be activated with uses. The base of the Office Building would incorporate design elements to reflect the Arts District neighborhood's industrial past while transitioning to a modern Office Building. The ground floor would be constructed with a rough concrete finish and large bi-fold doors opening the restaurant uses to the street. The parking podium would be shielded with inoperable steel-framed windows set in unfinished concrete. Murals that allude to the Arts District vernacular would be painted on the southern and western facades. Further, the Project's passageway and ground floor courtyard would allow for increased pedestrian activity from Hewitt Street to Colyton Street through the Site, thereby opening up the block, which is approximately 620 feet deep. The ground floor courtyard would also provide access to the ground floor restaurant uses and Office Building lobby with seating for visitors and employees.



*Parking Podium Design Viewed from the Corner of 4<sup>th</sup> and Colyton Streets*

Climate-Adaptive Design

The Project’s design would incorporate energy-efficient features, such as high performance window glazing and window frames; passive energy efficiency strategies, such as deep balconies which provide self-shading, “shadow boxes” that would provide 25 to 50 percent shading, a “cool roof”, and reduced glazing areas for solar mitigation. Shadow boxes would block sun in a way that is similar to deep recessed balconies. The windows on the southern façade and the southern part of the western façade would feature 55 percent opaque facades. This would be achieved through multiple spandrel panels placed between the glazed windows. The Reduced Glazing figure below demonstrates the reduced glazing as the windows on the south façade and the southern portion of the west façade appear to be smaller resulting in less reflectivity. The Project would comply with the Los Angeles Green Building Code and 30 percent of the total useable roof area will be solar ready.



*Climate Adapted Features Including Deep Balconies (left) and Reduced Glazing (right)*

### *Urban Design Studio Professional Volunteer Program*

The Project was presented to the Department of City Planning's Urban Design Studio and Professional Volunteer Program (PVP), which acts as a venue for City Planning staff to gather project-specific urban design advice and insight from local architecture professionals. Comments received by staff about the Project are as follows, with responses immediately below each comment.

#### Pedestrian-First Design

- *Consider adding an entrance to the Hewitt Street corner below the focal point to open up the Project to the public realm and add a presence to the street.*

The Project proposes a chamfer at the corner of the building at the Hewitt Street and 4th Street intersection, which will have a bench at the ground level and a mural above it. A pedestrian entrance at the corner would not make practical sense as the applicant anticipates a healthy pedestrian volume and would like to protect the pedestrian refuge and line of sight at the corner so as not contribute to any other obstructions.

- *Consider a robust wayfinding program to help visitors navigate and adding trees and landscaping to provide shade and a welcoming gathering area. Also, consider adding a a café or restaurant to attract people in the paseo.*

There would be adequate way-finding to direct visitors to all the uses and amenities on the project site. In response to this comment, the plaza area was revised to include more landscaping and shade elements, such as a pergola and additional potted shade trees and other shrubs to soften the hardscape and provide a green, shaded area for the outdoor seating. Because the majority of the courtyard area is over a subterranean DWP vault area, in-ground planting is limited in this area.

- *The area in front of the bow-truss building (from the paseo side) that indicates two parallel parking spots should be designed as a perpendicular pedestrian connection to the street.*

The existing structure that is to remain at the northwest corner has a pedestrian entrance facing Colyton Street that is a few feet above grade, which requires a ramp to its entrance. The Project would maintain the current ramp on the west side and provide an additional ramp along the building's south edge that both lead up to the elevated deck on the west edge where the building entrance is located. The south ramp on the plans was indicated by two arrows, which may have been misidentified as two parallel parking spaces.

#### 360 Degree Design

- *Since the Project is located close to the Regional Connector future transit stop consider using LAMC relief to remove some of the parking to reduce the parking podium levels. Consider placing the parking underground, adding mechanical/automated parking and make sure that if there is a podium, it is designed in a way that it can be adapted to a different use in the future.*

The Project reduced its parking by 28 parking spaces by replacing vehicular parking with additional bicycle parking at the 1:4 ratio. Additional project features also provide a convenient and secured long-term bicycle parking room with a work station, bike lockers and showers/changing rooms at the ground floor level. The podium parking levels have also been



designed to be flat and a minimum of 17 feet floor-to-floor so that it could easily be repurposed into an occupiable use in the future. The Project already proposes three levels of subterranean parking. Any additional subterranean levels beyond that would require increased amounts of grading and the applicant states this would not be financially feasible.

- *Reconsider the scale, treatment, and massing of the parking podium to better fit the overall context of the neighborhood.*

The Project was revised to shield all above-grade parking with industrial steel-frame opaque windows and incorporates murals to cover areas of blank concrete where windows are not allowed due to building code regulations. The Project's massing and aesthetics are built around a concept of a traditional Arts District contextual warehouse-like podium and a modern office building top. Other options that studied demolishing the existing contextual bow truss building on the site were rejected in order to maintain authenticity and create a mix of uses that are more appropriate for the Arts District.

- *Consider a different treatment for the long, blank wall of the parking podium. The super graphics indicated is not a desired solution.*

The Project has updated the treatment on several walls of the Office Building, including the south-facing podium walls. The south-facing and portion of the west-facing walls are at the property line and cannot have openings of any kind, pursuant to the LAMC. Project design updates to these solid walls include more visible articulation of the surfaces that will follow the window patterns of the north- and east-facing podium facades. The large-scale murals that are a tradition of the Arts District will also continue on these walls. The intent is to include murals on the project walls, not supergraphics.

- *Currently, the Project is employing two different kinds of architectural language (one for the parking and one for the building above) that don't talk to each other. Can this interaction between the parking podium base and the office building above achieved in a smoother way? Similarly, the two focal point elements to at the South Hewitt corner, even if they do add visual interest to the building, don't interact with each other or the overall design of the building. Is the brace structure in the top just decorative? Architectural elements of the building should be brought down into the parking structure.*

The Applicant team has worked on several iterations with DCP staff to address this comment. The building's design is an expression of the raw concrete structure from the base (which is a nod to the traditional warehouse buildings in the Arts District), that is carried through in a more modern form as the office space begins on level six. Material choices that tie the building together and cross between the two, such as glass, steel, and concrete. The idiosyncratic elements of the design are reflective of the industrial history of the Arts District buildings.

### Climate-Adapted Design

- *Consider additional opportunities for landscaping in the paseo area.*

As discussed above, the Project enhanced the plaza area, including additional landscaping in response to PVP comments.

- *Consider adding brise soleil elements.*

The southern and south-western façades are partially composed of insulated spandrel panels (or shadow boxes) incorporated into the skin (providing 25-50 percent shading). Additional areas include deep balconies that provide self-shading more effective than a brise soleil.

- *Consider the building's solar orientation in the façade treatment especially for the southern and western facades. The continuous curtain wall doesn't help the energy performance of the building.*

See above. The Project has provided solar relief on the southern and western facades.

- *Consider additional native landscaping and trees.*

The Project proposes to incorporate all native and drought-resistant landscaping throughout the site.

- *Provide solar on the roof.*

Roof plan has been updated to be able to accommodate solar equipment on 30 percent of the usable roof area.

- *Remove the power lines on Hewitt and place them underground.*

The Applicant would be undergrounding the power lines around the Project Site.

## **Entitlement Analysis**

In order to develop the Project, the Applicant has requested the following land use entitlements:

- A **General Plan Amendment** to amend the land use designation from Heavy Industrial to Regional Commercial;
- A **Vesting Zone and Height District Change** from M3-1-RIO to C2-2-RIO to allow for commercial uses in a previously industrial zone and to allow for an FAR of up to 6:1 in lieu of 1.5:1;
- A **Main Conditional Use Permit** to allow the sale and dispensing a full line of alcoholic beverages for on-site consumption for up to six (6) establishments; and
- A **Site Plan Review** approval for a project resulting in an increase of more than 50,000 gross square feet of non-residential floor area.

### *General Plan Amendment, Vesting Zone and Height District Change*

The Project Site is currently designated as Heavy Industrial and zoned as M3-1-RIO in the Central City North Community Plan. The Community Plan designates the M3 Zone as a corresponding zone for the Heavy Industrial land use designation. The FAR for the Project's existing Height District and Zone is further limited to a maximum 1.5:1 FAR. The Project is requesting a General Plan Amendment and Vesting Zone Change to change the land use designation to Regional Commercial and the zone to C2-2-RIO for the entirety of the Project Site. The proposed General Plan Amendment and Vesting Zone Change would permit commercial uses at a greater scale than what the current zoning and land use would allow up to a maximum FAR of 6:1, and would allow for the construction of an 18-story Office Building comprised of 8,149 square feet of ground floor restaurant space, 308,527 square feet of office use, 16,249

square feet of covered exterior employee common areas and a 3,500 square-foot ground floor courtyard accessible from Colyton Street. The Project would total 340,770 square feet of floor area, comprised of an existing 7,800 square-foot building (the bow-truss building) and an 332,970 square-foot Office Building, on a 1.3-acre lot (a Floor Area Ratio of 6:1) and a building height of 292 feet to the top of the parapet. As the existing land use designation and zone would not permit an FAR greater than 1.5:1, the amendment is necessary to permit the redevelopment of the Site.

The Project would be an infill development located in the Arts District neighborhood which is rapidly transforming from an area with low- and mid-rise industrial and manufacturing uses to an area with a more intense mix of residential, office, retail, and restaurant uses. Additionally, the existing bow-truss building that is being retained as part of the Project is no longer being used for industrial uses and was most recently used as a museum space. The Vesting Zone Change would result in a Project that retains and introduces new employment-generating uses on the Site that complement existing uses in the vicinity, while also providing restaurant uses and open space on the ground floor. The Project promotes a more walkable lifestyle by locating commercial uses within proximity of transit and existing residential uses, job centers, and services.

#### *Main Conditional Use – Alcoholic Beverages*

The Project proposes the off- and on-site sale of a full line of alcohol beverages within six establishments on the ground floor of the restaurant uses in the Office Building and within the existing bow-truss building. The sale of alcoholic beverages would be in line with the expected services and products provided from restaurants. The Project's commercial uses would primarily front Colyton Street and Hewitt Street and along an inner-facing publicly accessible courtyard, adjacent to other commercial uses. The proposed on-site alcohol sales would support the growing number of residents and visitors to Arts District neighborhood and would complement the other existing mixed-use developments in the areas that serve alcohol. The proposed sales and services would continue to add to the diversification of commercial activities being conducted in the area and would not adversely affect the surrounding neighborhood.

#### *Site Plan Review*

The building's design would be consistent with the design policies set forth in the Citywide Design Guidelines. The building elevations utilize a variety of architectural features, building materials and changes in depth to break up massing and create a consistent architectural theme for the development. The publicly accessible pedestrian passageway would provide connectivity through the Site from Hewitt Street to Colyton Street. In addition, the ground floor restaurant uses would be designed to be connected to the street through large bi-fold door that open onto 4<sup>th</sup> Street and Hewitt Street. The publicly-accessible ground floor courtyard would be accessible from the pedestrian passageway and would provide a public meeting space as well as provide direct access to the ground floor restaurant uses and the Office Building lobby. Overall, the Project's utilitarian and contemporary architecture complements and enhances the surrounding neighborhood.

#### **Environmental Analysis**

The City of Los Angeles released the Final Environmental Impact Report (EIR), ENV-2017-470-EIR (SCH No. 2017091054), in July 2023, detailing the relevant environmental impacts resulting from the Project. The EIR includes the Draft EIR for the 4<sup>th</sup> and Hewitt Project published in May 2022 and the Final EIR dated July 2023.

The EIR identified the following significant and unavoidable impacts: Off-road Construction Noise, Construction Composite Noise, Construction Vibration – Structural Damage from Off-road

Construction Equipment, Construction Vibration – Human Annoyance from On-road Haul Route Trucks, and Cumulative Construction Noise (Excluding Structural Damage from Off-road Construction Vibration).

The EIR was certified by the Deputy Advisory Agency on September 1, 2023, in conjunction with the approval of Case No. VTT-74745. The decision was subsequently appealed and is pending decision by the City Planning Commission immediately prior to the subject case.

### **Public Hearing and Communications**

A joint public hearing was held virtually via Zoom by the Deputy Advisory Agency and Hearing Officer on behalf of the City Planning Commission on August 16, 2023 for the Project entitlements and subdivision and was attended by approximately 70 individuals. At the public hearing, testimony was provided by the Project team; 25 people spoke, of which 21 were in favor of the Project, including the Southwest Mountain States Regional Council of Carpenters (SWMSRCC), three commenters were in opposition including Supporters Alliance for Environmental Responsibility (SAFER), and one commenter who spoke on behalf of Coalition of Responsible Equitable Economic Development (CREED LA) noted they had submitted a comment on the Draft EIR and were reviewing the Final EIR to ensure their comments were addressed.

In general, support for the project centered on the Project's benefits in terms of commitment to local hire, overall design, and creation of new jobs.

Opposition to the Project centered on concerns regarding, the scale, height, and compatibility of the Project and environmental issues relating to potential adverse effects to public health, air quality, GHG, noise, and biological resources. Concerns in regard to scale and compatibility of the Project, environmental issues relating to adverse effects to public health, air quality, greenhouse gases, and biological resources are discussed in the "Issues" section immediately following below and have been addressed in the appeal report for Case Number VTT-74745-1A.

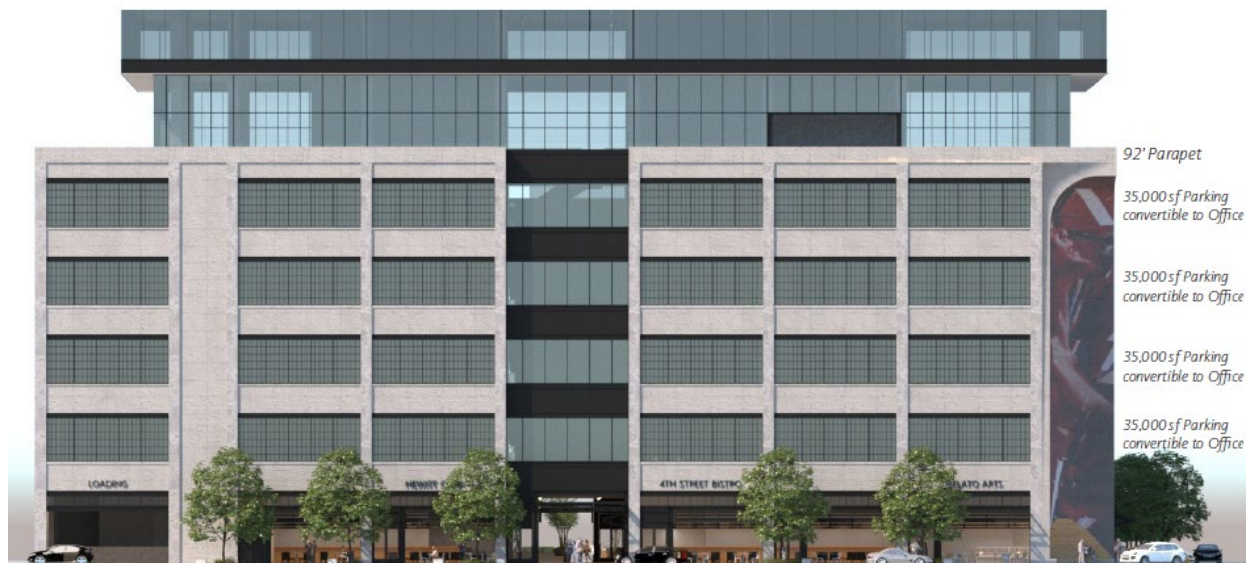
### **ISSUES**

#### **Parking Podium Design**

The design of the parking podium has evolved from the time of filing in 2017 to its current iteration which complies with the intent of the City Planning Commission's *Updated Advisory Notice Relative to Above-Grade Parking (2022)* (Parking Memo) to address concerns and the potential impact that above-grade parking facilities can have on the quality of the public realm and the pedestrian environment through a set of design strategies for projects that include above-grade parking. If all the parking cannot feasibly be reduced or placed underground, then design strategies to minimize the impact to the public realm through intentional site planning and design and enhancement of the sustainability of the parking facility should be followed.

Consistent with the Parking Memo's intention to minimize impacts on the public realm, the Project would design the podium to be fully-integrated into the design as a whole to fully conceal the parking within the podium, and would incorporate murals into the design. The design of the parking podium's design intention is to not only shield the public from parked vehicles but to blend seamlessly with the surrounding neighborhoods' traditional warehouse massing. The parking podium would feature non-operable steel frame windows inset into unfinished concrete walls at the facades that face public streets and also a portion of the west façade that faces the existing bow-truss building. The windows would be frosted to eliminate glare from headlights onto surrounding streets. The podium facades that face property lines, particularly the southern façade and the southern portion of the western façade, would need to have a rated wall with no

openings due to building code requirements. However, the walls would be articulated in a manner that continues the window patterns on the street-facing facades. Additionally, the surfaces would continue the tradition of large murals in the Arts District by using these surfaces as a large canvas for murals. At the top of the podium on the Level 6, where the office floors begin, there would be an outdoor terrace with landscaping.



*Parking Podium Eastern Façade (Hewitt Street)*

Consistent with the Parking Memo's intention to enhance the sustainability of the parking facility, the Project would build the parking podium in a manner that could easily convert the parking levels for future office uses. The floors of the parking levels would be level and the slabs would be designed to office/live load standards. The height of each level would be 17 feet tall from floor to floor. The large windows incorporated into the podium would be appropriate for office use and the office elevators are proposed to stop on all parking floors. Each of the four parking levels could be converted to 35,000 square-feet of office space. Also, consistent with the Parking Memo's sustainability strategies, the Project would provide 30 percent of its required parking spaces to be EV-ready (198 spaces), and 10 percent of its required parking spaces would be provided chargers for EV (66 spaces).

### **Air Quality, Greenhouse Gas Emissions, Biological Resources, and Public Health**

A comment letter was submitted on August 15, 2023 (one day prior to the joint public hearing), by Lozeau Drury, LLP on behalf of Supporters Alliance for Environmental Responsibility (SAFER). The letter provided comments on a variety of environmental topics, including air quality, biological resources, greenhouse gas (GHG) emissions, and public health and included a technical letter from Matt Hagemann, P.G., C.Hg. and Paul E. Rosenfeld, Ph.D. of Soil/Water/Air Projection Enterprise (SWAPE). The City reviewed the comment letter (dated August 15, 2023), and provided written responses to the comment, available as part of the City's administrative case file. The City determined that the comments do not result in any new significant environmental impacts or a substantial increase in any of the severity of significant impacts identified in the Draft EIR.

### **Office Building Height**

Several comments received state that the 18-story Office Building's maximum height of 292 feet to the top of the parapet (a maximum height of 297 feet to the top of the elevator overrun) would be out of character with the surrounding neighborhood. As stated above, the Project Site is

located in an urbanized area and generally surrounded by industrial and commercial buildings as well as a few residential buildings. While the area immediately around the Site is generally surrounded by low- and medium-rise industrial and commercial buildings, the Project Site is located in the Arts District neighborhood which continues to undergo changes to the development landscape, including the introduction of taller buildings with a variety of new uses such as residential, live/work, restaurant, retail, and office uses being introduced within an area previously concentrated with commercial and industrial uses.

Several proposed and approved projects located within one mile of the Project Site, including the height/number of stories of each project, are provided in the table and figure below. As shown, the recently approved and proposed projects range in height from 110 feet to a maximum of 378 feet. Thus, the Project's maximum height of 297 feet would be within the range of heights for the recently approved or proposed project's heights.

<b><i>Height and Number of Stories of Proposed/Approved Nearby Projects</i></b>			
<b>Project Location</b>	<b>Number of Stories / Height</b>	<b>Distance from 4<sup>th</sup>/Hewitt Site</b>	<b>Approved/Proposed</b>
2143 Violet Street	31 stories/343 feet	1 mile	Approved
2045 Violet Street	13 stories/273 feet	1 mile	Proposed
1101 East 5 <sup>th</sup> Street	12 stories/170 feet	0.2 miles	Approved
1100 East 5 <sup>th</sup> Street	8 stories/110 feet	0.2 miles	Proposed
4 <sup>th</sup> Street and Central Avenue <sup>1</sup>	2-44 stories/max height of 500 feet	0.3 miles	Proposed
670 Mesquit Street <sup>2</sup>	26-32 stories/max height of 378 feet	0.7 miles	Proposed
527 Colyton Street	12 stories/148 feet	0.3 miles	Proposed
<sup>1</sup> This project would include 10 buildings. <sup>2</sup> This project would include five buildings.			



*Height and FAR for Approved and Proposed Projects in the Vicinity of the Project Site*

## Conclusion

The 4th and Hewitt Project results in a development that would provide 308,527 square feet of office space, 8,149 square feet of restaurant space, and a 3,500 square foot publicly-accessible courtyard and retention of an existing bow-truss building. The Project would increase the employment opportunities in the surrounding area, improving the ratio of residential to office uses. The proposed office and restaurant uses would be in proximity to quality transit and residential buildings, thereby, reducing the amount of single-occupancy vehicles on the road. The proposed ground floor restaurant uses, ground floor courtyard, and sidewalk improvements would help enliven the streets and enhance the pedestrian experience.

The approval of the General Plan Amendment, Vesting Zone Change and Height District Change, Main Conditional Use Permit for alcohol sales, and Site Plan Review would all help facilitate a beneficial mix of uses and allow for a cohesive site design, creating an active and safe pedestrian environment, and offering a variety of, amenities, including public and private open space features. The Project's mix of uses, design, location, and other features would be complementary with the surrounding neighborhood, and would not adversely affect public health, welfare, and safety. Furthermore, overriding considerations of economic, social, aesthetic, and environmental benefits for the Project justify its adoption and certification of the EIR. Based on the information submitted, the testimony received at the public hearing, the analysis in the EIR, and the whole of the record, staff recommends that the City Planning Commission adopt and approve the Project as conditioned herein.

## **CONDITIONS FOR EFFECTUATING (T) TENTATIVE CLASSIFICATION REMOVAL**

Pursuant to Section 12.32 G of the Municipal Code, the (T) Tentative Classification shall be removed by the recordation of a final parcel or tract map or by posting of guarantees through the B-permit process of the City Engineer to secure the following without expense to the City of Los Angeles, with copies of any approval or guarantees provided to the Department of City Planning for attachment to the subject planning case file.

### Dedications/Improvements and Responsibilities/Guarantees.

Dedications and Improvements herein contained in these conditions which are in excess of street improvements contained in either the Mobility Element 2035 or any future Community Plan amendment or revision may be reduced to meet those plans with the concurrence of the Department of Transportation and the Bureau of Engineering:

1. As part of early consultation, plan review, and/or project permit review, the applicant/developer shall contact the responsible agencies to ensure that any necessary dedications and improvements are specifically acknowledged by the applicant/developer.
2. **Bureau of Engineering.** Street Dedications and Improvements shall be provided to the satisfaction of the City Engineer.
3. **Sewer.** Construction of necessary sewer facilities, or payment of sewer fees, shall be to the satisfaction of the City Engineer.
4. **Drainage.** Construction of necessary drainage and storm water runoff drainage facilities to the satisfaction of the City Engineer.
5. **Driveway/Parkway Area Plan.** Preparation of a parking plan and driveway plan to the satisfaction of the appropriate District Offices of the Bureau of Engineering and the Department of Transportation.
6. **Fire.** Incorporate into the building plans the recommendations of the Fire Department relative to fire safety, which includes the submittal of a plot plan for approval by the Fire Department either prior to the recordation of a final map or the approval of a building permit.
7. **Cable.** Make any necessary arrangements with the appropriate cable television franchise holder to assure that cable television facilities will be installed in City rights-of-way in the same manner as is required of other facilities, pursuant to Municipal Code Section 17.05 N to the satisfaction of the Department of Telecommunications.
8. **Recreation and Park Fees.** The Park Fee paid to the Department of Recreation and Parks be calculated as a Subdivision (Quimby in-lieu) fee. The application for Vesting Tentative Tract Map No. 74745 was deemed complete on March 3, 2017.
9. **Lighting.** Street lighting facilities shall be provided to the satisfaction of the Bureau of Street Lighting.
10. **Street Trees.** All trees in the public right-of-way shall be provided per the current Urban Forestry Division Standards.



Notice: Certificates of Occupancy for the subject property will not be issued by the City until the construction of all the public improvements (streets, sewers, storm drains, etc.), as required herein, are completed to the satisfaction of the City Engineer.

### (Q) QUALIFIED CONDITIONS

Pursuant to Section 12.32 G of the Municipal Code, the following limitations are hereby imposed upon the use of the subject property, subject to the “Q” Qualified classification.

1. **Site Development.** The use and development of the property shall be in substantial conformance with the plans submitted with the application and marked Exhibit A, dated October 12, 2023 (hereafter referred to as Exhibit A) and attached to the subject case file. No change to the plans (except as conditioned) will be made without prior review by the Department of City Planning, Major Projects Section and written approval by the Director of Planning. Each change shall be identified and justified in writing. Minor deviations may be allowed in order to comply with the provisions of the LAMC or the Project conditions. The project shall be in substantial conformance with the following project description:
  - a. Retention of the 7,800 square-foot bow-truss building
  - b. 8,149 square feet of ground floor restaurant space;
  - c. 308,527 square feet of office space; and
  - d. 16,249 square feet of covered exterior employee common areas.
2. **Pedestrian Passageway and Courtyard.** A minimum 3,500 square-foot ground floor, publicly accessible landscaped courtyard and passageway shall provide public access from Colyton Street to Hewitt Street, as shown in Exhibit A, Architectural and Landscape Plans, dated October 12, 2023. The courtyard and passageway shall remain open and accessible to the public during business hours, seven days a week. No motorized vehicles shall be permitted, except for emergency vehicles used during an emergency. The courtyard and passageway area will be maintained in good condition for the life of the Project.

## CONDITIONS OF APPROVAL

Pursuant to LAMC Sections 12.24 W.1 and 16.05, the following conditions are hereby imposed upon the use of the subject property.

### **Main Conditional Use Permit for Alcohol Conditions**

1. The use and development of the property shall be in substantial conformance with the plot plan and floor plan submitted with the application and marked Exhibit A dated October 12, 2023, except as may be revised as a result of this action.
2. Authorized herein is the sale and dispensing of a full line of alcoholic beverages for on-site and consumption, in conjunction with a maximum of six commercial establishments and associated outdoor dining areas.
3. Main Plan Approval (MPA) Requirement. Each individual venue shall be subject to a Main Plan Approval (MPA) determination pursuant to Section 12.24 M of the Los Angeles Municipal Code in order to implement and utilize the Main Conditional Use authorization granted. The purpose of the Main Plan Approval determination is to review each proposed venue in greater detail and to tailor site-specific conditions of approval for each of the premises including but not limited to hours of operation, seating capacity, size, security, live entertainment, the length of a term grant and/or any requirement for a subsequent MPA application to evaluate compliance and effectiveness of the conditions of approval. The Zoning Administrator may impose more restrictive or less restrictive conditions on each individual tenant at the time of review of each Plan Approval application.
4. MPA Public Hearing Requirement. A public hearing for any Main Plan Approval (MPA) request may be waived at the discretion of the Chief Zoning Administrator.
5. Notwithstanding approved Exhibit A, dated October 12, 2023, and the Conditions above, this grant recognizes that there may be changes resulting from identified tenants, which may result in smaller or larger restaurants, different locations, and/or a reduced number of restaurants than those originally proposed and identified in Exhibit A. Such outcome is permitted provided that the other conditions noted herein, specifically those related to the combined maximum interior floor areas, maximum interior and exterior seating numbers, maximum (total) number of establishments authorized under this grant, and the maximum number of establishments approved for each type of grant in the Conditions above are not exceeded. The sale and dispensing of beer and wine may be provided in lieu of a full line of alcoholic beverages at any of the establishments approved for a full line of alcoholic beverages (but not the reverse), provided that the maximum (total) number of establishments authorized for alcoholic beverages is not exceeded, and subject to all other conditions of this grant.
6. After hour use shall be prohibited, except routine clean-up. This includes but is not limited to private or promotional events, special events, excluding any activities which are issued film permits by the City.
7. There shall be no Adult Entertainment of any type pursuant to LAMC Section 12.70.
8. The authorized use shall be conducted at all times with due regard for the character of the surrounding district, and the right is reserved to the Department of City Planning to impose additional corrective conditions, if, it is determined by the Department of City

Planning that such conditions are needed for the protection of person in the neighborhood or occupants of adjacent property.

9. A camera surveillance system shall be installed and operating at all times to monitor the interior, entrance, exits and exterior areas, in front of and around the premises. Recordings shall be maintained for a minimum period of 30 days.
10. **STAR/LEAD/RBS Training.** Within the first six months of operation, all employees involved with the sale of alcohol shall enroll in the Los Angeles Police Department "Standardized Training for Alcohol Retailers" (STAR) or Department of Alcoholic Beverage Control "Licensee Education on Alcohol and Drugs" (LEAD) training program or the Responsible Beverage Service (RBS) Training Program. Upon completion of such training, the applicant shall request the Police Department or Department of Alcohol Beverage Control to issue a letter identifying which employees completed the training. STAR or LEAD or RBS training shall be conducted for all new hires within three months of their employment.
11. The Applicant shall be responsible for monitoring both patron and employee conduct on the premises and within the parking areas under his/her/their control to assure such conduct does not adversely affect unreasonably, or detract unreasonably from the quality of life for adjoining residents, property owners, or business.
12. Loitering is prohibited on the premises or the area under the control of the applicant. "No Loitering or Public Drinking" signs shall be posted in and outside of the subject facility.
13. The Applicant shall be responsible for maintaining the premises and adjoining sidewalk free of debris or litter.
14. An electronic age verification device shall be purchased and retained on the premises to determine the age of any individual and shall be installed on at each point-of-sales location. This device shall be maintained in operational condition and all employees shall be instructed in its use.
15. The owner or the operator shall comply with California Labor Code 6404.5 which prohibits the smoking of tobacco or any non-tobacco substance, including from electronic smoking devices or hookah pipes, within any enclosed place of employment.
16. All deliveries shall be made in the loading area along Hewitt Street. No loading or unloading of deliveries shall be permitted along 4<sup>th</sup> or Colyton Street.
17. Trash pick-up, compacting, loading and unloading and receiving activities shall be limited to 7:00 a.m. to 6:00 p.m. Monday through Friday and 10:00 a.m. to 4:00 p.m. on Saturday. No deliveries or trash pick-up shall occur on Sunday.
18. The outside disposal of glass bottles and shall only occur between the hours of 7:00 a.m. to 6:00 p.m.
19. **MViP – Monitoring Verification and Inspection Program.** Prior to the effectuation of this grant, fees required per L.A.M.C Section 19.01 E.3 - Monitoring of Conditional Use Permits, Inspection, and Field Compliance for Review of Operations and Section 19.04 - Miscellaneous ZA Sign Offs shall be paid to the City.
  - a. Within 24 months from the beginning of operations or issuance of a Certificate of Occupancy, a City inspector will conduct a site visit to assess compliance with, or

- violations of, any of the conditions of this grant. Observations and results of said inspection will be documented and included in the administrative file.
- b. The owner and operator shall be notified of the deficiency or violation and required to correct or eliminate the deficiency or violation. Multiple or continued documented violations or Orders to Comply issued by the Department of Building and Safety which are not addressed within the time prescribed, may result in additional corrective conditions imposed by the Zoning Administrator.
20. Should there be a change in the ownership and/or the operator of the business, the property owner and/or the business owner or operator shall provide the prospective new property owner and the business owner or operator with a copy of the conditions of this action prior to the legal acquisition of the property and/or the business. Evidence that a copy of this determination including the conditions required herewith has been provided to the prospective property owner and/or business owner/operator shall be submitted to the Department of City Planning in a letter from the new property owner and/or business owner or operator indicating the date that the new property owner and/or business owner or operator/management began and attesting to the receipt of this approval and its conditions. The new property owner and/or business owner or operator shall submit this letter to the Department of City Planning within 30 days of the beginning day of his/her/their new operation of the establishment along with any proposed modifications to the existing floor plan, seating arrangement or number of seats of the new operation.
  21. At any time during the period of validity of this grant, should documented evidence be submitted showing continued violation of any condition of this grant and/or the ABC license of the location, resulting in an unreasonable level of disruption or interference with the peaceful enjoyment of the adjoining and neighboring properties, the Zoning Administrator (upon his/her/their initiative, or upon written request by LAPD or Department of ABC) reserves the right to call for a public hearing requiring the applicant to file for a plan approval application together with associated fees pursuant to LAMC Section 19.01 E, the purpose of which will be to review the applicant's compliance with and the effectiveness of these conditions. The applicant shall prepare a radius map and cause notification to be mailed to all owners and occupants of properties within a 500-foot radius of the property, the Council Office and the Los Angeles Police Department's corresponding division. The applicant shall also submit a summary and any supporting documentation of how compliance with each condition of this grant has been attained. Upon this review, the Zoning Administrator may modify, add or delete conditions, and reserves the right to conduct this public hearing for nuisance abatement/revocation purposes.

### **Site Plan Review Conditions**

22. **Site Development.** The use and development of the Property shall be in substantial conformance with the plans stamped Exhibit A, dated October 12, 2023. No change to the plans will be made without prior review by the Department of City Planning, Major Projects, and written approval by the Director of Planning. Each change shall be identified and justified in writing. Minor deviations may be allowed in order to comply with the provisions of the Municipal Code or the Project conditions.
23. **Landscaping.** Prior to the issuance of a building permit, a landscape and irrigation plan shall be submitted to the Department of City Planning for approval. The landscape plan shall be in substantial conformance with the landscape plan stamped Exhibit A, dated October 12, 2023. Minor deviations from the requirements provided below may be permitted by the Department of City Planning to permit the existing landscaping conditions provided that the plantings are well established and in good condition.

**24. Vehicular Parking.**

- a. Any above-grade parking structure shall be designed to be utilized and easily repurposed to other uses.
- b. Entrances, elevators, and stairs for parking structures shall be easily accessible and highlighted architecturally.
- c. The height of the parking level shall have sufficient clearance to be adaptable to non-parking uses. Once converted, the building shall permit a minimum floor to ceiling height of nine feet for commercial uses and eight feet for residential uses.
- d. All above-grade levels of the Project's parking podium shall be consistent with the façade treatments in the plans stamped as "Exhibit A", dated October 12, 2023.

**25. Tree Wells.** The minimum depth of tree wells and planters on the rooftop, any above grade open space, and above a subterranean structure shall be as follows:

- a. Minimum depth for trees shall be 42 inches.
- b. Minimum depth for shrubs shall be 30 inches.
- c. Minimum depth for herbaceous plantings and ground cover shall be 18 inches.
- d. Minimum depth for an extensive green roof shall be three inches.

The minimum amount of soil volume for tree wells shall be based on the size of the tree at maturity as follows:

- a. 220 cubic feet for a tree 15 - 19 feet tall at maturity.
- b. 400 cubic feet for a tree 20 - 24 feet tall at maturity.
- c. 620 cubic feet for a medium tree or 25 - 29 feet tall at maturity.
- d. 900 cubic feet for a large tree or 30 - 34 feet tall at maturity.

**26. Tree Maintenance.** New trees planted within the public right-of-way shall be spaced not more than an average of 30 feet on center, unless otherwise permitted by the Urban Forestry Division, Bureau of Public Works.**27. Cool Roof.** The Project shall implement a "cool roof" which would be comprised of light colored, reflective roofing materials over the mechanical equipment area and the roof of the vertical circulation.**28. Utilities.** All utilities shall be fully screened from view of any abutting properties and the public right-of-way.**29. River Improvement Overlay.** The Project shall comply with the River Improvement Overlay requirements set forth in LAMC Section 13.17. RIO approval shall be obtained prior to the issuance of Building Permits per the instructions as shown in ZI-2358.**30. Glare.** The exterior of the proposed structure shall be constructed of materials such as, but not limited to, high-performance and/or non-reflective tinted glass (no mirror-like tints or films) and pre-cast concrete or fabricated wall surfaces to minimize glare and reflected heat.**31. Reflectivity.** Glass used in building façades shall be non-reflective or treated with a non-reflective coating in order to minimize glare from reflected sunlight.**32. Signage.** There shall be no off-site commercial signage on construction fencing during construction.

33. **Lighting.** Outdoor lighting shall be designed and installed with shielding, such that the light source cannot be seen from adjacent residential properties, the public right-of-way, nor from above.
  - a. Areas where nighttime uses are located shall be maintained to provide sufficient illumination of the immediate environment so as to render objects or persons clearly visible for the safety of the public and emergency response personnel.
  - b. All pedestrian walkways, storefront entrances, and vehicular accessways shall be illuminated with lighting fixtures.
  - c. Light fixtures located on the Project Site (and not in the public right-of-way) shall be harmonious with the building design. Wall mounted lighting fixtures to accent and complement architectural details at night shall be installed on the building to provide illumination to pedestrians and motorists.
34. **Construction Generators.** The Project contractor shall use power construction equipment with state-of-the-art noise shielding and muffling devices. The Project construction contractor shall use on-site electrical sources and solar generators to power equipment rather than diesel generators, where feasible.
35. **Mechanical Equipment.** All mechanical equipment shall be fully screened from view of any abutting properties and the public right-of-way.
36. **Trash/Storage.** All trash collecting and storage areas shall be located on-site and not visible from the public right-of-way. Trash receptacles shall be enclosed and/or covered at all times. Trash/recycling containers shall be locked when not in use.
37. **Graffiti Removal.** All graffiti on the site shall be removed or painted over to match the color of the surface to which it is applied within 24 hours of its occurrence.
38. **Construction Traffic Management Plan.** The Applicant shall prepare a Construction Traffic Management Plan which will include a construction work site traffic control plan, DOT recommends that the construction work site traffic control plan be submitted to LADOT's Citywide Temporary Traffic Control Section or Permit Plan Review Section for review and approval prior to the start of any construction work.
39. **Downtown/Arts District Transportation Management Organization (TMO).** The Applicant shall contribute to the formation and marketing of and participation in the Downtown/Arts District TMO. That Applicant shall provide its seed funding for the TMO in the first year to cover the cost of launching the TMO and continue to commit to nine additional years (10 years total) as a charter member with annual dues.
40. **Art Murals.** The proposed art murals shall be in compliance with the conceptual plans stamped as "Exhibit A" dated October 12, 2023 and with all applicable City regulations, pursuant to Section 22.119 of the Los Angeles Administrative Code and including approval from the Department of Cultural Affairs.
41. **Development Review Fees.** The Applicant shall comply with any applicable fees pursuant to LAMC Section 19.15 including, but not limited to, traffic study review, condition clearance, and permit issuance.
42. **Street Trees and Pedestrian Lighting.** Street trees and pedestrian lighting in the public right-of-way shall be selected and designed to be consistent with existing trees and lighting

in the area as approved the Department Public Works – Urban Forestry Division and the Department of Cultural Affairs.

### **Environmental Conditions**

43. **Implementation.** The Mitigation Monitoring Program (MMP), attached as “Exhibit D” and part of the case file, shall be enforced throughout all phases of the Project. The Applicant shall be responsible for implementing each Project Design Features (PDF) and Mitigation Measure (MM) and shall be obligated to provide certification, as identified below, to the appropriate monitoring and enforcement agencies that each PDF and MM has been implemented. The Applicant shall maintain records demonstrating compliance with each PDF and MM. Such records shall be made available to the City upon request.
44. **Construction Monitor.** During the construction phase and prior to the issuance of building permits, the Applicant shall retain an independent Construction Monitor (either via the City or through a third-party consultant), approved by the Department of City Planning, who shall be responsible for monitoring implementation of PDFs and MMs during construction activities consistent with the monitoring phase and frequency set forth in this MMP.

The Construction Monitor shall also prepare documentation of the Applicant’s compliance with the PDFs and MMs during construction every 90 days in a form satisfactory to the Department of City Planning. The documentation must be signed by the Applicant and Construction Monitor and be included as part of the Applicant’s Compliance Report. The Construction Monitor shall be obligated to immediately report to the Enforcement Agency any non-compliance with the MMs and PDFs within two businesses days if the Applicant does not correct the non-compliance within a reasonable time of notification to the Applicant by the monitor or if the non-compliance is repeated. Such non-compliance shall be appropriately addressed by the Enforcement Agency.

45. **Substantial Conformance and Modification.** After review and approval of the final MMP by the Lead Agency, minor changes and modifications to the MMP are permitted, but can only be made subject to City approval. The Lead Agency, in conjunction with any appropriate agencies or departments, will determine the adequacy of any proposed change or modification. This flexibility is necessary in light of the nature of the MMP and the need to protect the environment. No changes will be permitted unless the MMP continues to satisfy the requirements of CEQA, as determined by the Lead Agency.

The Project shall be in substantial conformance with the PDFs and MMs contained in this MMP. The enforcing departments or agencies may determine substantial conformance with PDFs and MMs in the MMP in their reasonable discretion. If the department or agency cannot find substantial conformance, a PDF or MM may be modified or deleted as follows: the enforcing department or agency, or the decision maker for a subsequent discretionary project related approval finds that the modification or deletion complies with CEQA, including CEQA Guidelines Sections 15162 and 15164, which could include the preparation of an addendum or subsequent environmental clearance, if necessary, to analyze the impacts from the modifications to or deletion of the PDFs or MMs. Any addendum or subsequent CEQA clearance shall explain why the PDF or MM is no longer needed, not feasible, or the other basis for modifying or deleting the PDF or MM, and that the modification will not result in a new significant impact consistent with the requirements of CEQA. Under this process, the modification or deletion of a PDF or MM shall not, in and of itself, require a modification to any Project discretionary approval unless the Director of Planning also finds that the change to the PDF or MM results in a substantial change to the Project or the non-environmental conditions of approval.



46. **Tribal Cultural Resource Inadvertent Discovery.** In the event that objects or artifacts that may be tribal cultural resources are encountered during the course of any ground disturbance activities (excavating, digging, trenching, plowing, drilling, tunneling, quarrying, grading, leveling, removing peat, clearing, driving posts, augering, backfilling, blasting, stripping topsoil or a similar activity), all such activities shall temporarily cease on the project site until the potential tribal cultural resources are properly assessed and addressed pursuant to the process set forth below:
- a. Upon a discovery of a potential tribal cultural resource, the Applicant shall immediately stop all ground disturbance activities and contact the following: (1) all California Native American tribes that have informed the City they are traditionally and culturally affiliated with the geographic area of the proposed project; (2) and the Department of City Planning.
  - b. If the City determines, pursuant to Public Resources Code Section 21074 (a)(2), that the object or artifact appears to be tribal cultural resource, the City shall provide any effected tribe a reasonable period of time, not less than 14 days, to conduct a site visit and make recommendations to the Applicant and the City regarding the monitoring of future ground disturbance activities, as well as the treatment and disposition of any discovered tribal cultural resources.
  - c. The Applicant shall implement the tribe's recommendations if a qualified archaeologist and a culturally affiliated tribal monitor, both retained by the City and paid for by the Applicant, reasonably conclude that the tribe's recommendations are reasonable and feasible.
  - d. The Applicant shall submit a tribal cultural resource monitoring plan to the City that includes all recommendations from the City and any affected tribes that have been reviewed and determined by the qualified archaeologist and by a culturally affiliated tribal monitor to be reasonable and feasible. The Applicant shall not be allowed to recommence ground disturbance activities until this plan is approved by the City.
  - e. If the Applicant does not accept a particular recommendation determined to be reasonable and feasible by the qualified archaeologist or by a culturally affiliated tribal monitor, the Applicant may request mediation by a mediator agreed to by the Applicant and the City who has the requisite professional qualifications and experience to mediate such a dispute. The Applicant shall pay any costs associated with the mediation.
  - f. The Applicant may recommence ground disturbance activities outside of a specified radius of the discovery site, so long as this radius has been reviewed by the qualified archaeologist and by a culturally affiliated tribal monitor and determined to be reasonable and appropriate.
  - g. Copies of any subsequent prehistoric archaeological study, tribal cultural resources study or report, detailing the nature of any significant tribal cultural resources, remedial actions taken, and disposition of any significant tribal cultural resources shall be submitted to the South Central Coastal Information Center (SCCIC) at California State University, Fullerton.
  - h. Notwithstanding the above, any information determined to be confidential in nature, by the City Attorney's office, shall be excluded from submission to the SCCIC or the general public under the applicable provisions of the California Public Records Act, California Public Resources Code, and shall comply with the City's AB 52 Confidentiality Protocols.
47. **Paleontological Resource Inadvertent Discovery.** In the event that any subsurface paleontological resources are encountered unexpectedly at the project site during construction or the course of any ground disturbing activities, all such activities shall halt immediately, at which time the applicant shall notify the City and consult with a qualified

paleontologist to implement the following procedures associated with the inadvertent discovery of paleontological resources:

- a. The project applicant shall retain a qualified paleontologist meeting the Society of Vertebrate Paleontology Standards (SVP) to complete a treatment and disposition plan for any discovered paleontological resource. The qualified paleontologist shall retain a paleontological monitor who shall be present during further ground disturbing activities on the project site, including peripheral activities, such as sidewalk replacement, utilities work, and landscaping, which may occur adjacent to the project site.
- b. A 50-foot buffer around any find shall be established, subject to modification by the qualified paleontologist, within which construction activities shall not be allowed to continue around the find until work is allowed to resume in accordance with the treatment and disposition plan. Ground-disturbing activities shall be halted or diverted away from the vicinity of the find so that the find can be evaluated as part of a treatment and disposition plan. Work shall be allowed to continue outside of the buffer area.
- c. All paleontological resources unearthed by project development activities shall be evaluated by the qualified paleontological. The qualified paleontologist or designated paleontological monitor shall recover intact fossils consistent with the treatment plan and notify the City of any fossil salvage and recovery efforts. Typically, fossils can be safely salvaged quickly by a single paleontologist and not disrupt future construction activity. In some cases, larger fossils (such as complete skeletons or large mammal fossils) require more extensive excavation and longer salvage periods. In this case the paleontologist shall have the authority to temporarily direct, divert or halt construction activity to ensure that the fossil(s) can be removed in a safe and timely manner. Any fossils shall be handled and deposited consistent with the treatment and disposition plan prepared by the paleontological monitor.
- d. The frequency of required paleontological monitoring shall be based on the rate of excavation and grading activities, the materials being excavated (younger sediments vs. older sediments), the depth of excavation, and, if found, the abundance and type of archaeological resources encountered. Full-time monitoring may be reduced to part-time inspections, or ceased entirely, if determined adequate by the qualified paleontologist. Prior to any further ground disturbing activities on the project site, Paleontological Resource Sensitivity Training shall be given for applicable construction personnel. The training session shall be carried out by the qualified archaeologist and shall focus on how to identify paleontological resources that may be encountered during earthmoving activities and the procedures to be followed in such an event.
- e. All artifacts, other cultural remains, records, photographs, and other documentation shall be curated by an appropriate curation facility. All fieldwork, analysis, report production, and curation shall be fully funded by the applicant.
- f. The treatment and disposition plan shall be submitted to the City prior to any further ground disturbing activities continue within the buffer area. Recommendations contained therein shall be implemented throughout any further ground disturbance activities.

### **Administrative Conditions of Approval**

48. **Approval, Verification and Submittals.** Copies of any approvals guarantees or verification of consultations, review or approval, plans, etc., as may be required by the subject conditions, shall be provided to the Planning Department for placement in the subject file.
49. **Code Compliance.** Area, height and use regulations of the zone classification of the subject property shall be complied with, except where herein conditions are more restrictive.

50. **Covenant.** Prior to the issuance of any permits relative to this matter, an agreement concerning all the information contained in these conditions shall be recorded in the County Recorder's Office. The agreement shall run with the land and shall be binding on any subsequent property owners, heirs or assign. The agreement must be submitted to the Planning Department for approval before being recorded. After recordation, a copy bearing the Recorder's number and date shall be provided to the Planning Department for attachment to the file.
51. **Definition.** Any agencies, public officials or legislation referenced in these conditions shall mean those agencies, public officials, legislation or their successors, designees or amendment to any legislation.
52. **Enforcement.** Compliance with these conditions and the intent of these conditions shall be to the satisfaction of the Planning Department and any designated agency, or the agency's successor and in accordance with any stated laws or regulations, or any amendments thereto.
53. **Building Plans.** Page 1 of the grants and all the conditions of approval shall be printed on the building plans submitted to the Department of City Planning and the Department of Building and Safety.
54. **Project Plan Modifications.** Any corrections and/or modifications to the project plans made subsequent to this grant that are deemed necessary by the Department of Building and Safety, Housing Department, or other Agency for Code compliance, and which involve a change in Site Plan, floor area, parking, building height, yards or setbacks, building separations, or lot coverage, shall require a referral of the revised plans back to the Department of City Planning for additional review and final sign-off prior to the issuance of any building permit in connection with said plans. This process may require additional review and/or action by the appropriate decision-making authority including the Director of Planning, City Planning Commission, Area Planning Commission, or Board.
55. **Indemnification and Reimbursement of Litigation Costs.** The Applicant shall do all of the following:
  - (i) Defend, indemnify and hold harmless the City from any and all actions against the City relating to or arising out of, in whole or in part, the City's processing and approval of this entitlement, including but not limited to, an action to attack, challenge, set aside, void, or otherwise modify or annul the approval of the entitlement, the environmental review of the entitlement, or the approval of subsequent permit decisions, or to claim personal property damage, including from inverse condemnation or any other constitutional claim.
  - (ii) Reimburse the City for any and all costs incurred in defense of an action related to or arising out of, in whole or in part, the City's processing and approval of the entitlement, including but not limited to payment of all court costs and attorney's fees, costs of any judgments or awards against the City (including an award of attorney's fees), damages, and/or settlement costs.
  - (iii) Submit an initial deposit for the City's litigation costs to the City within 10 days' notice of the City tendering defense to the Applicant and requesting a deposit. The initial deposit shall be in an amount set by the City Attorney's Office, in its sole discretion, based on the nature and scope of action, but in no event shall the initial deposit be less than \$50,000. The City's failure to notice or collect the deposit does not relieve the Applicant from responsibility to reimburse the City pursuant to the requirement in paragraph (ii).

- (iv) Submit supplemental deposits upon notice by the City. Supplemental deposits may be required in an increased amount from the initial deposit if found necessary by the City to protect the City's interests. The City's failure to notice or collect the deposit does not relieve the Applicant from responsibility to reimburse the City pursuant to the requirement in paragraph (ii).
  - (v) If the City determines it necessary to protect the City's interest, execute an indemnity and reimbursement agreement with the City under terms consistent with the requirements of this condition.
56. The City shall notify the applicant within a reasonable period of time of its receipt of any action and the City shall cooperate in the defense. If the City fails to notify the applicant of any claim, action, or proceeding in a reasonable time, or if the City fails to reasonably cooperate in the defense, the applicant shall not thereafter be responsible to defend, indemnify or hold harmless the City.
57. The City shall have the sole right to choose its counsel, including the City Attorney's office or outside counsel. At its sole discretion, the City may participate at its own expense in the defense of any action, but such participation shall not relieve the applicant of any obligation imposed by this condition. In the event the Applicant fails to comply with this condition, in whole or in part, the City may withdraw its defense of the action, void its approval of the entitlement, or take any other action. The City retains the right to make all decisions with respect to its representations in any legal proceeding, including its inherent right to abandon or settle litigation.

For purposes of this condition, the following definitions apply:

"City" shall be defined to include the City, its agents, officers, boards, commissions, committees, employees, and volunteers.

"Action" shall be defined to include suits, proceedings (including those held under alternative dispute resolution procedures), claims, or lawsuits. Actions includes actions, as defined herein, alleging failure to comply with any federal, state or local law.

Nothing in the definitions included in this paragraph are intended to limit the rights of the City or the obligations of the Applicant otherwise created by this condition.

## FINDINGS

### **General Plan/Charter Findings**

The Director-initiated General Plan Amendment and requested Zone and Height District Change are in substantial conformance with the purposes, intent, and provisions of the General Plan as explained below. The General Plan Amendment Will Further the Purposes, Intent and Provisions of the General Plan (Charter Section 556).

#### **1. General Plan Land Use Designation**

The Project Site is located within the Central City North Community Plan, which was adopted by the City Council on December 15, 2000. The subject property is comprised of six contiguous parcels, totaling 1.31-acres in size. The Community Plan currently designates the Project Site for Heavy Industrial land uses, corresponding to the M3 Zone. The Site is presently zoned M3-1-RIO, consistent with the range of zones under the land use designation.

As proposed, the General Plan Amendment would re-designate the Project Site from Heavy Industrial to Regional Commercial land uses. In addition, a Vesting Zone Change and Height District Change would modify the existing zoning from M3-1-RIO to (T)(Q)C2-2-RIO. The proposed Regional Commercial land use designation has the following corresponding zones: CR, C1.5, C2, C4, RAS3, RAS4, R3, R4, and R5. Thus, the recommended (T)(Q)C2-2-RIO Zone would be consistent with the adoption of the proposed land use designation and in substantial conformance with the purpose, intent, and provisions of the General Plan as it is reflected within the Central City North Community Plan.

#### **2. General Plan Text**

The Los Angeles General Plan sets forth goals, objectives and programs that guide both citywide and community specific land use policies. The General Plan is comprised of a range of State-mandated elements, including, but not limited to, Land Use, Transportation, Noise, Safety, Housing and Conservation. The City's Land Use Element is divided into 35 community plans that establish parameters for land use decisions within those sub-areas of the City. The Project is in compliance with the following Elements of the General Plan: Framework Element, Noise Element, Mobility Element, Housing Element, Health and Wellness and Air Quality Elements and the Land Use Element – Central City North Community Plan.

#### **Framework Element**

The Framework Element for the General Plan (Framework Element) was adopted by the City of Los Angeles in December 1996 and re-adopted in August 2001. The Framework Element provides guidance regarding policy issues for the entire City of Los Angeles, including the Project Site. The Framework Element also sets forth a Citywide comprehensive long-range growth strategy and defines Citywide policies regarding such issues as land use, housing, urban form, neighborhood design, open space, economic development, transportation, infrastructure, and public services. The Project is consistent with the objectives and policies of the Framework Element as described below:

### Chapter 3: Land Use

The Land Use Chapter of the Framework Element identifies objectives and supporting policies relevant to the Project Site. Those objectives and policies seek, in part, to encourage the development of commercial uses and structures that integrate a mix of commercial uses. The Project supports and will be generally consistent with the General Plan Framework Land Use Chapter as it accommodates development of office and restaurant uses in accordance with the applicable policies of the Central City North Community Plan. Specifically, the Project would be consistent with the following goals, objective and policies, as set forth in the General Plan Framework Land Use Chapter:

**Goal 3A:** *A physically balanced distribution of land uses that contributes towards and facilitates the City's long-term fiscal and economic viability, revitalization of economically depressed areas, conservation of existing residential neighborhoods, equitable distribution of public resources, conservation of natural resources, provision of adequate infrastructure and public services, reduction of traffic congestion and improvement of air quality, enhancement of recreation and open space opportunities, assurance of environmental justice and a healthful living environment, and achievement of the vision for a more livable city.*

**Objective 3.1:** *Accommodate a diversity of uses that support the needs of the City's existing and future residents, businesses, and visitors.*

**Objective 3.2:** *Provide for the spatial distribution of development that promotes an improved quality of life by facilitating a reduction of vehicular trips, vehicle miles traveled, and air pollution.*

**Policy 3.2.3:** *Provide for the development of land use patterns that emphasize pedestrian/bicycle access and use in appropriate locations.*

**Objective 3.4:** *Encourage new multi-family residential, retail commercial, and office development in the City's neighborhood districts, community, regional, and downtown centers as well as along primary transit corridors/boulevards, while at the same time conserving existing neighborhoods and related districts.*

The Project would support the above goal, objectives and policy of the Framework Element through the provision of a complementary mix of commercial uses on site, including the construction of a new 18-story Office Building with ground floor restaurant uses and the retention of the bow-truss building for planned creative office uses. A 3,500 square-foot ground floor outdoor courtyard accessible from Colyton Street would be included as part of the new on-site improvements and would connect to a publicly accessible ground floor pedestrian passageway that would provide connectivity between Colyton Street and Hewitt Street. Further, the Office Building would provide employees with 18,294 square feet of covered exterior employee commons areas.

The Project Site is located in close proximity to public transit, including multiple local and regional bus lines, within walking distance to several bus stops along 1<sup>st</sup> Street, 3<sup>rd</sup> Street, 4<sup>th</sup> Street, 4<sup>th</sup> Place, 6<sup>th</sup> Street, 7<sup>th</sup> Street, Traction Avenue, Olympic Boulevard, Central Avenue, Alameda Street, Boyle Avenue, and Soto Street. Public transit service in the immediate Project vicinity is currently provided by Metro, LADOT DASH and Montebello Bus Lines. The bus lines include Metro Local Lines 18, 30, 53, 60, 62, 66, 106, 251, Metro Shuttle 605, the LADOT DASH A bus, and Montebello bus lines M40 and M90. Additionally, the Project Site is located 0.5 miles southeast of the Metro A and E (Formerly Gold) Line Little Tokyo/Arts District Station. The availability and proximity of various bus

lines and the Metro A and E Line Little Tokyo/Arts District Station would provide employees/visitors opportunities to use public transit to access the Site, in place of a vehicle.

The Project would provide 40 short-term and 72 long-term on-site bicycle parking spaces for employees and visitor use. While there are no existing bicycle lanes, paths, or routes adjacent to the Project Site, Class III and II bicycle lanes are located in the Project vicinity. Dedicated Class II bicycle lanes are located along 3<sup>rd</sup> Street between 4<sup>th</sup> Place and Santa Fe Avenue and become a Class III bicycle lane east of Garey Street. Dedicated Class III bicycle lanes are located along 2<sup>nd</sup> Street between Alameda Avenue and Santa Fe Avenue and Class II bicycle lanes are installed along 1<sup>st</sup> Street from Beaudry Avenue to Mission Street.

The retention of the bow-truss building and introduction of new office and restaurant uses, as well as the provision of a publicly accessible outdoor ground floor courtyard and bicycle facilities within close proximity to public transit would provide residences and/or visitors of the Arts District and/or Downtown Los Angeles a new and nearby location to work, eat, and gather, creating a more livable city.

***Policy 3.14.6:*** Consider the potential re-designation of marginal industrial land for alternative uses by amending the community plans based on the following criteria:

- a. *Where it can be demonstrated that the existing parcelization precludes effective use for industrial or supporting functions and where there is no available method to assemble parcels into a unified Site that will support viable industrial development;*
- b. *Where the size and/or the configuration of assembled parcels are insufficient to accommodate viable industrial development;*
- c. *Where the size, use, and/or configuration of the industrial parcels adversely impact adjacent residential neighborhoods;*
- d. *Where the conversion of industrial lands to an alternative use will not create a fragmented pattern of development and reduce the integrity and viability of existing industrial areas;*
- e. *Where it can be demonstrated that the reduction of industrial lands will not adversely impact the City's ability to accommodate sufficient industrial uses to provide jobs for the City's residents or incur adverse fiscal impacts; and/or*

The Project includes a request to amend the General Plan land use designation for the Project Site from Heavy Industrial to Regional Commercial. The Regional Commercial land use designation would allow for the development of an 18-story Office Building comprised of 8,149 square feet of ground floor restaurant space, 308,527 square feet of office, 16,249 square feet of covered exterior employee common areas and a 3,500 square-foot ground floor courtyard accessible from Colyton Street and Hewitt Street. The Project would total 340,770 square feet of gross floor area, comprised of an existing 7,800 square-foot building (the bow-truss building) and an 332,970 square-foot Office Building, on a 1.3-acre lot (for an FAR of 6:1) and a building height of 292 feet to the top of the parapet. An existing office building, two storage/garage buildings, and surface parking lots would be demolished to allow for the development of the Project.

As indicated in the Economic Development Chapter of the Framework Element, some existing industrially zoned lands may be inappropriate for new industries and should be converted for other land uses. Where such lands are to be converted, their appropriate

use shall be the subject of future planning studies. This is satisfied in several ways. First, the CEQA process included an Environmental Impact Report, which assessed the surrounding land uses, as well as an analysis of the proposed land use in relation to the General Plan and the CEQA Guidelines threshold for land use compatibility. As demonstrated therein, while the majority of properties in the surrounding area are designated and zoned heavy industrial and manufacturing, the implementation of the Adaptive Reuse Ordinance has allowed for residential uses within the live/work components, with neighborhood commercial uses to complement the residential population. The entitlement process further analyzes in detail the Goals, Objectives, and Policies of the General Plan and demonstrates that the Project meets the overall intent of the General Plan. As demonstrated herein, the land use pattern in the immediate neighborhood of the subject property has evolved from its historic industrial and manufacturing uses to light industrial and manufacturing uses and new technologies along with compatible and complementary non-industrial uses including residential, creative office, and related retail / cultural / entertainment uses, reflecting larger employment and economic trends

Second, around 2007, the Department of City Planning, in conjunction with the Community Redevelopment Agency drafted an Industrial Land Use Policy (ILUP) that was intended to preserve certain industrially-zoned land in the City for industrial use. The ILUP addressed multiple areas of the City, among them the Greater Downtown, which encompasses the Project Site, located within Analysis Area 3 of the ILUP. The ILUP Map for Analysis Area 3 shows that the Project Site land use at the time was predominantly light industrial and auto uses. Currently, the Site is comprised of a two office buildings, two storage/garage buildings and surface parking lots. The Project is consistent with and would complement the land use patterns and uses in the immediate area, which is developed with a variety of commercial, residential, and live/work uses on properties zoned for industrial uses. The 1.31-acre Site is also not well-suited for modern large-scale industrial operations as it is developed with two one-story office buildings, two small storage/garage buildings, and scattered surface parking lots. Additionally, the Site is no longer being fully utilized for industrial related purposes. The Project would include community benefits, such as a 3,500 square-foot ground floor courtyard accessible from Colyton Street and Hewitt Street and EV parking, recommended in the ILUP Memo for approving the conversion of industrially zoned land in designated Industrial/Commercial Mixed Use (IMU) Districts. The IMU District is applied to areas appropriate for a mix of industrial and commercial activities and provides for a full range of goods and services to the community located along portions of industrial/commercial thoroughfares, in conformance with the general plan. This District allows for a mix of industrial and commercial or just industrial or commercial (standalone) land uses. In addition, the Project would incorporate ILUP guidelines for providing community benefits through jobs-producing space. Therefore, the Project would not conflict with the policies of the ILUP.

Third, the City began an update in 2014 of the Central City and Central City North Community Plans which includes studying land use and zoning in those plans, as well as the supporting policies and objectives. The Project Site is proposed to be designated "Hybrid Industrial (HI)" under the Downtown Community Plan. According to the Draft Plan, Hybrid Industrial areas preserve existing structures that characterize the existing unique form and development patterns, promote productive, creative, manufacturing, fabrication, and light industrial uses, encourage the development of live/work units, and support walkable neighborhoods with active and livable pedestrian realm.

The Project includes a request to amend the General Plan land use for the subject property from Heavy Industrial to Regional Commercial. Although the Project Site is currently zoned industrial, as described above, the existing uses of the Site are not prime industrial uses



that generate a significant number of quality industrial jobs. Specifically, while the ILUP survey categorized the Project Site as currently containing “light industry” uses, existing uses are a combination of industrial and office. The Project would include approximately 340,770 square feet of commercial uses (general commercial, restaurant, office), which would generate 1,282 jobs, a net increase of 1,270 jobs. As such, the Project would be consistent with the purpose of the ILUP to implement Goals 7A and 7B of the Framework Element for industrial growth that provides job opportunities for the City’s residents and maintain the City’s fiscal viability, including the City’s intent to: protect industrial zoned land; retain and expand existing businesses (by bringing new customers to existing businesses); attract new uses that provide job opportunities for the City’s residents; and maintain a healthy jobs/household ratio that supports the General Fund and its capacity to pay for essential services and programs for the City’s existing and future population.

Approval of the land use amendment and development of the Project would not result in a fragmented pattern of development. Specifically, the surrounding area is surrounded by low- and medium-rise industrial and commercial buildings. To the north of the Project Site, across 4th Street, is a one-story automotive repair shop and warehouse. Across 4th Place is a seven-story parking structure and a three-story office building. To the east of the Project Site across Hewitt Street are one-story commercial and manufacturing uses as well as a surface parking lot. To the south of the Project Site are one- and two-story commercial and manufacturing uses, and to the west of the Project Site across Colyton Street are one-story manufacturing uses. Additionally, there are several residential uses located less than 0.5 miles from the Project Site, including a two-story structure with a rooftop-mounted single trailer used for residential purposes located approximately 80 feet southwest of the Project Site, a six-story multi-unit residential building located on the northeast corner of Seaton Street and 4<sup>th</sup> Street, approximately 200 feet northwest of the Site, and a live-work building located more than 200 feet south of the Project Site. Given this mix of surrounding uses, the conversion of industrial land to commercial land uses, will be compatible with and complementary to adjacent land uses.

According to the Central City North Community Plan, there are 914 acres planned for industrial use (approximately 45.5 percent of the 2,010 acre total) in the Community Plan area. The Project Site comprises 1.31 acres or approximately 0.14 percent of the industrially-zoned land in the Community Plan. The Project would result in a net increase of 1,270 new employees and would generate ongoing revenue to the City in the form of sales and property taxes. Therefore, the Project would not adversely impact the City’s ability to accommodate sufficient industrial uses to provide jobs for the City’s residents or cause the City to incur adverse fiscal impacts. Furthermore, for this specific site, based on the immediately surrounding uses and the small lot size of the Project Site, the Project Site is not suited to a large-scale industrial operation. Therefore, the Project would not conflict with policy listed above.

In sum, the Project is consistent with the directions relative to industrial land set forth in the Framework Element in that it will provide for a mix of job-producing uses that are allowed for in industrial zones, as is recognized in the adopted Central City North Community Plan and the Downtown Community Plan Update. The proposed uses are compatible with and complement the existing mix of industrial, live/work, arts production, and retail uses within the immediately surrounding area.

Chapter 5: Urban Form and Neighborhood Design

**Goal 5A:** *A livable City for existing and future residents and one that is attractive to future investment. A City of interconnected, diverse neighborhoods that builds on the strengths of those neighborhoods and functions at both the neighborhood and citywide scales.*

**Objective 5.5:** *Enhance the livability of all neighborhoods by upgrading the quality of development and improving the quality of the public realm.*

**Objective 5.9:** *Encourage proper design and effective use of the built environment to help increase personal safety at all times of the day.*

The Project proposes a number of pedestrian improvements that would enhance the walkability and livable pedestrian realm for visitors and employees of the Site. The Project would add sidewalks, landscaping, lighting, street trees, and include a 3,500 square-foot ground floor courtyard accessible from Colyton Street which would connect to a publicly accessible ground floor pedestrian passageway that would provide connectivity between Colyton Street and Hewitt Street. The new Office Building would include 8,149 square feet of ground floor restaurant uses and 16,249 square feet of covered exterior employee common areas. Additionally, short-term bicycle parking would be dispersed throughout the ground floor of the Site and the long-term bicycle parking would be located on the ground floor inside the Office Building.

The Project would incorporate design elements at the ground level to reflect the Arts District industrial past while transitioning to a modern office building. The ground floor would be constructed with a rough concrete finish and minimal utilitarian detail. The ground floor restaurants would have large bifold doors mirroring the industrial warehouse feel of the Arts District. The four levels of above-grade parking would be fully enclosed on three sides and screened on the fourth side. with non-operable, industrial steel frame windows, set in board form concrete walls, hidden from view behind board form concrete, and/or accented with murals, similar to other murals found throughout the Arts District. The exterior of the Office Building would be encased in a glass curtain wall with concrete accents to help tie in the industrial base design. Sliding glass panels would lead to outdoor balconies on each office level providing an indoor/outdoor feel. The Level 6 balcony would wrap around the entire Office Building while the remaining office level balconies would primarily face the street levels. The metal screen railings surrounding the balconies would tie into the design language of the metal screens and frame windows of the parking levels. Overall, the Project's industrial and contemporary architecture complements and enhances the surrounding developments, originally comprised of low-scale manufacturing and industrial uses, but which now includes nearby proposed projects, such as the proposed 670 Mesquit Project with a 32-story tower, 4<sup>th</sup> and Central Project with a 44-story tower and the Palmetto Project with a 21-story building.

In addition, the Project would introduce new commercial uses to the Project Site that would provide for continuous activity during commercial business hours through the development of restaurant uses. The Project has been designed such that the ground floor outdoor courtyard is visible from Colyton Street and accessible by Project employees and visitors. Appropriate lighting and other security measures would be incorporated into the design of the building.

As such, the Project would be consistent with the Land Use Chapter of the Framework Element as it would provide a complimentary mix of commercial uses on site, is sited near public transit, increases the economic vitality of the area through a net increase in jobs,

provides public infrastructure improvements, and would incorporate design elements to reflect the Arts District industrial history.

*Chapter 7: Economic Development*

The Project's consistency with the relevant goals, objectives, and policies in the of the Framework Element (Chapter 7), is provided below:

**Goal 7A:** *A vibrant economically revitalized City.*

**Goal 7B:** *A City with land appropriately and sufficiently designed to sustain a robust commercial and industrial base.*

**Objective 7.2:** *Establish a balance of land uses that provides for commercial and industrial development which meets the needs of local residents, sustains economic growth, and assures maximum feasible environmental quality.*

**Policy 7.2.2:** *Concentrate commercial development entitlements in areas best able to support them, including community and regional centers, transit stations and mixed-use corridors. This concentration prevents commercial development from encroaching on existing residential neighborhoods.*

Consistent with Goal 7A, the proposed Project would include ground floor restaurant use, office use, and publicly accessible ground floor outdoor courtyard that would revitalize the Project Site which is currently developed with office uses and a vacant building. Further, the public improvements, including the wider sidewalk, new street trees, and street lighting along 4<sup>th</sup> Street, Colyton Street, and Hewitt Streets (or alternatively the Living Street Conditions) would economically revitalize the Project Site and would contribute to the City's vibrant atmosphere.

The Project would support this Goal 7B, Objective 7.2, and Policy 7.2.2 by providing a new 18-story Office Building comprised of 8,149 square feet of ground floor restaurant space, 308,527 square feet of office, 16,249 square feet of covered exterior employee common areas and a 3,500 square-foot ground floor courtyard accessible from Colyton Street and Hewitt Street, that would serve the community and future businesses. The ground floor restaurant uses, and office use would complement the employment base of the Central City North Community Plan area, meet the needs of local residents, and foster continued economic investment. In addition, the Project Site would have convenient access to public transit and opportunities for walking and biking, thereby facilitating a reduction in vehicle trips, vehicle miles traveled, and air pollution to ensure maximum feasible environmental quality. Furthermore, the Project would integrate sustainable and green building techniques by incorporating various standards and guidelines to reduce resources and energy consumption.

The Project would provide for a balanced mix of job-producing uses that are permitted in commercial zones that provides for productive commercial development including office and restaurant uses that would result in an increase of 1,270 net new on-site jobs and would generate ongoing revenue to the City in the form of sales and property taxes, and meet the needs of local residents, sustains economic growth, and assures maximum feasible environmental quality.

The proposed uses are compatible with and complement the existing mix of commercial and manufacturing uses within the immediate surrounding area. The development is located within an infill site in proximity to employment, entertainment, and various amenities within the Arts District neighborhood. Access to public transit is provided throughout Downtown, including multiple local lines and regional lines via stops within walking distance along 1<sup>st</sup> Street, 3<sup>rd</sup> Street, 4<sup>th</sup> Street, 4<sup>th</sup> Place, 6<sup>th</sup> Street, 7<sup>th</sup> Street, Traction Avenue, Olympic Boulevard, Central Avenue, Alameda Street, Boyle Avenue, and Soto Street. Public transit service in the immediate Project vicinity is currently provided by Metro, LADOT DASH, and Montebello Bus Lines. The bus lines include Metro Local Lines 18, 30, 53, 60, 62, 66, 106, 251, Metro Shuttle 605, the LADOT DASH A bus, and Montebello bus lines M40 and M90. Additionally, the Project Site is located 0.5 miles southeast of the Metro A and E (Formerly Gold) Line Little Tokyo/Arts District Station.

The Project is retaining job-producing uses as well as including new commercial uses that would sustain economic growth and a robust commercial base in an area where similar uses currently exist, and is developing an underutilized Site, all of which satisfies numerous objectives and policies regarding the provision of new housing and productive commercial uses within proximity to existing activity centers and transit.

**Goal 7C:** *A City with thriving and expanding businesses.*

**Objective 7.3:** *Maintain and enhance the existing businesses in the City.*

**Policy 7.3.2:** *Retain existing neighborhood commercial activities within walking distance of residential areas.*

The Project would be consistent with the above goal, objective and policy as it would retain the existing 7,800 square-foot bow-truss building and construct a new 18-story Office Building comprised of 8,149 square feet of ground floor restaurant space, 308,527 square feet of office, 16,249 square feet of covered exterior employee common areas and a 3,500 square-foot ground floor courtyard accessible from Colyton Street and Hewitt Street, thus supporting a City with thriving and expanding businesses. These commercial activities would be sited in proximity to existing residential and employment centers in the adjacent Downtown Center, as well as to residential uses within walking distance of the Site, such as the six-story multi-unit residential building located at 825 East 4<sup>th</sup> Street, the Amp Lofts (695 South Santa Fe Avenue), the Brick Lofts (562 Mateo street), the Toy Factory Lofts (1855 Industrial Street) and the Biscuit Company Lofts (1850 Industrial Street). The proposed mix of commercial uses would support the goal of a City with a renewed and enhanced base of expanding businesses in a manner that is complementary to the existing mix of uses which would be retained onsite and in the immediately surrounding area.

### **Housing Element**

The City's Housing Element for 2021-2029 (Housing Element) was adopted by the Los Angeles City Council on November 24, 2021. While the Project does not propose any residential uses, it would be in conformance with Senate Bill (SB) 166, which was adopted on September 29, 2017 and amended Government Code Section 65863, and also known as the No Net Loss Law, to require sufficient adequate sites to be available at all times through the Housing Element Planning period to meet a jurisdiction's remaining unmet Regional Housing Needs Assessment (RHNA) goals for each income category. Pursuant to SB 166, as jurisdictions make decisions regarding zoning and land use, or development occurs, jurisdictions must assess their ability to accommodate new housing in each income category on the remaining sites in their housing element site inventories. A

jurisdiction must add additional sites to its inventory if land use decisions or development results in a shortfall of sufficient sites to accommodate its remaining housing need for each income category. In particular, a jurisdiction may be required to identify additional sites if a jurisdiction rezones a site or approves a project at a different income level or lower density than showing in the site's inventory.

The Housing Element identified the Project Site as a "candidate site" to which 9.1 Very Low Income Units, 9.1 Low Income Units, and 229 Above-Moderate Income Units have been allocated. SB 166 requires that the reduction on an individual project site in allocated density is consistent with the City's General Plan and that the remaining sites identified in the Housing Element are adequate to meet the City's RHNA's allocation, which are demonstrated below.

The Housing Element anticipated that the Project Site could accommodate up to 9.1 Very Low Income Units, 9.1 Low Income Units, and 229 Above-Moderate Income Units, the City has capacity to accommodate the remaining RHNA Allocation for the 2021-2029 Planning period. As of April 1, 2023, the City's remaining RHNA Allocation for the 2021-2029 Planning period is as follows: 112,281 Very Low Income Units, 67,086 Low Income Units, 74,964 Moderate Income Units, and 168,892 Above-Moderate Income Units. As of April 1, 2023, the City has a remaining capacity of 330,056 Very Low Income Units, 332,096 Low Income Units, 63,107 Moderate Income Units, and 907,466 Above-Moderate Income Units. The excess Above-Moderate Income Unit capacity may accommodate both Moderate and Above-Moderate Unit RHNA Allocations.

**The reduction is consistent with the General Plan including the Housing Element.**

Consistency with the Housing Element

**Goal 1:** *A City where housing production results in an ample supply of housing to create more equitable and affordable options that meet existing and projected needs.*

**Objective 1.1:** *Forecast and plan for existing and projected housing needs over time with the intention of furthering Citywide Housing Priorities.*

**Policy 1.1.2:** *Account for existing housing needs when planning for future development by conducting analysis to develop and incorporate a buffer above household projections.*

As described above, as of April 1, 2023, the City's remaining RHNA Allocation for the 2021-2029 Planning period is as follows: 112,281 Very Low Income Units, 67,086 Low Income Units, 74,964 Moderate Income Units, and 168,892 Above-Moderate Income Units. As of April 1, 2023, the City has a remaining capacity of 330,056 Very Low Income Units, 332,096 Low Income Units, 63,107 Moderate Income Units, and 907,466 Above-Moderate Income Units. The excess Above-Moderate Income Unit capacity may accommodate both Moderate and Above-Moderate Unit RHNA Allocations. Therefore, the City's remaining RHNA Allocation for the 2021-2029 Planning period for Very Low Income Units represents 34 percent of the City's total remaining capacity of Very low Income Units, 20.2 percent of the total remaining capacity for Low Income Units, 7.7 percent of the total remaining capacity for Moderate Income Units, and 18.6 percent of the total remaining capacity for Above-Moderate Income Units; less than half of the City's total capacity for each affordable housing type. While the Housing Element identified the Project Site as being able to accommodate 9.1 Very Low Income Units, 9.1 Low Income Units, and 229 Above-Moderate Income Units and the Project would reduce the number of City parcels

that allow for residential uses, the City's share of the RHNA Allocation for the 2021-2029 Planning period can be located on other parcels throughout the City. Therefore, the City finds that there are adequate remaining sites in the Housing Element to accommodate the remaining RHNA Allocation for the 2021-2029 Planning period. Thus, consistent with Goal 1, Objective 1.1, and Policy 1.1.2, the City has forecasted for existing and projected housing needs and developed a buffer above household projections that would meet existing and projected needs.

***Objective 1.2: Facilitate the production of housing, especially projects that include Affordable Housing and/or meet Citywide Housing Priorities.***

***Policy 1.2.2: Facilitate the construction of a range of different housing types that addresses the particular needs of the city's diverse households.***

Consistent with Objective 1.2 and Policy 1.2.2, as stated above, the City could meet the remaining RHNA Allocation for the 2021-2029 Planning period for Very Low Income, Low Income, Moderate Income, and Above-Moderate Income Units through development of the identified remaining sites located throughout the City. While the Housing Element determined that 9.1 Very Low Income Units, 9.1 Low Income Units, and 229 Above-Moderate Income Units could be located on the Project Site, the City maintains ample capacity to meet the remaining RHNA Allocation for Very Low Income, Low Income, Moderate, and Above-Moderate Units. Thus, the 9.1 Very Low Income Units, 9.1 Low Income Units, and 229 Above-Moderate Income Units that were originally allocated to the Project Site could be accommodated on other parcels located throughout the City. Therefore, the reduction would not constrict the Citywide production of Affordable Housing and/or the construction of a range of different housing types, and the Project is consistent with Objective 1.2 and Policy 1.2.2.

Lastly, as discussed in the applicable portions of Finding No. 2 , the Project would be consistent with the purposes, intent and provisions of the General Plan and its elements, including the Framework Element, Noise Element, Mobility Element, Health and Wellness Element and Air Quality Element, and the Land Use Element – Central City North Community Plan that relate to commercial and economic vitality. Therefore, the reduction is consistent with the General Plan including the Housing Element.

**The remaining sites identified in the Housing Element are adequate to meet the requirements of Section 65583.2 and to accommodate the jurisdiction's share of the regional housing need pursuant to Section 65584. The finding shall include a quantification of the remaining unmet need for the jurisdiction's share of the regional housing need at each income level and the remaining capacity of sites identified in the housing element to accommodate that need by income level.**

The Project is located on a parcel identified in the Inventory of Sites prepared for the Housing Element and was anticipated to accommodate 9.1 Very Low Income Units, 9.1 Low Income Units, and 229 Above-Moderate Income Units. As the Project does not propose a residential component, the Project would not develop any affordable housing units on the Project Site and would not meet the allocated number of affordable units as identified in the Housing Element.

Pursuant to Government Code Section 65863(b)(2), the City finds that while the Project would result in fewer units by income category on the Project Site than those identified in the Inventory of Sites prepared for the Housing Element, the remaining sites identified in

the Housing Element are adequate to meet the requirements of Government Code Section 65583.2 and to accommodate the City's share of the regional housing need pursuant to Government Code Section 65584. As of April 1, 2023, the City's remaining RHNA Allocation for the 2021-2029 Planning period is as follows: 112,281 Very Low Income Units, 67,086 Low Income Units, 74,964 Moderate Income Units, and 168,892 Above-Moderate Income Units. As of April 1, 2023, the City has a remaining capacity of 330,056 Very Low Income Units, 332,096 Low Income Units, 63,107 Moderate Income Units, and 907,466 Above-Moderate Income Units. The excess Above-Moderate Income Unit capacity may accommodate both Moderate and Above-Moderate Unit RHNA Allocations. Thus, the City's RHNA allocation for the 2021-2029 Planning period for Very Low, Low, Moderate, and Above-Moderate Income Units makes up 34 percent, 20.2 percent, 7.7 percent, and 18.6 percent of the City's remaining housing capacity, respectively. Therefore, the City finds that there are adequate remaining sites identified in the Housing Element and located throughout the City to accommodate the remaining RHNA Allocation for the Planning Period.

### **Noise Element**

The Noise Element, adopted in November 1999 addresses noise mitigation regulations, strategies and programs and delineates federal, state and city jurisdiction relative to rail, automotive, aircraft and nuisance noise. The Noise Element sets forth noise management goals, objectives, policies and City programs.

***Objective 2: Reduce or eliminate nonairport related intrusive noise, especially relative to noise sensitive uses.***

***Policy 2.2: Enforce and/or implement applicable city, state and federal regulations intended to mitigate proposed noise producing activities, reduce intrusive noise and alleviate noise that is deemed a public nuisance.***

As stated in the Project's Draft EIR, Section IV.I Noise on pages IV.I-36 and IV.I-38 noise generated by off-road construction equipment and composite construction noise levels (combined effect of the Project's on- and off-site construction noise sources at each sensitive receptor) would be significant without mitigation. While the Project includes a mitigation measure that would reduce construction noise impacts, there are no feasible mitigation measures to reduce the Project's significant and unavoidable impacts.

The Project would be required to comply with all applicable federal, state, and city noise regulations, including the Occupational Safety and Health Act of 1970, California Office of Planning and Research Guidelines for Noise Compatible Land Use, and the LAMC. Further the Project would be required to comply with Project Design Features NOI-PDF-1 through NOI-PDF-5, which would require the Project to equip all feasible diesel-powered construction vehicles with noise reducing devices, equip all power construction equipment, with state-of-the-art noise shielding and muffling devices to ensure no additional noise due to worn or improperly maintained parts, would be generated, and to use rubber-tired equipment rather than metal-tracked equipment. Further, the on-site construction manager will be responsible for responding to local complaints regarding construction noise and construction supervisors will be informed of Project-specific noise requirements.

***Objective 3: Reduce or eliminate noise impacts associated with proposed development of land and changes in land use.***

The Project includes a requested General Plan Amendment to change the Site's land use designation from Heavy Industrial to Regional Commercial. As stated in the Project's Draft EIR, Section IV.I Noise on pages IV.I-38 through IV.I-49, the Project will not result in operational noise impacts. Thus, operation of the Project's office and restaurant uses (uses that would be permitted under the Regional Commercial land use designation) would not result in noise impacts.

### **Mobility Plan 2035**

The Mobility Element 2035 (Mobility Element), adopted in September 2016, guides development of a citywide transportation system with the goal of ensuring the efficient movement of people and goods and recognizes that primary emphasis must be placed on maximizing the efficiency of existing and proposed transportation infrastructure through advanced transportation technology, reduction of vehicle trips, and focused growth in proximity to public transit. The Mobility Plan 2035 includes goals that define the City's high-level mobility priorities and sets forth objectives and policies to establish a citywide strategy to achieve long-term mobility and accessibility within the City of Los Angeles. The Proposed Project would be in conformance with following objectives and policies of the Mobility Element as described below.

#### **Chapter 2: World Class Infrastructure**

***Policy 2.3: Recognize walking as a component of every trip and ensure high-quality pedestrian access in all site planning and public right-of-way modifications to provide a safe and comfortable walking environment.***

#### **Chapter 3: Access for All Angelenos**

***Policy 3.1: Recognize all modes of travel, including pedestrian, bicycle, transit, and vehicular modes - including goods movement - as integral components of the City's transportation system.***

***Policy 3.3: Promote equitable land use decisions that result in fewer vehicle trips by providing greater proximity and access to jobs, destinations, and other neighborhood services.***

***Policy 3.5: Support "first-mile, last-mile solutions" such as multi-modal transportation services, organizations, and activities in the areas around transit stations and major bus stops (transit stops) to maximize multi-modal connectivity and access for transit riders.***

***Policy 3.8: Provide bicyclists with convenient, secure and well-maintained bicycle parking facilities***

The Project would provide access for all modes of travel, including vehicular, pedestrian, and bicycles. The Project Site is located in a Transit Priority Area and adjacent to several bus lines and in the vicinity of the Metro A and E (Formerly Gold) Line Little Tokyo/Arts District Station (0.5 miles northwest of the Project Site), increasing accessibility to and from the Site via walking and transit. The Project would introduce new commercial uses such as new office space and restaurants to the area, which would reduce the vehicle



miles travel to the Site as on-site employees, nearby residents and visitors would be able to walk to the restaurant uses.

Pedestrian entrances would continue to be located along Colyton Street (to access the existing bow-truss building), and 4<sup>th</sup> Street, and Hewitt Street to access the Office Building ground floor lobby and the ground floor restaurant uses. Long-term bicycle parking would be provided on the ground floor. The short-term bicycle parking would be dispersed throughout the Site, while the long-term bicycle parking, showers, and lockers would be provided in the lobby of the Office Building. The Project would provide 660 vehicle parking spaces in three levels of subterranean parking and four levels of above-grade parking (accessible from 4<sup>th</sup> Street).

As mentioned below, the Project would also improve the streetscape surrounding the Project Site with improved sidewalks, street lighting, and street trees. The Project would further activate the street with a 3,500 square-foot ground floor courtyard and provide a new pedestrian mid-block cut through access point from Colyton Street to Hewitt Street. The Office Building's orientation would contribute to a walkable environment as the entrances to the commercial uses are located directly on 4<sup>th</sup> Street, Hewitt Street and from the pedestrian passageway connecting Colyton Street and 4<sup>th</sup> Street. In general, the site design creates an active environment by supporting bicycle and pedestrian activities, and creating a building orientation that is easily accessible from adjacent public streets and open spaces. These Project features and improvements would lend themselves to create a safe and engaging pedestrian environment, and would enrich the quality of the public realm, consistent with the objectives of the Mobility Element.

The Project's related Vesting Tentative Tract Map also includes BOE Conditions of Approval that would require improvements the adjacent public right-of-way along 4<sup>th</sup> Street, Colyton Street, and Hewitt Street. Along 4<sup>th</sup> Street, BOE would require a 6-foot-wide strip of land be dedicated, except where the existing bow-truss building would remain, to complete a 36-foot-wide half public right-of-way, including a concrete curb, gutter and 13-foot sidewalk with tree wells and a 15-foot by 15-foot property line cut corner at the intersection with Hewitt Street; along Colyton Street BOE would require a 4-foot-wide strip of land be dedicated to complete a 34-foot-wide half public right-of-way, including the construction of a concrete curb and a 10-foot-wide concrete sidewalk with tree wells; and along Hewitt Street a 2-foot to 4-foot variable width strip of land be dedicated to complete a 34-foot-wide half public right-of-way including a concrete curb and a 10-foot-wide concrete sidewalk with tree wells.

In addition to the BOE Conditions of Approval, the Deputy Advisory Agency approved separate Living Street Conditions that allows the Applicant the option of implementing in place of BOE's conditions. The Living Streets typology is a developing street treatment to retain the original feel of the Arts District roadways instead of reconstructing the roadways with a traditional curb, gutter, and sidewalks. If the Applicant chooses to implement the Living Streets conditions, the improvements along 4<sup>th</sup> Street would include a 3-foot-wide sidewalk easement, except where the existing bow-truss building is to remain, to complete a 30-foot-wide half public right-of-way, including a concrete curb, gutter, and 10-foot sidewalk with tree wells adjacent to the bow-truss building, a 13-foot sidewalk with tree wells along the remainder of the site, and a 15-foot by 15-foot property line cut corner at the intersection with Hewitt Street; the improvements along Colyton would include suitable surfacing to join the existing pavement to complete a 14-foot half roadway from the centerline, a minimum 15-foot by 15-foot building line cut corner at the intersection with 4<sup>th</sup> Street, a near-flush curb, and 16-foot-wide sidewalk with tree wells; and the improvements along Hewitt Street would include a merger of the existing partial 2-foot-wide sidewalk easement, maintaining the existing 30-foot-wide half right-of-way, suitable

surfacing to join the existing pavement to complete a 14-foot half roadway from the centerline, a near-flush curb and 16-foot-wide concrete sidewalk with tree wells.

Furthermore, the Project incorporates mobility-friendly design elements such as expanded, landscaped sidewalks, a ground floor courtyard and pedestrian passageway connecting Colyton Street and Hewitt Street, and bicycle parking facilities consistent with the City's Bicycle Parking Ordinance to provide friendly, safe, and convenient access to nearby neighborhood uses and various nearby transit options. These Project and neighborhood elements would further support the purpose of the Streets Standards Committee's guidelines.

As such, the Project would be consistent with the Mobility Element through its proximity to public transit options, infrastructure improvements, pedestrian and bicycle amenities, and pedestrian-friendly design.

### **Health and Wellness Element and Air Quality Element**

Adopted in March 2015, the Plan for a Healthy Los Angeles lays the foundation to create healthier communities for all Angelenos. As the Health and Wellness Element of the General Plan, it provides high-level policy vision, along with measurable objectives and implementation programs, to elevate health as a priority for the City's future growth and development. Through a new focus on public health from the perspective of the built environment and City services, the City of Los Angeles will strive to achieve better health and social equity through its programs, policies, plans, budgeting, and community engagement. The Project is consistent with the following:

#### **Chapter 2: A City Built for Health**

***Policy 2.2:*** *Promote a healthy built environment by encouraging the design and rehabilitation of buildings and sites for healthy living and working conditions, including promoting enhanced pedestrian-oriented circulation, lighting, attractive and open stairs, healthy building materials and universal accessibility using existing tools, practices, and programs.*

#### **Chapter 5: An Environment Where Life Thrives**

***Policy 5.1:*** *Reduce air pollution from stationary and mobile sources; protect human health and welfare and promote improved respiratory health.*

***Policy 5.7:*** *Promote land use policies that reduce per capita greenhouse gas emissions, result in improved air quality and decreased air pollution, especially for children, seniors and other susceptible to respiratory diseases.*

#### **Air Quality Element**

***Policy 4.2.3*** *Ensure that new development is compatible with pedestrians, bicycles, transit, and alternative fuel vehicles.*

***Policy 5.1.2*** *Effect a reduction in energy consumption and shift to non-polluting sources of energy in its buildings and operations,*

The Project would comply with applicable provisions of the CALGreen Code and the Los Angeles Green Building Code, which would serve to reduce the Project's energy usage. Furthermore, in compliance with Code requirements, a minimum of 30 percent of the total

code-required parking spaces would be capable of supporting future electric vehicle supply equipment (EVSE), and 10 percent of the total code-required parking spaces would be equipped with EV chargers.

The addition of over 300,000 square feet of office space to a neighborhood that is rapidly adding residential uses and is 0.5 miles southeast of the Little Tokyo/Arts District Station for the A and E light rail Metro lines, would provide employment opportunities to nearby residents, thereby, reducing commute times to work and reducing reliance on single-occupancy vehicle trips. The Project would also include a 3,500 square-foot publicly-accessible courtyard which would provide a recreation space for employees, visitors, and nearby residents to enjoy passive recreational opportunities. Additionally, a publicly-accessible passageway is proposed through the Site which would provide an east-west link between Colyton Street and Hewitt Street. The proposed ground floor restaurant space would enliven the Project Site frontages and provide a place for local residents, employees, and visitors to enjoy a meal. Additionally, the Project would provide short- and long-term bicycle parking throughout the ground floor of the Site. Bicycle amenities include lockers and showers adjacent to the long-term bicycle parking within the lobby of the Office Building.

In addition to adhering to smart growth principles of locating infill development adjacent to existing employment centers and public transportation options, the Project would incorporate a wide range of building technologies and design features, such as high efficiency toilet and urinals, low flow showerheads and private and commercial faucets, drought tolerant and native plants, drip/subsurface, zoned irrigation with weather-based irrigation controllers, a cool roof, high-efficiency that would protect the environment by saving energy (which would also reduce air emissions associated with electricity generation), reducing water consumption, making use of recycled materials, and producing better indoor and outdoor environmental quality.

The Project's energy efficiency features and location near major transit facilities, which designates it in a Transit Priority Area, could help reduce the energy and emission footprint of the Project and the per capita GHG emissions of the employees and visitors from private automobile travel. With existing code-requirements and as conditioned, the Project would be consistent with the aforementioned policies, as well as Policy 5.1.2 of the Air Quality Element mentioned above, by ensuring that future developments are energy efficient and shift to efficient and non-polluting sources of energy. The EV parking also provides a convenient service amenity to the employees and visitors who utilize electricity on site for other functions. As such, the Project would provide service amenities to improve habitability for on-site employees of the Project and to minimize impacts on neighboring properties. Therefore, the Project would promote a healthy built environment, encourage healthy working conditions, reduce air pollution, and promote land use policies that reduce per capita greenhouse gas emissions.

#### **Land Use Element – Central City North Community Plan.**

The development of the Project would support the overarching goals of the Central City North Community Plan. The proposed development furthers the following Community Plan objectives and policies:

## Commercial

**Objective 2-1:** *To conserve and strengthen viable commercial development in the community and to provide additional opportunities for new commercial development and services.*

**Policy 2-1.1:** *New commercial uses shall be located in existing established commercial areas or existing shopping centers.*

**Policy 2-1.4:** *Require that projects be designed and developed to achieve a high level of quality, distinctive character, and compatibility with existing uses and development.*

**Objective 2-2:** *To attract uses which strengthen the economic base and expand market opportunities for existing and new businesses.*

**Policy 2-2.2:** *New development needs to add to and enhance the existing pedestrian street activity.*

**Policy 2-2.3:** *Require that the first floor street frontage of structures, including mixed use projects and parking structures located in pedestrian oriented districts, incorporate commercial uses.*

**Policy 2-4.2:** *Preserve community character, scale, and architectural diversity.*

**Policy 2-4.3:** *Improve safety and aesthetics of parking areas in commercial areas.*

The Project's development of the new 18-story Office Building comprised of 8,149 square feet of ground floor restaurant space, 308,527 square feet of office, 16,249 square feet of covered exterior employee common areas and a 3,500 square-foot ground floor courtyard, and the retention of the existing 7,800 square-foot bow-truss building, would strengthen existing nearby commercial development, provide opportunities for new commercial development and services, and strengthen the economic base by expanding market opportunities for existing and new businesses. These commercial activities would be sited in proximity to existing residential and employment centers in the nearby Downtown Center, as well as to existing nearby residential uses located throughout the Arts District neighborhood.

The ground floor street frontages along the west, north, and east elevations of the Project would mostly be comprised of the restaurant uses, as well as the existing bow-truss building and the proposed 3,500 square-foot ground floor courtyard. The restaurant uses and ground floor paseo would both draw interested from pedestrians walking along Colyton, 4<sup>th</sup>, and Hewitt Streets, especially as the restaurant uses would include large bifold doors mirroring the industrial warehouse feel of the Arts District. The Project would create an inviting pedestrian environment through the provision of a 3,500 square foot ground floor courtyard that would be activated with a variety of seating, planters, and lighting.

According to the Central City North Community Plan, there are 914 acres planned for industrial use (approximately 45.5 percent of the 2,010 acre total) in the Community Plan area. The Project Site comprises 1.31-acres or approximately 0.14 percent of the industrially-zoned land in the Community Plan. The Project would include approximately 340,770 square feet of commercial uses (general commercial, restaurant, office), which would generate 1,282 jobs, a net increase of 1,270 jobs that would generate ongoing

revenue for the City in the form of sales and property taxes. Therefore, the Project would not adversely impact the ability of the Community Plan Area to provide sufficient land for a variety of industrial uses with maximum employment opportunities. Furthermore, for this specific site, based on the immediately surrounding uses and the size and lot configuration, the Project Site is not well-suited to a large-scale industrial operation.

In order to provide the new commercial uses the Project would not retain the current industrial land use designation; however, the Project includes job producing uses that would maximize employment opportunities which are safe for the environment and the work force, and which have minimal adverse impact on adjacent uses. Surrounding uses include industrial and commercial buildings. The proposed uses are compatible with and complement the existing mix of industrial, and commercial uses within the immediately surrounding area.

In sum, the Project is consistent with the above commercial policies in that it would provide for a commercial development with job-producing uses that are allowed for in commercial zones, as is recognized in the adopted Central City North Community Plan and the Downtown Community Plan update.

For the reasons discussed above, the Project would be consistent with the overarching objectives and policies of the Central City North Community Plan.

### **River Improvement Overlay District**

The Project Site is located within the River Improvement Overlay Zone (RIO) and would be required to comply with the Los Angeles River Design Guidelines, which establishes best practices for designing projects located within the RIO District. The Los Angeles River Design Guidelines consist of overarching objectives followed by a list of specific implementation strategies. These strategies address river-adjacent development. Although the Project is located within the boundaries of the RIO District, the Project Site is approximately 0.4 miles west of the Los Angeles River. Nevertheless, the Project would further the relevant objectives of the Los Angeles River Design Guidelines, including employing high quality, attractive and distinguishable architecture.

### **3. Sewerage Facilities Element**

The Sewerage Facilities Element of the General Plan will not be affected by the recommended action. The sewer system will be able to accommodate the total flows for the Project, further detailed gauging and evaluation may be needed as part of the permit process to identify a specific sewer connection point. If the public sewer has insufficient capacity then the Applicant would be required to build sewer lines to a point in the sewer system with sufficient capacity. A final approval for sewer capacity and connection permit will be made at that time. Ultimately, this sewage flow will be conveyed to the Hyperion Treatment Plant, which has sufficient capacity for the Project.

4. **City Charter Finding 555.** The General Plan may be amended in its entirety, by subject elements or parts of subject elements, or by geographic areas, provided that the part or area involved has significant social, economic or physical identity.

**Amendment in Whole or in Part.** The requested General Plan Amendment is an Amendment in Part of the Central City North Community Plan, as it is for the subject site and not the entire Community Plan. The Project's requested amendment from Heavy Manufacturing to Regional Commercial, and concurrent Vesting Zone Change and Height

District Change from M3-1-RIO to (T)(Q)C2-2-RIO, would support the City's efforts to provide commercial and office uses in appropriate infill locations near existing activity centers.

The Arts District is a neighborhood of Downtown Los Angeles that has historically been characterized by industrial, warehousing, and distribution uses, and is an important part of the City's physical identity. The requested General Plan Amendment would allow the Project Site to provide a mix of commercial uses to strengthen the economic identity of the surrounding area, which currently includes these uses. The Project would result in an active street life, would continue to provide office uses on the Site, while also introducing restaurant uses on the Project Site, thereby contributing to the conservation of the significant economic identity of the Site while introducing new commercial uses that contribute to the significant social and physical identity of the district.

The Project Site is comprised of six contiguous parcels totaling 1.31-acres and is currently improved with an existing office building, a vacant building (the bow-truss building), two storage/garage buildings, and surface parking lots that total 54,581 square feet in building floor area. The Project Site is located in an urbanized area, on the corners of Colyton Street and Hewitt Street along 4<sup>th</sup> Street, and generally surrounded by low- and medium-rise industrial and commercial buildings as well as a few residential buildings. The proposed uses are compatible with and complement the existing mix of commercial and industrial uses within the immediate vicinity.

Although the Site, as well as several other properties in the immediate area, are designated for heavy industrial uses, the Art's District neighborhood is characterized by a mix of office, residential, industrial, retail, restaurants, and other commercial uses. The Project Site itself is no longer being utilized for industrial-related purposes, as one of the buildings is occupied by office uses and the other building is vacant. The Arts District neighborhood continues to undergo changes to the development landscape, including the introduction of taller buildings with a variety of new uses such as residential, live/work, restaurant, retail, and office uses being introduced within an area previously concentrated with commercial and industrial uses such as the proposed 670 Mesquit Project with a 32-story tower, 4<sup>th</sup> and Central Project with a 44-story tower and the Palmetto Project with a 21-story building. Nearby residential buildings include the six-story multi-unit residential building located at 825 East 4<sup>th</sup> Street, the Amp Lofts (695 South Santa Fe Avenue), the Brick Lofts (562 Mateo Street), the Toy Factory Lofts (1855 Industrial Street) and the Biscuit Company Lofts (1850 Industrial Street).

The Project Site's location would help the City achieve land use goals of increasing the number of jobs near transit and existing activity centers. The Project Site is located within a Transit Priority Area and would be located in close proximity to public transit, including multiple local and regional bus lines, several of which provide connections to the Metro A and E (Formerly Gold) Little Tokyo/Arts District Station, located 0.5 miles northwest of the Site. Public transit service in the vicinity of the Project Site is currently provided by multiple local lines and regional lines via stops within walking distance along to several bus stops along 1<sup>st</sup> Street, 3<sup>rd</sup> Street, 4<sup>th</sup> Street, 4<sup>th</sup> Place, 6<sup>th</sup> Street, 7<sup>th</sup> Street, Traction Avenue, Olympic Boulevard, Central Avenue, Alameda Street, Boyle Avenue, and Soto Street. Public transit service in the immediate Project vicinity is currently provided by Metro, LADOT DASH and Montebello Bus Lines. The bus lines include Metro Local Lines 18, 30, 53, 60, 62, 66, 106, 251, Metro Shuttle 605, the LADOT DASH A bus, and Montebello bus lines M40 and M90. Additionally, the Project Site is located 0.5 miles southeast of the Metro A and E (Formerly Gold) Line Little Tokyo/Arts District Station.

The Project represents an opportunity to develop a new 18-story Office Building comprised of 8,149 square feet of ground floor restaurant space, 308,527 square feet of office, 16,249

square feet of covered exterior employee common areas and a 3,500 square-foot ground floor courtyard, which are uses that are compatible and complementary to the immediate area. The Project would total 340,770 square feet of gross floor area, comprised of an existing 7,800 square-foot building (the bow-truss building) and an 332,970 square-foot Office Building. The Project would appropriately locates office and restaurant uses near transit, jobs, entertainment and services within Downtown, creates new commercial uses for the neighborhood, and promotes pedestrian activity in the general vicinity.

Re-designating the land use of the Project Site reinforces an area that has its own economic and physical identity by furthering the General Plan Land Use Element's policies to focus development in areas that are accessible to existing activity centers and transit, providing compatible and complementary economically productive uses, which supports the provisions of the Central City North Community Plan.

Therefore, the General Plan should be amended in part through the Central City North Community Plan as the Project would contribute to and strengthen an area which has significant social, economic or physical identity.

5. **City Charter Finding 556.** When approving any matter listed in Section 558, the City Planning Commission and the Council shall make findings showing that the action is in substantial conformance with the purposes, intent and provisions of the General Plan. If the Council does not adopt the City Planning Commission's findings and recommendations, the Council shall make its own findings.

The Project Site is located within the Central City North Community Plan area, which is one of the 35 community plans that comprise the Land Use Element of the General Plan. The Community Plan designates the Site as Heavy Industrial, which has a corresponding zone of M3. The site is presently zoned M3-1-RIO, consistent with the land use designation. As proposed, the Project would re-designate the Project Site from Heavy Industrial to Regional Commercial. The amendment, and corresponding Vesting Zone and Height District Change to (T)(Q)C2-2-RIO, would allow the redevelopment of the Site with a new 18-story Office Building comprised of 8,149 square feet of ground floor restaurant space, 308,527 square feet of office, 16,249 square feet of covered exterior employee common areas, a 3,500 square-foot ground floor courtyard, and associated parking facilities.

The Project would be consistent with the land use patterns and uses in the immediate area, which is developed with a variety commercial and industrial uses on properties zoned for industrial uses. Further, the 1.31-acre Project Site is not well-suited for modern large-scale industrial operations, and the Project's proposed uses, including restaurant and office uses, are generally consistent with the overarching vision of the adopted Community Plan.

Pursuant to LAMC Section 12.36 D, when acting on multiple applications for a project, when appropriate, findings may be made by reference to findings made for another application involving the same Project. As detailed above in Findings No. 2 and 3 the requested General Plan Amendment complies with Los Angeles City Charter Section 556 in that it is in substantial conformance with the purposes, intent and provisions of the General Plan and its elements, including the Framework Element, Noise Element, Mobility Element, Health and Wellness and Air Quality Elements and the Land Use Element – Central City North Community Plan. The Project would introduce new land uses while providing economically productive uses that are complementary to similar uses in the immediate vicinity; and enhance the pedestrian environment by activating ground floor uses, within proximity to transit and existing employment, amenity, and commercial centers within and around Downtown.

6. **City Charter Finding 558.** The proposed Amendment to the Central City North Community Plan will be in conformance with public necessity, convenience, general welfare and represents good zoning practice.

The recommended General Plan Amendment to the Central City North Community Plan would re-designate the land use from Heavy Industrial to Regional Commercial. In conjunction with the Amendment, the corresponding Vesting Zone and Height District Change from M3-1-RIO to (T)(Q)C2-2-RIO would permit development of the Project Site with a new 18-story Office Building comprised of 8,149 square feet of ground floor restaurant space, 308,527 square feet of office, 16,249 square feet of covered exterior employee common areas, a 3,500 square-foot ground floor courtyard, and associated parking facilities.

Public Necessity, Convenience, and General Welfare.

The Project would be an infill development in an area developed with low- and medium-rise industrial and commercial buildings as well as a few residential buildings of widely varying age. To the north are one- and three-story buildings and a seven story parking garage, to the east are one-story buildings and surface parking lots, to the south are one- and two-story buildings, and to the west are one-story buildings. While the area immediately around the Site is generally surrounded by low and medium rise industrial and commercial buildings, the Project Site is located in an urbanized area and the Arts District neighborhood continues to undergo changes to the development landscape, including the introduction of taller buildings with a variety of new uses such as residential, live/work, restaurant, retail, and office uses being introduced within an area previously concentrated with commercial and industrial uses. The Project Site itself is no longer being utilized for industrial related purposes, as it currently incorporates the vacant bow-truss building, an occupied office building, and a surface parking lot.

According to the Central City North Community Plan, there are 914 acres planned for industrial use (approximately 45.5 percent of the 2,010 acre total) in the Community Plan area. The Project Site comprises 1.31 acres or approximately 0.14 percent of the industrially-zoned land in the Community Plan. The Project would generate 1,282 jobs, a net increase of 1,270 jobs that would generate ongoing revenue for the City in the form of sales and property taxes. Therefore, the Project would not adversely impact the ability of the Community Plan Area to provide sufficient land for a variety of industrial uses with maximum employment opportunities. Furthermore, based on the immediately surrounding uses and the lot configuration, the Project Site is not well-suited to a large-scale industrial operation. In order to provide the new 18-story Office Building, the Project would change the current industrial land use designation to allow for commercial uses in order to maximize employment opportunities which are safe for the environment and the work force, and which have minimal adverse impact on adjacent uses. Surrounding uses include low- and medium-rise industrial and commercial buildings as well as a few residential buildings .

Furthermore, while not required, the Project would provide 3,500 square feet of open space in the form of a ground floor courtyard and 16,249 square feet of covered exterior employee common areas. As proposed, the Project would improve the livability and general welfare of the future employees and visitors of the development through the provision of open space, amenities, and commercial uses. The Project would further promote foot traffic through the development of ground floor restaurant uses, new street trees, and a ground floor paseo through the Site that would be open to the public and connect to a pedestrian passageway that would provide pedestrian access between Colyton Steet and Hewitt Street.



Approval of the requested General Plan Amendment from Heavy Industrial to Regional Commercial would allow the Project to make more efficient use of land by providing new employment opportunities in direct proximity to residential uses, while accommodating for projected population growth in the area that is compatible with its evolving surrounding uses. Therefore, the requested General Plan Amendment would be in conformity with public necessity, convenience, and general welfare.

### Good Zoning Practice

Although the Site, as well as a number of other properties in the area, are designated for industrial uses, development in the immediate vicinity is characterized by a mix of commercial and industrial uses. The Project Site itself is no longer being utilized for industrial-related purposes, as it currently occupied by a vacant building and office building with associated surface parking lots. Nearby residential buildings residential uses within walking distance of the Site include the six-story multi- unit residential building located at 825 East 4<sup>th</sup> Street, the Amp Lofts (695 South Santa Fe Avenue), the Brick Lofts (562 Mateo street ), the Toy Factory Lofts (1855 Industrial Street) and the Biscuit Company Lofts (1850 Industrial Street).

As the Arts District neighborhood is developed with a mixture of residential, commercial, office and light manufacturing uses, the Project would be compatible with the range of uses that exist in the vicinity. The proposed 18-story Office Building is similar in height to other buildings in the immediate surrounding area, including those recently approved and proposed projects included in the table below.

<b><i>Height and Number of Stories of Proposed/Approved Nearby Projects</i></b>			
<b>Project Location</b>	<b>Number of Stories/Height</b>	<b>Distance from 4<sup>th</sup>/Hewitt Site</b>	<b>Approved/Proposed</b>
2143 Violet Street	31 stories/343 feet	1 mile	Approved
2045 Violet Street	13 stories/273 feet	1 mile	Proposed
1101 East 5 <sup>th</sup> Street	12 stories/170 feet	0.2 miles	Approved
1100 East 5 <sup>th</sup> Street	8 stories/110 feet	0.2 miles	Proposed
4 <sup>th</sup> Street and Central Avenue <sup>1</sup>	2-44 stories/max height of 500 feet	0.3 miles	Proposed
670 Mesquit Street <sup>2</sup>	26-32 stories/max height of 378 feet	0.7 miles	Proposed
527 Colyton Street	12 stories/148 feet	0.3 miles	Proposed
<sup>1</sup> This project would include 10 buildings.			
<sup>2</sup> This project would include five buildings.			

As such, approval of the Project would allow for the development and use of a commercial building consistent with the scale of existing and proposed developments in the surrounding neighborhood.

Additionally, the Project would provide new commercial building and would offer amenities that would improve the quality of life for future employees as well as the surrounding community. The General Plan Amendment would result in a Project that would retain the

existing bow-truss building, which would complement existing uses in the vicinity, while also providing much needed ground floor commercial floor area. The Project promotes a more walkable neighborhood by locating office and restaurant uses within proximity of transit, existing job centers and services, and similar commercial uses. Furthermore, the amendment would allow for commercial uses that are consistent with the objectives and policies of the Central City North Community Plan, as discussed in Findings No. 2. Therefore, the requested General Plan Amendment would represent good zoning practices and development patterns in this portion of Downtown Los Angeles.

## **ENTITLEMENT FINDINGS**

### **7. Vesting Zone Change, Height District Change, and “T” and “Q” Classification**

- a. Pursuant to Section 12.32 C of the Los Angeles Municipal Code (LAMC), and based on these findings, the recommended action is deemed consistent with public necessity, convenience, general welfare and good zoning practice.**

Pursuant to LAMC 12.32 the City Planning Commission is required to provide a recommendation regarding the relation of the proposed land use ordinance to the General Plan. The proposed Zone Change from M3-1-RIO to C2-2-RIO would be consistent with the requested General Plan Amendment to change the Site's land use designation from Heavy Industrial to Regional Commercial as detailed above in Findings No. 2 and 3.

Public Necessity. The subject property is currently designated as Heavy Industrial and zoned M3-1-RIO in the Central City North Community Plan. The Applicant is requesting a General Plan Amendment, Vesting Zone Change and Height District Change to modify the land use designation to Regional Commercial and the zone to C2-2-RIO for the entirety of the Project Site. The proposed Zone Change and Height District Change would allow for commercial uses up to a maximum FAR of 6:1.

The Project is for the construction of an 18-story Office Building comprised of 8,149 square feet of ground floor restaurant space, 308,527 square feet of office, 16,249 square feet of covered exterior employee common areas and a 3,500 square-foot ground floor courtyard accessible from Colyton Street that would connect to a publicly accessible ground floor pedestrian passageway that would allow for a mid-block pedestrian cut-through from Colyton Street to Hewitt Street. The Project would total 340,770 square feet of gross floor area, comprised of an existing 7,800 square-foot building (the bow-truss building) and an 332,970 square-foot Office Building, on a 1.3-acre lot and a building height of 292 feet to the top of the parapet. The Project would be an infill development within an immediate vicinity which is rapidly transforming from an area with low-rise warehouses with industrial uses and a few residential buildings to an area with a more intense mix of residential, office, retail, and restaurant uses. Additionally, the existing bow-truss building that is being retained as part of the Project is no longer being used for industrial uses was most recently used as a museum space, and is proposed to be used for creative offices. The proposed Vesting Zone Change and Height District Change would be consistent with public necessity as it would bring a greater variety of jobs to an area going through extraordinary residential growth and allow an increase in FAR for an underdeveloped site that could otherwise support the economic goals for the City and the Community Plan area. As such, the proposed Vesting Zone and Height District Change would be in conformity with public necessity.

Convenience. Approval of the Vesting Zone and Height District Change would permit the construction of a new commercial development, comprised of 308,527 square feet

of office space, 8,149 square feet of ground floor restaurant space, , 16,249 square feet of covered exterior employee common areas and a 3,500 square-foot ground floor courtyard publicly-accessible from Colyton Street that would connect to a publicly accessible ground floor pedestrian passageway and allow for a mid-block pedestrian cut-through from Colyton Street to Hewitt Street. The Project would total 340,770 square feet of gross floor area, inclusive of the existing 7,800 square-foot bow-truss building which would remain as part of the Project. The Project would locate additional employment opportunities, publicly-accessible open space, and restaurant uses to a centrally-located neighborhood near housing and transit.

The addition of over 300,000 square feet of office space to a neighborhood that is rapidly adding residential uses and is 0.5 miles southeast of the Little Tokyo/Arts District Station for the A and E light rail Metro lines, would provide employment opportunities to nearby residents, thereby, reducing commute times to work and reducing reliance on single-occupancy vehicle trips.

The Project would also include a 3,500 square-foot publicly-accessible courtyard which would provide a recreation space for employees, visitors, and nearby residents to enjoy passive recreational opportunities. Additionally, a publicly-accessible passageway is proposed through the Site which would provide a east-west link between Colyton Street and Hewitt Street. The proposed ground floor restaurant space would enliven the Project Site frontages and provide a place for local residents, employees, and visitors to enjoy a meal.

In sum, the Project locates additional employment opportunities, open space, and restaurant uses to a transit-rich, centrally-located neighborhood experiencing a growing residential population. Accordingly, the proposed Vesting Zone and Height District Change would be in conformity with the public convenience.

General Welfare. Approval of the requested Vesting Zone and Height District Change would allow for the construction of a new commercial development, comprised of 308,527 square feet of office space, 8,149 square feet of ground floor restaurant space, 16,249 square feet of covered exterior employee common areas and a 3,500 square-foot ground floor courtyard publicly-accessible from Colyton Street that would connect to a publicly accessible ground floor pedestrian passageway that would allow for a mid-block pedestrian cut-through from Colyton Street to Hewitt Street. The Project would total 340,770 square feet of gross floor area, inclusive of the existing 7,800 square-foot bow-truss building which would remain as part of the Project.

The proposed Vesting Zone and Height District Change would be consistent with the general welfare by locating office and restaurant uses to a transit-rich, centrally-located neighborhood with an expanding residential base. Locating the proposed uses of the Project in this area would reduce congestion and air pollution in the area as the office uses would expand employment opportunities near residences and public transit, including light rail and local and regional bus lines. The Project would also include publicly-accessible open space to a dense urban area for use by employees of the site as well as neighboring residents and visitors. As such, the proposed Vesting Zone Change and Height District Change would be consistent with general welfare.

Good Zoning Practice. The Project Site is designated by the Central City North Community Plan for Heavy Industrial land uses and is zoned M3-1-RIO. This zoning is consistent with the existing land use designation. The corresponding zone for Heavy Industrial is the M3 Zone according to the Community Plan. Although the Site is designated for heavy industrial uses, the immediate vicinity is characterized by a mix of

office, residential, restaurant, and retail uses. Additionally, the Project Site is no longer being utilized for industrial-related purposes, as it currently consists of the vacant bow-truss building, surface parking lot, garage/storage buildings, and a one-story building that is currently being used as offices.

The proposed Vesting Zone and Height District Change would allow for the Project to add 1,282 jobs to the surrounding area, improving the ratio of residential and office uses. The proposed office and restaurant uses would be in proximity to public transit, including light rail and local and regional bus lines, and residential buildings, thereby, reducing the amount of single-occupancy vehicles on the road. The Project would also provide publicly-accessible open space which would provide a location for local residents, employees, and visitors to interact and recreate. The ground floor restaurant space would have large bi-fold doors that open to the street, enhancing the pedestrian experience and enlivening the surrounding block. The Project would be consistent with evolving nature of the Arts District neighborhood, originally comprised of low-scale manufacturing and industrial uses, but which now includes nearby proposed projects, such as the proposed 670 Mesquit Project with a 32-story tower, 4<sup>th</sup> and Central Project with a 44-story tower and the Palmetto Project with a 21-story building.

As proposed, the Project would introduce new amenities that would improve the quality of life for future employees as well as the surrounding community. The Vesting Zone Change and Height District Change would result in a Project that retains employment-generating uses on the Site that complement existing uses in the vicinity, while also providing restaurant uses and open space on the ground floor. The Project promotes a more walkable lifestyle by locating commercial uses within proximity of transit and existing job centers and services. Furthermore, the Vesting Zone Change would allow the development of the Site with a commercial use that is consistent with the objectives and policies of the Community Plan as discussed in Findings No. 2 and 3 and is compatible with the existing and proposed development of the surrounding area. Therefore, the Vesting Zone and Hight District Change would be in conformity with good zoning practices and with development patterns in the immediate area.

In addition, the Project Site is located within the River Improvement Overlay Zone (RIO) and would be required to comply with the Los Angeles River Design Guidelines, which establishes best practices for designing projects located within the RIO Zone. The requested Vesting Zone and Height District Change would maintain the RIO designation on the site. The Los Angeles River Design Guidelines consist of overarching objectives followed by a list of specific implementation strategies. These strategies address river-adjacent development. Although the Project is located within the boundaries of the RIO Zone, the Project Site is separated from the Los Angeles River by existing roads and railroad tracks and is not immediately adjacent or accessible from the River. Nevertheless, the Project would further the relevant objectives of the Los Angeles River Design Guidelines, including employing high quality, attractive and distinguishable architecture and minimizing the quantity and appearance of parking and loading areas by locating all parking underground and placing loading areas within the building or away from public view.

**b. “T” and “Q” Classification Findings.**

Per LAMC Sections 12.32 G.1 and 2, the current action, as recommended, has been made contingent upon compliance with new “T” and “Q” conditions of approval imposed herein for the Project. The “T” Conditions are necessary to ensure the identified dedications, improvements, and actions are undertaken to meet the public’s needs, convenience, and general welfare served by the actions required. These actions and

improvements will provide the necessary infrastructure to serve the proposed community at this Site. The “Q” conditions that limit the scale and scope of future development on the Site are also necessary to protect the best interests of and to assure a development more compatible with surrounding properties and the overall pattern of development in the community, to secure an appropriate development in harmony with the General Plan, and to prevent or mitigate the potential adverse environmental effects of the subject recommended action.

### **Main Conditional Use (On-Site Alcohol Sales) Findings**

In conjunction with the development of the Project, the Applicant is requesting a Main Conditional Use Permit (MCUP) to permit the sale and dispensing of a full line of alcoholic beverages for on-site consumption within up to six establishments and associated outdoor dining areas. The following are the required findings to permit the sale and dispensing of alcoholic beverages as required by LAMC 12.24 E and 12.24 W.1.

#### **8. The project will enhance the built environment in the surrounding neighborhood or will perform a function or provide a service that is essential or beneficial to the community, city, or region**

The Project would involve the demolition of an existing office building, two storage/garage buildings, and surface parking lots, and the construction of an 18-story Office Building. The Project would total 340,770 square feet of gross floor area, comprised of an existing 7,800-square-foot building and a 332,970-square-foot Office Building, which would include 8,149 square feet of ground floor restaurant space, 308,527 square feet of commercial office space, and 16,294 square feet of office exterior common areas. The Main Conditional Use Permit would allow for the sale and dispensing of a full line of alcoholic beverages of alcohol for on-site consumption at six establishments within the Project, including the existing 7,800 square foot bow-truss building and 8,149 square feet of ground floor restaurant uses in the Office Building.

The Project Site is located within the Central City North Community Plan area, and is currently designated for Heavy Industrial land uses corresponding to the M3-1-RIO Zone. The Applicant is requesting a General Plan Amendment to change the land use designation on the Project Site to Regional Commercial, and a corresponding Vesting Zone and Height District Change to change the zoning on the Project Site to C2-2-RIO.

The area surrounding the Project Site is urbanized and improved with a range of industrial, residential, restaurants, and commercial uses. The Project Site is generally bounded by Colyton Street to the west, East 4<sup>th</sup> Street to the north, Hewitt Street to the east, and various industrial and commercial uses to the south. Uses immediately surround the Site to the north across 4th Street is a one-story automotive repair shop and warehouse. Across 4th Place is a seven-story parking structure and a three-story office building. To the east of the Project Site across Hewitt Street are one-story commercial and manufacturing uses as well as a surface parking lot. To the south of the Project Site are one- and two-story commercial and manufacturing uses. To the west of the Project Site across Colyton Street are one-story manufacturing uses.

The proposed Regional Commercial land use designation and corresponding C2-2-RIO Zone would allow for the proposed commercial uses, which would be desirable to the public convenience and welfare as they would be in an infill location accessible to nearby residents, employees, and visitors. The Project would provide and add to the number of available dining venues eating establishments to serve the residents, employees, and visitors in the

area,. The offering of food and alcohol in conjunction with the proposed uses would be a benefit as an amenity to current and future residents and visitors in the area.

The ability for restaurant tenants to offer a full-line of alcoholic beverages would also allow the restaurants to remain competitive with other similar uses serving the same area, since alcohol service is commonly expected by patrons as part of a restaurant. Further, patrons are drawn to the area due to the shopping, entertainment, and dining experiences available to them, and offering a full-line of alcoholic beverages at these uses on the Project Site would enhance the dining and entertainment experience for visitors, employees, and residents in the vicinity. Alcohol sales at this location would provide a function and beneficial service to patrons visiting the site, which would enhance the character of the area and would be appropriate in the proposed Regional Commercial land use designation and C2 Zone.

The Main Conditional Use Permit provides an umbrella entitlement with conditions that apply to the Project Site and in general to the restaurant uses and the bow-truss building. These conditions include, but are not limited to, security measures, such as a camera surveillance system, appropriate lighting in the evening hours, and prohibiting adult entertainment. In addition, all music, sound or noise which is under the control of the individual tenants shall be in compliance with the Citywide Noise Ordinance. Further, loitering is prohibited on and around the premises, and the Applicant would be required to maintain the premises and sidewalk in good condition. These conditions would be supplemented by more specific conditions designed to address the characteristics of each individual establishment, and a Plan Approval would be required, prior to the effectuation of the approval for each respective tenant, including more specific physical and operational restrictions. Under these Plan Approvals, the Zoning Administrator and City of Los Angeles Police Department will have the opportunity to recommend conditions, including the maximum number of indoor seats, as determined by the City of Los Angeles Department of Building and Safety.

As such, the on-site service of alcoholic beverages at up to six establishments within the Project's ground floor restaurant uses, would enhance the built environment in the surrounding neighborhood, and would provide a function that is beneficial and compatible with the character of the community, and the commercial viability of the region as a whole.

**9. The project's location, size, height, operations and other significant features will be compatible with and will not adversely affect or further degrade adjacent properties, the surrounding neighborhood, or the public health, welfare, and safety.**

The subject property is located within the Central City North Community Plan area and the Project includes a General Plan Amendment to amend the Community Plan to Regional Commercial land uses, with a corresponding zone change to C2-2-RIO. The Project would be an infill development in an area developed with low- and medium-rise industrial and commercial buildings as well as a few residential buildings of widely varying age. To the north are one- and three-story buildings and a seven story parking garage, to the east are one-story buildings and surface parking lots, to the south are one- and two-story buildings, and to the west are one-story buildings. While the area immediately around the Site is generally surrounded by low and medium rise industrial and commercial buildings, the Project Site is located in an urbanized area and the Arts District neighborhood continues to undergo changes to the development landscape, including the introduction of taller buildings with a variety of new uses such as residential, live/work, restaurant, retail, and office uses being introduced within an area previously concentrated with commercial and industrial uses. Thus, the Project would introduce new commercial uses to the Site, which would provide new amenities to the surrounding businesses and residents in the area, add to the multiple commercial establishments in the area, support the residential uses in close proximity to the Site , as well as support projected growth. The proposed hours of operation

are typical of establishments of this type and are reasonable to expect in a commercial development in an area that contains commercial uses. Alcohol sales in conjunction with restaurant uses would be compatible with and continue to add to the diversification of commercial activities, which further supports the growing residential population in the neighborhood as well as visitors to the area.

The sales of alcohol would not be detrimental to nearby residential uses, as the establishments serving alcohol would be carefully controlled and monitored. Nearby residential buildings within walking distance of the Site include the six-story multi-unit residential building located at 825 East 4<sup>th</sup> Street, the Amp Lofts (695 South Santa Fe Avenue), the Brick Lofts (562 Mateo street ), the Toy Factory Lofts (1855 Industrial Street) and the Biscuit Company Lofts (1850 Industrial Street). The conditions recommended herein would ensure that the establishments would not adversely affect or further degrade the surrounding neighborhood, or the public health, welfare, and safety. These conditions would be supplemented by more specific conditions designed to address the characteristics of each individual establishment, and a Plan Approval would be required, prior to the effectuation of the approval for each respective tenant, including more specific physical and operational restrictions. Under these Plan Approvals, the Zoning Administrator and LAPD will have the opportunity to recommend conditions, including the maximum number of indoor seats, as determined by the City of Los Angeles Department of Building and Safety. Further, the sale of alcohol is regulated by the State of California through the issuance of an Alcohol Beverage Control (ABC) license. Other commercial uses in the area provide similar functions, and no evidence was presented at the hearing or in writing that alcohol-sales would be materially detrimental to the immediate neighborhood.

Thus, the Project would be compatible with development on adjacent properties and its location, size height, and operations will be compatible with and would not adversely affect or further degrade surrounding properties and/or the public health, welfare, and safety.

**10. The project substantially conforms with the purpose, intent and provisions of the General Plan, the applicable community plan, and any applicable specific plan.**

Pursuant to LAMC Section 12.36 D, when acting on multiple applications for a project, when appropriate, findings may be made by reference to findings made for another application involving the same Project. This finding is substantially identical to the finding found earlier in this document as General Plan/Charter Findings Number 2 and 3 and in accordance with Sections 12.24 E and 12.24 W.1 of the LAMC, is hereby incorporated by reference. As discussed in Findings No. 2 and 3, the Project would be consistent with the purposes, intent and provisions of the General Plan and its elements, including the Framework Element, Noise Element, Mobility Element, Health and Wellness Element, Air Quality Element, Sewerage Facilities Element and the Land Use Element – Central City North Community Plan that relate to commercial land uses. Approval of the Project would enhance the built environment in the surrounding neighborhood and would provide a function that is fitting and compatible with the character of the surrounding community and commercial viability of the region as a whole.

The Project Site is located within the Central City North Community Plan area, which designates the Site for Heavy Industrial land uses corresponding to the M3 Zone. The Site is currently zoned M3-1-RIO, consistent with the land use designation. The Project is requesting a General Plan Amendment to change the land use designation from Heavy Industrial to Regional Commercial, and a corresponding Vesting Zone and Height District Change from M3-1-RIO to C2-2-RIO, in order to implement the Project.

The Community Plan does not contain policies that specifically address requests for the sale of alcoholic beverages; however, the sale of alcohol is inherent in the operation of similar commercial uses within the vicinity of the Site. The proposed request for the sale and dispensing of a full line of alcoholic beverages for off- and on-site consumption at up to six establishments is consistent with the following Central City North Community Plan goal and objective:

**Goal 2:** A strong and competitive commercial sector which best serves the needs of the community through maximum efficiency and accessibility while preserving the historic commercial and cultural character of the district.

**Objective 2-1:** To conserve and strengthen viable commercial development in the community and to provide additional opportunities for new commercial development and services.

The Project would contribute to the area's viable commercial development by introducing new restaurant and office uses adjacent to other similar uses. The area surrounding the Project Site is urbanized and improved with a range of residential, commercial, and restaurants. Alcohol sales for on-site consumption as part of the Project would be desirable to the public convenience and welfare as the uses are in an infill location accessible to nearby residents, workers, and visitors.

The commercial uses would create an active environment for surrounding residents by increasing the walkability of the streets. The Project's commercial uses would directly front 4<sup>th</sup> and Hewitt Streets, and a publicly-accessible courtyard, activating the streets with pedestrian activity and creating an environment that would be safe, clean, attractive and lively. The ability for the Site to offer a full line of alcoholic beverages would allow the restaurant uses to remain competitive with other similar uses serving the same area, as alcohol service is a common as part of restaurant uses. Further, patrons are drawn to the Arts District neighborhood and nearby Downtown neighborhoods due to the shopping, entertainment, and dining experiences available to them, and offering a full line of alcoholic beverages at these uses on the Project Site would be an added amenity for patrons of the Project that would help to strengthen economic development, and support a strong and competitive commercial sector in the Community Plan area.

Based on the above, the requested Main Conditional Use Permit for the dispensing of alcohol would substantially conform with the purpose, intent and provisions of the General Plan and the applicable community plan.

**Additional findings required per LAMC Section 12.24 W.1 (Conditional Use for Alcoholic Beverages):**

**11. The proposed use will not adversely affect the welfare of the pertinent community.**

The Project requests a Main Conditional Use Permit to allow for the sales and service of a full-line of alcoholic beverages for on-site consumption, in conjunction with six commercial establishments and associated outdoor dining areas. While the specific tenants or uses have not yet been identified, the Project would be limited to commercial uses permitted in the C2 Zone, pending approval of the Project's requested General Plan Amendment, Vesting Zone, and Height District Change entitlements. The Main Conditional Use Permit provides an umbrella entitlement with general conditions that apply to up to six commercial establishments serving the Project. These conditions include, but are not limited to, security measures such as a camera surveillance system, appropriate lighting in the evening hours, hours of operation, prohibition of after-hours use, except routine clean-up, and prohibition



of dancing and adult entertainment. Additionally, within the first six months of operation, all employees involved with the sale of alcohol shall enroll in the City of Los Angeles Police Department (LAPD) "Standardized Training for Alcohol Retailers" (STAR) or Department of Alcoholic Beverage Control "Licensee Education on Alcohol and Drugs" (LEAD) training program or the Responsible Beverage Service (RBS) Training Program.

Additionally, the surrounding neighborhood contains similar buildings that provide commercial uses which serve alcohol on-site and off-site, therefore the introduction of another such establishment would not create an adverse or unique condition. The Project's new establishments would help to enhance the availability of dining and entertainment options in the neighborhood, as well as provide options for the Office Building tenants. All music, sound, or noise which is under the control of the tenant shall be in compliance with the Citywide Noise Ordinance. Further, loitering is prohibited on and around the premises, and the Applicant would be required to maintain the premises and sidewalk in good condition. These conditions would be supplemented by more specific physical and operational restrictions to address the characteristics of each individual establishment implemented through a Plan Approval required prior to the effectuation of the grant for each respective tenant. Under these Plan Approvals, the Zoning Administrator and LAPD have the opportunity to recommend conditions, including the maximum number of indoor seats, as determined by the City of Los Angeles Department of Building and Safety.

Therefore, as conditioned, the request to allow the sale of alcoholic beverages for off- and on-site consumption within up to six establishments in conjunction with the proposed uses would not adversely affect the welfare of the community.

**12. The granting of the application will not result in an undue concentration of premises for the sale or dispensing for consideration of alcoholic beverages, including beer and wine, in the area of the City involved, giving consideration to applicable State laws and to the California Department of Alcoholic Beverage Control's guidelines for undue concentration; and also giving consideration to the number and proximity of these establishments within a one thousand foot radius of the site, the crime rate in the area (especially those crimes involving public drunkenness, the illegal sale or use of narcotics, drugs or alcohol, disturbing the peace and disorderly conduct), and whether revocation or nuisance proceedings have been initiated for any use in the area.**

According to the California State Department of Alcoholic Beverage Control (ABC) licensing criteria, two on-site and one off-site licenses are authorized for the subject Census Tract Number 2060.51. Within the subject Census Tract there are currently a total of 64 active licenses, including, 54 on-sale and 10 off-sale licenses. As such, the number of existing on-sale licenses within the Census Tract where the Project Site is located exceeds ABC guidelines.

According to statistics provided by the LAPD's Central Division Vice Unit, within Crime Reporting District No. 159, which has jurisdiction over the Project Site, a total of 508 crimes were reported in 2022 (457 Part I and 51 Part II crimes), compared to the Citywide average of 156 crimes and the total High Crime Reporting Districts average of 187 crimes for the same reporting period. The vast majority of Part I Crimes are reported as larceny. Part II Crimes reported include, Narcotics (14), Liquor Laws (5), Public Drunkenness (0), Disturbing the Peace (0), Disorderly Conduct (2), Gambling (0), DUI related (6) and other offenses (11).

Concentration can be undue when the addition of a license would negatively impact a neighborhood. Concentration is not undue when the approval of a license does not negatively impact an area, but rather such a license benefits the public welfare and

convenience. The ABC has discretion to approve an application if there is evidence that normal operations would not be contrary to the public welfare and would not interfere with the quiet enjoyment of property by residents in the area. Negative impacts commonly associated with the sale of alcoholic beverages, such as criminal activity, public drunkenness, and loitering are mitigated by the imposition of conditions requiring responsible management and deterrents against loitering, public drinking, driving under the influence, and public drunkenness. As stated above, the number of existing on-site licenses within the census tract where the Project Site is located exceeds ABC guidelines. However, as conditioned, allowing the sale, dispensing, and consumption of a full line of alcoholic beverages for on-site consumption in conjunction with the proposed restaurant and retail uses is not anticipated to create a law enforcement issue. Consequently, this approval would not result in an undue concentration of premises selling, dispensing, and consumption of a full-line of alcoholic beverages.

Additionally, the six proposed establishments are part of a larger development, which would benefit from oversight of the building complex as a whole. Moreover, included in this grant are a number of general conditions that would act to minimize any impacts that might be generated by alcohol serving establishments including that each individual venue seeking to utilize a permit to sell alcoholic beverages for on-site consumption as a part of this Main Conditional Use Permit must apply for a Plan Approval. The Plan Approval process would allow the Department of City Planning to tailor conditions to each individual Applicant and establishment, and create measures, which would minimize impacts. Likewise, concerns associated with any individual establishment can be addressed in more detail through the Plan Approval process, which would provide an opportunity to consider more specific operational characteristics when a tenant is identified and the details of each establishment are highlighted.

The Project would not adversely affect community welfare because the proposed restaurants are desirable uses, and would be located in an area that permits commercial uses. Conditions are in place to ensure security measures and public welfare, such as a camera surveillance system, appropriate lighting in the evening hours, restrictive hours for loading and unloading, and prohibiting loitering. In addition, all music, sound or noise which is under the control of the individual tenants shall be in compliance with the Citywide Noise Ordinance. As such, as conditioned, the proposed uses in conjunction with the on-site sale, dispensing, and consumption of a full-line of alcoholic beverages would be compatible with the surrounding development and will not adversely affect the welfare of the surrounding community.

**13. The proposed use will not detrimentally affect nearby residentially zoned communities in the area of the City involved, after giving consideration to the distance of the proposed use from residential buildings, churches, schools, hospitals, public playgrounds and other similar uses, and other establishments dispensing, for sale or other consideration, alcoholic beverages, including beer and wine.**

The Project Site is located in an urbanized area and generally surrounded by low- and medium-rise industrial and commercial buildings as well as a few residential buildings. The area has been undergoing changes to the development landscape, with new residential, live/work, restaurant, retail, and office uses being introduced within an area previously more concentrated with commercial and industrial use

The following sensitive uses are located within 1,000-foot radius of the Project Site:

- Arts District Park located at 501 Hewitt Street;
- Saint Francis Xavier Church & School located at 222 Hewitt Street;
- Barker Block Lofts located at 510 Hewitt Street;
- Residential Trailer located at 428 Hewitt Street;
- Beacon Lofts located at 825 4<sup>th</sup> Street
- Exodus3 LA/City Light Church located at 801 4<sup>th</sup> Place; and
- Arts District Dog Park located at 1004 4<sup>th</sup> Street.

As such, the proposed commercial uses are located within proximity of sensitive uses, including residential and park uses. Consideration has been given to the distance of the subject establishment from the above-referenced sensitive uses. As conditioned, the Project would provide adequate security measures to discourage loitering, theft, vandalism and other nuisances as imposed; all sales employees would receive training in responsible alcohol sales; age verification devices and prompts would be part of the Point-of-Sale system to assist cashiers in prevention of sales to minors; and potential effects of excessive noise or disruptive behavior. Furthermore, the surrounding area is an urban environment that contains similar buildings with residents in close proximity to restaurants and other establishments that serve alcohol. While the sale of alcoholic beverages is important to the restaurants that would be located on the Project Site, it would be incidental to primary operations and, as such, no detrimental effects should be expected from the Project.

Plans submitted by the Applicant show that the six potential establishments for which alcohol sales is being requested, would front 4<sup>th</sup>, Colyton and Hewitt Streets as well as the Project's proposed internal facing public courtyard. While properties along 4<sup>th</sup>, Colyton, and Hewitt Streets are developed with industrial and commercial uses, the proposed on-site sale of a full line of alcoholic beverages would be compatible with the existing and proposed neighborhood-serving commercial uses within the Central City North Community Plan area. Moreover, included in this grant are several conditions that would act to minimize any impacts that might be generated by alcohol serving establishments, including that each individual venue seeking to utilize a permit to sell alcoholic beverages for on-site consumption as a part of this Main Conditional Use Permit must apply for a Plan Approval. The Plan Approval process would allow the Department of City Planning to tailor conditions to each individual establishment, which would minimize any impact that may be generated. Therefore, as conditioned, the Project would not detrimentally affect residentially zoned properties or any other sensitive uses in the area.

### **Site Plan Review Findings**

#### **14. The project is in substantial conformance with the purposes, intent and provisions of the General Plan, applicable community plan, and any applicable specific plan.**

The Project would involve the demolition of an existing office building, two storage/garage buildings, and a surface parking lot to accommodate the construction of an 18-story Office Building comprised of 8,149 square feet of ground floor restaurant space, 308,527 square feet of office, 16,249 square feet of covered exterior employee common areas and a 3,500 square-foot ground floor courtyard accessible from Colyton Street that would connect to a publicly accessible ground floor pedestrian passageway creating a mid-block pedestrian cut-through from Colyton Street to Hewitt Street. The Project would total 340,770 square feet of gross floor area, comprised of an existing 7,800 square-foot building (the bow-truss

building) and an 332,970 square-foot Office Building, on a 1.3-acre lot (an FAR of 6:1) and a building height of 292 feet to the top of the parapet.

The Los Angeles General Plan sets forth goals, objectives and programs that guide both Citywide and community-specific land use policies. The General Plan is comprised of a range of State-mandated elements, including, but not limited to Housing and Conservation, Land Use, Noise, Safety, and Transportation. The City's Land Use Element is divided into 35 Community Plans that establish parameters for land use decisions within those sub-areas of the City. The Project is consistent with the following Elements of the General Plan: Framework Element, Mobility Element, Health and Wellness Element, Air Quality Element and the Land Use Element - Central City North Community Plan.

Pursuant to LAMC Section 12.36 D, when acting on multiple applications for a project, when appropriate, findings may be made by reference to findings made for another application involving the same Project. This finding is substantially identical to the finding found earlier in this document as General Plan/Charter Findings Number 2 and 3 and in accordance with Section 16.05 F of the LAMC, is hereby incorporated by reference. As discussed in Findings No. 2 and 3, the Project would be consistent with the purposes, intent and provisions of the General Plan and its elements, including the Framework Element, Noise Element, Mobility Element, Health and Wellness Element, Air Quality Element, Sewerage Facilities Element and the Land Use Element – Central City North Community Plan that relate to commercial land uses. Approval of the Project would enhance the built environment in the surrounding neighborhood and would provide a function that is fitting and compatible with the character of the surrounding community and commercial viability of the region as a whole.

Additionally, the Project Site is located within the River Improvement Overlay Zone (RIO) and would be required to comply with the Los Angeles River Design Guidelines, which establishes best practices for designing projects located within the RIO Zone. The Los Angeles River Design Guidelines consist of overarching objectives followed by a list of specific implementation strategies. These strategies address river-adjacent development. Although the Project is located within the boundaries of the RIO Zone, the Project Site is separated from the Los Angeles River by roads and railroad tracks and is not immediately adjacent or accessible from the River. Nevertheless, the Project would further the relevant objectives of the Los Angeles River Design Guidelines, including employing high quality, attractive and distinguishable architecture and minimizing the quantity and appearance of parking and loading areas by locating all parking and loading areas underground or screened from public view.

Based on the above, the Project is in substantial conformance with the purposes, intent and provisions of the General Plan, applicable community plan, and any applicable specific plan.

**15. The project consists of an arrangement of buildings and structures (including height, bulk and setbacks), off-street parking facilities, loading areas, lighting, landscaping, trash collection, and other such pertinent improvements that is or will be compatible with existing and future development in neighboring properties.**

The Project Site is located within the Central City North Community Plan Area. The area surrounding the Project Site is urbanized and improved with low and medium rise industrial and commercial buildings as well as a few residential buildings. The Site is bounded by 4<sup>th</sup> Street to the north, Hewitt Street to the east, Colyton Street to the west, and various industrial and commercial uses to the south. Vehicular access to the Site is currently available at driveways along 4<sup>th</sup> Street, Hewitt Street, and Colyton Street.

The Project Site is currently developed with an existing office building, the bow-truss building, two storage/garage buildings, and surface parking lots. To accommodate the Project, the existing uses, excluding the vacant bow-truss building located at the southwest corner of Colyton Street and 4<sup>th</sup> Street, would be demolished. The Project would include the construction of a new 18-story Office Building and the retention of the vacant 7,800 square-foot bow-truss building. The Office Building would include 8,149 square feet of ground floor restaurant space and 16,294 square feet of office exterior common areas, a 3,500 square-foot ground floor outdoor courtyard accessible from Colyton Street that would connect to a publicly accessible ground floor pedestrian passageway to allow for a mid-block pedestrian cut-through from Colyton Street to Hewitt Street. The Office Building would have a building height of 292 feet to the top of the parapet, (a maximum height of 297 feet to the top of the elevator overrun) and Project's FAR would be 6:1.

The Project would provide 660 vehicle parking spaces within three subterranean levels and four levels of above-grade parking contained within a parking podium. The Project would also provide a total of 112 total bicycle parking spaces, five more spaces than what would be required. The 40 short-term bicycle parking spaces would be located at the northwest corner of the Site adjacent to the bow-truss building, in the corridor between the existing building and the proposed Office Building, and in the courtyard and passageway that connect to the Office Building lobby. The 72 long-term bicycle parking spaces would be located on the ground floor inside the Office Building, accessible from the pedestrian passageway.

### Height

The proposed building would be 292 feet to the top of the parapet (a maximum height of 297 feet to the top of the elevator overrun). The Project Site is located in an urbanized area and generally surrounded by industrial and commercial buildings as well as a few residential buildings. To the north are one- and three-story buildings and a seven story parking garage, to the east are one-story buildings and surface parking lots, to the south are one- and two-story buildings, and to the west are one-story buildings. While the area immediately around the Site is generally surrounded by low and medium rise industrial and commercial buildings, the Project Site is located in the Arts District neighborhood which continues to undergo changes to the development landscape, including the introduction of taller buildings with a variety of new uses such as residential, live/work, restaurant, retail, and office uses being introduced within an area previously concentrated with commercial and industrial uses.

Several proposed and approved projects located within one mile of the Site, including the height/number of stories of each project, are provided in the table below. As shown, the recently approved and proposed projects range in height from 110 feet to a maximum of 378 feet. Thus, the Project's maximum height of 297 feet would be within the range of heights for the recently approved or proposed project's heights.

<b><i>Height and Number of Stories of Proposed/Approved Nearby Projects</i></b>			
<b>Project Location</b>	<b>Number of Stories/Height</b>	<b>Distance from 4<sup>th</sup>/Hewitt Site</b>	<b>Approved/Proposed</b>
2143 Violet Street	31 stories/343 feet	1 mile	Approved
2045 Violet Street	13 stories/273 feet	1 mile	Proposed
1101 East 5 <sup>th</sup> Street	12 stories/170 feet	0.2 miles	Approved

1100 East 5 <sup>th</sup> Street	8 stories/110 feet	0.2 miles	Proposed
4 <sup>th</sup> Street and Central Avenue <sup>1</sup>	2-44 stories/max height of 500 feet	0.3 miles	Proposed
670 Mesquit Street <sup>2</sup>	26-32 stories/max height of 378 feet	0.7 miles	Proposed
527 Colyton Street	12 stories/148 feet	0.3 miles	Proposed
<sup>1</sup> This project would include 10 buildings.			
<sup>2</sup> This project would include five buildings.			

### Bulk & Mass

The area surrounding the Project Site is characterized by a range of one- to nine-story buildings. The Project is for an 18-story Office Building and would retain an existing one-story bow-truss building at the northwest corner of the Project Site. The Office Building is intended to be complementary to surrounding structures and reflect a utilitarian warehouse massing, form, and exterior façade design at the base of the structure, transitioning to a modern office building above the parking podium. Where the ground floor Office Building directly abuts 4<sup>th</sup> Street and Hewitt Street, the façade will feature large bi-fold doors that open and provide access to the ground floor restaurant uses. The indoor-outdoor feel that the doors would provide help break up the bulk of the building. The one-story bow-truss building would also break up the bulk and mass of the Project while providing a link to the lower-scale industrial past.

Additionally, the Project would include a substantial setback as it transitions from the podium levels to the office levels. The western facing façade would feature a maximum setback of approximately 45 feet, the northern façade would be set back approximately 9 feet, and the eastern and southern facades would be set back approximately 7.5 feet. A pedestrian passageway is proposed through the ground floor of the Office Building into the proposed publicly-accessible outdoor courtyard. The passageway would provide a link between Hewitt and Colyton Streets and minimize the perceived massing and density of the Office Building. The courtyard abuts Colyton Street at the rear of the bow-truss building and could also be accessed from the passageway on Hewitt Street. The courtyard would be open-to-the-sky with a pergola and potted trees providing shade. The open-air plaza would create an open buffer within the Project Site, thereby, reducing the perceived massing of the Project.

Finally, the design of the parking podium would screen the visibility of parked cars and mirror the surrounding neighborhood's traditional warehouse massing. The parking podium would feature non-operable steel frame windows inset into unfinished concrete walls at the facades that face public streets and also a portion of the west façade that faces the existing bow-truss building. The podium facades that face property lines, particularly the southern façade and the southern portion of the west façade, would need to have a rated wall with no openings due to building code requirements. However, the walls would be articulated in a manner that continues the window patterns on the street-facing facades. Additionally, the surfaces would continue the tradition of large murals in the Arts District by using these surfaces as a large canvas for murals. At the top of the podium where the office floors begin, there would be an outdoor terrace with landscaping. Additional outdoor terraces are provided at the northwest corner of the Office Building from Levels 7 through 15. These terraces would include landscaping and help break up the mass of the Office Building massing. Level 16 would provide an outdoor terrace at the northeast corner of the building; and Levels 17 and 18 would provide a large terrace on both the western portion of the floor and at the northwest corners. The cutouts for the terraces within the Office Building structure

not only provide a unique design element but also soften the building with landscaping and help break up the mass and bulk of the structure. In sum, the proposed bulk and mass of the Project would be consistent with the scale of existing and future proposed developments within the surrounding neighborhood.

### Setbacks

Pursuant to LAMC Section 12.14, setbacks are not required in the C2 Zone for commercial uses.

### Off-Street Parking and Loading Area

Vehicular access to the Site would be provided from two driveways on 4<sup>th</sup> Street, including an ingress/egress to the subterranean parking garage and another ingress/egress to the upper levels of the garage. Because 4<sup>th</sup> Street is a one-way eastbound street, both driveways would be restricted to right-turn in and out turn movements. An at-grade loading dock would be accessible from Hewitt Street. All driveways and access would be designed in accordance with the City of Los Angeles Department of Transportation (LADOT) standards.

The City cannot require the Project to provide vehicle parking for the proposed commercial uses as the Site meets the requirements of Government Code Section 65863.2 (AB 2097) and is 0.5 miles from the Metro A and E Line Little Tokyo/Arts District Station. However, the Applicant has chosen to voluntarily provide vehicle parking for the proposed commercial uses at rates consistent with the East Los Angeles State Enterprise Zone and the bicycle replacement provisions of LAMC 12.21 A.4, which sets a commercial and office parking ratio of two parking spaces per 1,000 square feet of floor area, with bicycle parking reductions to replace 28 vehicular spaces. After the bicycle replacement provisions and continuing to provide parking for the bow-truss building to be retained, a total of 660 vehicular parking spaces would be provided within three subterranean levels and four above-grade parking levels contained within a parking podium. In addition, the Project would provide 30 percent of its required parking spaces to be electric-vehicle ready (198 spaces), and ten percent of its required parking spaces would be provided chargers for electric vehicles (66 spaces).

Vehicle parking would be provided within three subterranean levels and four levels of above grade parking. The parking levels visible from 4<sup>th</sup> Street and Hewitt Street façades, would be designed with non-operable, industrial steel frame opaque windows set in exposed concrete walls. The parking level along the southern façade would be fully enclosed with articulated unfinished concrete and would be accented with murals, similar to other murals found throughout the Arts District neighborhood. As seen in the rendering above, the parking podium's western façade, adjacent to the bow-truss building and outdoor courtyard, would include non-operable opaque windows set in concrete similar to the north and east facades. Additionally, the west façade would include black mesh screens to screen the parking ramps, as they are right up against the west façade. The mesh screens would become a background element for a mural. Further, the western façade includes two exit stairs that scale the building. Finally, the southern portion of the west façade's podium adjacent to the neighboring property would be fully enclosed in concrete decorated with a mural.

### Landscaping

The Project would not be required to provide open space (as defined in the LAMC), as only commercial uses are proposed. However the Project would provide a publicly accessible ground floor courtyard and tenant amenity spaces. As mentioned, a 3,500 square-foot courtyard would be located adjacent to the bow-truss building, accessible from Colyton

Street (or from Hewitt Street via the pedestrian passageway), would include a pergola and be landscaped with potted plants and trees. As a Los Angeles Department of Water and Power (LADWP) vault would be located beneath the courtyard, landscaping cannot be planted in the ground.

Approximately 1,000 square feet of the ground floor would be landscaped with shrubs and groundcover. The three existing non-protected street trees (Brisbane box trees) along 4<sup>th</sup> Street would be removed and replaced with five street trees on 4<sup>th</sup> Street, five street trees on Hewitt Street, and two trees on Colyton Street. The surrounding street trees species shall be considered when selecting the Project's street trees species as conditioned above. Three additional trees would be planted on-site: two trees would be planted adjacent to the Colyton Street frontage and one potted tree would be located in the courtyard area. Tree removals and replacements are subject to the approval of the City of Los Angeles Board of Public Works Urban Forestry Division.

Additional office tenant open space areas would be located on Levels 6 through 18, ranging in size from 524 square feet to 10,839 square feet.

#### Trash Collection

As conditioned, all trash and recycling areas would be enclosed and screened from public view, accessed via the loading corridor on Hewitt Street, at the southeast corner of the proposed Office Building.

#### Lighting & Building Signage

The Project would add new restaurant and office uses that would include accent and security lighting, in compliance with LAMC requirements, and similar to that provided for existing adjacent commercial, and industrial uses. Additionally, the Project has been designed and conditioned to incorporate pedestrian lighting that is similar in design to pedestrian lighting in the Project vicinity, to protect nearby uses from potential lighting related impacts, including conditions for outdoor lighting to shine downward, be installed with shielding, and be directed onto the Project Site. All other signage shall be required to comply with LAMC requirements. In addition, the Project has been conditioned so that there shall be no off-site commercial signage on construction fencing during construction.

The Project, as an infill commercial project, would be compatible with existing and future development on adjacent and neighboring properties with regards to height, bulk, and setbacks, off-street parking facilities, loading areas, lighting, landscaping, trash collection, and other such pertinent improvements. Therefore, the arrangement of the development would be consistent and compatible with existing and future development in neighboring properties.

#### **16. That any residential project provides recreational and service amenities in order to improve habitability for the residents and minimize impacts on neighboring properties.**

The Project does not contain any residential units; therefore, this finding does not apply.



## **CALIFORNIA ENVIRONMENTAL QUALITY ACT (CEQA) FINDINGS**

### **Introduction**

The City of Los Angeles (the "City"), as Lead Agency, has evaluated the environmental impacts of the 4<sup>th</sup> and Hewitt Project by preparing an environmental impact report (EIR) ENV-2017-470-EIR (SCH No. 2017091054). The EIR was prepared in compliance with the California Environmental Quality Act of 1970, Public Resources Code Section 21000 et seq. (CEQA) and the California Code of Regulations Title 14, Division 6, Chapter 3 (the "CEQA Guidelines").

The 4<sup>th</sup> and Hewitt Project EIR, consisting of the Draft EIR and Final EIR, is intended to serve as an informational document for public agency decision-makers and the general public regarding the objectives and impacts of the 4<sup>th</sup> and Hewitt Project (Project), located at 900-926 East 4<sup>th</sup> Street; 406- 414 Colyton Street; 405-423 South Hewitt Street, (Project Site). The Project as analyzed in the EIR, proposes the demolition of an existing office building, two storage/garage buildings, and surface parking lots, to allow for the construction of an 18-story office building (Office Building) comprised of 8,149 square feet of ground floor restaurant space, 308,527 square feet of office, 16,249 square feet of covered exterior employee common areas and a 3,500 square-foot ground floor courtyard accessible from Colyton Street and Hewitt Street. The Project would total 340,770 square feet of gross floor area, comprised of an existing 7,800 square-foot building (the bow-truss building) and an 332,970 square-foot Office Building, on a 1.3-acre lot (a FAR of 6:1) and a building height of 292 feet to the top of the parapet. Vehicle parking would be provided within three subterranean levels and four levels of above grade parking.

The Draft EIR was circulated for a 47-day public comment period beginning on May 26, 2022 and ending on July 11, 2022. A Notice of Availability (NOA) was distributed on May 26, 2022 to all property owners within 500 feet of the Project Site and interested parties, which informed them of where they could view the document and how to comment. The Draft EIR was available to the public at the City of Los Angeles, Department of City Planning, and could be accessed and reviewed by members of the public by appointment with the Planning Department. Additionally, due to the circumstances created by the COVID-19 pandemic, copies of the Draft EIR were made available to the public on CD-ROM or in hard copy upon request to the Department of City Planning at the contact information listed on the NOA. A copy of the document was also posted online at <https://planning.lacity.org>. Notices were filed with the County Clerk on May 24, 2022 as result of the COVID-19 pandemic pursuant to the Governor's Executive Order No. N-54-40.

The Final EIR was then distributed in July 2023. The Deputy Advisory Agency certified the EIR on September 1, 2023 ("Certified EIR") in conjunction with the approval of the Project's Tract Map (VTT-74745). In connection with the certification of the EIR, the Deputy Advisory Agency adopted CEQA findings and a mitigation monitoring program. The Advisory Agency adopted the mitigation monitoring program in the EIR as a condition of approval. This decision was appealed in a timely manner on September 11, 2023 by Lozeau & Drury LLP on behalf of the Supporter's Alliance for Environmental Responsibility (SAFER) and the City Planning Commission denied the appeal and certified the EIR at its October 12, 2023 meeting. All mitigation measures in the Mitigation Monitoring Program are also imposed on the Project through Conditions of Approval of CPC-2017-469-GPA-VZC-MCUP-SPR, to mitigate or avoid significant effects of the Project on the environment and to ensure compliance during implementation of the Project.

### **NO SUPPLEMENTAL OR SUBSEQUENT REVIEW IS REQUIRED**

CEQA and the State CEQA Guidelines (California Code of Regulations, Title 14, Chapter 3, Sections 15000-15387) allow the City to rely on the previously certified EIR unless a Subsequent or Supplemental EIR is required. Specifically, CEQA Guidelines Sections 15162 and 15163

require preparation of a Subsequent or Supplemental EIR when an EIR has been previously certified or a negative declaration has previously been adopted and one or more of the following circumstances exist:

- 1) Substantial changes are proposed in the project which will require major revisions of the previous EIR or negative declaration due to the involvement of new significant environmental effects or a substantial increase in the severity of previously identified significant effects;
- 2) Substantial changes occur with respect to the circumstances under which the project is undertaken which will require major revisions of the previous EIR or negative declaration due to the involvement of new significant environmental effects or a substantial increase in the severity of previously identified significant effects; or
- 3) New information of substantial importance, which was not known and could not have been known with the exercise of reasonable diligence at the time the previous EIR was certified as complete or the negative declaration was adopted, shows any of the following:
  - A. The project will have one or more significant effects not discussed in the previous EIR or negative declaration;
  - B. Significant effects previously examined will be substantially more severe than shown in the previous EIR;
  - C. Mitigation measures or alternatives previously found not to be feasible would in fact be feasible and would substantially reduce one or more significant effects of the project, but the project proponents decline to adopt the mitigation measure or alternative; or Mitigation measures or alternatives which are considerably different from those analyzed in the previous EIR would substantially reduce one or more significant effects on the environment, but the project proponents decline to adopt the mitigation measure or alternative.

None of the above changes or factors has arisen since the approval of the Project. There are no substantial changes to the Project, and it is substantially the same as the approved project. No substantial changes have been identified to the surrounding circumstances, and no new information of substantial importance has been identified since the approval of the Project. There is no evidence of new or more severe significant impacts, and no new mitigation measures are required for the project.

Accordingly, there is no basis for changing any of the impact conclusions referenced in the certified EIR's CEQA Findings. Similarly, there is no basis for changing any of the mitigation measures referenced in the certified EIR's CEQA Findings, all of which have been implemented as part of the conditions of approval. There is no basis for finding that mitigation measures or alternatives previously rejected as infeasible are instead feasible. There is also no reason to change the determination that the overriding considerations referenced in the certified EIR's CEQA Findings, and each of them considered independently, continue to override the significant and unavoidable impacts of the Project.

Therefore, as the Project was assessed in the previously certified EIR, and pursuant to CEQA Guidelines Section 15162, no supplement or subsequent EIR or subsequent mitigated negative declaration is required, as the whole of the administrative record demonstrates that no major revisions to the EIR are necessary due to the involvement of new significant environmental effects or a substantial increase in the severity of a previously identified significant effect resulting from changes to the project, changes to circumstances, or the existence of new information. In addition,

no addendum is required, as no changes or additions to the EIR are necessary pursuant to CEQA Guidelines Section 15164.

## **RECORD OF PROCEEDINGS**

The record of proceedings for the decision includes the Record of Proceedings for the original CEQA Findings, including all items included in the case files, as well as all written and oral information submitted at the hearings on this matter. The documents and other materials that constitute the record of proceedings on which the City of Los Angeles' CEQA Findings are based are located at the Department of City Planning, 221 North Figueroa Street, Suite 1350, Los Angeles, CA 90021. This information is provided in compliance with CEQA Section 21081.6(a)(2).

In addition, copies of the Draft EIR, Final EIR, and Errata, are available on the Department of City Planning's website at <https://planning.lacity.org/project-review/environmental-review/published-documents> (to locate the documents, select "Environmental Impact Reports (EIRs)" and search for the environmental case number).

## **PUBLIC HEARING AND COMMUNICATIONS**

### **Public Hearing**

A joint public hearing was held virtually via Zoom by the Deputy Advisory Agency (DAA) and Hearing Officer on behalf of the City Planning Commission on August 16, 2023 for the Project entitlements and subdivision and was attended by approximately 70 individuals. At the public hearing, testimony was provided by the Project team; 25 people spoke, of which 21 were in favor of the Project, including the Southwest Mountain States Regional Council of Carpenters (SWMSRCC), 3 commenters were in opposition including Supporters Alliance for Environmental Responsibility (SAFER), and one commenter who spoke on behalf of Coalition of Responsible Equitable Economic Development (CREED LA) noted they had submitted a comment on the Draft EIR, were reviewing the Final EIR to ensure their comments were addressed.

#### Summary of Public Hearing Testimony

At the hearing, the Project team presented the following information:

- Project Background;
- Overview of Project entitlement requests;
- Overview of the Project environmental impacts;
- Overview of the Tract Map request;
- Overview of community outreach;
- Overview of the commercial uses and design elements; and
- The ground floor activation through use of the ground floor paseo and restaurant spaces.

Support for the Project focused on the following:

- Commitment to local higher
- Compatibility of the Project's design with the Arts Districts characteristics
- The Project would provide construction and operational jobs during development and operation of the Project;
- The addition of ground floor restaurant space and pedestrian paseo will attract additional visitors to the Arts District neighborhood and increase pedestrian safety in the neighborhood
- The new uses will create new jobs for the neighborhood; and people would be able to live and work in the area and reduce traffic and greenhouse gas emissions
- Will attract world class employers to the Arts District

Opposition focused on the following:

- Lack of compatibility of the Project's design with the Arts Districts characteristics
- Negative impact on the environment and public health
- Concerns about the impacts on air quality, greenhouse gases and biological resources
- The Project would not adopt all feasible mitigation measures regarding air quality, biological, and greenhouse gas impacts
- The Project would result in significant and unavoidable noise impacts
- The City has not identified the Project's economic benefits

### **Communications Received**

Prior to the public hearing, a comment letter was submitted by Lozeau and Drury, LLP on behalf of SAFER. The letter provided comments on a variety of environmental topics, including air

quality, biological resources, greenhouse gas emissions, and public health and included a technical letter from Matt Hagemann, P.G., C.Hg. and Paul E. Rosenfeld, Ph.D. of Soil/Water/Air Projection Enterprise (SWAPE). The City reviewed the comment letter (dated August 15, 2023), and provided written responses to the comment, available as part of the City's administrative case file. The City determined that the comments do not result in any new significant environmental impacts or a substantial increase in any of the severity of significant impacts identified in the Draft EIR.

Following the public hearing six comments were received via email in opposition to the Project and one of the comments included a link to an online petition opposing the Project. The commenters raised concerns with the Office Building's height, including the introduction of a new high-rise, proposed office use, design including the massing and scale, traffic congestion and number of parking levels, the condition of existing infrastructure, glare from the proposed materials. The petition noted similar concerns, specifically regarding the Office Building's height the proposed office use and concern over the potential for future office space to remain vacant.

**EXHIBIT A**  
**CPC-2017-469-GPA-VZC-HD-MCUP-SPR**  
**October 12, 2023**



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## ENTITLEMENT SET

OCTOBER 12, 2023

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**SHEET INDEX**

SHEET NUMBER	SHEET NAME	REV
0- GENERAL		
G0.000	COVER	•
G0.002	SHEET INDEX	•
G0.003	PROJECT INFORMATION/ ARCHITECTURAL NOTES	•
G0.004	DEVELOPMENT SUMMARY	•
G0.100	PROJECT INFORMATION/ ABBREVIATIONS/ ARCHITECTURAL SYMBOLS	•
G0.901	FLOOR AREA PLANS	•
G0.902	FLOOR AREA PLANS	•
G0.903	FLOOR AREA PLANS	•
G0.904	FLOOR AREA PLANS	•
1-CIVIL		
C1.0	PRELIMINARY GRADING AND DRAINAGE	•
C1.1	VESTING TENTATIVE TRACT NO. 74745	•
C1.2	VESTING TENTATIVE TRACT NO. 74745	•
C1.3	VESTING TENTATIVE TRACT NO. 74745	•
C1.4	VESTING TENTATIVE TRACT NO. 74745	•
2 - DEMOLITION		
DM0.100	DEMOLITION PLAN	•
3 - ARCHITECTURE		
A0.101	EAST ELEVATION	•
A0.102	SOUTH ELEVATION	•
A0.103	WEST ELEVATION	•
A0.104	NORTH ELEVATION	•
A1.2B1	PLAN - OVERALL - BASEMENT 1	•
A1.2B2	PLAN - OVERALL - BASEMENT 2	•
A1.2B3	PLAN - OVERALL - BASEMENT 3	•
A1.200	PLOT PLAN	•
A1.201	PLAN - OVERALL - GROUND FLOOR	•
A1.201.M	PLAN - OVERALL - GROUND FLOOR MEZZANINE	•
A1.202	PLAN - OVERALL - LEVEL 2	•
A1.203	PLAN - OVERALL - LEVEL 3	•
A1.204	PLAN - OVERALL - LEVEL 4	•
A1.205	PLAN - OVERALL - LEVEL 5	•
A1.206	PLAN - OVERALL - LEVEL 6	•
A1.206.M	PLAN - OVERALL - LEVEL 6 MEZZANINE	•
A1.207	PLAN - OVERALL - LEVEL 7	•
A1.208	PLAN - OVERALL - LEVEL 8	•
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A1.215	PLAN - OVERALL - LEVEL 15	•
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A2.202	SECTION - TRANSVERSAL AT ELEVATOR LOBBY	•
A2.203	SECTION - LONGITUDINAL AT CORE	•
4 - LANDSCAPE		
L0.01	LANDSCAPE NOTES, SYMBOLS AND ABBREVIATIONS	•
L1.01	LANDSCAPE SITE PLAN	•
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L2.01	SITE SECTIONS	•
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L4.01	COURTYARD PLAN ENLARGEMENT	•
L4.02	AMENITY LEVEL PLANTING	•
L4.03	AMENITY LEVEL PLANTING	•
L5.01	REFER TO DETAIL FOR SCALE	•
L5.02	REFER TO DETAIL FOR SCALE	•

**EXHIBIT A**  
**CPC-2017-469-GPA-VZC-HD-MCUP-SPR**  
**October 12, 2023**

Date	Description
6 05/31/2019	ENTITLEMENT SET
7 10/02/2019	ENTITLEMENT SET
8 04/07/2020	ENTITLEMENT SET
11 06/16/2023	ENTITLEMENT SET
12 10/12/2023	ENTITLEMENT SET

Seal / Signature

**NOT FOR CONSTRUCTION**

Project Name  
401 S Hewitt

Project Number  
05.1291.000

Description  
SHEET INDEX

Scale

**G0.002**





AREA CALCULATIONS PER LEVEL

PROJECT FLOOR AREA CALCULATION						
	Existing Structure FA	Restaurant FA	Office Covered Balconies (FA)	Office FA	Floor to Floor	Elevation
Elevator Overrun						297.0'
18 - Mechanical Floor				1,025 sf	23.0'	274.0'
17				10,930 sf	18.0'	256.0'
16				24,177 sf	14.5'	241.5'
15			779 sf	24,675 sf	14.5'	227.0'
14			1,689 sf	24,019 sf	14.5'	212.5'
13			1,735 sf	24,019 sf	14.5'	198.0'
12			956 sf	24,786 sf	14.5'	183.5'
11			956 sf	24,786 sf	14.5'	169.0'
10			983 sf	22,949 sf	14.5'	154.5'
9			2,819 sf	22,882 sf	14.5'	140.0'
8			524 sf	24,899 sf	14.5'	125.5'
7			524 sf	24,899 sf	14.5'	111.0'
6 Mezzanine				7,090 sf	11.5'	99.5'
6			5,329 sf	20,275 sf	11.5'	88.0'
5				592 sf	17.0'	71.0'
4				592 sf	17.0'	54.0'
3				592 sf	17.0'	37.0'
2				592 sf	17.0'	20.0'
Ground Mezzanine				2,025 sf	10.0'	10.0'
Ground	7,800 sf	8,149 sf		21,307 sf	10.0'	0.0'
B1				540 sf	11.0'	-11.0'
B2				438 sf	9.0'	-20.0'
B3				438 sf	9.0'	-29.0'

<b>TOTAL</b>	<b>7,800 sf</b>	<b>8,149 sf</b>	<b>16,294 sf</b>	<b>308,527 sf</b>
<b>TOTAL FLOOR AREA</b>	<b>340,770 sf</b>			

OVERALL PROGRAM CALCULATIONS - FAR & PARKING

OVERALL PROJECT CALCULATION			
Site Baseline	Site Area	FAR	Total Floor Area
Existing Structure to Remain	17,418 sf	6.00	104,508 sf
4th/Hewitt Property	39,377 sf	6.00	236,262 sf
<b>Total</b>	<b>56,795 sf</b>	<b>6.00</b>	<b>340,770 sf</b>

Overall Program		Total Floor Area
Existing Structure to Remain		7,800 sf
Restaurant		8,149 sf
Office		308,527 sf
Office (Covered Exterior)		16,294 sf
<b>Total</b>		<b>340,770 sf</b>

Parking Reduction Per Bicycles		
Parking Required by 2/1000 Ratio		682 stalls
<b>Parking Required based upon AB2097</b>		<b>0 stalls</b>
Bicycle Required Office Long Term 1/5,000		65 bikes
Bicycle Required Restaurant Long Term 1/2,000		4 bikes
<b>Long Term Bicycle Required</b>		<b>69 bikes</b>
Bicycle Required Office Short Term 1/10,000		32 bikes
Bicycle Required Restaurant Short Term 1/2,000		4 bikes
<b>Short Term Bicycle Required</b>		<b>37 bikes</b>
<b>Total Bicycle Required</b>		<b>106 bikes</b>
Bicycle Provided Long Term		72 bikes
Bicycle Provided Short Term		40 bikes
<b>Total Bicycle Provided</b>		<b>112 bikes</b>
Parking Reduction By Bicycle provided 1/4 ratio		28 stalls
<b>Parking Minimum Requirement</b>		<b>0 stalls</b>

Detailed Parking Requirement Calculation		
Standard Stalls Required: 60% of Total Parking		0 stalls
Standard Stalls (including ADA) provided		472 stalls
ADA Parking Stalls Required 2%		13 stalls
<b>ADA Parking Stalls Provided</b>		<b>14 stalls</b>
Minimum EV Charging Stations Required 10%		66 stalls
<b>EV Charging Stations Provided</b>		<b>66 stalls</b>
EV Spaces Required 30%		198 stalls
<b>EV Spaces Provided</b>		<b>198 stalls</b>

	Parking Provided Per Level			Overall
	Standard	Compact	ADA	
Level 5	57 stalls	20 stalls	3 stalls	80 stalls
Level 4 (WITH STACKERS)	63 stalls	23 stalls	3 stalls	89 stalls
Level 3 (WITH STACKERS)	81 stalls	33 stalls	3 stalls	117 stalls
Level 2 (WITH STACKERS)	82 stalls	32 stalls	3 stalls	117 stalls
Ground	0 stalls	0 stalls	0 stalls	0 stalls
B1	56 stalls	28 stalls	2 stalls	86 stalls
B2	58 stalls	28 stalls	0 stalls	86 stalls
B3	61 stalls	28 stalls	0 stalls	85 stalls
<b>Totals</b>	<b>458 stalls</b>	<b>192 stalls</b>	<b>14 stalls</b>	<b>660 stalls</b>

### ZIMAS

**Identify Parcel**

**Address/Legal**

Site Address: 407 S HEWITT ST  
 Site Address: 411 S HEWITT ST  
 ZIP Code: 90011  
 PIN Number: 127-54215-259  
 Lot/Parcel Area (Calculated): 8,103.8 (sq ft)  
 Thomas Brothers, Inc. PLOT 84 - GRID H5  
 Assessor Parcel No. (APN): 418020202  
 Tract: P P HOWARD AND CO'S SUBDIVISION OF THE BLISS TRACT  
 Map Reference: M R 12-42  
 Block: E  
 Lot: FR 8  
 Arb (Lot Out Reference): None  
 Map Sheet: 372-24312

**Jurisdictional**

**Permitting and Zoning Compliance**

**Planning and Zoning**

Special Notes: None  
 Zoning: M2-LRDP  
 Zoning Information (Z): 41-242 Local Emergency Temporary Regulations - Time Limits and Parking Requirements 10.02.1  
 Zoning Information (Z): 41-242 State Enterprise Zone, East Los Angeles  
 Zoning Information (Z): 41-242 River Implementation Overlay District (R2)  
 Zoning Information (Z): 41-242 Transit Priority Area in the City of Los Angeles  
 Zoning Information (Z): 41-242 Transit Priority Area in the City of Los Angeles  
 General Plan Land Use: Heavy Manufacturing  
 General Plan (Notes): XAA  
 Hillside Area (Zoning Code): N/A  
 Specific Plan Area: None  
 Subarea: None  
 Special Land Use / Zoning: None  
 Historic Preservation Review: No  
 Historic Preservation: XAA  
 CDC Community Design Overlay: None  
 CPIO Community Plan Imp: None  
 Quarterly: Subarea: None  
 CIOU Clean Up/Stream Up: None  
 HCR Historic Construction: No  
 Regulation: No  
 NOD Neighborhood Stabilization: No  
 Overlay: None  
 POC Pedestrian Oriented District: None  
 RBP Restaurant Beverage Program Eligible Area: None  
 RPA Residential Floor Area: None  
 District: None  
 R2 River Implementation Overlay: Yes  
 SN Sign District: No  
 SB 9 Eligibility: Yes  
 SB 15 Eligibility: Yes  
 AB 2334 Very Low VMT: Yes  
 AB 2097 Reduced Parking Areas: Yes  
 Streetscape: No  
 Adaptive Reuse Incentive Area: None  
 Affordable Housing Incentive Fee: Residential Market Area: Medium-High  
 Non-Residential Market Area: Medium  
 Trans Oriented Communities (TOC): 24.3  
 TOD: Not Eligible  
 RPA Redevelopment Project Area: None  
 Central City Parking: Yes  
 Downtown Parking: No  
 Building Line: None  
 800 Ft School Zone: No  
 500 Ft Park Zone: No

DESCRIPTION: AB 2097 Reduced Parking Requirements

Pursuant to Assembly Bill (AB) 2097, the City of Los Angeles is prohibited from imposing or enforcing minimum parking requirements on any residential, commercial or other development project (excluding event centers, hotels and similar transient lodging) that are within a one-half mile radius of a Major Transit Stop. The Department of City Planning issued a memorandum on December 31, 2022 which serves as guidance for project applicants and staff on the implementation of AB 2097.

AB 2097 eligibility is shown in ZIMAS within the Planning and Zoning tab under the Table of Contents and is titled "AB 2097: Reduced Parking Area". Eligible properties will have a "Yes" populated in the "AB 2097: Reduced Parking Area" field. To utilize AB 2097, the applicant shall provide a copy of the ZIMAS parcel profile report, including the AB 2097: Reduced Parking Area field showing eligibility, at the time of filing an application with the Department of Building and Safety for a ministerial approval or with the Department of City Planning for an administrative or discretionary approval. Verification of eligibility shall take place at the time an application is accepted and fees are paid.

Note: AB 2097 eligibility in ZIMAS is periodically updated as bus routes are updated. If more than 180 days have transpired from the date shown on the ZIMAS AB 2097 map and when application fees are paid, the project applicant will need to produce a more recent date-stamped ZIMAS parcel property report to be able to utilize reduced parking requirements per AB 2097.

Available Documents:  
 AB 2097 DCP Memorandum

SHEET NOTES

**EXISTING SITE INFORMATION**  
 Existing Land Use Dest.: Heavy Industrial / Heavy Manufacturing  
 Existing Zoning: M3  
 Existing Height District: Height District No. 1  
 Applicable Plans: Central City North Comm. Plan  
 River Improv. Overlay (RIO)  
 LA State Enterprise Zone

**PROPOSED SITE INFORMATION**  
 Proposed Land Use Dest.: Regional Center  
 Proposed Zoning: C2  
 Proposed Height District: Height District No. 2  
 Applicable Plans: Central City North Comm. Plan  
 River Improv. Overlay (RIO)  
 LA State Enterprise Zone

**FAR AREA DEFINITION**  
 The floor area ratio (FAR) is a ratio establishing relationship between a property and the amount of development permitted for that property, and is expressed as a percentage or a ratio of the buildable Area or Lot size.

**FLOOR AREA DEFINITION**  
 The area in square feet confined within the exterior walls of a building, but not including the area of the following: exterior walls, stairways, shafts, rooms housing Building-operating equipment or machinery, parking areas with associated driveways and ramps, space dedicated to bicycle parking, space for the landing and storage of helicopters, and Basement storage areas.



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555 Flower St., Suite 4300  
 Los Angeles, CA 90071  
 Tel 213.223.1400

423 Gin Ling Way  
 Los Angeles, CA 90012  
 Tel 213.234.0057

Date	Description
1 02/06/2017	ENTITLEMENT SET
2 07/14/2017	ENTITLEMENT SET
3 10/25/2017	ENTITLEMENT SET
6 05/31/2019	ENTITLEMENT SET
7 10/02/2019	ENTITLEMENT SET
8 04/07/2020	ENTITLEMENT SET
9 03/04/2022	ENTITLEMENT SET
10 03/16/2022	ENTITLEMENT SET
11 06/16/2023	ENTITLEMENT SET
12 10/12/2023	ENTITLEMENT SET

**EXHIBIT A**  
**CPC-2017-469-GPA-VZC-HD-MCUB-SPR**  
**October 12, 2023**

GENERAL NOTES

Seal / Signature

**NOT FOR CONSTRUCTION**

Project Name

401 S Hewitt

Project Number

05.1291.000

Description

DEVELOPMENT SUMMARY

Scale

**G0.004**

GRAPHICS SYMBOLS

REFLECTED CLG

Table of symbols for Reflected Clg including ACOUS CLG AND GRID, CLG HEIGHT CHANGE, FIN CLG HEIGHT SYMBOL, GRID STARTPOINT SYMBOL, CLG FINISH TAG, MOTION SENSOR, CLG MTD SPEAKER, CLG MTD CAMERA, CLG MTD SPRINKLER HEAD, CLG MTD SMOKE DETECTOR, CLG MTD STROBE LT, WALL MTD EXIT SIGNS - PARALLEL, WALL MTD EXIT SIGNS - DENOTES EXISTING TO REMAIN, DENOTES EXIST TO BE RELOCATED, ACCESS DOOR, MECHANICAL FXTRS, RETURN AIR, SUPPLY AIR, CIRCULAR DIFFUSER, LINEAR DIFFUSER, EXHAUST FAN.

POWER & COMM. CONT

Table of symbols for Power & Comm. Cont including Legend of Common Symbol Modifiers, Legend of Common Symbols, Security Devices, and Wall MTD Devices.

SECTION INDICATIONS

Table of section indications including Acoustical Ceiling Tile, Aluminum, Brick, Carpet, Concrete, Concrete Masonry Unit, Cut Stone, Earth, Fabric Wrapped Panel, Glass, Gravel, Gypsum Plaster, Insulation (Loose or Batt), Insulation (Rigid), Metal, Plastic, Plywood, Pre-cast Panels, Sand or Grout, Stone, Wood (Finished), Wood (Continuous Member), Wood (Interrupted Member).

CONSTRUCTION

Table of construction symbols including Column Grid Reference Number, Millwork, Sheet Number, Area to be Detailed, Revision Reference Number, Sheetnote Reference, Wall Type Reference, Door Reference Number, Door Type, Door Number, Door Type | Hardware Type, Millwork Reference Number, Elevation Datum Reference, Match Line Symbols, Fire Warden Station Symbol, Fire Alarm Strobe Symbol, Fire Alarm Pull Symbol, Fire Extinguisher Cabinet, Fire Extinguisher, Fire Hose Cabinet, Fire Hose Cabinet, Fire Hose Cabinet, Fire Hose Cabinet.

FINISH

Table of finish symbols including Wall Finish Tag, Special Finish Tag, Floor Finish Tag, Ceiling Finish Tag, Change in Floor Finish.

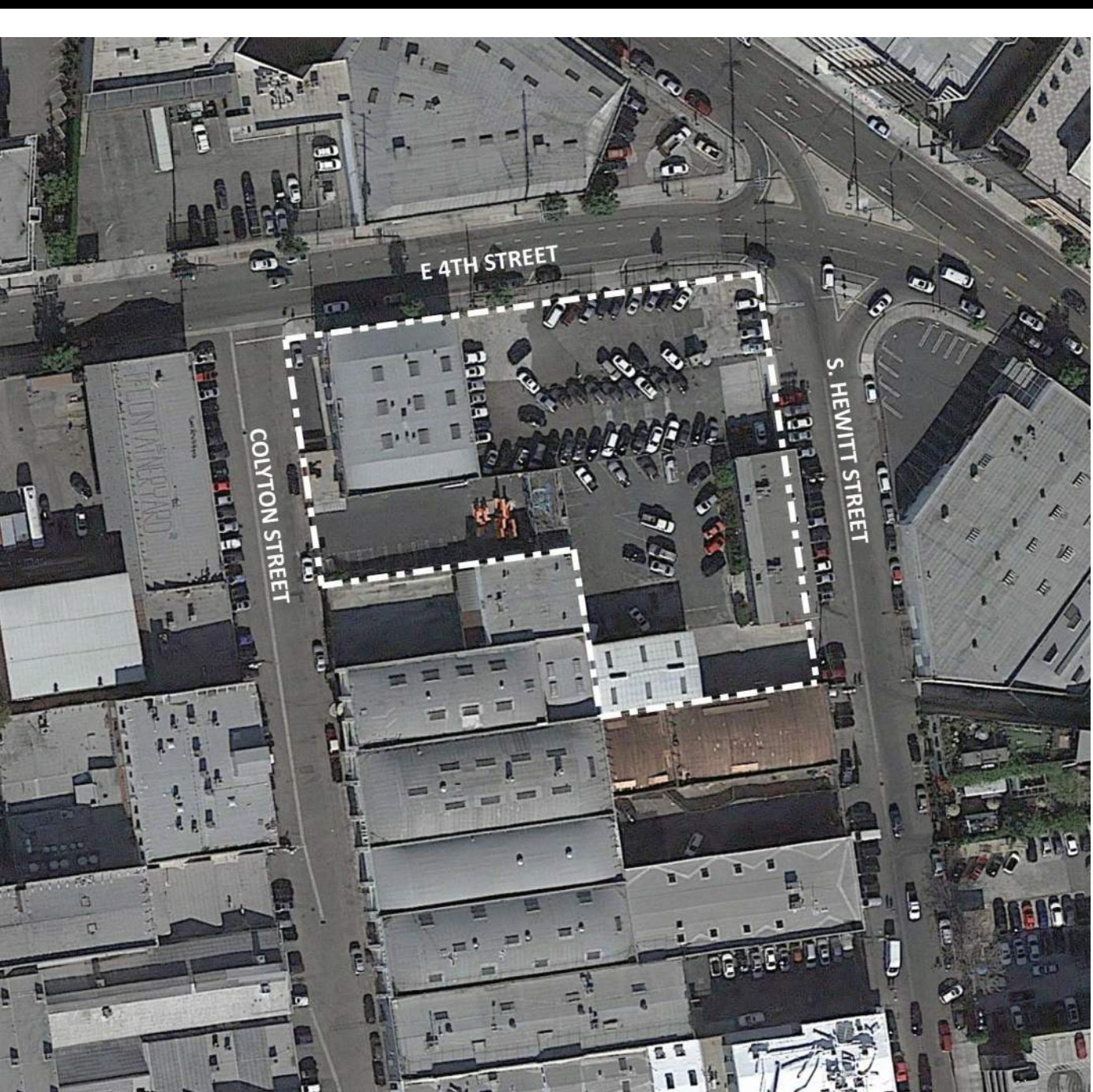
ABBREVIATIONS

Comprehensive list of abbreviations including ACCESSORY, ABOVE FINISHED FLOOR, ALUMINUM, ALTERNATE, ANNUNCIATOR, ANODIZED, APPLIANCE, AUTOMATIC, AVERAGE, BOARD, BUILDING, BLOCKING, BOLLARD, BROADLOOM, BUILT UP, CABINET, CEMENT, CERAMIC, CEILING, CLEAR, CONCRETE MASONRY UNIT, COATING, COILING, CONCRETE, CONSTRUCTION, CONTINUOUS, CONTRACTOR, COVER, CARPET, DOUBLE, DEPARTMENT, DESIGN, DETAIL, DRINKING FOUNTAIN, DIAMETER, DIFFUSER, DIMENSION, DISPENSER, DIVISION, DOWN, DOOR, DISCONNECT, DRAWER, EXISTING, ELASTOMERIC, ELECTRICAL, EMBEDDED, ENGINEER, ENTRANCE, EQUAL, EQUIPMENT, EXPANSION JOINT, EXPOSED, EXTERIOR, FABRICATION, FLOOR DRAIN, FIRE EXTINGUISHER, FIRE EXTINGUISHER AND CABINET, FIRE HOSE CABINET, FINISH, FOLDING, FLOORING, FIREPLACE, FIRE RATING, FRAMING, FURNITURE, FABRIC WALL COVERING, FIXED, FIXTURE, GAUGE, GLASS FIBER REINFORCED CONCRETE, GLASS FIBER REINFORCED GYPSUM, GLASS FIBER REINFORCED PLASTER, GLASS, GRADIENT, GYPSUM, HEAD, HARDWOOD, HARDWARE, HOLLOW METAL, HORIZONTAL, HIGH POINT, HEATING, VENTILATING, AND AIR CONDITIONING, INFILTRATION, INFORMATION, INSTRUMENT, INSULATION, INTERIOR, INTERLOCKING, JANITOR, KIT, KITCHEN, LAVATORY, POUND, LEASE LINE, LOUVER, LOW POINT, LIGHT, LEVELING, MAXIMUM MECHANICAL MEMBRANE, METAL, MEZZANINE, MANUFACTURED MANUFACTURER, MINIMUM MISCELLANEOUS, MILLWORK, MOISTURE, MOTORIZED, MOUNTED, NOT IN CONTRACT, NO NUMBER, NOT TO SCALE, ON CENTER, OVER, OCCUPANT, OPPOSITE HAND, OPENING, OPPOSITE, OPERABLE, ORNAMENTAL, OVERFLOW, OVERHEAD, PARTICLE BOARD, PEDESTRIAN, PLASTIC LAMINATE, PLASTER, PLASTIC, PLYWOOD, PANEL, POLYSTYRENE, PORTABLE, PREFABRICATED, PREFINISHED, PROTECTION, PARTITION, READER, RECESS, RECEPTACLE, REFERENCE, REFLECTED, REFRIGERATOR, REQUIRED, RESISTANT, ROOFING, ROOM, ROOF DRAIN, ROUGH OPENING, SCRIBE, SECURITY SQUARE FEET, SINGLE, SHORING, SHORING, SIMILAR, STAINLESS STEEL, STANDARD, STEEL, STOREFRONT, STRUCTURAL, SURFACE, SUSPENDED, SYSTEM, TONGUE AND GROOVE, TO BE DETERMINED, THICK, TOILET, TOP OF STEEL, TRAFFIC, TRANSPARENT, TREATED, TYPICAL, UNDERLAYMENT, UNLESS NOTED OTHERWISE, UTILITY, VEHICLE, VERTICLE, VERIFY IN FIELD, WITH, WITHOUT, WATER CLOSET, WOOD, WINDOW, WEIGHT, WATERPROOFING.

VICINITY MAP



LOCATION MAP



CODE INFORMATION

Table of applicable building codes including Building Code (2017 Los Angeles Building Code), Electrical Code (2016 California Electrical Code), Mechanical Code (2016 California Mechanical Code), Plumbing Code (2016 California Plumbing Code), Energy Code (2016 California Energy Code), Sustainability Code (2016 California Green Building Standards), Fire Code (2016 California Fire Code), and Fire-resistance rating requirements for building elements.

PROJECT DESCRIPTION

PROPOSED PROJECT INFORMATION: THE PROJECT WOULD CONSIST OF THE EXISTING STRUCTURE TO REMAIN AND THE REMAINDER OF THE PROPERTY THAT IS TO BE REDEVELOPED. REDEVELOPED PORTION OF THE PROPERTY WOULD INCLUDE NEW CONSTRUCTION OF A COMMERCIAL BUILDING THAT WOULD BE LOCATED AT THE CORNER OF 4TH AND HEWITT STREETS AND AN OUTDOOR PLAZA ALONG COLYTON STREET. THE COMMERCIAL BUILDING WOULD CONSIST OF GROUND FLOOR COMMERCIAL USES, OFFICE USE ON THE 6TH THROUGH 17TH FLOORS, PARKING ON THREE SUBTERRANEAN FLOORS, AND MECHANICAL EQUIPMENT LOCATED ON THE MECHANICAL ROOF. THE NEW GROUND FLOOR COMMERCIAL USES WOULD WRAP AROUND 4TH AND HEWITT STREETS WITH FRONTAGE AND ACCESS FROM BOTH STREETS. THE PROJECT WOULD INCLUDE APPROXIMATELY 687 PARKING SPACES. THE PARKING WOULD BE LOCATED ON THREE SUBTERRANEAN LEVELS AND ON THE 2ND THROUGH 5TH FLOORS. IN ADDITION, THE PROJECT WOULD ALSO PROVIDE 112 BICYCLE PARKING SPACES, COMPRISED OF 40 BICYCLE SPACES FOR SHORT TERM USE AND 72 FOR LONG TERM USE. TOP OF THE COMMERCIAL BUILDING IS AT THE 17TH FLOOR, 254 FEET ABOVE FINISHED GRADE AND THE TOP OF THE OVERRUN AT 295 FEET ABOVE FINISHED GRADE.

EXHIBIT A
CPC-2017-469-GPA-VZC-HD-MCUB-SPR
October 12, 2023

LEGENDARY INVESTORS GROUP, LLC
401 S. Hewitt, Los Angeles, CA 90013
Gensler
500 South Figueroa Street, Los Angeles, California 90071
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WALTER P MOORE
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ARC engineering
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Tel 818.508.6300 Fax 818.508.7050
PSOMAS
555 Flower St., Suite 4300, Los Angeles, CA 90071
Tel 213.223.1400
SALT
423 Gin Ling Way, Los Angeles, CA 90012
Tel 213.234.0057
Date Description
6 05/31/2019 ENTITLEMENT SET
7 10/02/2019 ENTITLEMENT SET
8 04/07/2020 ENTITLEMENT SET
10 03/16/2022 ENTITLEMENT SET
11 06/16/2023 ENTITLEMENT SET
12 10/12/2023 ENTITLEMENT SET
Seal / Signature
Project Name
401 S Hewitt
Project Number
05.1291.000
Description
PROJECT INFORMATION/ ABBREVIATIONS/ ARCHITECTURAL SYMBOLS
Scale
NOT TO SCALE
G0.100
© 2017 Gensler







Date	Description
6 05/31/2019	ENTITLEMENT SET
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8 04/07/2020	ENTITLEMENT SET
9 03/04/2022	ENTITLEMENT SET
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**SHEET NOTES**

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**CPC-2017-469-GPA-VZC-HD-MCUB-SPR**  
**October 12, 2023**

**GENERAL NOTES**

Seal / Signature

**NOT FOR CONSTRUCTION**

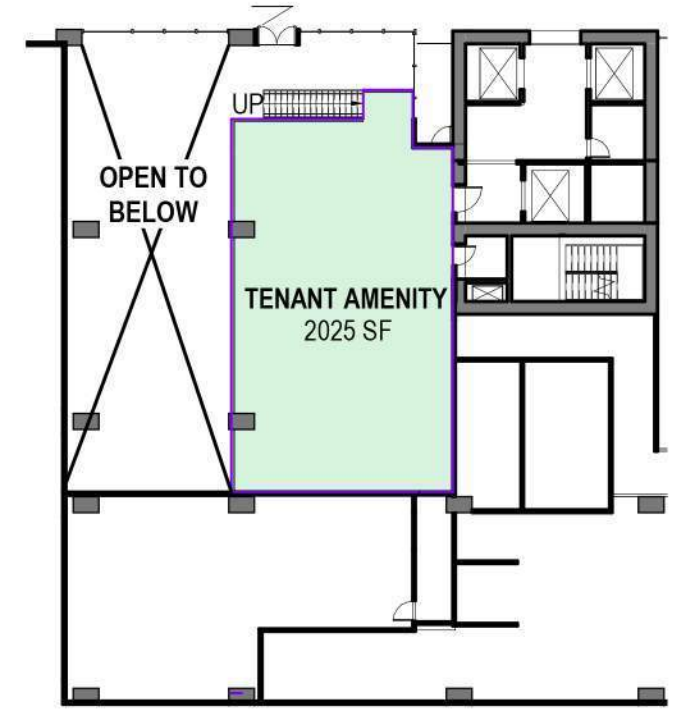
Project Name  
401 S Hewitt

Project Number  
05.1291.000

Description  
FLOOR AREA PLANS

Scale  
1" = 30'-0"

**G0.901**



Name	Level	Area
TENANT AMENITY	GROUND LEVEL MEZZANINE	2,025 SF

Name	Level	Area
FOOD & BEVERAGE	GROUND LEVEL	8,149 SF
EXISTING STRUCTURE TO REMAIN	GROUND LEVEL	7,800 SF
OFFICE	GROUND LEVEL	18,882 SF
TENANT AMENITY	GROUND LEVEL	2,425 SF

**GROUND LEVEL MEZZANINE**  
1" = 30'-0" SCALE: **5**

**GROUND LEVEL**  
1" = 30'-0" SCALE: **4**



Name	Level	Area
OFFICE	BASEMENT 2	438 SF

**BASEMENT 2**  
1" = 30'-0" SCALE: **2**



Name	Level	Area
OFFICE	BASEMENT 1	540 SF

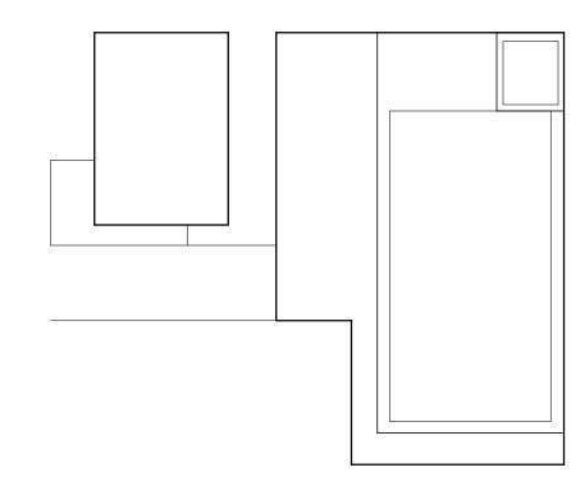
**BASEMENT 1**  
1" = 30'-0" SCALE: **3**

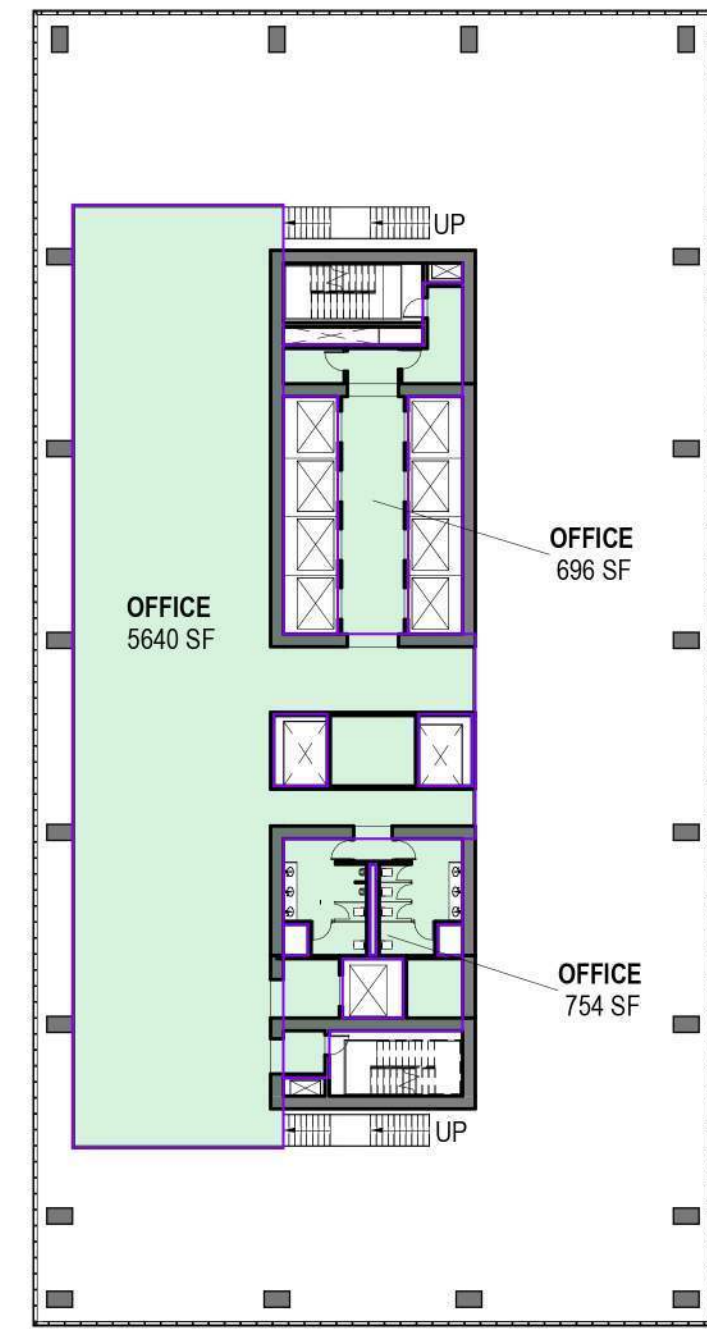


Name	Level	Area
OFFICE	BASEMENT 3	438 SF

**BASEMENT 3**  
1" = 30'-0" SCALE: **1**

**KEY PLAN**





Gross Floor Area Schedule (Level 6 Mezzanine)		
Name	Level	Area
OFFICE	LEVEL 6 MEZZANINE	7,090 SF

**LEVEL 6 MEZZANINE** 6  
1" = 30'-0" SCALE



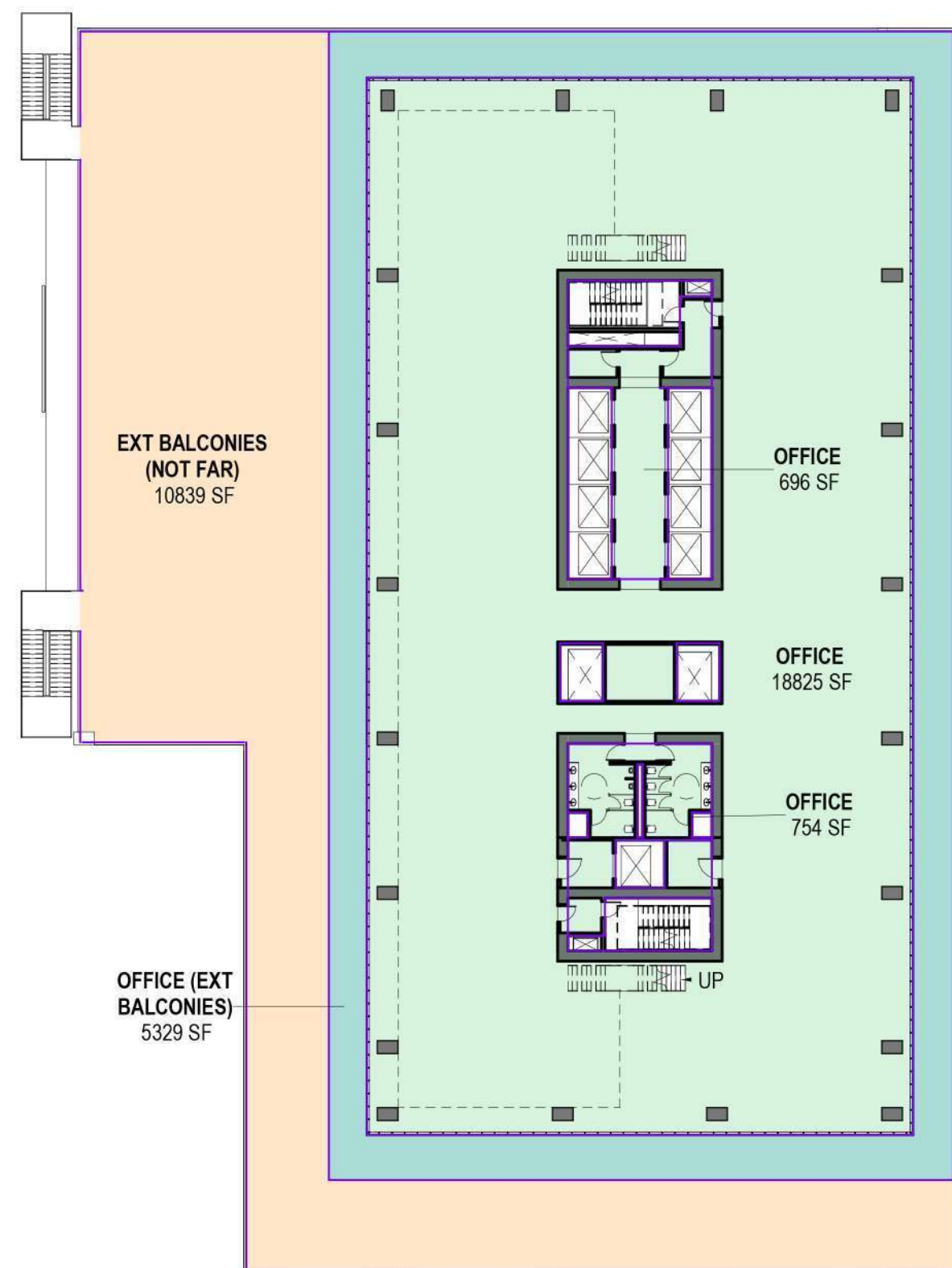
Gross Floor Area Schedule (Level 5)		
Name	Level	Area
OFFICE	LEVEL 5	592 SF

**LEVEL 5** 4  
1" = 30'-0" SCALE



Gross Floor Area Schedule (Level 3)		
Name	Level	Area
OFFICE	LEVEL 3	592 SF

**LEVEL 3** 2  
1" = 30'-0" SCALE



Gross Floor Area Schedule (Level 6)		
Name	Level	Area
EXT BALCONIES (NOT FAR)	LEVEL 6	10,839 SF
OFFICE	LEVEL 6	20,275 SF
OFFICE (EXT BALCONIES)	LEVEL 6	5,329 SF

**LEVEL 6** 5  
1" = 30'-0" SCALE



Gross Floor Area Schedule (Level 4)		
Name	Level	Area
OFFICE	LEVEL 4	592 SF

**LEVEL 4** 3  
1" = 30'-0" SCALE



Gross Floor Area Schedule (Level 2)		
Name	Level	Area
OFFICE	LEVEL 2	592 SF

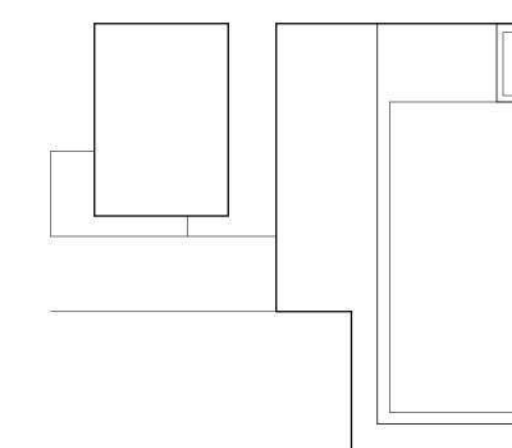
**LEVEL 2** 1  
1" = 30'-0" SCALE

**SHEET NOTES**

**EXHIBIT A**  
**CPC-2017-469-GPA-VZC-HD-MCUB-SPR**  
**October 12, 2023**

**GENERAL NOTES**

**KEY PLAN**



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**Gensler**

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Los Angeles, CA 90071  
Tel 213.223.1400



803 Broadway, Suite 600  
Los Angeles, CA 90014  
Tel 213.328.7489

Date	Description
6 05/31/2019	ENTITLEMENT SET
7 10/02/2019	ENTITLEMENT SET
8 04/07/2020	ENTITLEMENT SET
9 03/04/2022	ENTITLEMENT SET
11 06/16/2023	ENTITLEMENT SET
12 10/12/2023	ENTITLEMENT SET

Seal / Signature

**NOT FOR CONSTRUCTION**

Project Name  
401 S Hewitt

Project Number  
05.1291.000

Description  
FLOOR AREA PLANS

Scale  
1" = 30'-0"

**G0.902**

Date	Description
6 05/31/2019	ENTITLEMENT SET
7 10/02/2019	ENTITLEMENT SET
8 04/07/2020	ENTITLEMENT SET
9 03/04/2022	ENTITLEMENT SET
11 06/16/2023	ENTITLEMENT SET
12 10/12/2023	ENTITLEMENT SET

**SHEET NOTES**

**EXHIBIT A**  
**CPC-2017-469-GPA-VZC-HD-MCUB-SPR**  
**October 12, 2023**

**GENERAL NOTES**

Seal / Signature

**NOT FOR CONSTRUCTION**

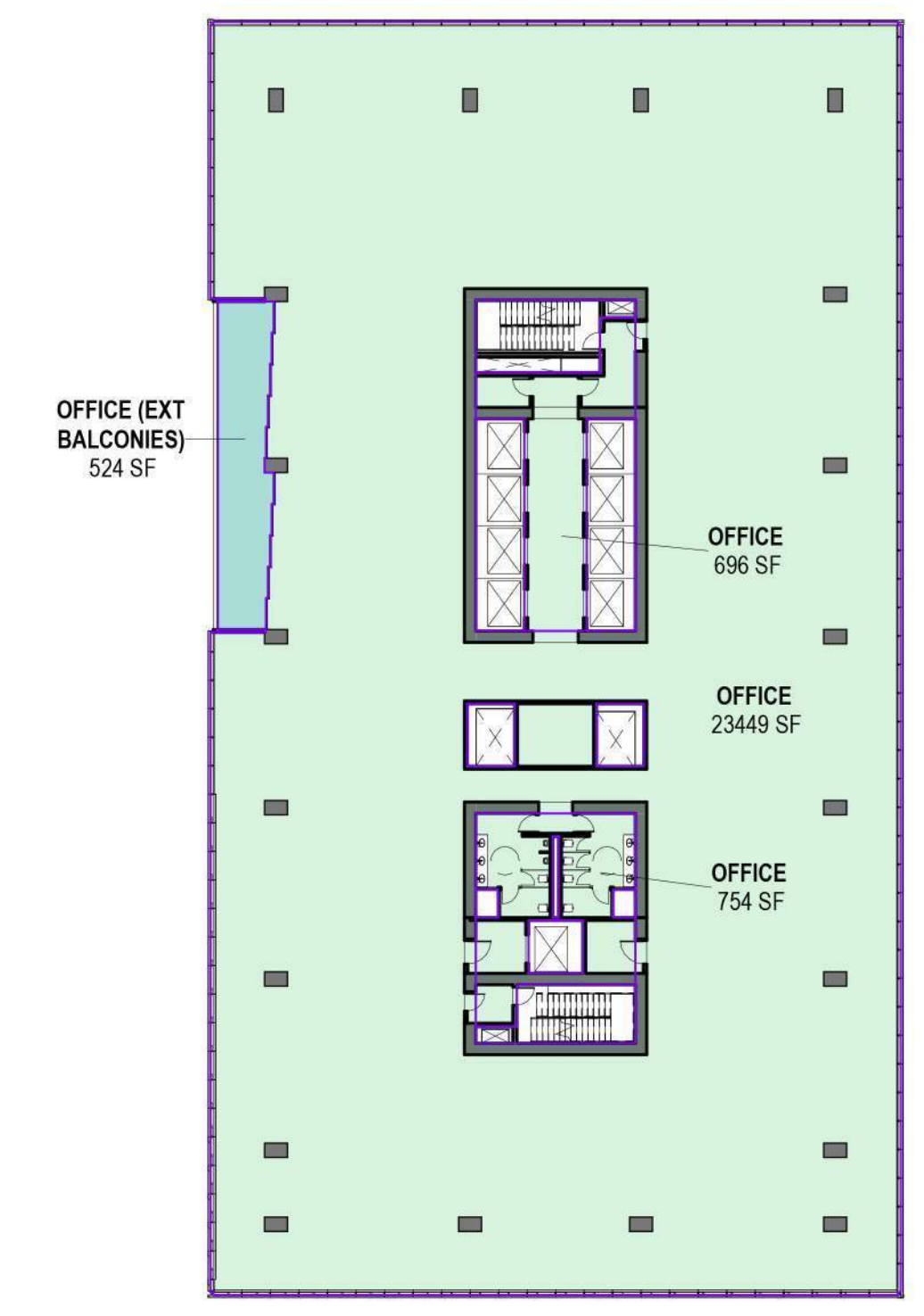
Project Name  
401 S Hewitt

Project Number  
05.1291.000

Description  
FLOOR AREA PLANS

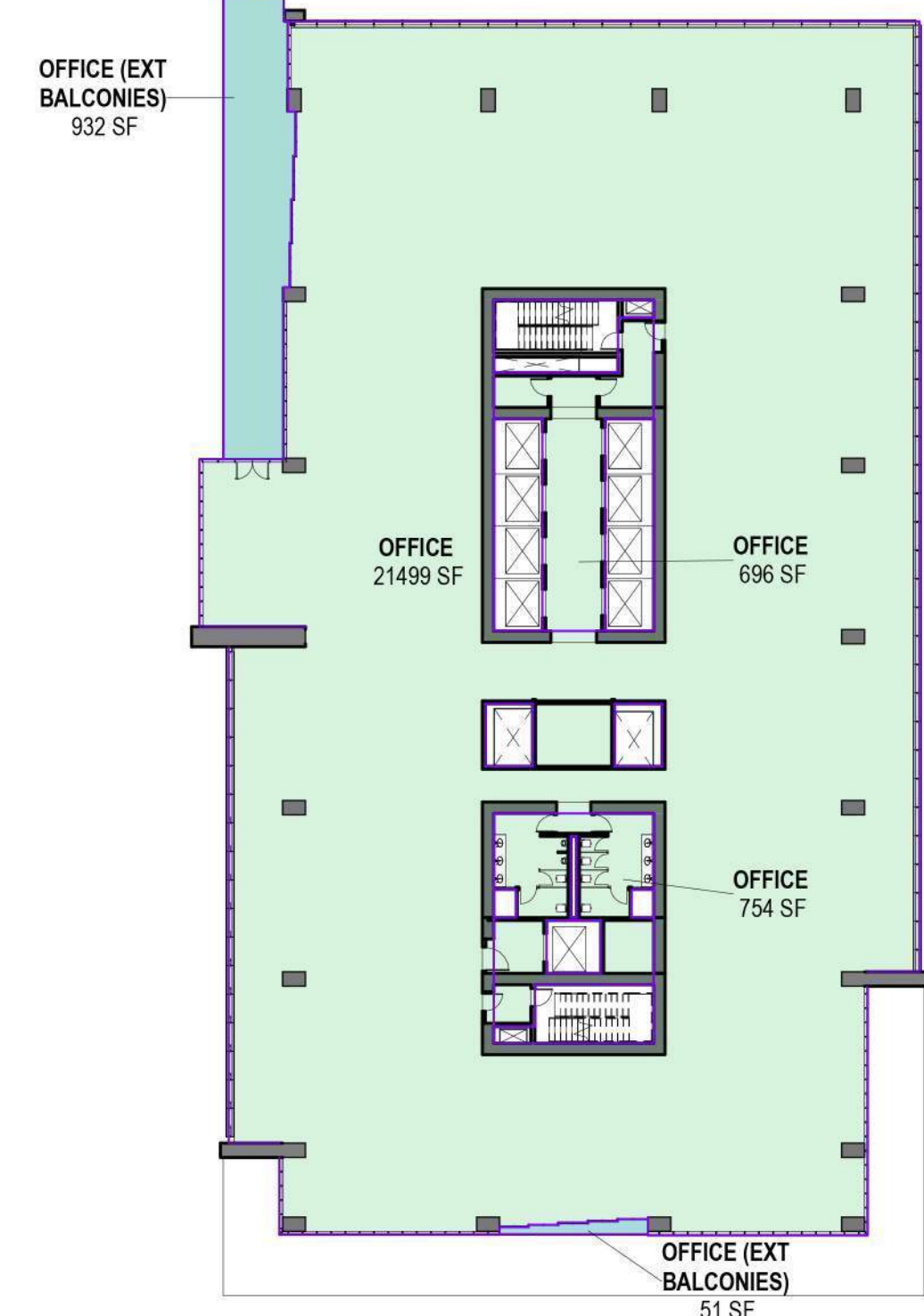
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**G0.903**



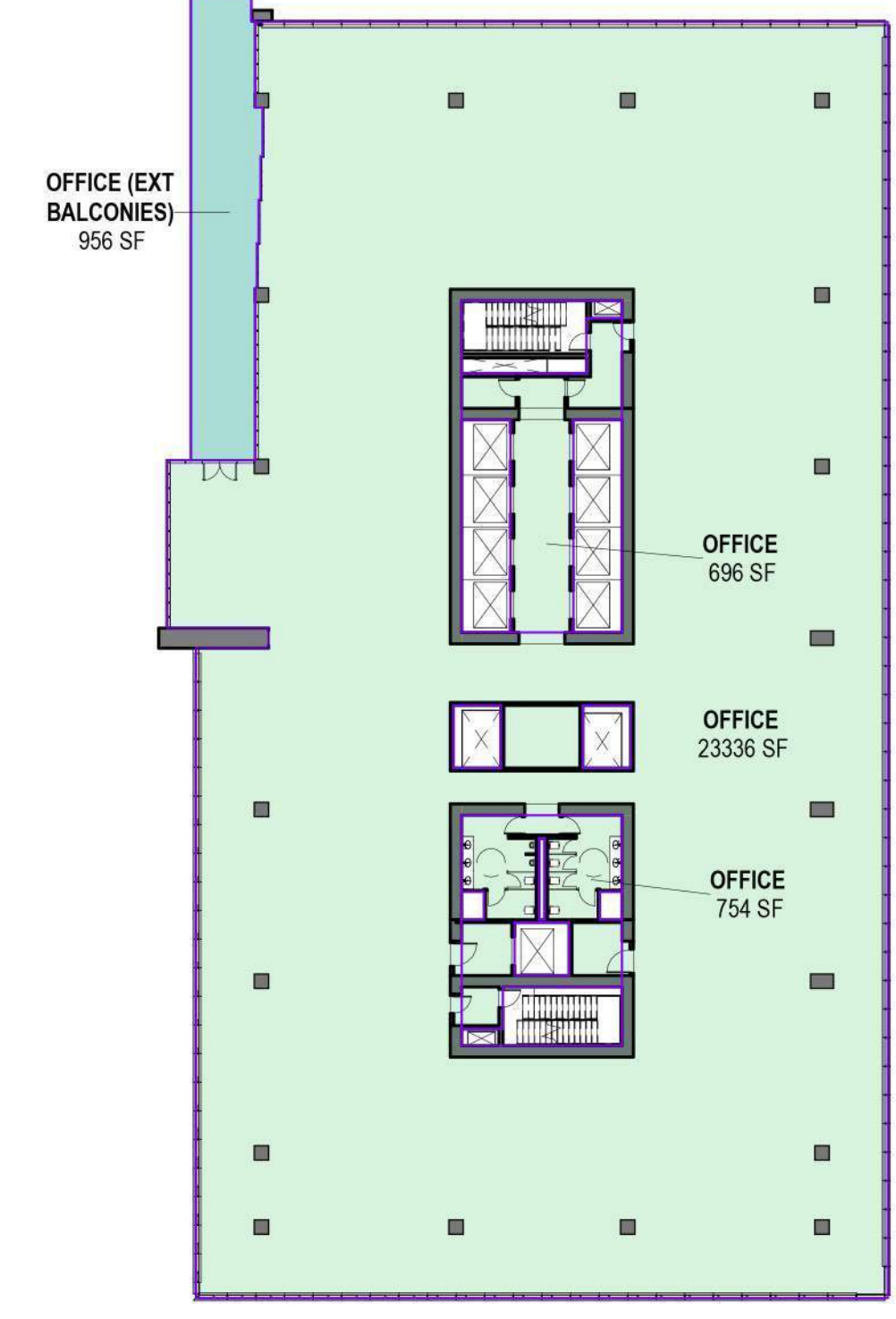
Name	Level	Area
OFFICE	LEVEL 8	24,899 SF
OFFICE (EXT BALCONIES)	LEVEL 8	524 SF

**LEVEL 8**  
1" = 30'-0" SCALE: 2



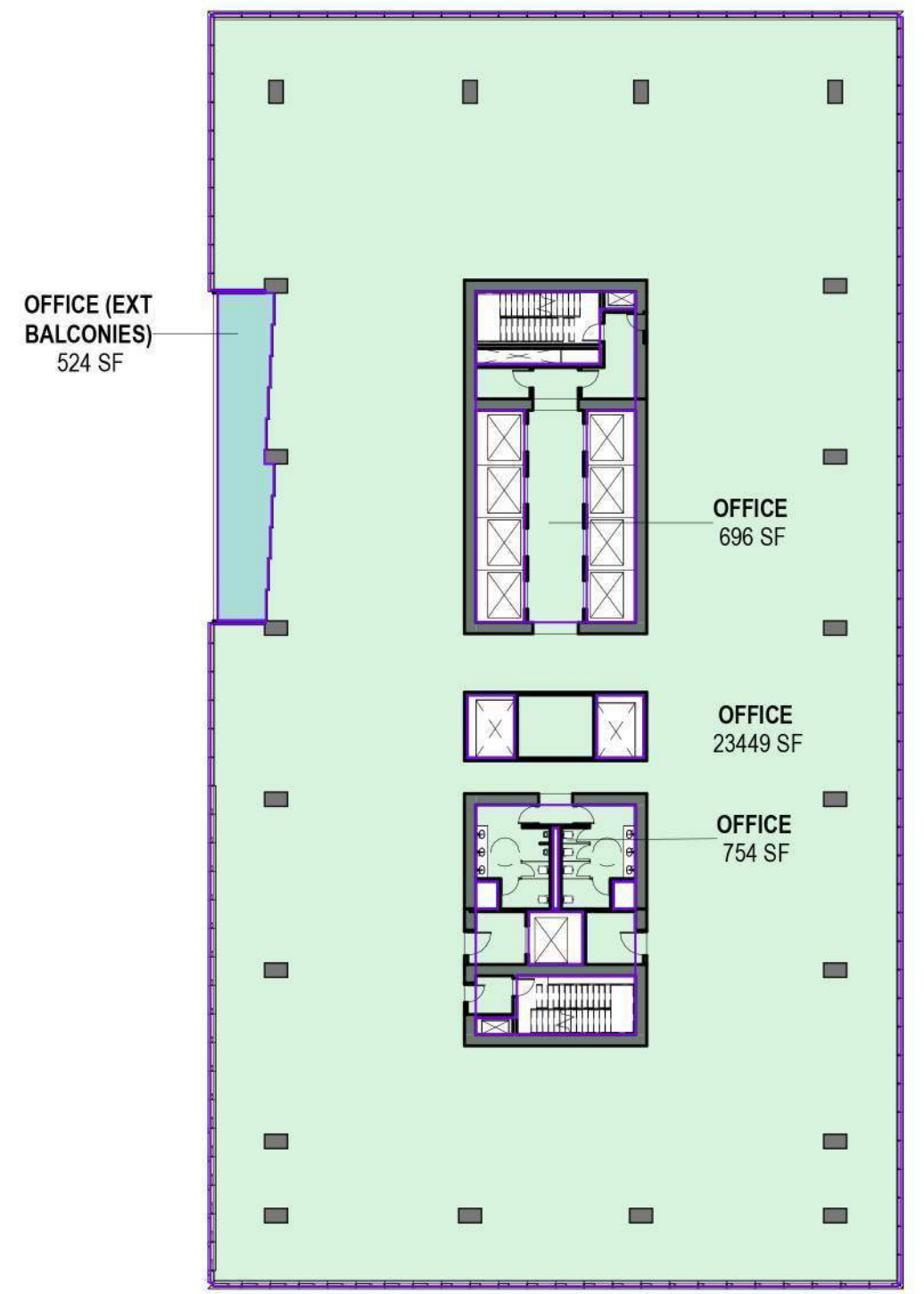
Name	Level	Area
OFFICE	LEVEL 10	22,949 SF
OFFICE (EXT BALCONIES)	LEVEL 10	983 SF

**LEVEL 10**  
1" = 30'-0" SCALE: 4



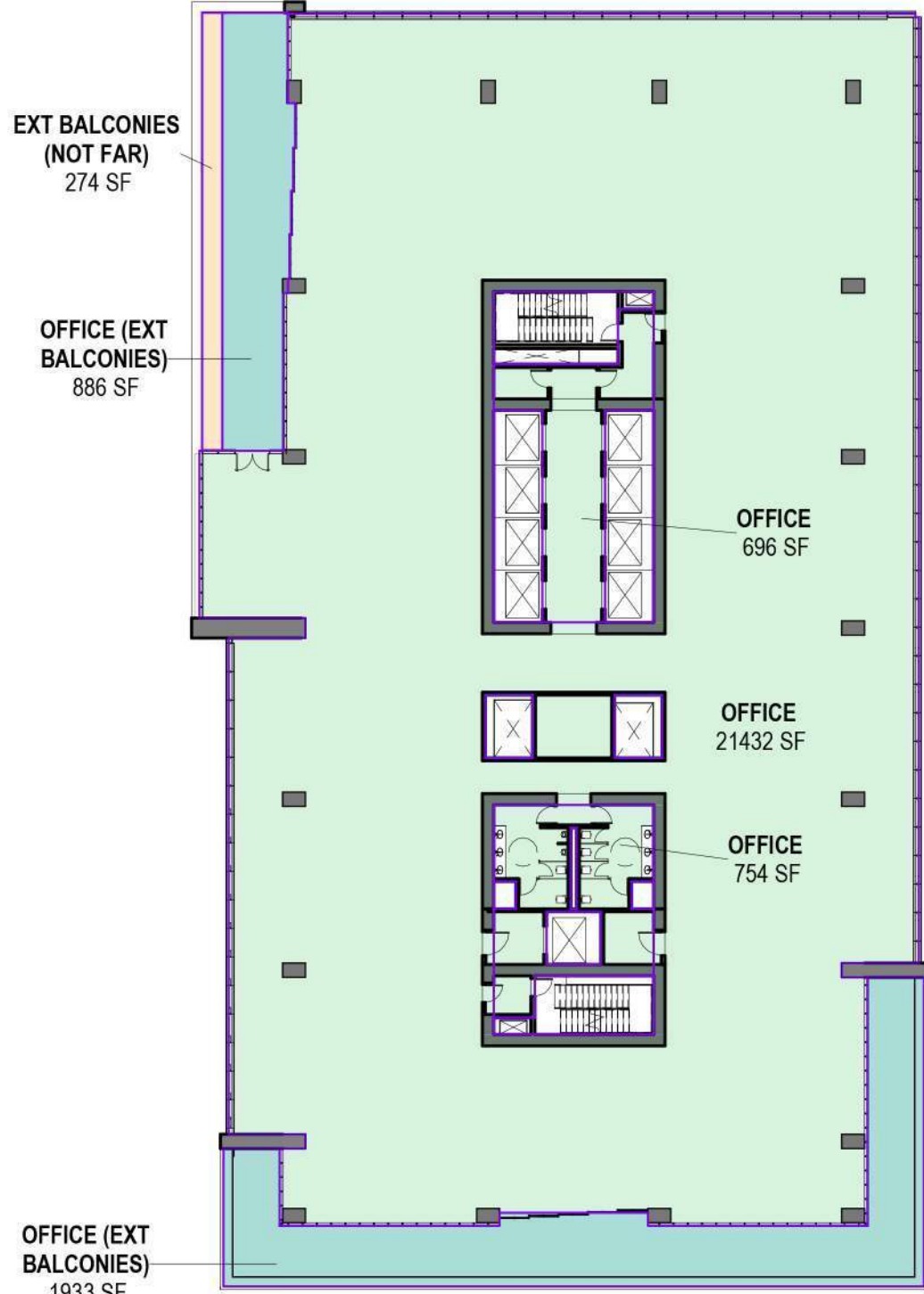
Name	Level	Area
OFFICE	LEVEL 12	24,786 SF
OFFICE (EXT BALCONIES)	LEVEL 12	956 SF

**LEVEL 12**  
1" = 30'-0" SCALE: 6



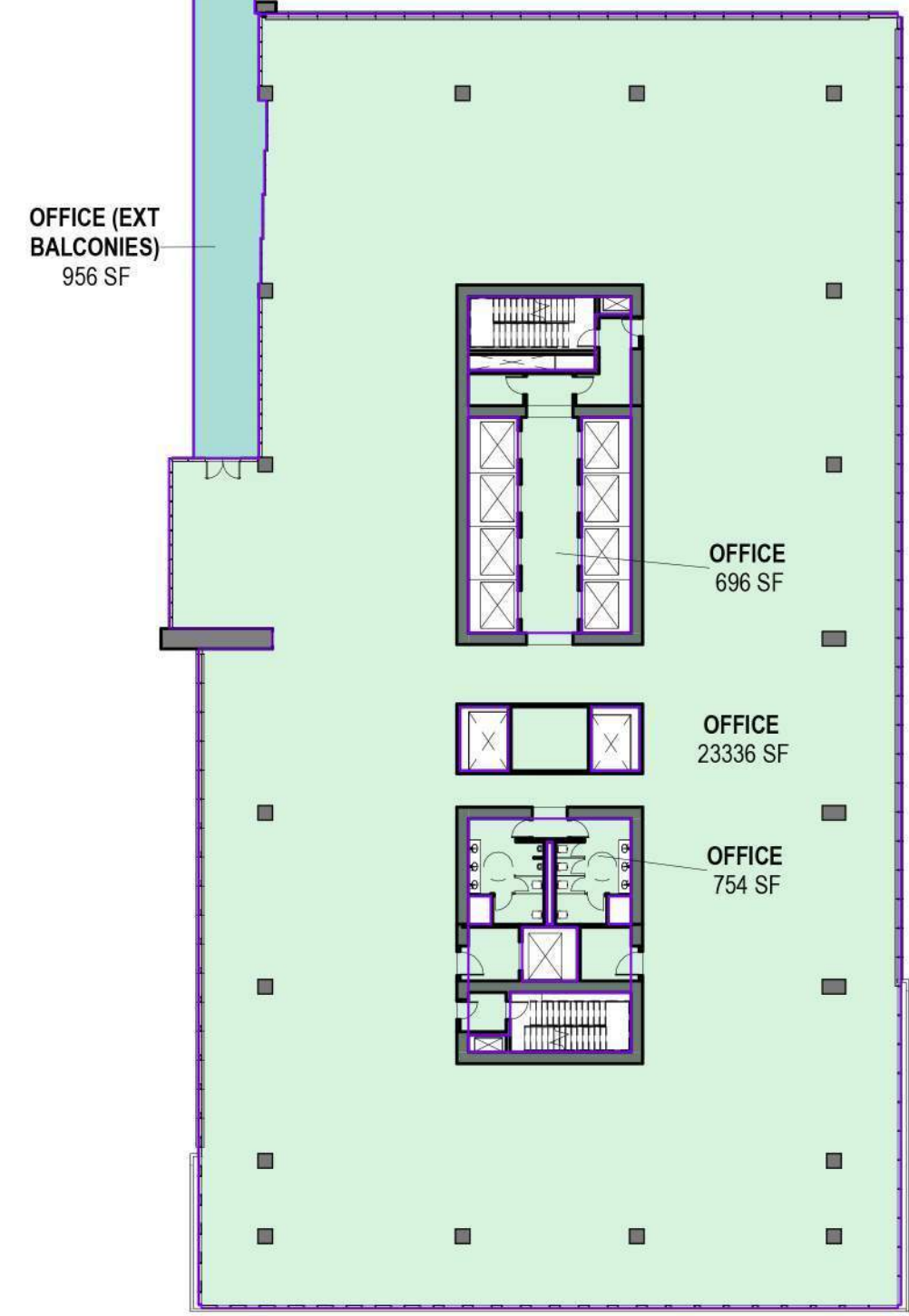
Name	Level	Area
OFFICE	LEVEL 7	24,899 SF
OFFICE (EXT BALCONIES)	LEVEL 7	524 SF

**LEVEL 7**  
1" = 30'-0" SCALE: 1



Name	Level	Area
EXT BALCONIES (NOT FAR)	LEVEL 9	274 SF
OFFICE	LEVEL 9	22,882 SF
OFFICE (EXT BALCONIES)	LEVEL 9	2,819 SF

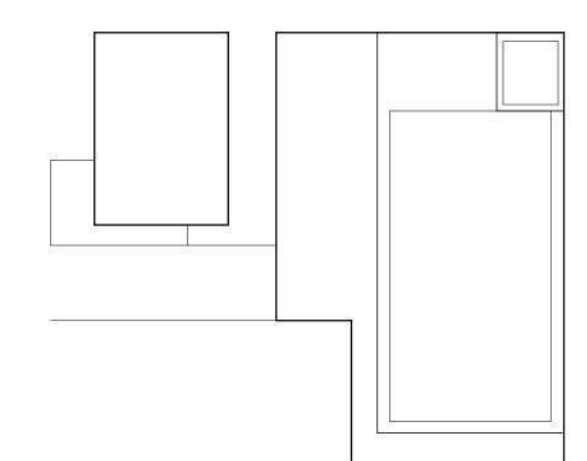
**LEVEL 9**  
1" = 30'-0" SCALE: 3



Name	Level	Area
OFFICE	LEVEL 11	24,786 SF
OFFICE (EXT BALCONIES)	LEVEL 11	956 SF

**LEVEL 11**  
1" = 30'-0" SCALE: 5

**KEY PLAN**





**SHEET NOTES**

- EXT BALCONIES (NOT FAR)
- MECHANICAL SPACE (NOT FAR)
- OFFICE

Date	Description
05/31/2019	ENTITLEMENT SET
10/02/2019	ENTITLEMENT SET
04/07/2020	ENTITLEMENT SET
06/16/2023	ENTITLEMENT SET
10/12/2023	ENTITLEMENT SET

**EXHIBIT A**  
**CPC-2017-469-GPA-VZC-HD-MCUP-SPR**  
**October 12, 2023**

**GENERAL NOTES**

Seal / Signature

**NOT FOR CONSTRUCTION**

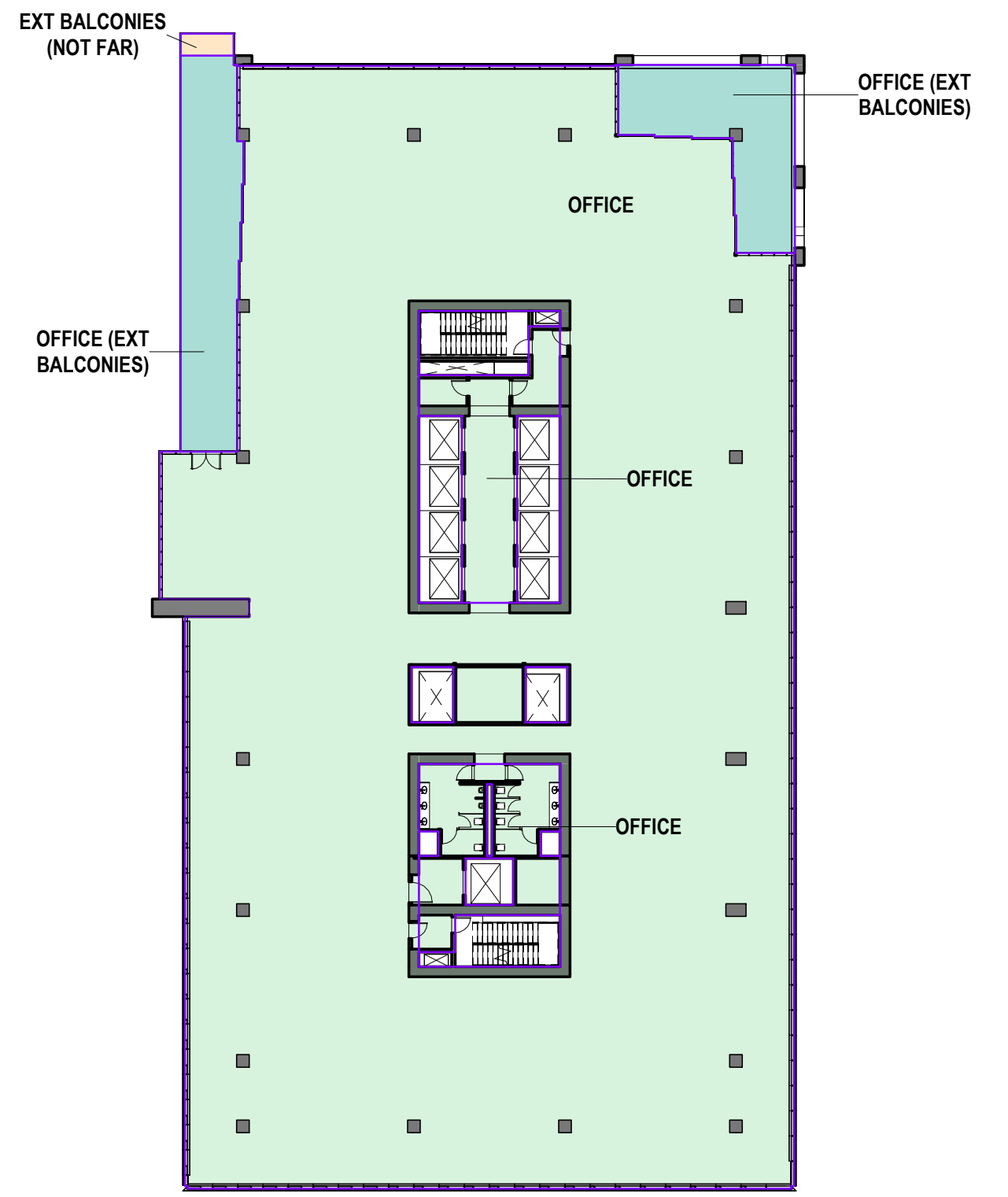
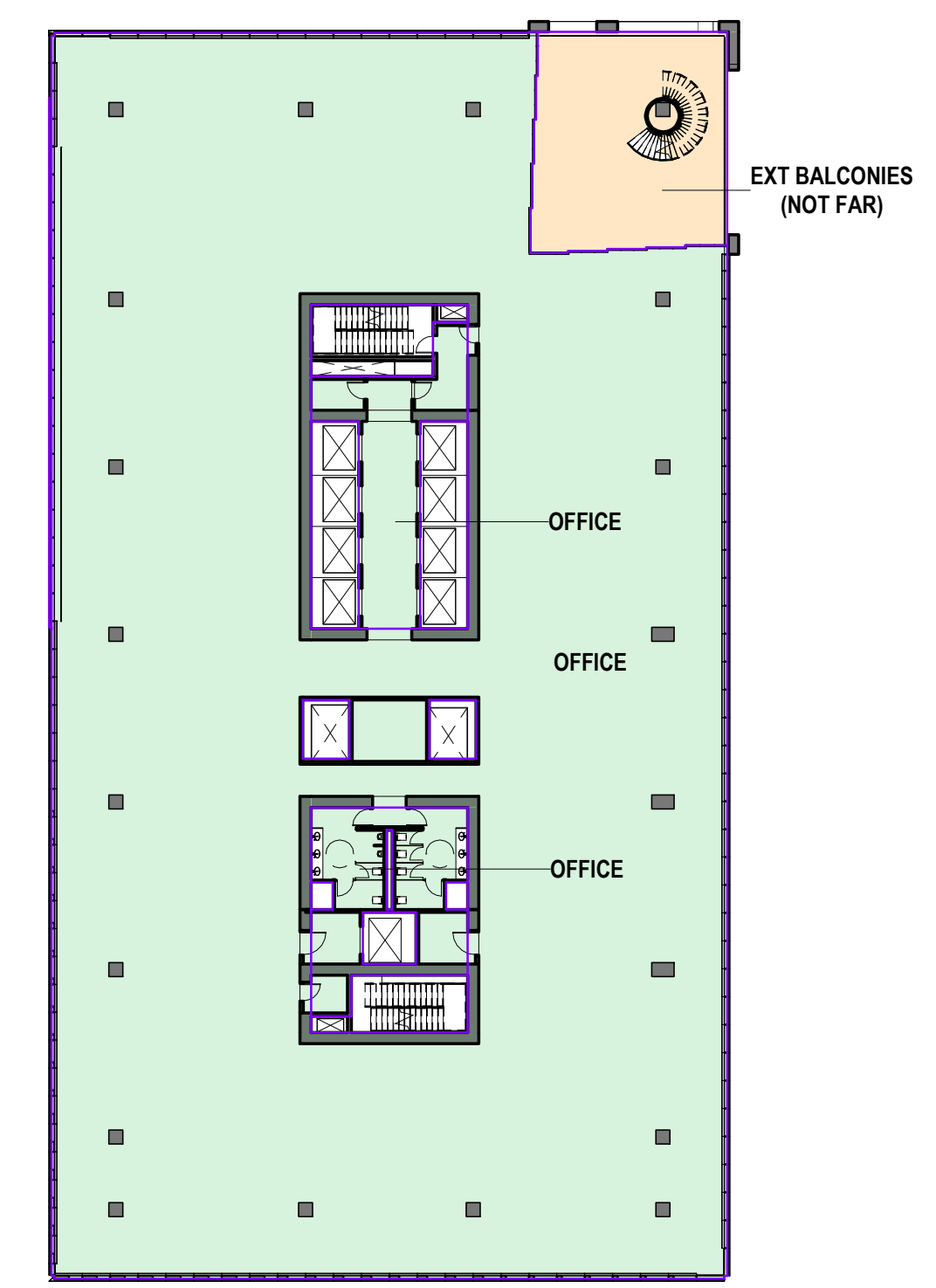
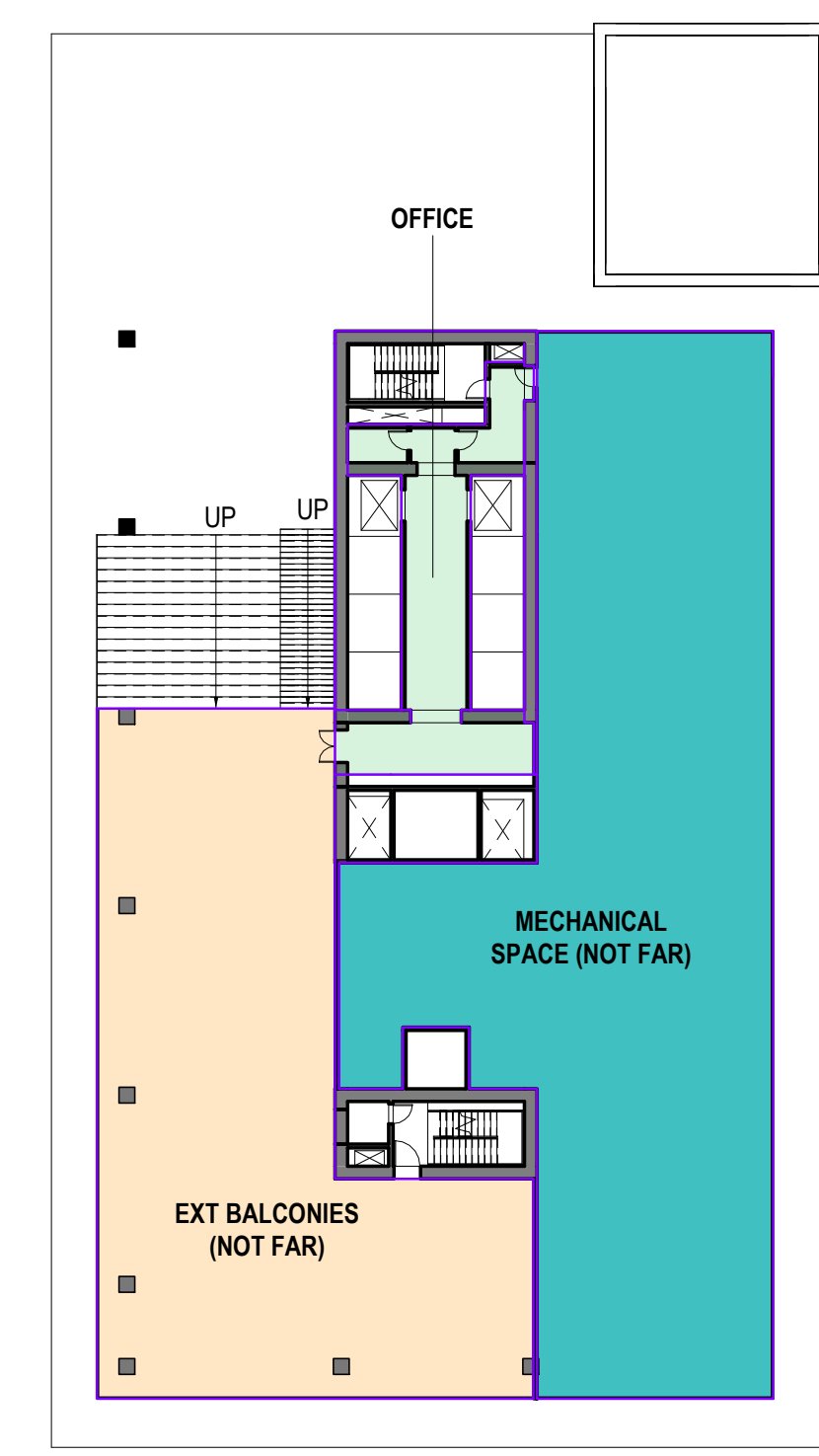
Project Name  
401 S Hewitt

Project Number  
05.1291.000

Description  
FLOOR AREA PLANS

Scale  
1" = 30'-0"

**G0.904**

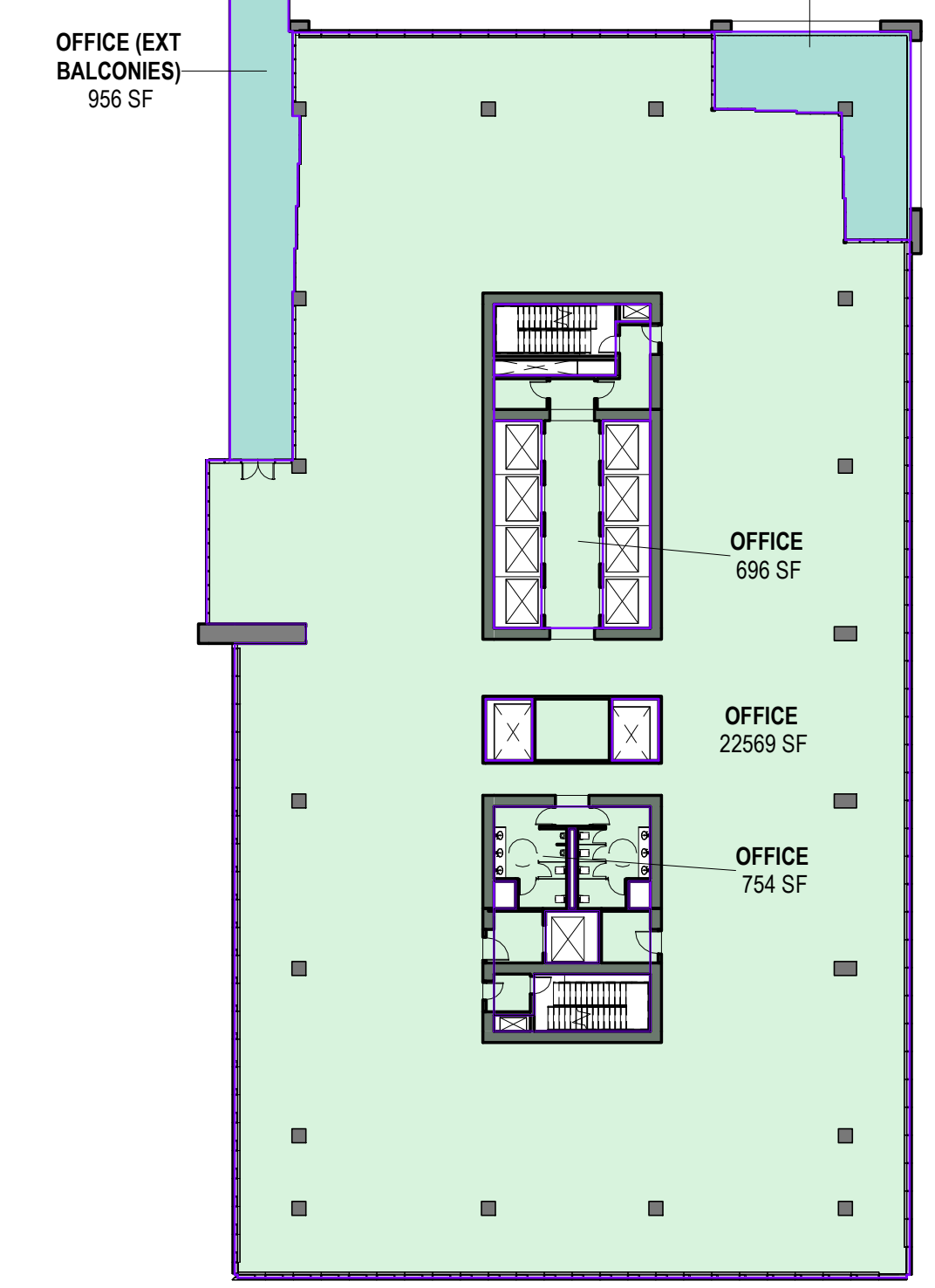
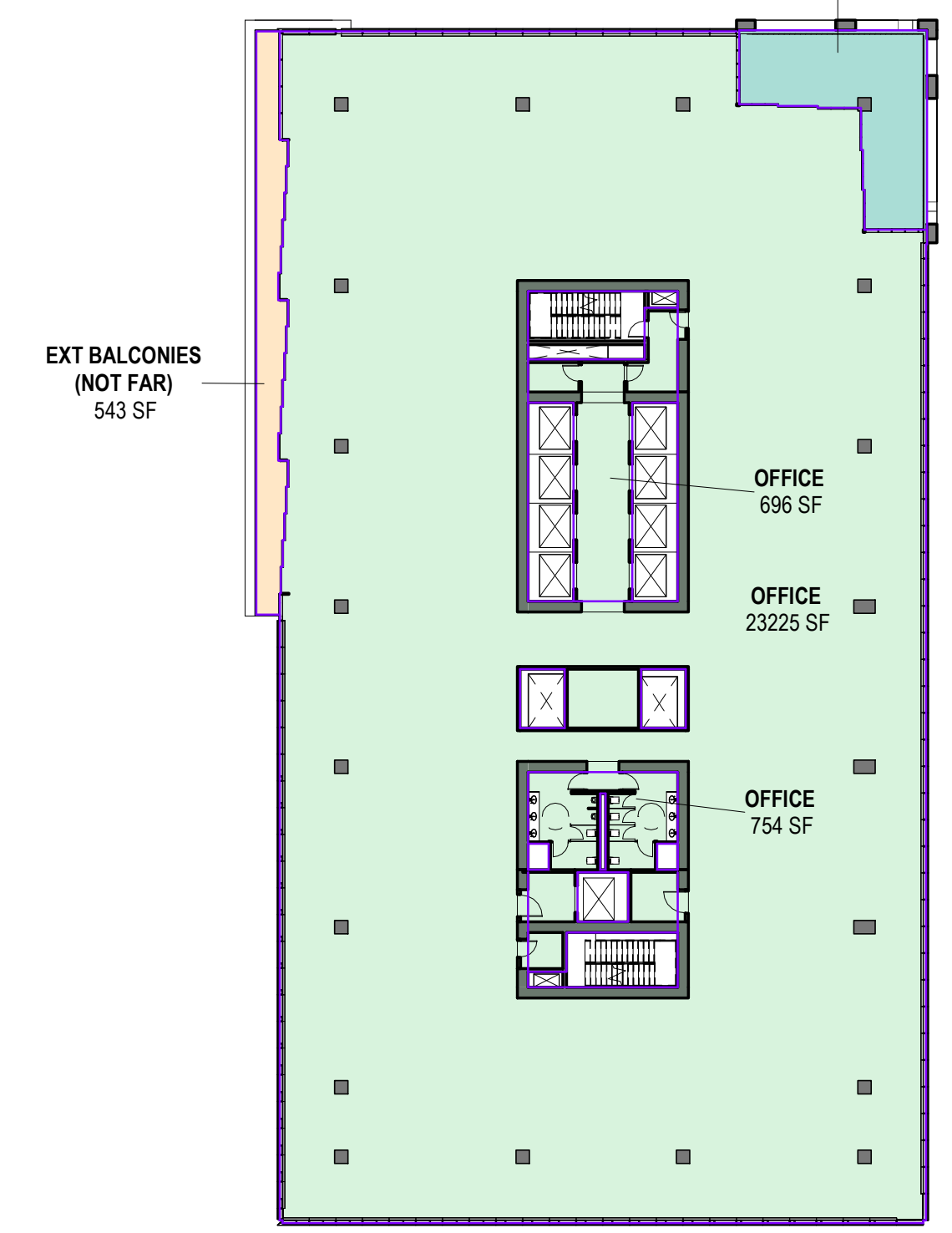
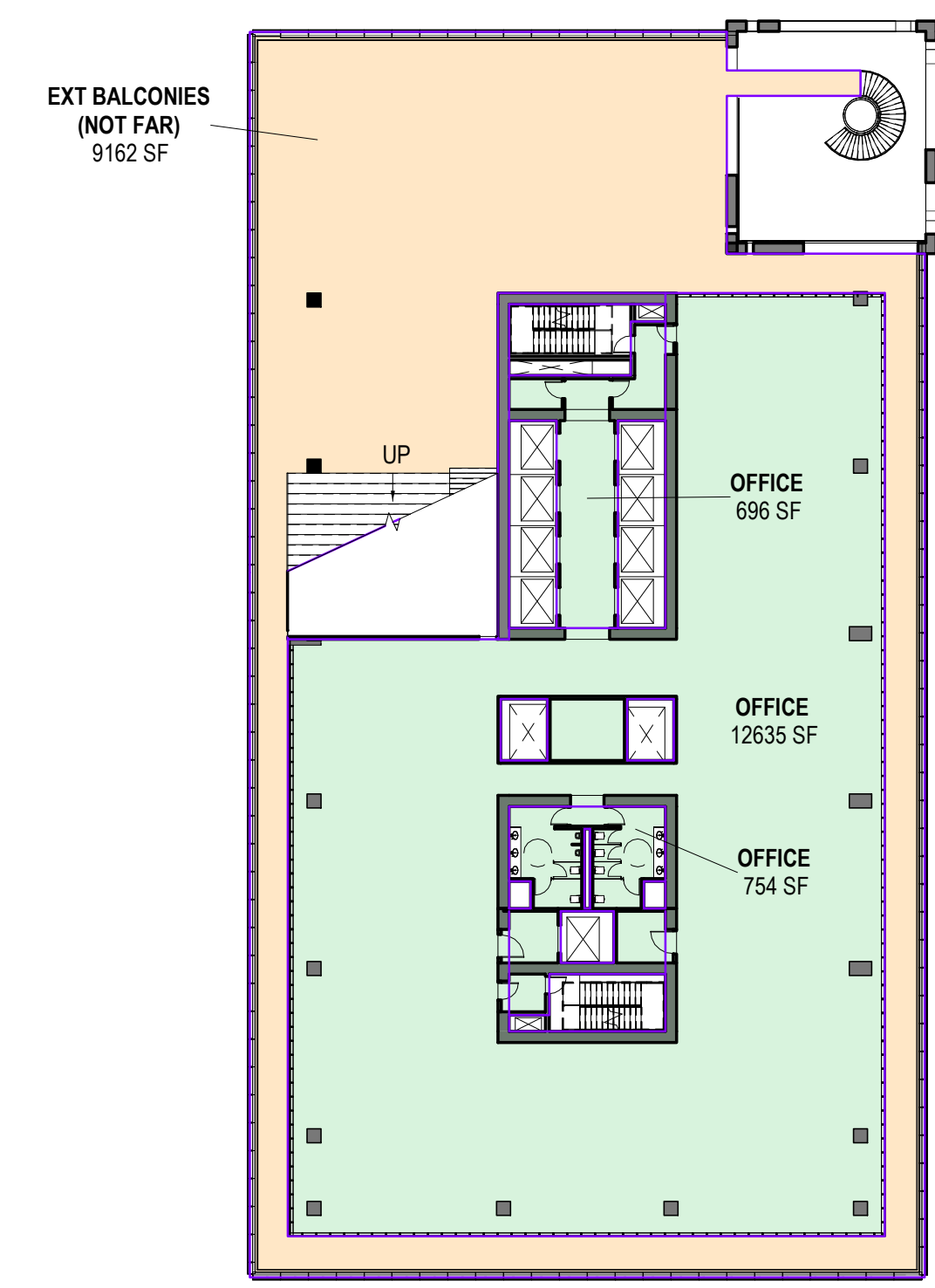
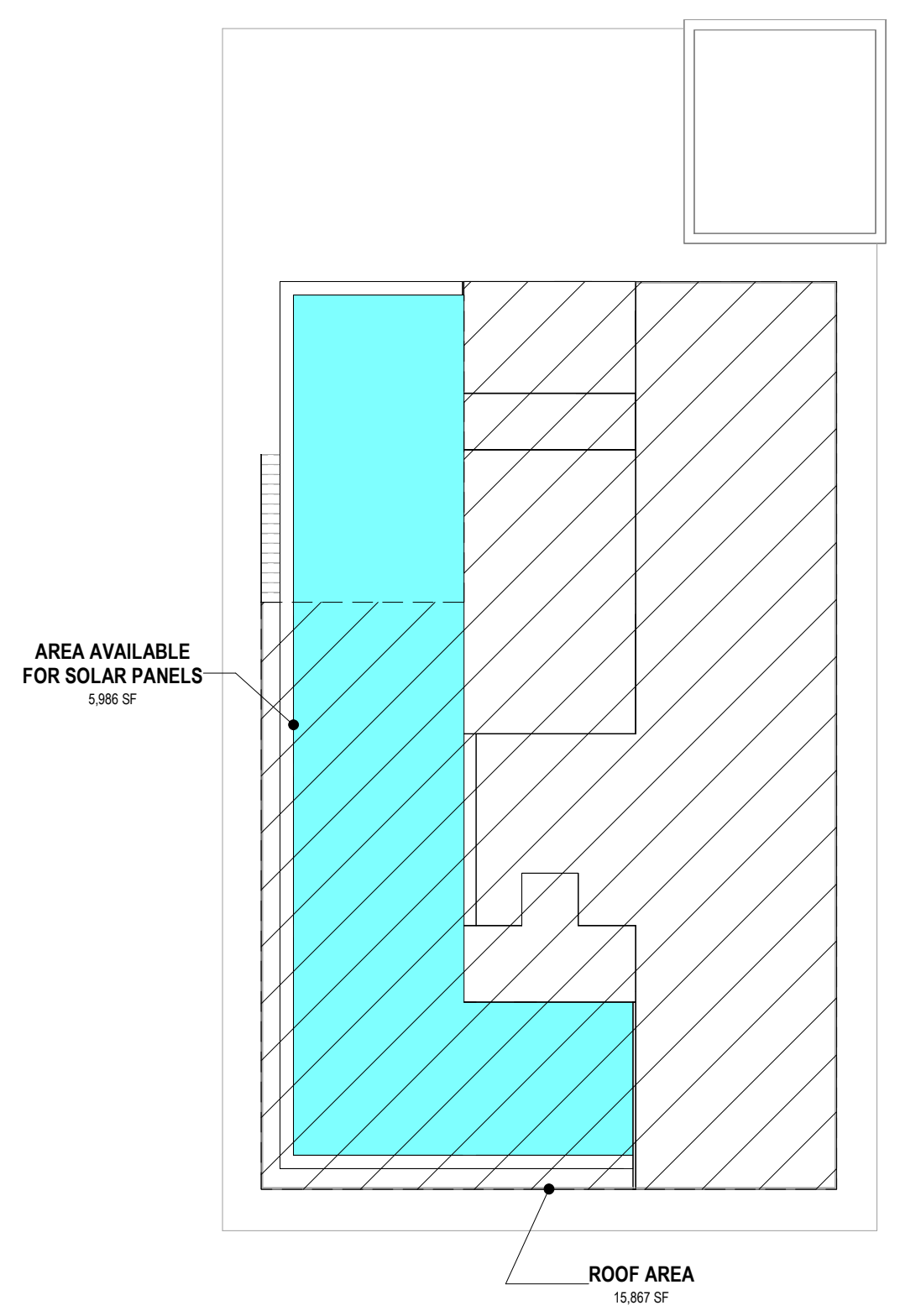
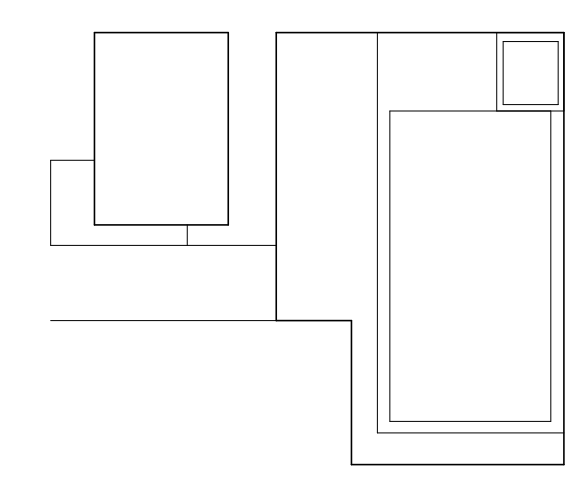


MECHANICAL LEVEL (LEVEL 18) 7  
1" = 30'-0" SCALE:

LEVEL 16 4  
1" = 30'-0" SCALE:

LEVEL 14 2  
1" = 30'-0" SCALE:

**KEY PLAN**

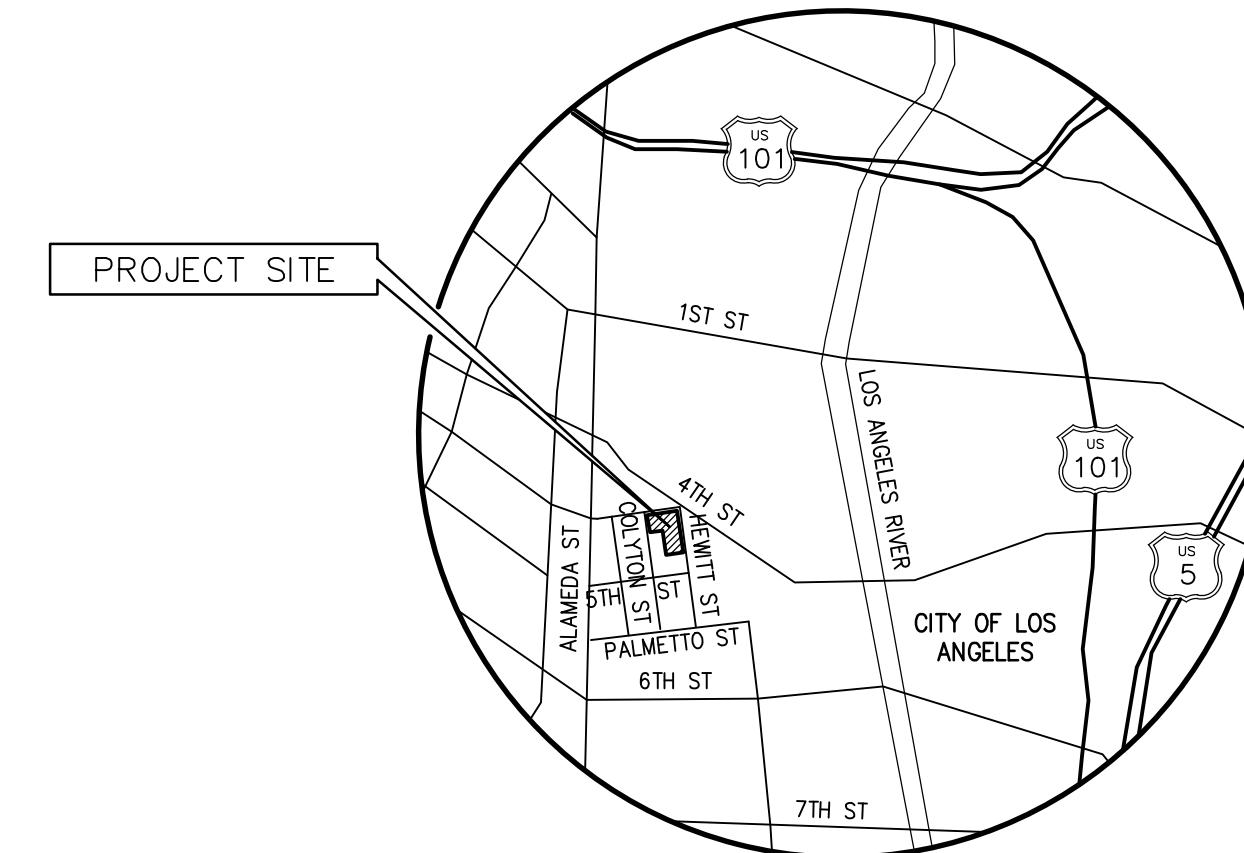


ROOF 8  
1" = 30'-0" SCALE:

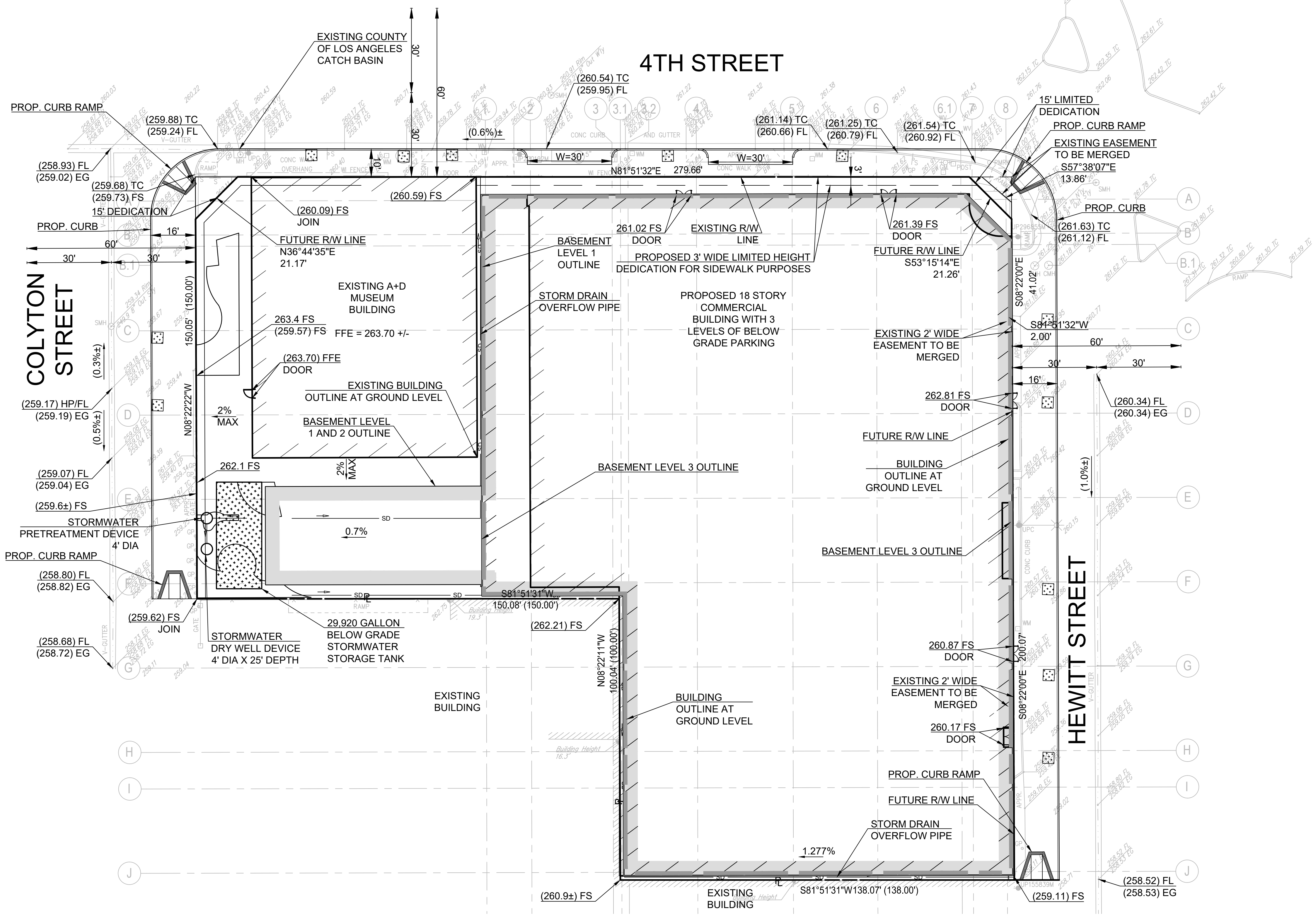
LEVEL 17 5  
1" = 30'-0" SCALE:

LEVEL 15 3  
1" = 30'-0" SCALE:

LEVEL 13 1  
1" = 30'-0" SCALE:



VICINITY MAP  
NOT TO SCALE



LEGEND:

- LIMIT OF STORM WATER TANK
- 4' DIA INFILTRATION DRYWELL
- SURFACE FLOW DIRECTION
- PIPE FLOW DIRECTION
- LIMIT OF BUILDING AT GROUND LEVEL
- LIMIT OF BASEMENT BELOW GRADE
- PROPOSED TREE WELLS

**EXHIBIT A**  
**CPC-2017-469-GPA-VZC-HD-MCUB-SPR**  
**October 12, 2023**

**Legendary Investors Group**  
401 S. Hewitt, Los Angeles, CA 90013

**Gensler**  
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Fax 818.508.7050

**WALKER PARKING CONSULTANTS**  
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Los Angeles, CA 90014  
Tel 213.488.4911

**PSOMAS**  
555 Flower St., Suite 4300  
Los Angeles, CA 90071  
Tel 213.223.1400

Date	Description
12/10/2023	ENTITLEMENT SET

Seal / Signature

**NOT FOR CONSTRUCTION**

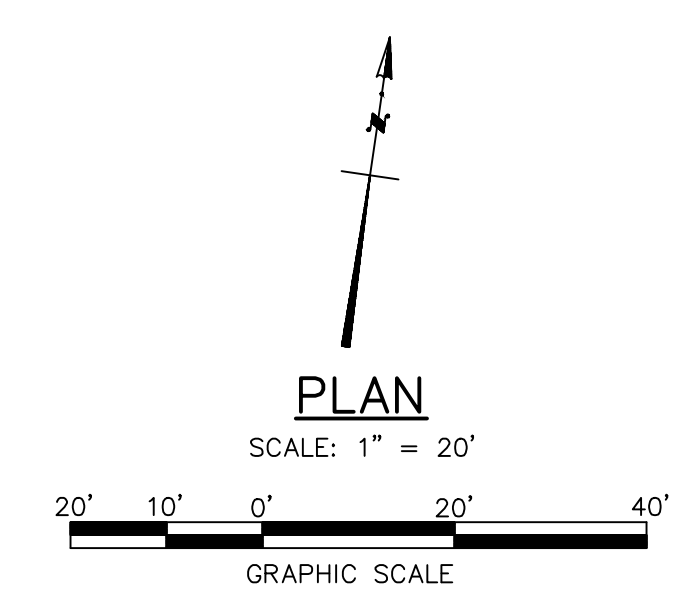
Project Name  
4th & Hewitt Complex

Project Number  
05.1291.000

Description  
PRELIMINARY GRADING AND DRAINAGE PLAN

Scale  
AS SHOWN

**C1.0**



**GENERAL NOTES:**

PLUMBING SYSTEM TO ROUTE STORM WATER RUNOFF TO STORAGE TANK. RUNOFF TO BE PLUMBED TO STORM WATER PRE-TREATMENT DEVICE AND DRY WELL INFILTRATION SYSTEM. OVERFLOW DISCHARGE TO HEWITT STREET AND 4TH STREET.

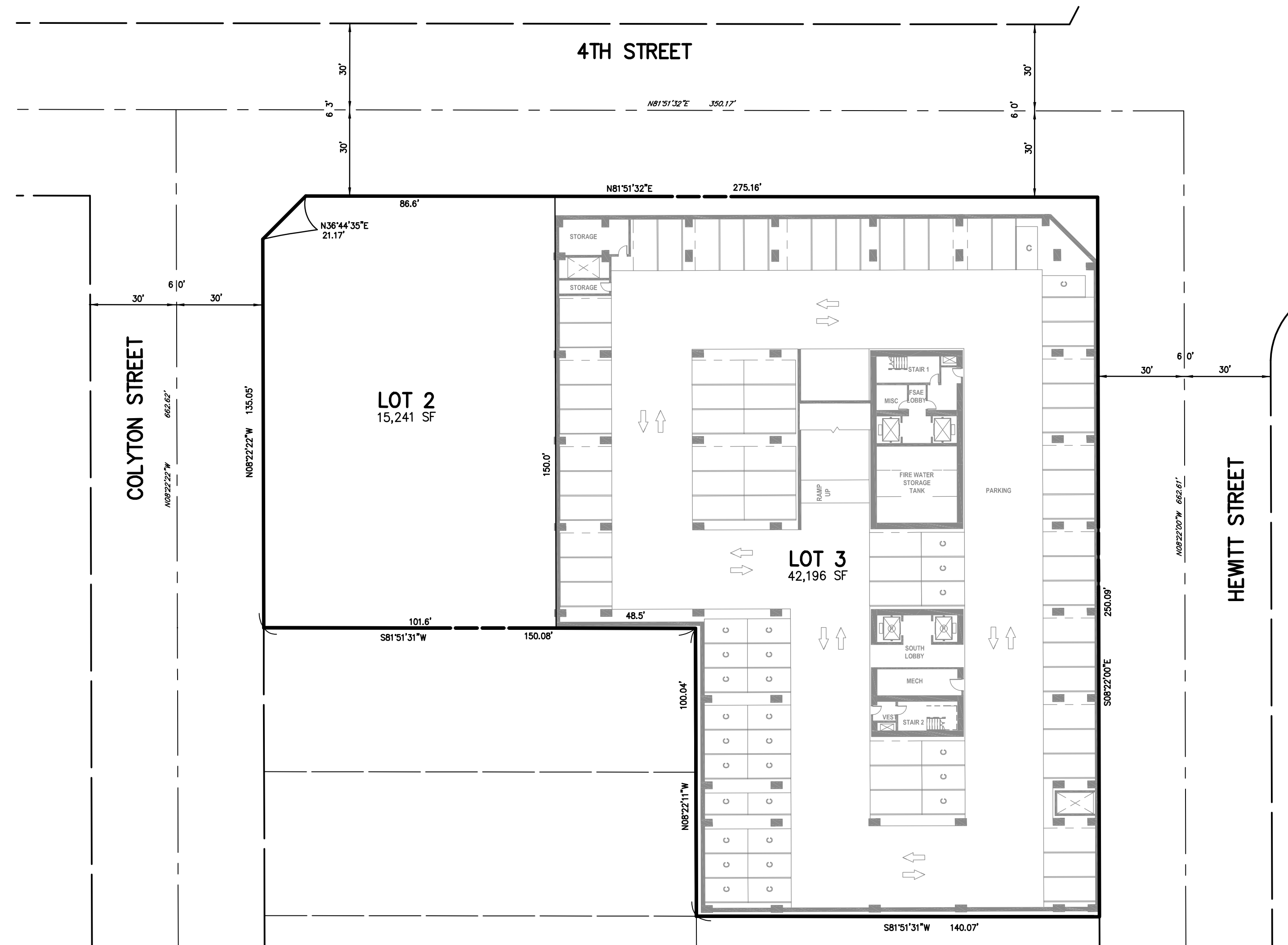
**ESTIMATED EARTHWORK QUANTITIES:**

RAW CUT 75,200 CY.  
RAW FILL 0 CY.  
OVEREXCAVATION AND RECOMPACTION @ 0' 0 CY.  
TOTAL EXPORT 75,200 CY.

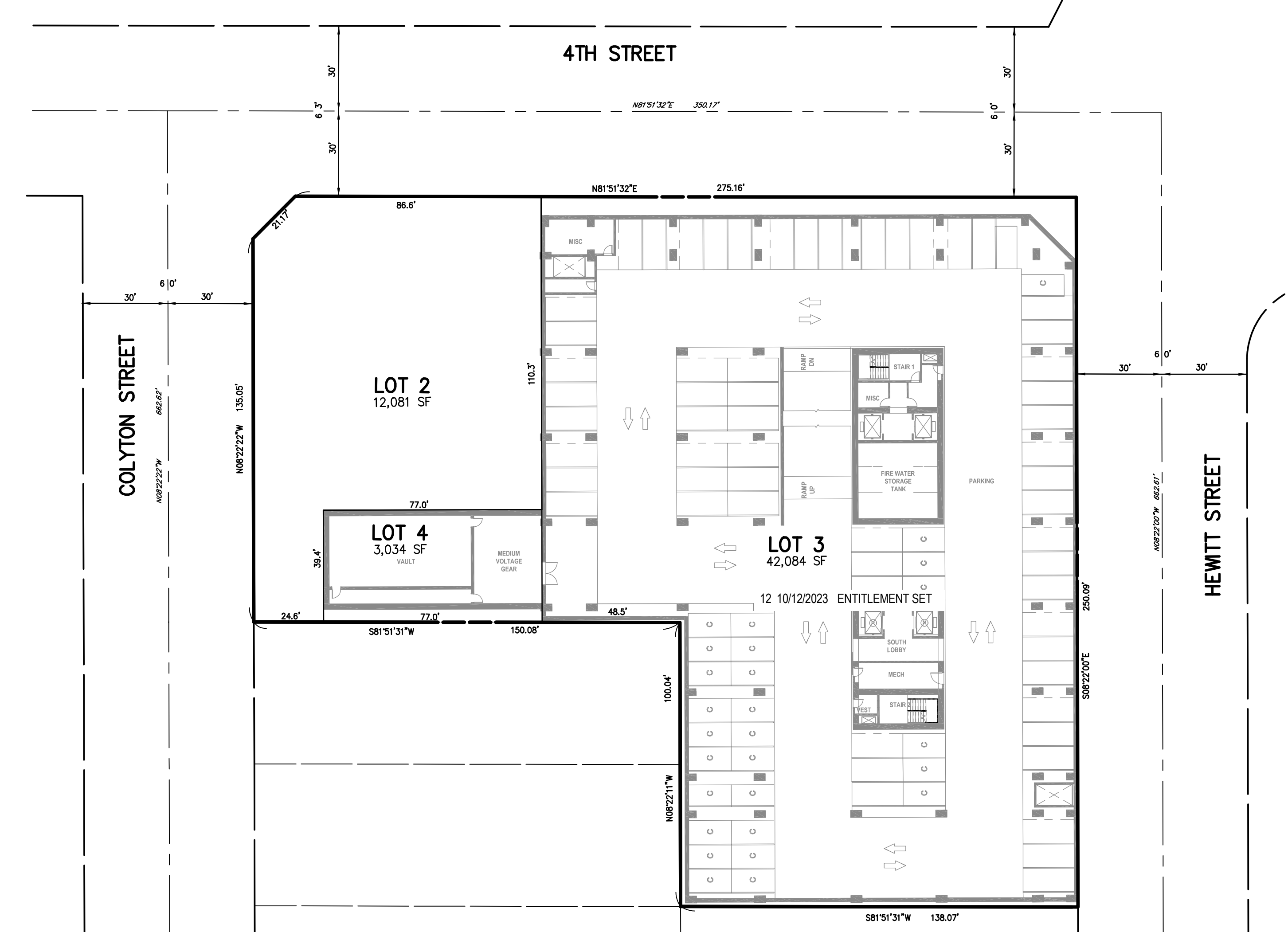
THE ABOVE LISTED QUANTITIES REFLECT THE ENGINEER'S ESTIMATE OF THE EARTHWORK VOLUMES. BULKING IS NOT CONSIDERED IN EXPORT.

THESE QUANTITIES ARE FOR DESIGN AND BONDING PURPOSES ONLY, AND NOT FOR CONTRACT PURPOSES. THE CONTRACTOR IS RESPONSIBLE FOR COMPUTING HIS OWN QUANTITIES.

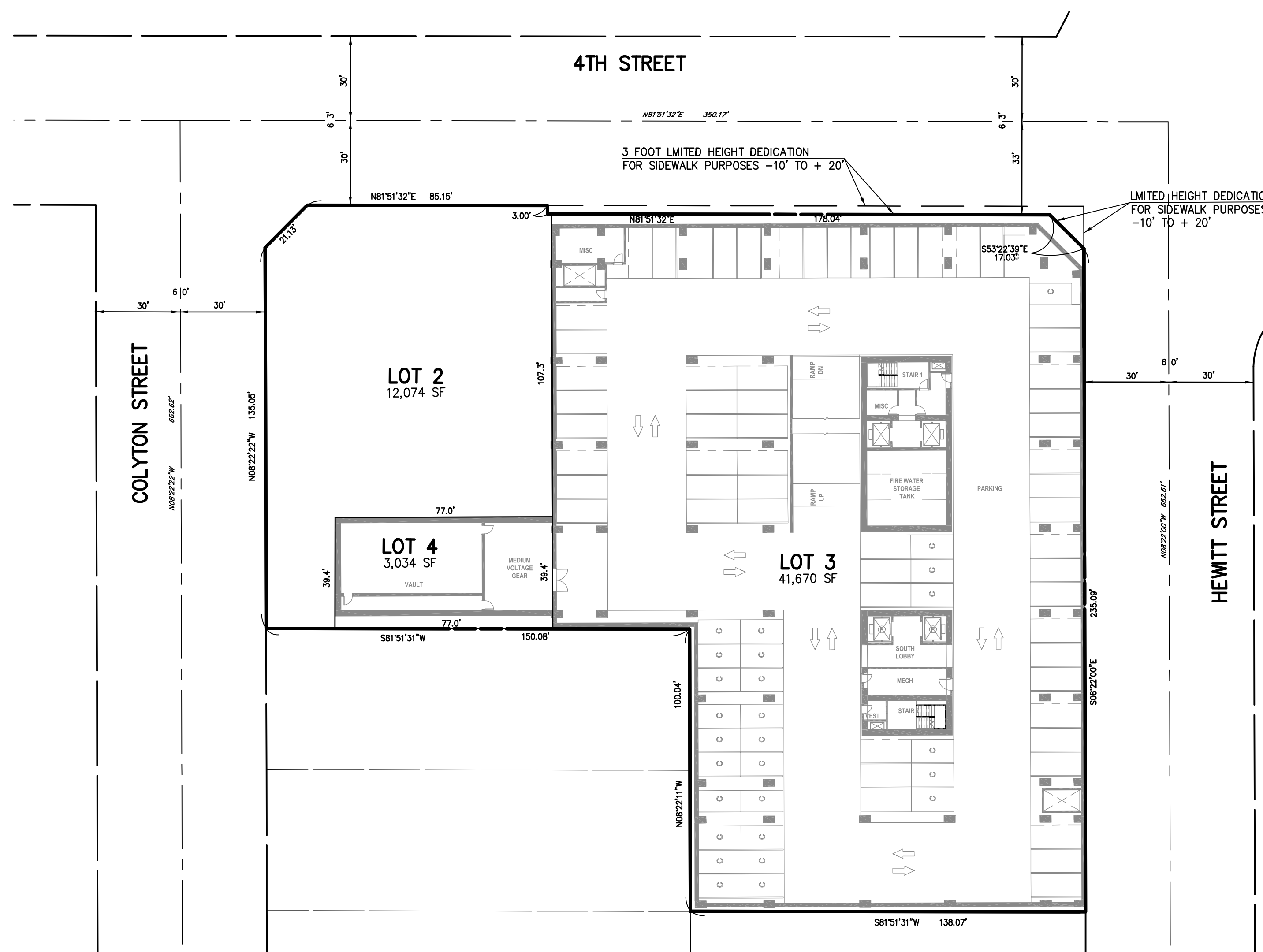
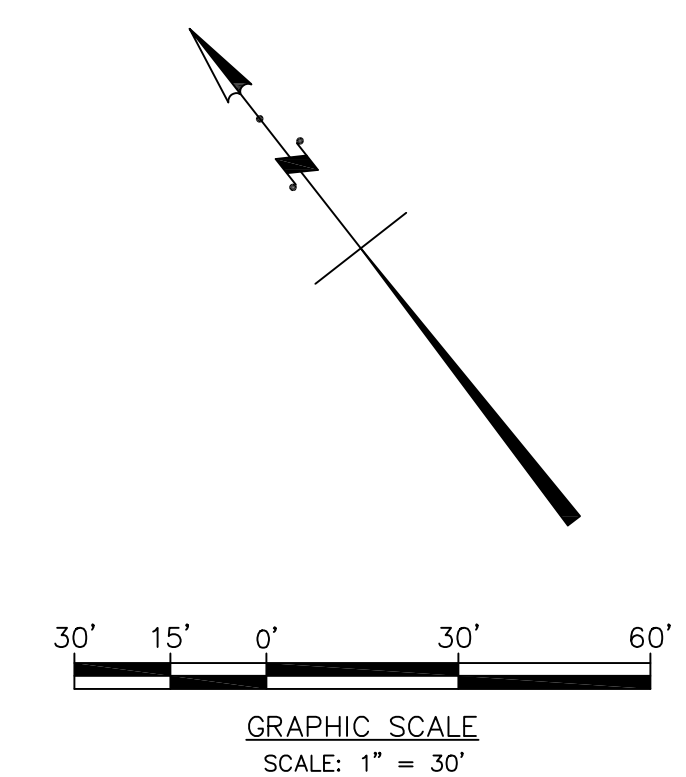




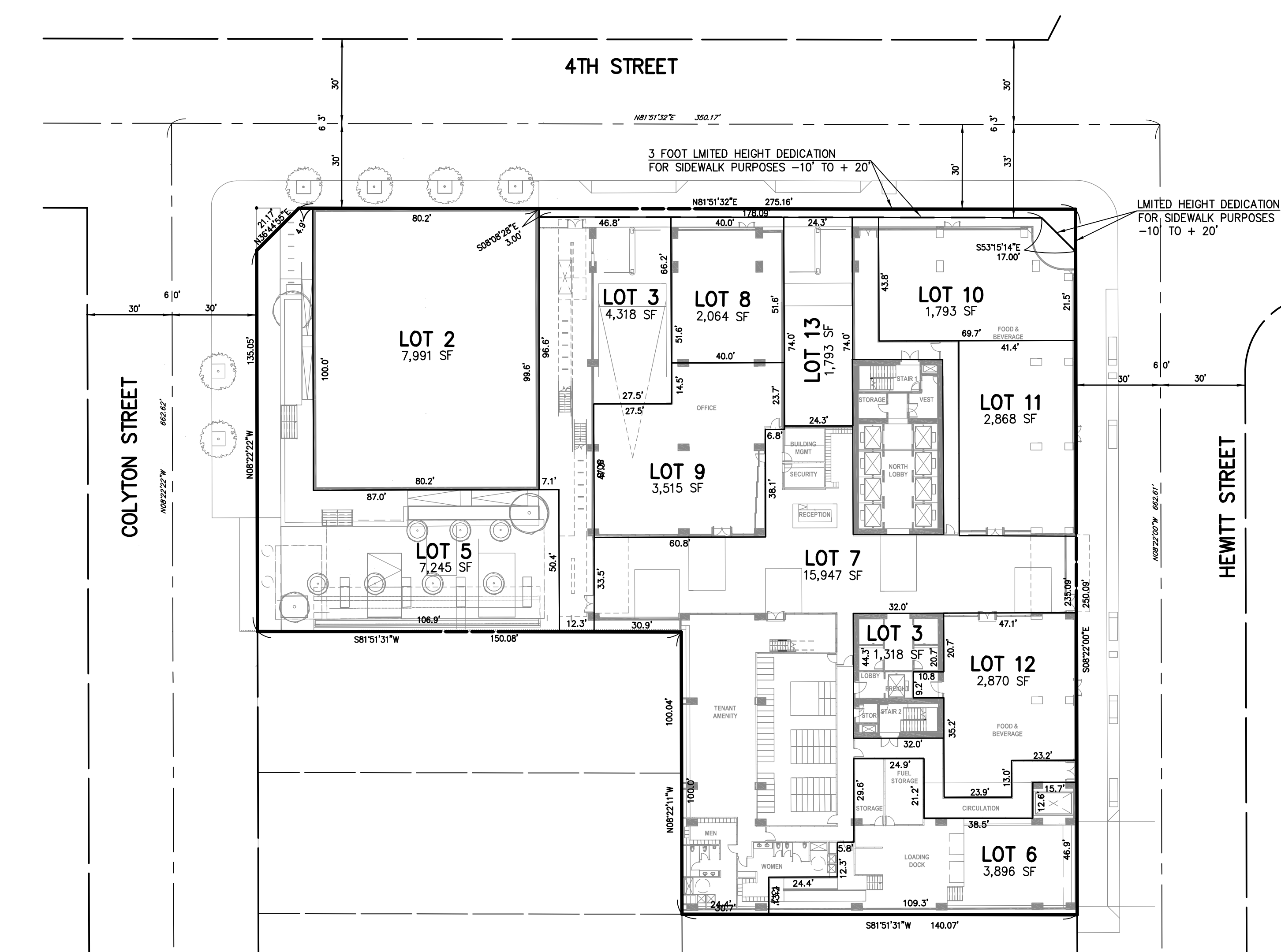
**BASEMENT 3**  
 UPPER ELEVATION: -20.0'  
 LOWER ELEVATION: -29.0'



**BASEMENT 2**  
 UPPER ELEVATION: -11.0'  
 LOWER ELEVATION: -20.0'



**BASEMENT 1**  
 UPPER ELEVATION: 0.0'  
 LOWER ELEVATION: -11.0'



**LEVEL 1**  
 UPPER ELEVATION: 20.0'  
 LOWER ELEVATION: 0.0'

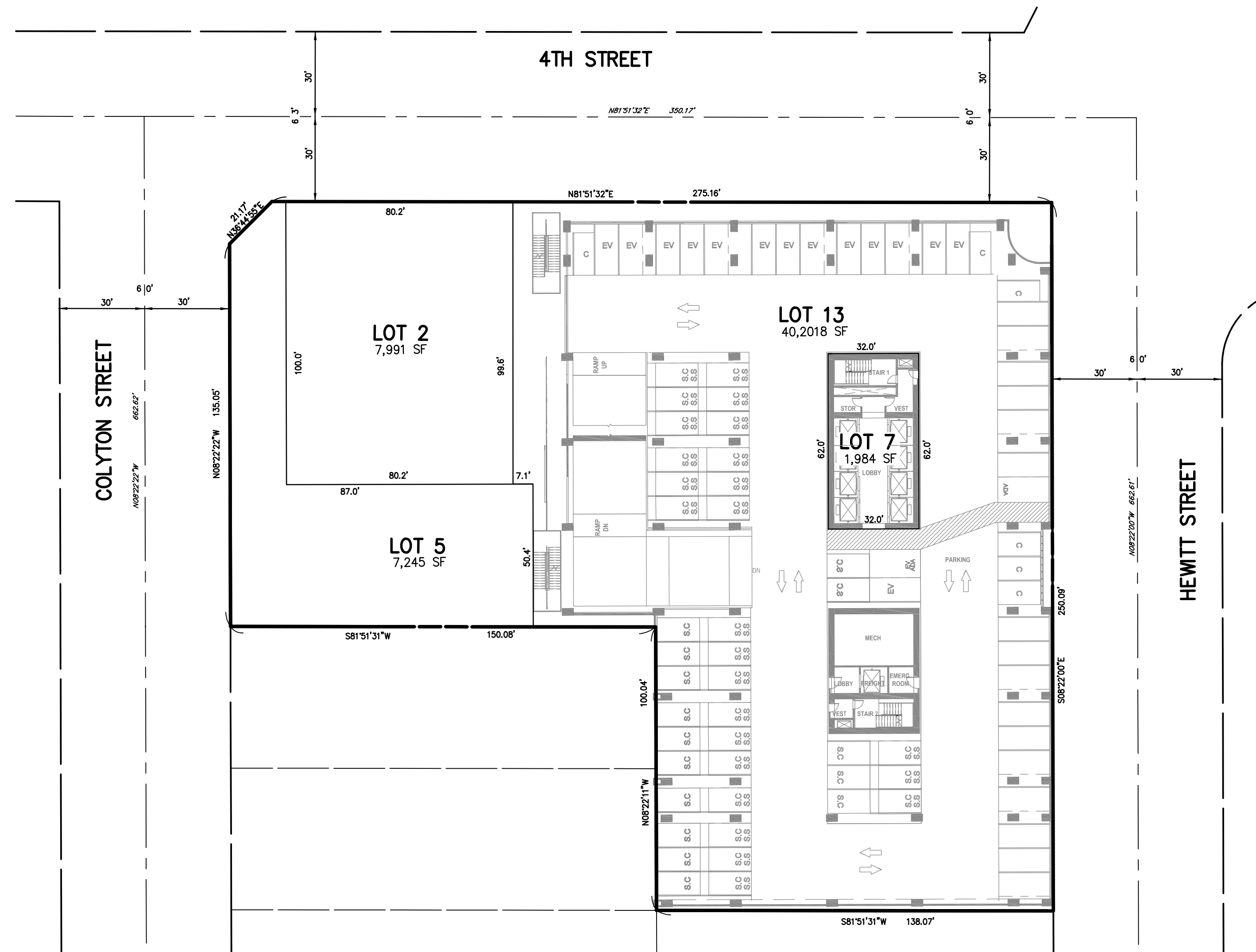
	DESIGNED	DLR						
	DRAFTED	DLR						
	CHECKED	DRH						
	REV	DATE	DESCRIPTION	DRH	DRH	LTL	DRH	APP'D
	06/20/23	ADDED NOTES REGARDING STREET DEDICATIONS/MERGER	DRH	DRH				
	06/13/23	UPDATED ARCH. DESIGN-ADDED 3 FT SIDEWALK DEDICATION	DRH	DRH				
	12/12/18	ARCHITECTURAL DESIGN REVISION - 12/07/18	LTL	DRH				

BENCHMARK	CITY OF LOS ANGELES BENCHMARK NO. 12-03812	
	WIRE SPK IN S CURB 4TH ST; 116.7 FT W OF HEWITT ST; 1.7 DT W OF CONC APRON FIRST DRIVEWAY W OF HEWITT ST.	
ELEVATION	260.784 FEET	ADJUSTMENT 2000 (NAVD88)
DATE	APR 23, 2023 - 02:34:11 (DWG Name: W:\11020010\SPR\1\SUBMISSION\EM1\1\PLN-11102.dwg Updated By: dhoward)	

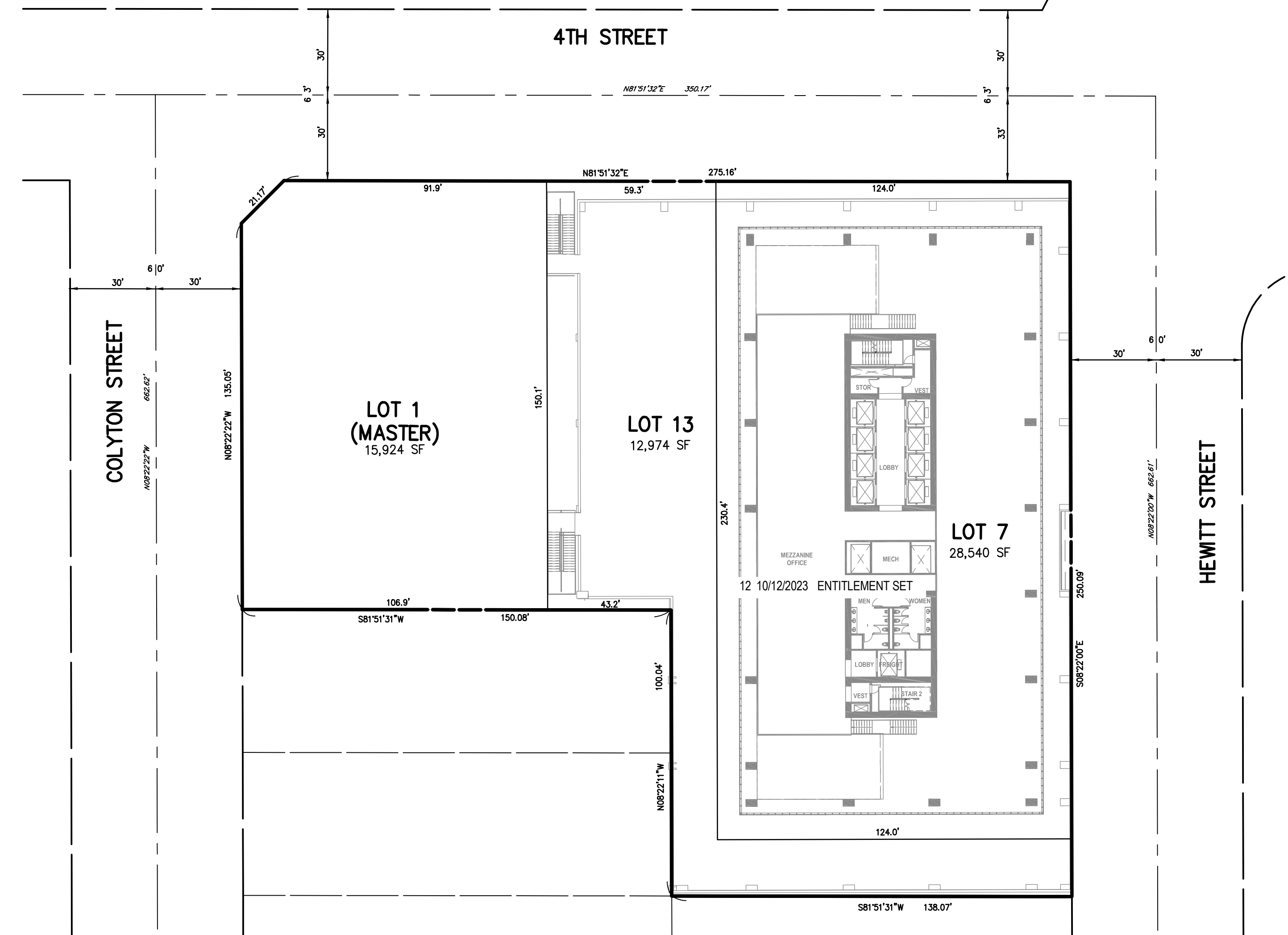
**PSOMAS**  
 555 South Flower Street, Suite 4300  
 Los Angeles, CA 90071  
 (213) 223-1400 (213) 223-1444 (FAX)  
 www.psomas.com

VESTING TENTATIVE TRACT FOR MERGER, VACATION, AND RE-SUBDIVISION PURPOSES:  
**VESTING TENTATIVE TRACT NO. 74745**  
 LIG-900, 910, 926 E. 4TH ST,  
 405, 144 S. HEWITT ST, LLC  
 IN THE CITY OF LOS ANGELES COUNTY OF LOS ANGELES STATE OF CALIFORNIA

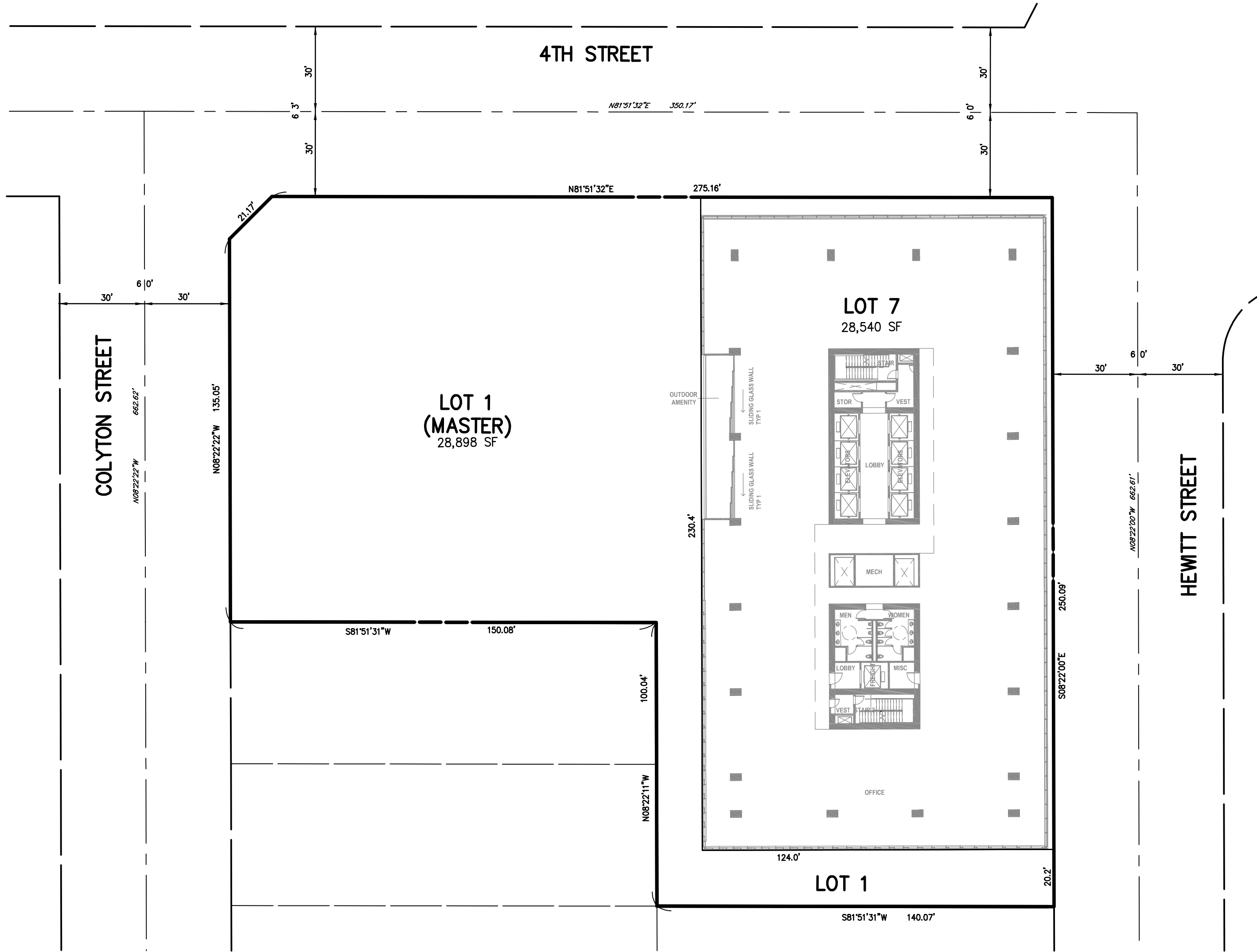
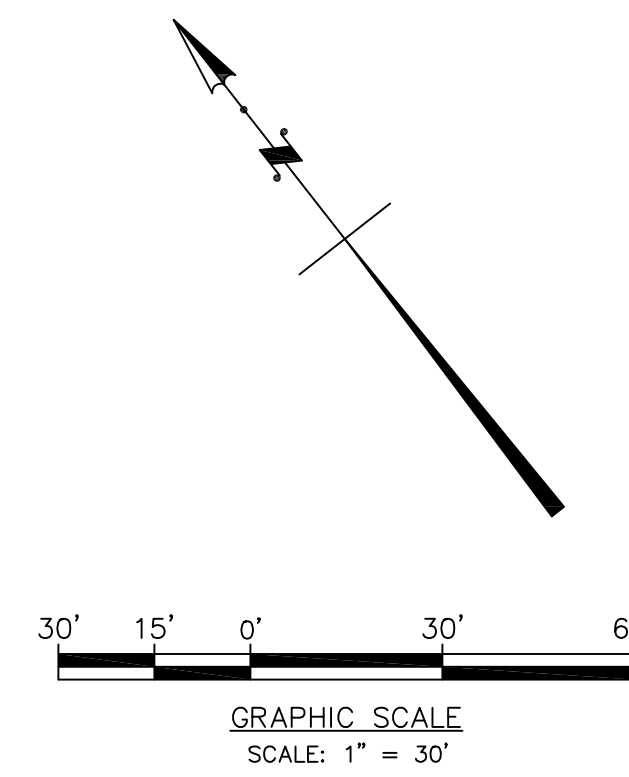
DATE:	JANUARY 6, 2017	SHEET
SCALE:	1" = 30'	2
PROJECT NUMBER:	1LIG060100, TASK 102	4



LEVEL 2 - 5  
UPPER ELEVATION: 80.0'  
LOWER ELEVATION: 20.0'



LEVEL 6  
UPPER ELEVATION: 95.0'  
LOWER ELEVATION: 80.0'



LEVELS 7 - 19  
UPPER ELEVATION: 300.0'  
LOWER ELEVATION: 95.0'

DATE PLOTTED: 10/12/2023 10:55:00 AM LEVEL: 7

DESIGNED	DLR						
DRAFTED	DLR						
CHECKED	DRH	06/20/23	ADDED NOTES REGARDING STREET DEDICATIONS/MERGER	DRH	DRH		
		06/13/23	UPDATED ARCH. DESIGN-ADDED 3 FT SIDEWALK DEDICATION	DRH	DRH		
		12/12/18	ARCHITECTURAL DESIGN REVISION - 12/07/18	LTL	DRH		
REV	DATE		DESCRIPTION	BY	APP'D		

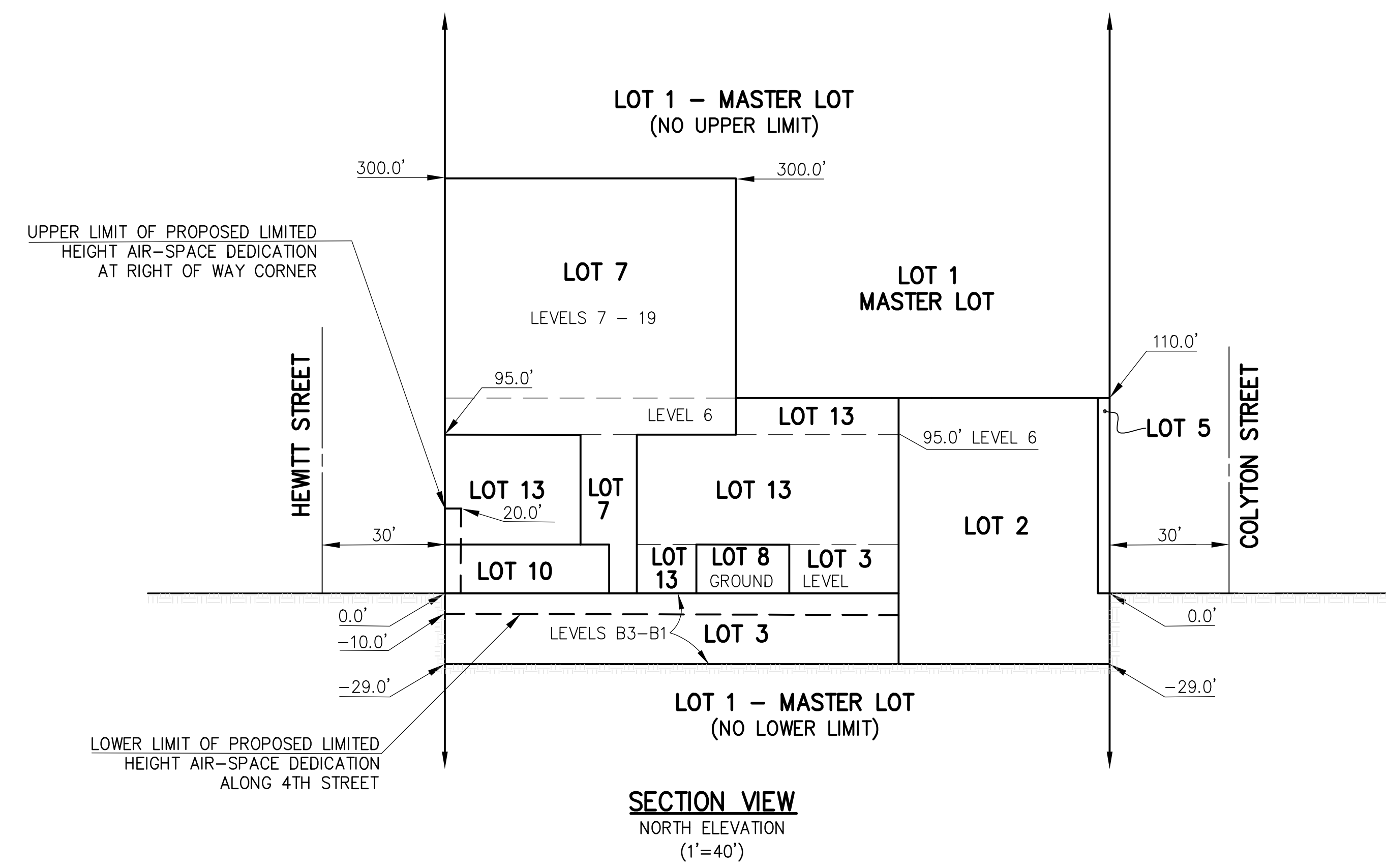
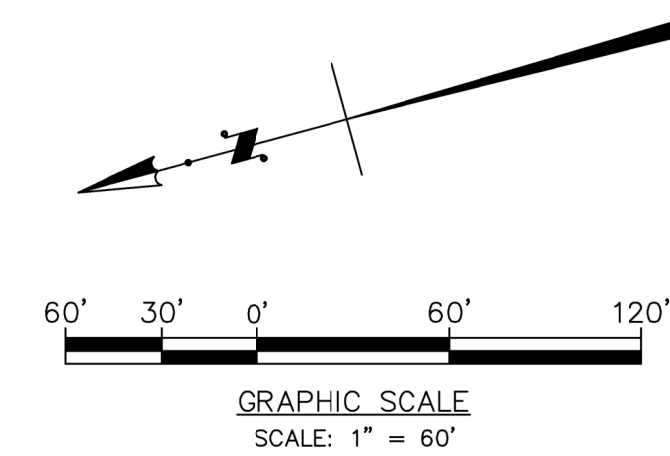
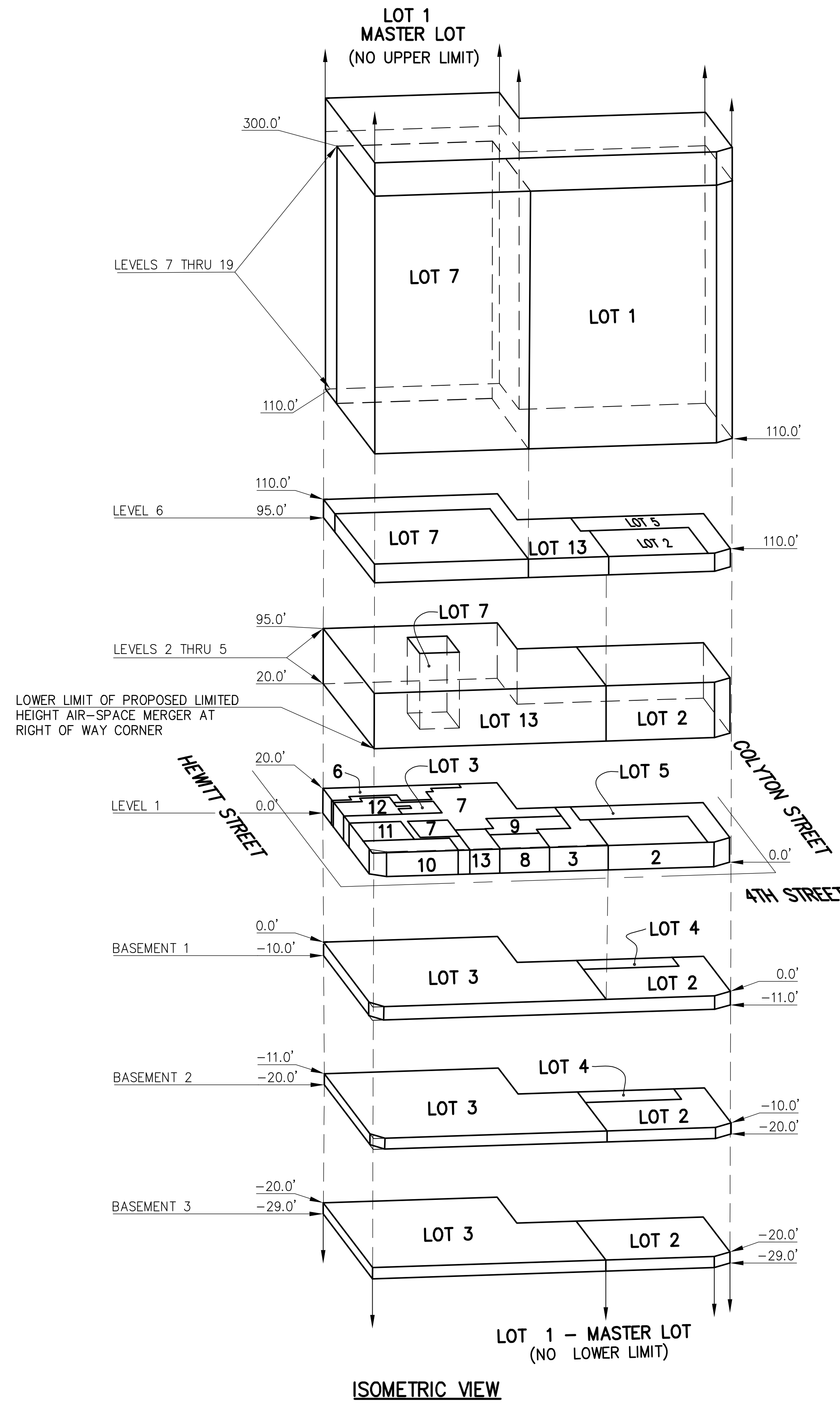
BENCHMARK	CITY OF LOS ANGELES BENCHMARK NO. 12-03812	
	WIRE SPK IN S CURB 4TH ST: 116.7 FT W OF HEWITT ST; 1.7 DT W OF CONC APRON FIRST DRIVEWAY W OF HEWITT ST.	
ELEVATION	260.784 FEET	ADJUSTMENT 2000 (NAVD88)
PATH/PLOT DATE	Jan. 26, 2023 - 13:48:37 DWG Name: \\L10200101\SERVER\GIS\ADMIN\SON\ENTR\T\N\PL-1103.dwg Updated by: dhoward	

**PSOMAS**  
555 South Flower Street, Suite 4300  
Los Angeles, CA 90071  
(213) 223-1400 (213) 223-1444 (FAX)  
www.psomas.com

VESTING TENTATIVE TRACT FOR MERGER, VACATION, AND RE-SUBDIVISION PURPOSES:  
**VESTING TENTATIVE TRACT NO. 74745**  
LIG-900, 910, 926 E. 4TH ST,  
405, 144 S. HEWITT ST, LLC  
IN THE CITY OF LOS ANGELES COUNTY OF LOS ANGELES STATE OF CALIFORNIA

DATE:	JANUARY 6, 2017	SHEET	
SCALE:	1" = 30'		3
PROJECT NUMBER	1LIG060100, TASK 102		4

12/10/22/2023 ENTITLEMENT SET



SEAL 	DESIGNED	DLR			
	DRAFTED	DLR			
	CHECKED	DRH			
	APP'D				

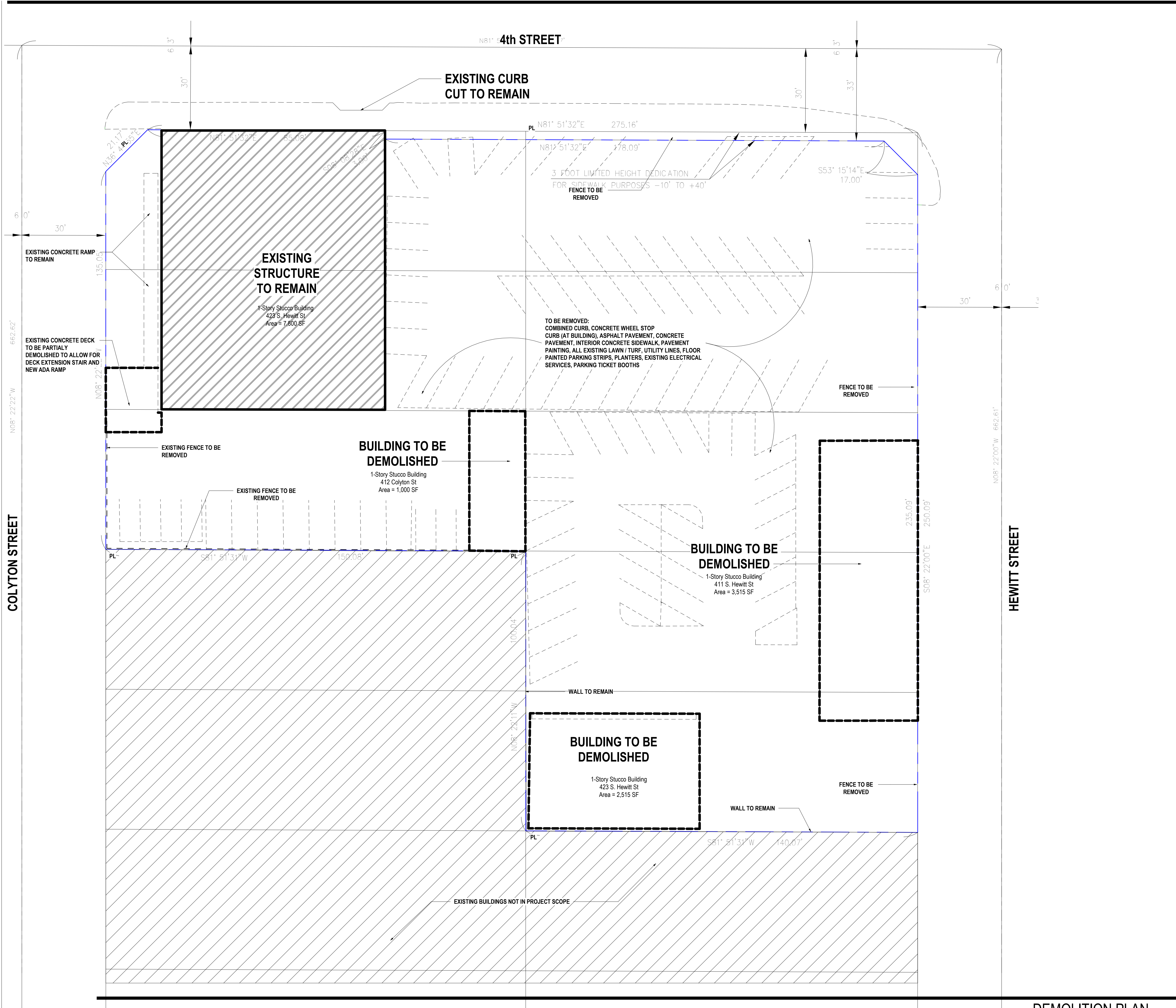
BENCHMARK	CITY OF LOS ANGELES BENCHMARK NO. 12-03812			
	WIRE SPK IN S CURB 4TH ST; 116.7 FT W OF HEWITT ST; 1.7 DT W OF CONC APRON FIRST DRIVEWAY W OF HEWITT ST.			
ELEVATION	260.784 FEET	ADJUSTMENT	2000 (NAVD88)	
DATE	12/12/18	DESCRIPTION	ARCHITECTURAL DESIGN REVISION - 12/07/18	

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VESTING TENTATIVE TRACT FOR MERGER, VACATION, AND RE-SUBDIVISION PURPOSES:  
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 LIG-900, 910, 926 E. 4TH ST,  
 405, 144 S. HEWITT ST, LLC  
 IN THE CITY OF LOS ANGELES COUNTY OF LOS ANGELES STATE OF CALIFORNIA

DATE:	JANUARY 6, 2017	SHEET	
SCALE:	AS SHOWN		4
PROJECT NUMBER:	1LIG060100, TASK 102		4

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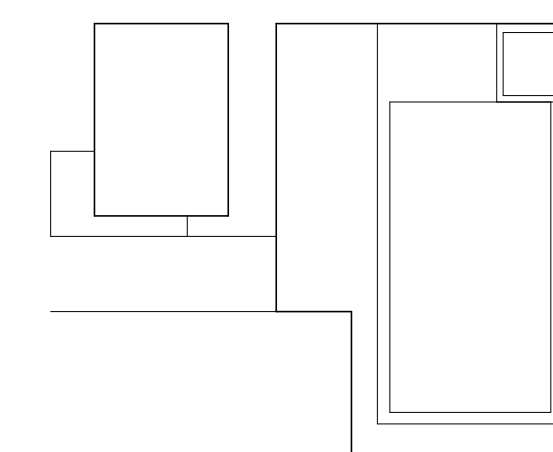
DEMOLITION PLAN 1  
1/16" = 1'-0" SCALE

### SHEET NOTES

**EXHIBIT A**  
**CPC-2017-469-GPA-VZC-HD-MCUP-SPR**  
**October 12, 2023**

### GENERAL NOTES

### KEY PLAN



401 S. Hewitt, Los Angeles, CA 90013

## Gensler

500 South Figueroa Street  
Los Angeles, California 90071  
United States  
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Fax 213.327.3601

WALTER P MOORE



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277 S. Lake Street  
Burbank, CA 91502  
Tel 818.508.6300  
Fax 818.508.7050



555 Flower St., Suite 4300  
Los Angeles, CA 90071  
Tel 213.223.1400



423 Gin Ling Way  
Los Angeles, CA 90012  
Tel 213.234.0057

Date	Description
02/06/2017	ENTITLEMENT SET
07/14/2017	ENTITLEMENT SET
10/25/2017	ENTITLEMENT SET
05/31/2019	ENTITLEMENT SET
10/02/2019	ENTITLEMENT SET
04/07/2020	ENTITLEMENT SET
03/16/2022	ENTITLEMENT SET
06/16/2023	ENTITLEMENT SET
10/12/2023	ENTITLEMENT SET

Seal / Signature

# NOT FOR CONSTRUCTION

Project Name  
401 S Hewitt

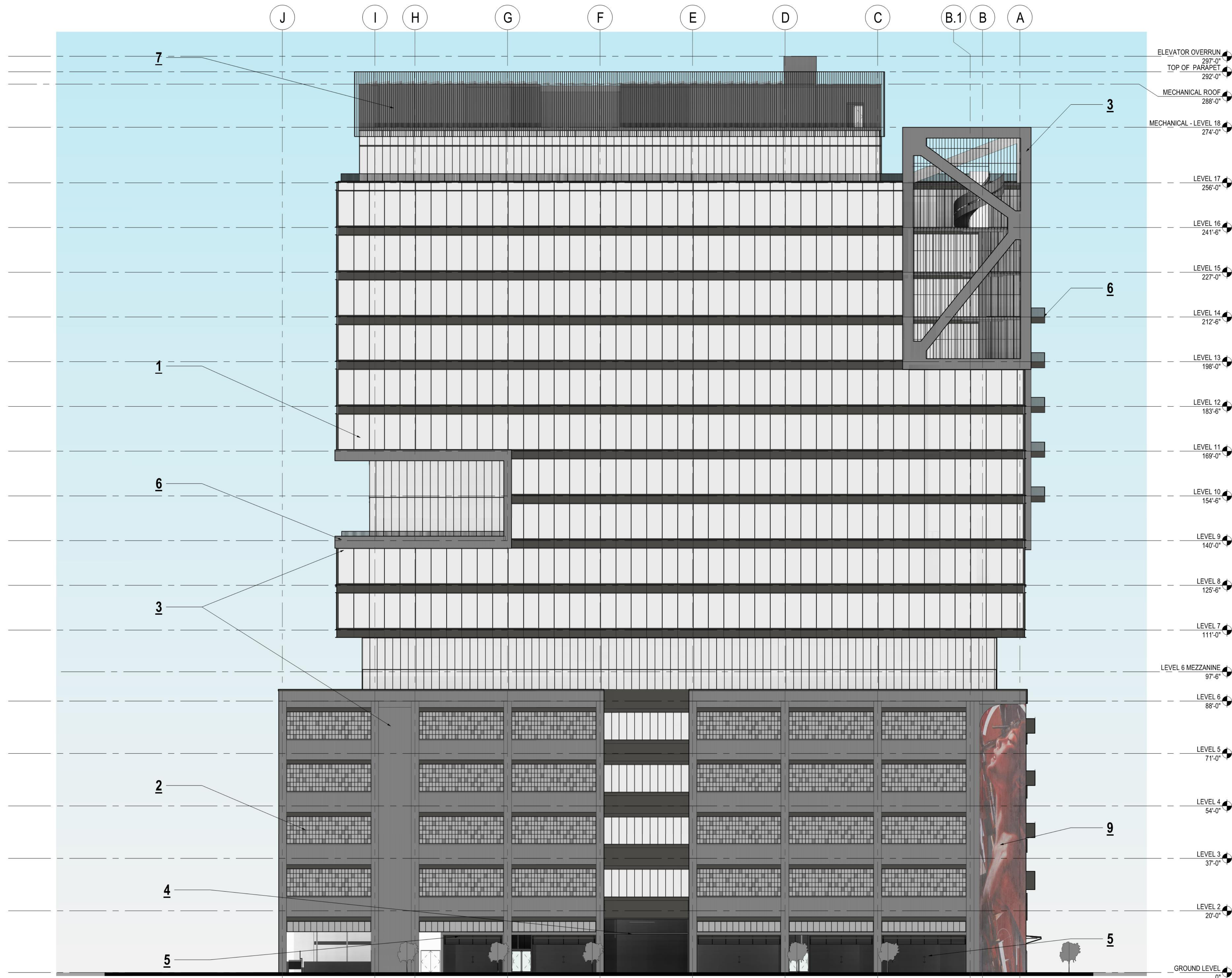
Project Number  
05.1291.000

Description  
DEMOLITION PLAN

Scale  
1/16" = 1'-0"

# DM0.100

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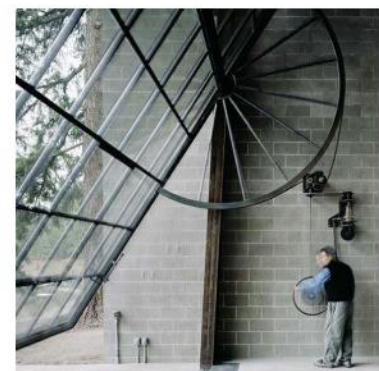
1 CURTAIN WALL SYSTEM



2 OBSCURE GLASS IN INDUSTRIAL WINDOW FRAME



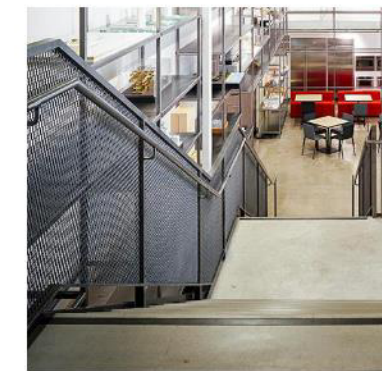
3 EXPOSED CONCRETE W/ FORM-LINER PATTERN



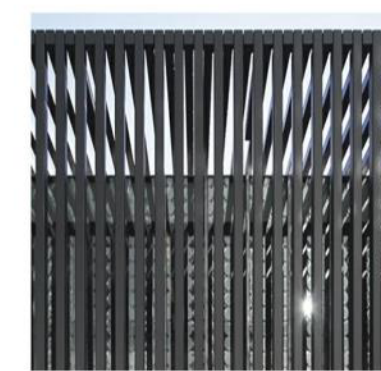
4 MONUMENTAL DOORS (NIGHT GATE)



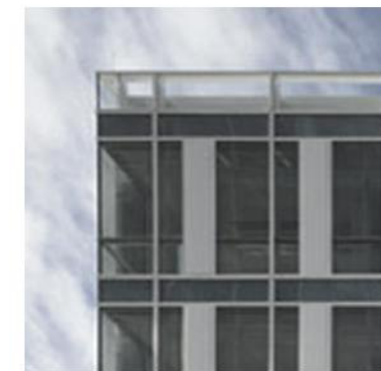
5 BI-FOLD DOORS



6 METAL SCREEN RAILING



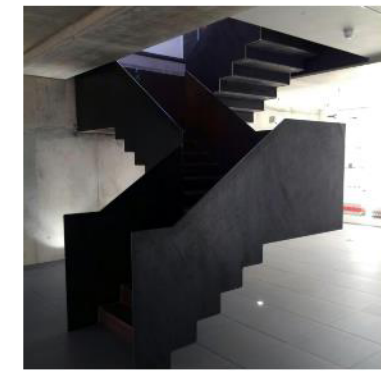
7 BLACK METAL SCREEN



8 SHADOWBOX



9 MURAL



10 METAL STAIR

**SHEET NOTES**

Finish Code	Type
-------------	------



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Los Angeles, CA 90071  
Tel 213.223.1400



423 Gin Ling Way  
Los Angeles, CA 90012  
Tel 213.234.0057

Date	Description
6 05/31/2019	ENTITLEMENT SET
7 10/02/2019	ENTITLEMENT SET
8 04/07/2020	ENTITLEMENT SET
9 03/04/2022	ENTITLEMENT SET
10 03/16/2022	ENTITLEMENT SET
11 06/16/2023	ENTITLEMENT SET
12 10/12/2023	ENTITLEMENT SET

**EXHIBIT A**  
**CPC-2017-469-GPA-VZC-HD-MCUP-SPR**  
**October 12, 2023**

**GENERAL NOTES**

Seal / Signature

**NOT FOR CONSTRUCTION**

Project Name  
401 S Hewitt

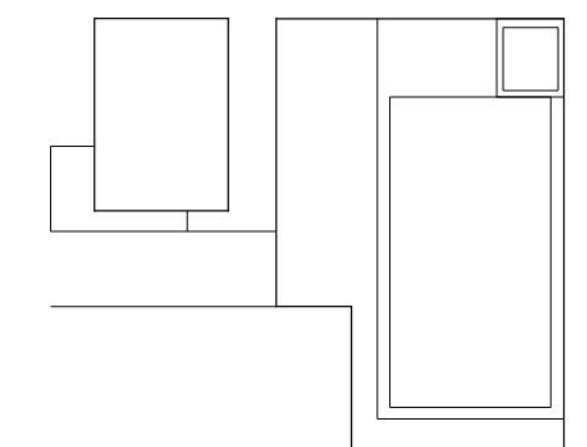
Project Number  
05.1291.000

Description  
EAST ELEVATION

Scale  
1/16" = 1'-0"

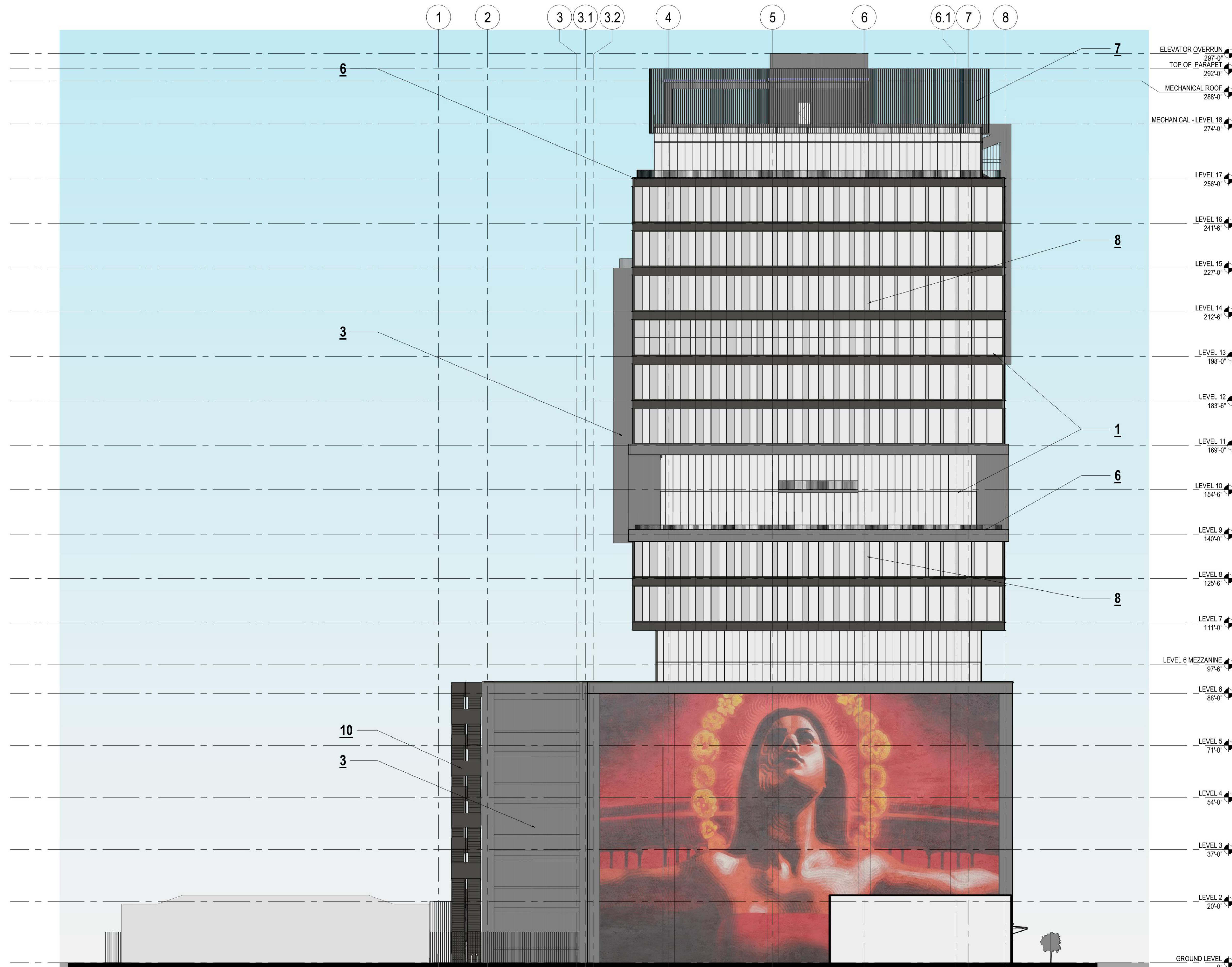
**A0.101**

**KEY PLAN**





3/15/2022 1:04:25 PM BIM\_360/005.1291.000 - 4th & Hewitt - Art District LA401 - Architecture\_F20.rvt



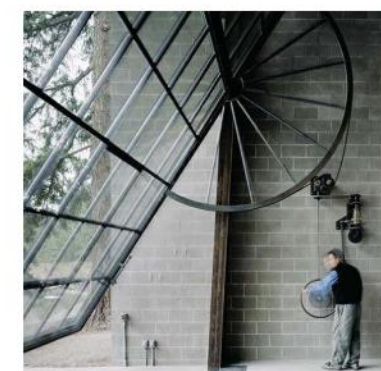
1 CURTAIN WALL SYSTEM



2 OBSCURE GLASS IN INDUSTRIAL WINDOW FRAME



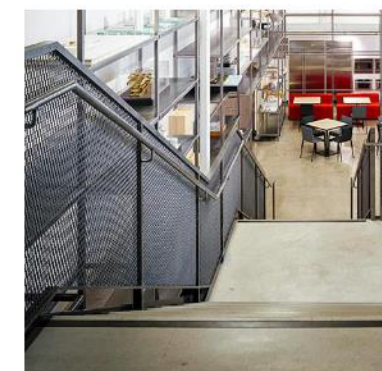
3 EXPOSED CONCRETE W/ FORM-LINER PATTERN



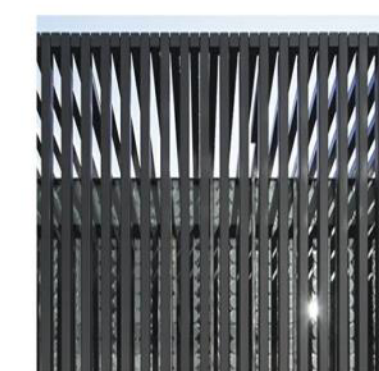
4 MONUMENTAL DOORS (NIGHT GATE)



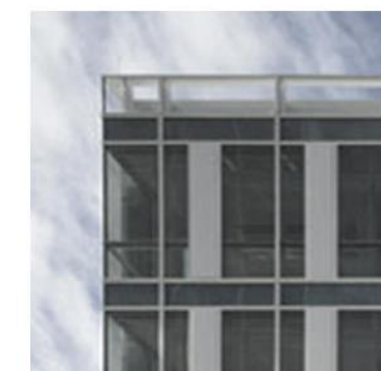
5 BI-FOLD DOORS



6 METAL SCREEN RAILING



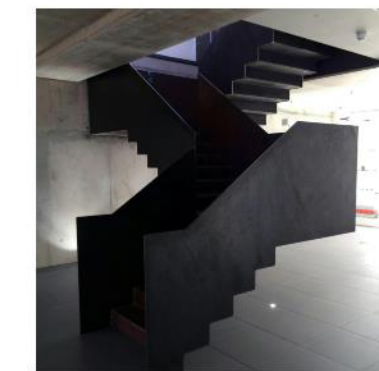
7 BLACK METAL SCREEN



8 SHADOWBOX



9 MURAL



10 METAL STAIR

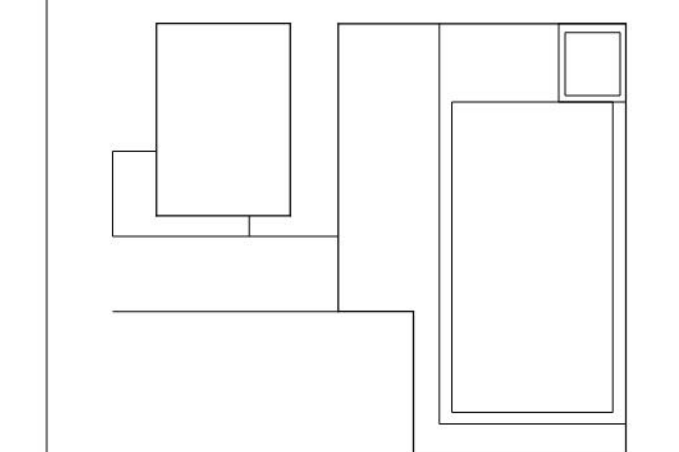
**SHEET NOTES**

Finish Code	Type
-------------	------

**EXHIBIT A**  
**CPC-2017-469-GPA-VZC-HD-MCUP-SPR**  
**October 12, 2023**

**GENERAL NOTES**

**KEY PLAN**



401 S. Hewitt, Los Angeles, CA 90013

**Gensler**

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 Los Angeles, CA 90071  
 Tel 213.223.1400



423 Gin Ling Way  
 Los Angeles, CA 90012  
 Tel 213.234.0057

Date	Description
6 05/31/2019	ENTITLEMENT SET
7 10/02/2019	ENTITLEMENT SET
8 04/07/2020	ENTITLEMENT SET
9 03/04/2022	ENTITLEMENT SET
10 03/16/2022	ENTITLEMENT SET
11 06/16/2023	ENTITLEMENT SET
12 10/12/2023	ENTITLEMENT SET

Seal / Signature

**NOT FOR CONSTRUCTION**

Project Name  
 401 S Hewitt

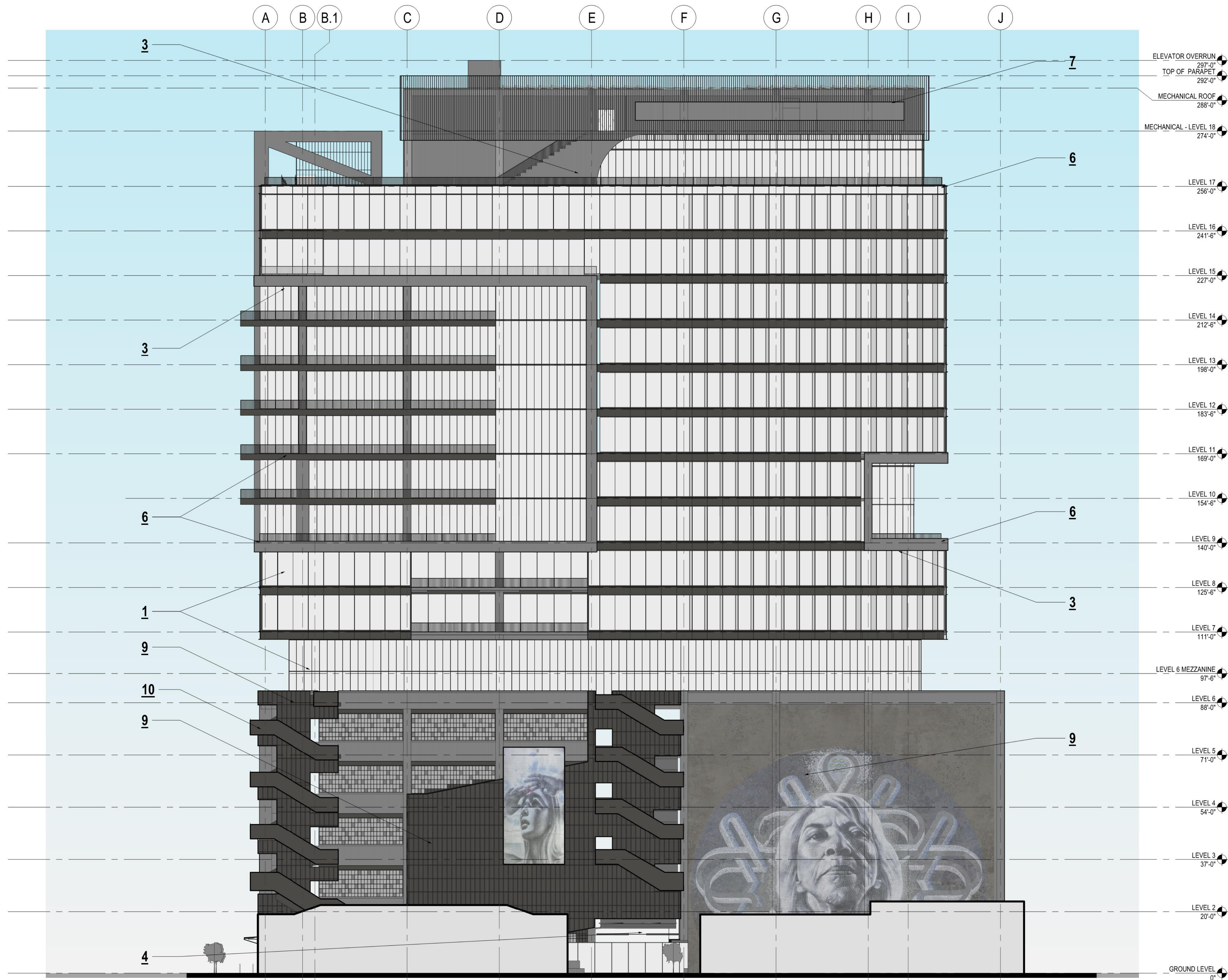
Project Number  
 05.1291.000

Description  
 SOUTH ELEVATION

Scale  
 1/16" = 1'-0"

**A0.102**

3/15/2022 1:06:59 PM BIM\_360/005.1291.000 - 4th & Hewitt - Art District LA401 - Architecture\_F20.rvt



**SHEET NOTES**

Finish Code	Type
-------------	------

- ELEVATOR OVERRUN 297'-0"
- TOP OF PARAPET 292'-0"
- MECHANICAL ROOF 288'-0"
- MECHANICAL - LEVEL 18 274'-0"
- LEVEL 17 256'-0"
- LEVEL 16 241'-6"
- LEVEL 15 227'-0"
- LEVEL 14 212'-6"
- LEVEL 13 198'-0"
- LEVEL 12 183'-6"
- LEVEL 11 169'-0"
- LEVEL 10 154'-6"
- LEVEL 9 140'-0"
- LEVEL 8 125'-6"
- LEVEL 7 111'-0"
- LEVEL 6 MEZZANINE 97'-6"
- LEVEL 6 88'-0"
- LEVEL 5 71'-0"
- LEVEL 4 54'-0"
- LEVEL 3 37'-0"
- LEVEL 2 20'-0"
- GROUND LEVEL 0"



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Tel 213.223.1400



423 Gin Ling Way  
Los Angeles, CA 90012  
Tel 213.234.0057

Date	Description
6 05/31/2019	ENTITLEMENT SET
7 10/02/2019	ENTITLEMENT SET
8 04/07/2020	ENTITLEMENT SET
9 03/04/2022	ENTITLEMENT SET
10 03/16/2022	ENTITLEMENT SET
11 06/16/2023	ENTITLEMENT SET
12 10/12/2023	ENTITLEMENT SET

**EXHIBIT A**  
**CPC-2017-469-GPA-VZC-HD-MCUP-SPR**  
**October 12, 2023**

**GENERAL NOTES**

Seal / Signature

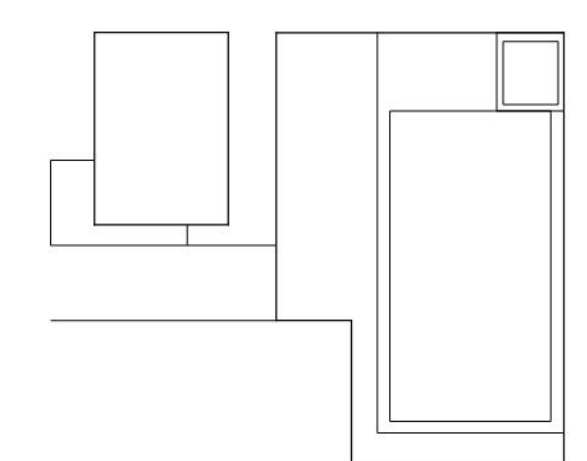
**NOT FOR CONSTRUCTION**

Project Name  
401 S Hewitt

Project Number  
05.1291.000

Description  
WEST ELEVATION

**KEY PLAN**



Scale  
1/16" = 1'-0"

**A0.103**



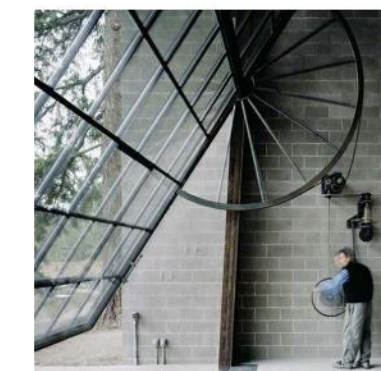
1 CURTAIN WALL SYSTEM



2 OBSCURE GLASS IN INDUSTRIAL WINDOW FRAME



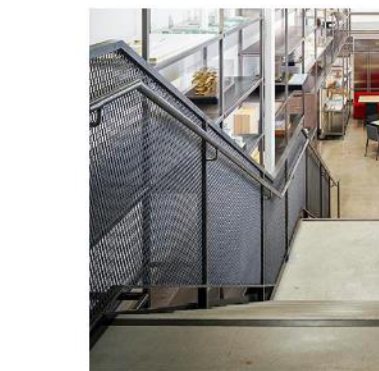
3 EXPOSED CONCRETE W/ FORMLINER PATTERN



4 MONUMENTAL DOORS (NIGHT GATE)



5 BI-FOLD DOORS



6 METAL SCREEN RAILING



7 BLACK METAL SCREEN



8 SHADOWBOX

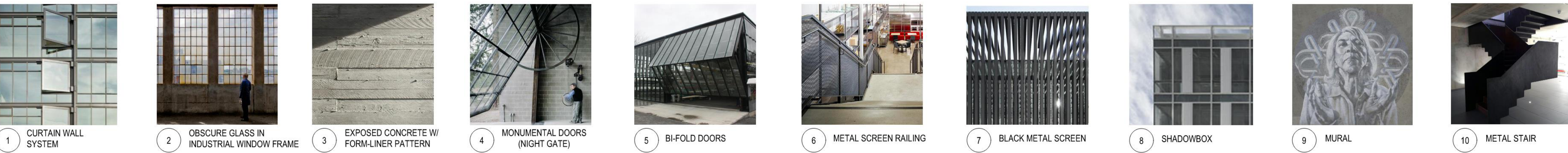
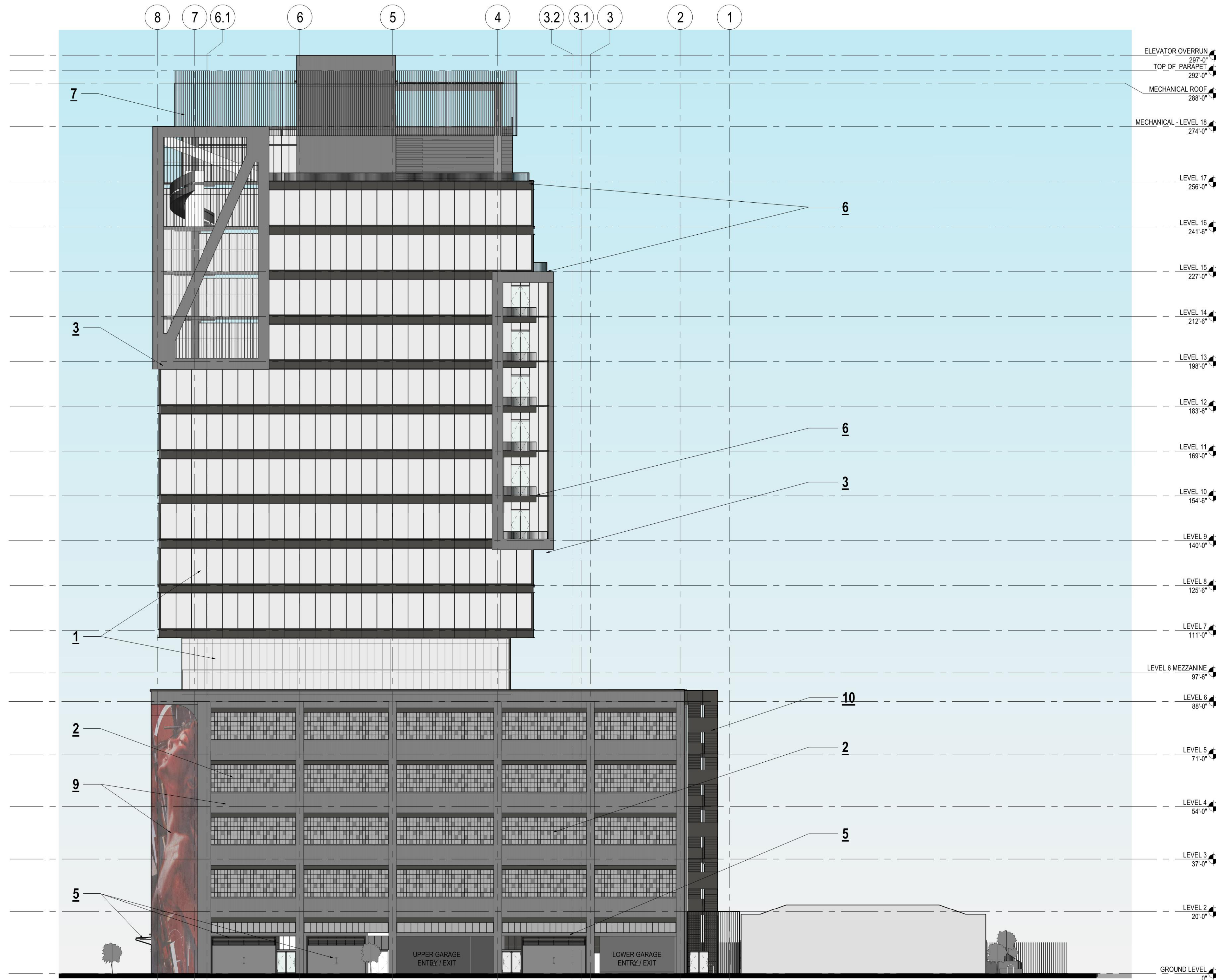


9 MURAL



10 METAL STAIR

3/15/2022 1:08:40 PM BIM\_360/005.1291.000 - 4th & Hewitt - Art District LA401 - Architecture\_F22.rvt



**SHEET NOTES**

Finish Code	Type
-------------	------

△	Date	Description
6	05/31/2019	ENTITLEMENT SET
7	10/02/2019	ENTITLEMENT SET
8	04/07/2020	ENTITLEMENT SET
9	03/04/2022	ENTITLEMENT SET
10	03/16/2022	ENTITLEMENT SET
11	06/16/2023	ENTITLEMENT SET
12	10/12/2023	ENTITLEMENT SET

**EXHIBIT A**  
**CPC-2017-469-GPA-VZC-HD-MCUP-SPR**  
**October 12, 2023**

**GENERAL NOTES**

Seal / Signature



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**NOT FOR CONSTRUCTION**

Project Name  
 401 S Hewitt

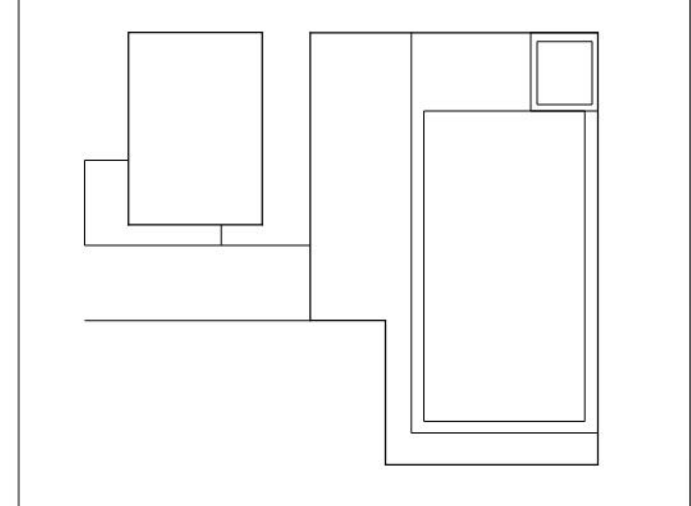
Project Number  
 05.1291.000

Description  
 NORTH ELEVATION

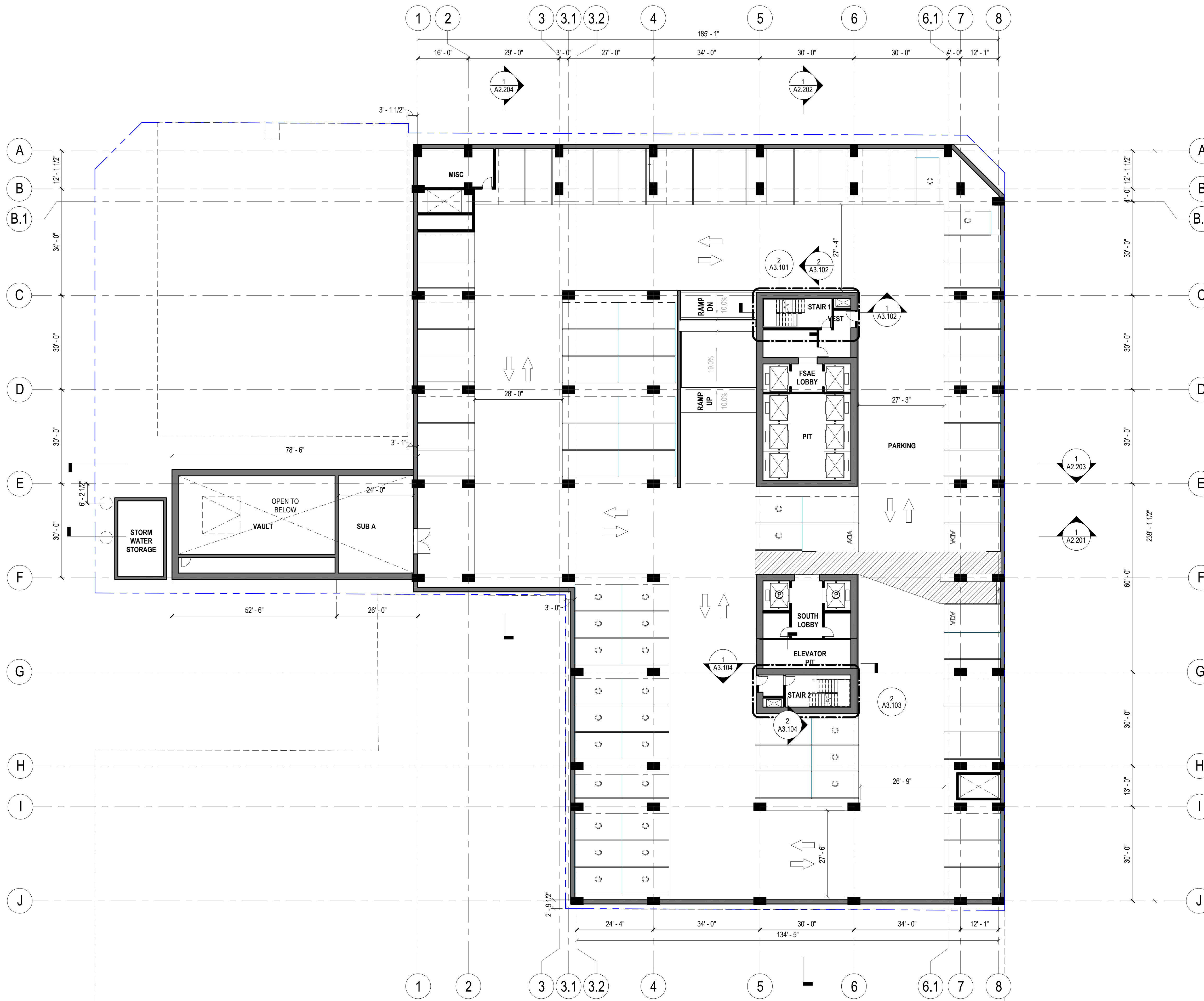
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**A0.104**

**KEY PLAN**



6/15/2023 2:23:17 PM BIM\_360/1005.1291.000 - 4th & Hewitt - Art District LA401 - Architecture\_F20.rvt



**SHEET NOTES**

Parking Schedule - Basement 1		
Level	Count	Type
BASEMENT 1	3	ADA, 9' x 18' (5' Aisle)
BASEMENT 1	4	Compact, 7' 6" x 15' - 90 deg
BASEMENT 1	23	Compact, 8' 6" x 15' - 90 deg
BASEMENT 1	53	Standard, 8' 6" x 18' - 90 deg
<b>Grand total</b>	<b>83</b>	



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Tel 213.234.0057

Date	Description
1 02/06/2017	ENTITLEMENT SET
2 07/14/2017	ENTITLEMENT SET
3 10/25/2017	ENTITLEMENT SET
6 05/31/2019	ENTITLEMENT SET
7 10/02/2019	ENTITLEMENT SET
8 04/07/2020	ENTITLEMENT SET
11 06/16/2023	ENTITLEMENT SET
12 10/12/2023	ENTITLEMENT SET

**EXHIBIT A**  
**CPC-2017-469-GPA-VZC-HD-MCUP-SPR**  
**October 12, 2023**

**GENERAL NOTES**

NOTE:  
All areas shown in plan are Net areas.  
C: Compact Parking Stall  
EV: Electric Vehicle Parking Stall  
S.S.: Stacking Standard Parking Stall  
S.C.: Stacking Compact Parking Stall

Seal / Signature

**NOT FOR CONSTRUCTION**

Project Name  
401 S Hewitt

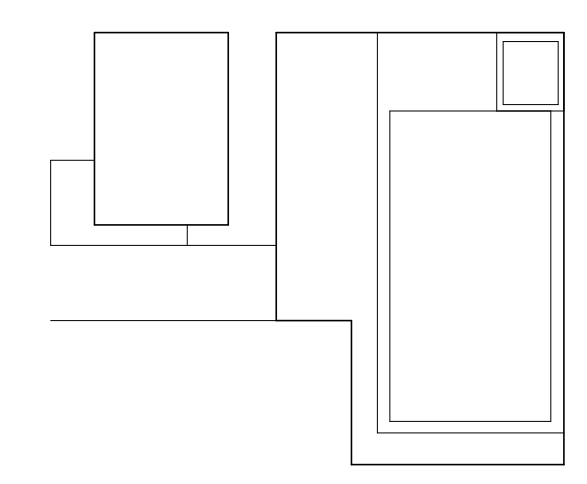
Project Number  
05.1291.000

Description  
PLAN - OVERALL - BASEMENT 1

Scale  
1/16" = 1'-0"

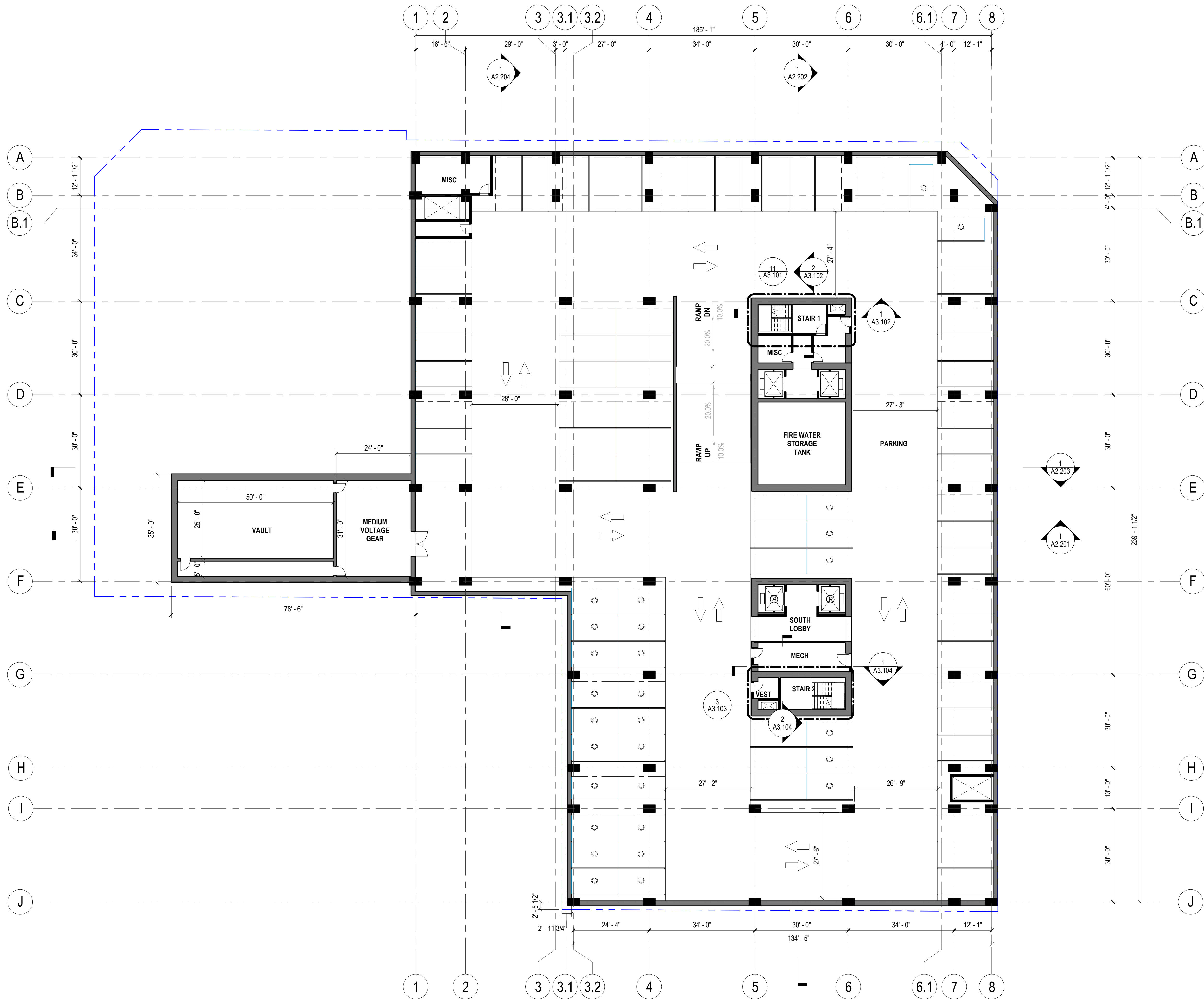
**A1.2B1**

**KEY PLAN**



**BASEMENT 1**  
1/16" = 1'-0" SCALE. 1

6/15/2023 2:23:24 PM BIM\_360/1005.1291.000 - 4th & Hewitt - Art District LA401 - Architecture\_F20.rvt



**SHEET NOTES**

Parking Schedule - Basement 2		
Level	Count	Type
BASEMENT 2	4	Compact, 7' 6" x 15' - 90 deg
BASEMENT 2	24	Compact, 8' 6" x 15' - 90 deg
BASEMENT 2	59	Standard, 8' 6" x 18' - 90 deg
Grand total	87	



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Date	Description
1 02/06/2017	ENTITLEMENT SET
2 07/14/2017	ENTITLEMENT SET
3 10/25/2017	ENTITLEMENT SET
6 05/31/2019	ENTITLEMENT SET
7 10/02/2019	ENTITLEMENT SET
8 04/07/2020	ENTITLEMENT SET
11 06/16/2023	ENTITLEMENT SET
12 10/12/2023	ENTITLEMENT SET

**EXHIBIT A**  
**CPC-2017-469-GPA-VZC-HD-MCUP-SPR**  
**October 12, 2023**

**GENERAL NOTES**

NOTE:  
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C: Compact Parking Stall  
EV: Electric Vehicle Parking Stall  
S.S: Stacking Standard Parking Stall  
S.C: Stacking Compact Parking Stall

Seal / Signature

**NOT FOR CONSTRUCTION**

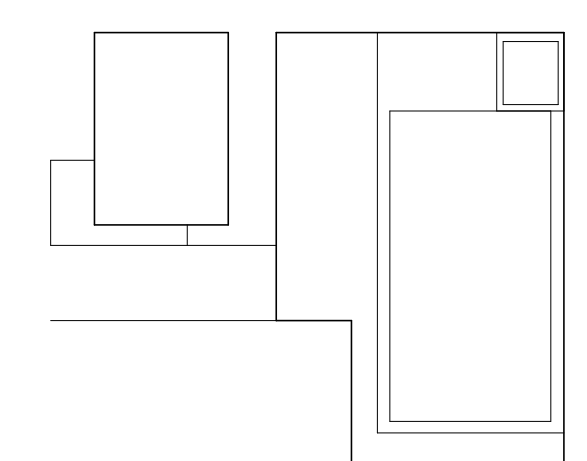
Project Name  
401 S Hewitt

Project Number  
05.1291.000

Description  
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Scale  
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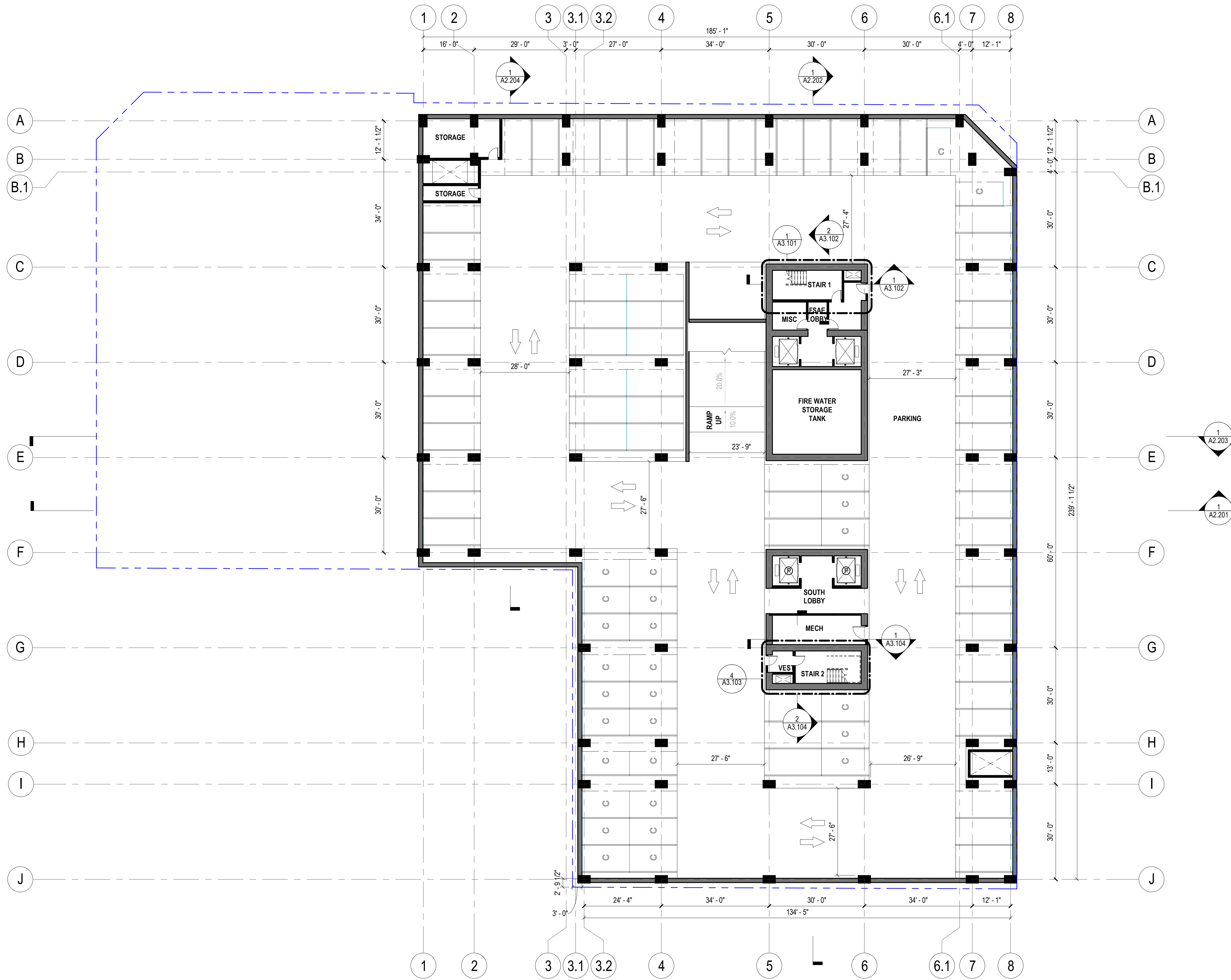
**KEY PLAN**



**A1.2B2**

**BASEMENT 2**  
1/16" = 1'-0" SCALE.

6/15/2023 2:23:32 PM BIM\_360/005.1291.000 - 4th & Hewitt - Art District LA401 - Architecture\_F20.rvt



**SHEET NOTES**

Parking Schedule - Basement 3		
Level	Count	Type
BASEMENT 3	4	Compact, 7' 6" x 15' - 90 deg
BASEMENT 3	24	Compact, 8' 6" x 15' - 90 deg
BASEMENT 3	62	Standard, 8' 6" x 18' - 90 deg
Grand total	90	



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Tel 213.234.0057

Date	Description
1 02/06/2017	ENTITLEMENT SET
2 07/14/2017	ENTITLEMENT SET
3 10/25/2017	ENTITLEMENT SET
6 05/31/2019	ENTITLEMENT SET
7 10/02/2019	ENTITLEMENT SET
8 04/07/2020	ENTITLEMENT SET
11 06/16/2023	ENTITLEMENT SET
12 10/12/2023	ENTITLEMENT SET

**EXHIBIT A**  
**CPC-2017-469-GPA-VZC-HD-MCUP-SPR**  
**October 12, 2023**

**GENERAL NOTES**

NOTE:  
All areas shown in plan are Net areas.  
C: Compact Parking Stall  
EV: Electric Vehicle Parking Stall  
S.S: Stacking Standard Parking Stall  
S.C: Stacking Compact Parking Stall

Seal / Signature

**NOT FOR CONSTRUCTION**

Project Name  
401 S Hewitt

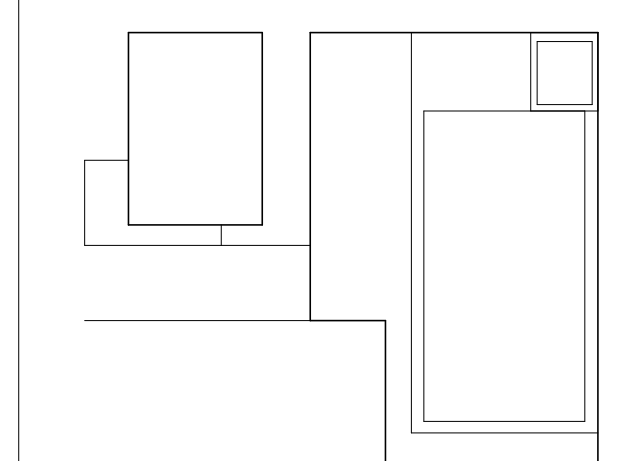
Project Number  
05.1291.000

Description  
PLAN - OVERALL - BASEMENT 3

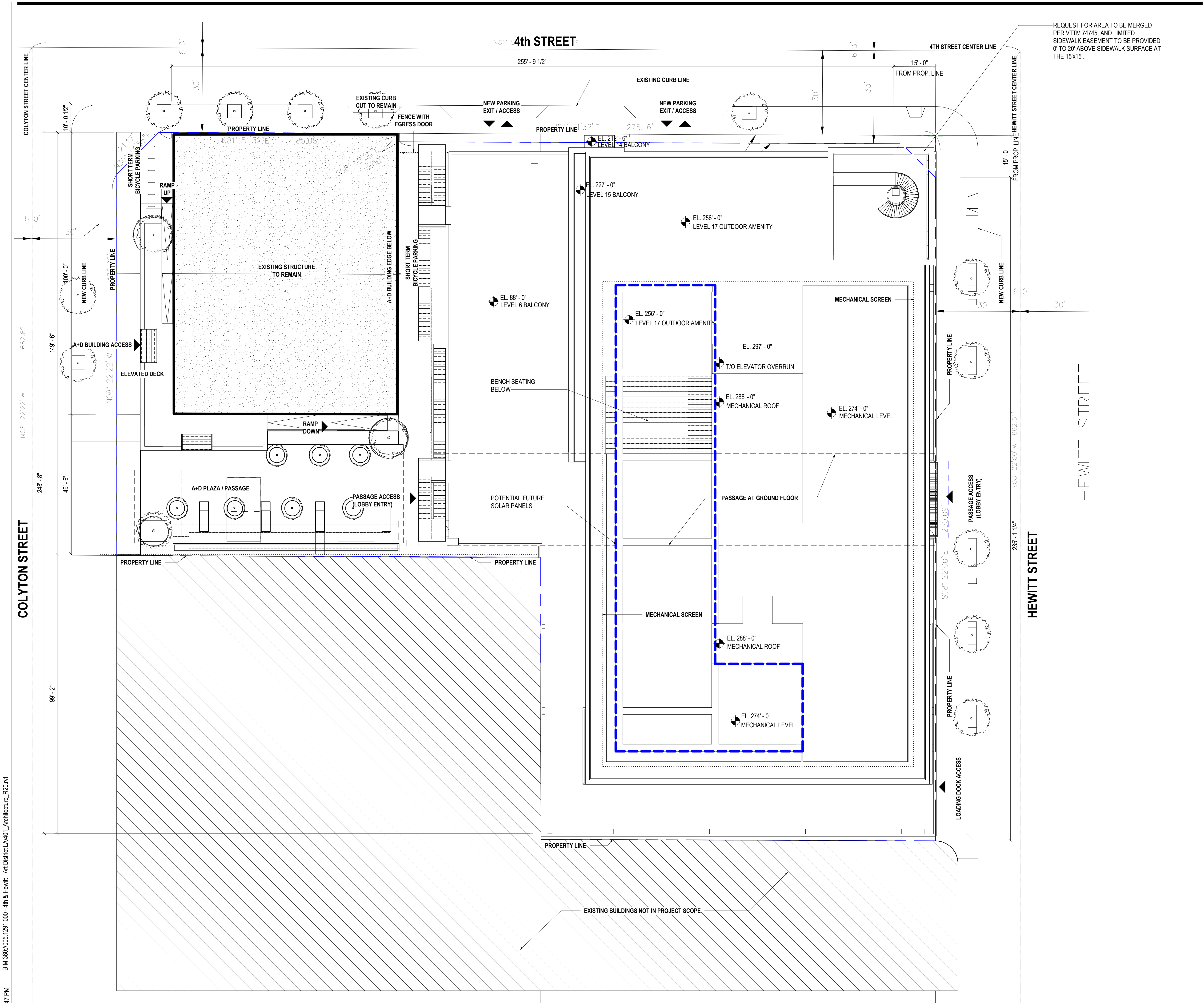
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**A1.2B3**

**KEY PLAN**



**BASEMENT 3**  
1/16" = 1'-0" SCALE: **1**



REQUEST FOR AREA TO BE MERGED PER VTTM 74745, AND LIMITED SIDEWALK EASEMENT TO BE PROVIDED 0' TO 20' ABOVE SIDEWALK SURFACE AT THE 15x15'.

**SHEET NOTES**

**LOT COVERAGE**

LOT AREA	57,325 SF - 100%
BUILDING AREA	48,700 SF - 85%
HARDSCAPE AREA	6,810 SF - 12%
LANDSCAPE AREA	1,815 SF - 3%

**SET BACK**

ALL SET BACKS FROM PROPERTY LINE TO BE EQUAL

**SECTION 3202 ENCROACHMENTS**  
 SECTION 3202.3.2  
 WHERE THE VERTICAL CLEARANCE ABOVE GRADE TO PROJECTING WINDOWS, BALCONIES, ARCHITECTURAL FEATURES OR MECHANICAL EQUIPMENT IS MORE THAN 8 FEET, 1 INCH OF CLEARANCE ABOVE 8 FEET, BUT THE MAXIMUM ENCROACHMENT SHALL BE 4 FEET.



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Date	Description
1 02/06/2017	ENTITLEMENT SET
2 07/14/2017	ENTITLEMENT SET
3 10/25/2017	ENTITLEMENT SET
4 11/21/2018	EIR SET
6 05/31/2019	ENTITLEMENT SET
7 10/02/2019	ENTITLEMENT SET
8 04/07/2020	ENTITLEMENT SET
10 03/16/2022	ENTITLEMENT SET
11 06/16/2023	ENTITLEMENT SET
12 10/12/2023	ENTITLEMENT SET

**EXHIBIT A**  
**CPC-2017-469-GPA-VZC-HD-MCUP-SPR**  
**October 12, 2023**

**GENERAL NOTES**

Seal / Signature

**NOT FOR CONSTRUCTION**

Project Name  
 401 S Hewitt

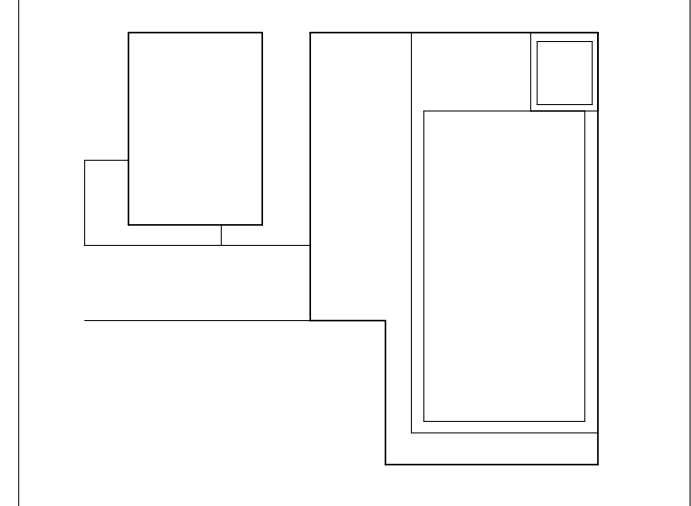
Project Number  
 05.1291.000

Description  
 PLOT PLAN

Scale  
 1/16" = 1'-0" Ref North

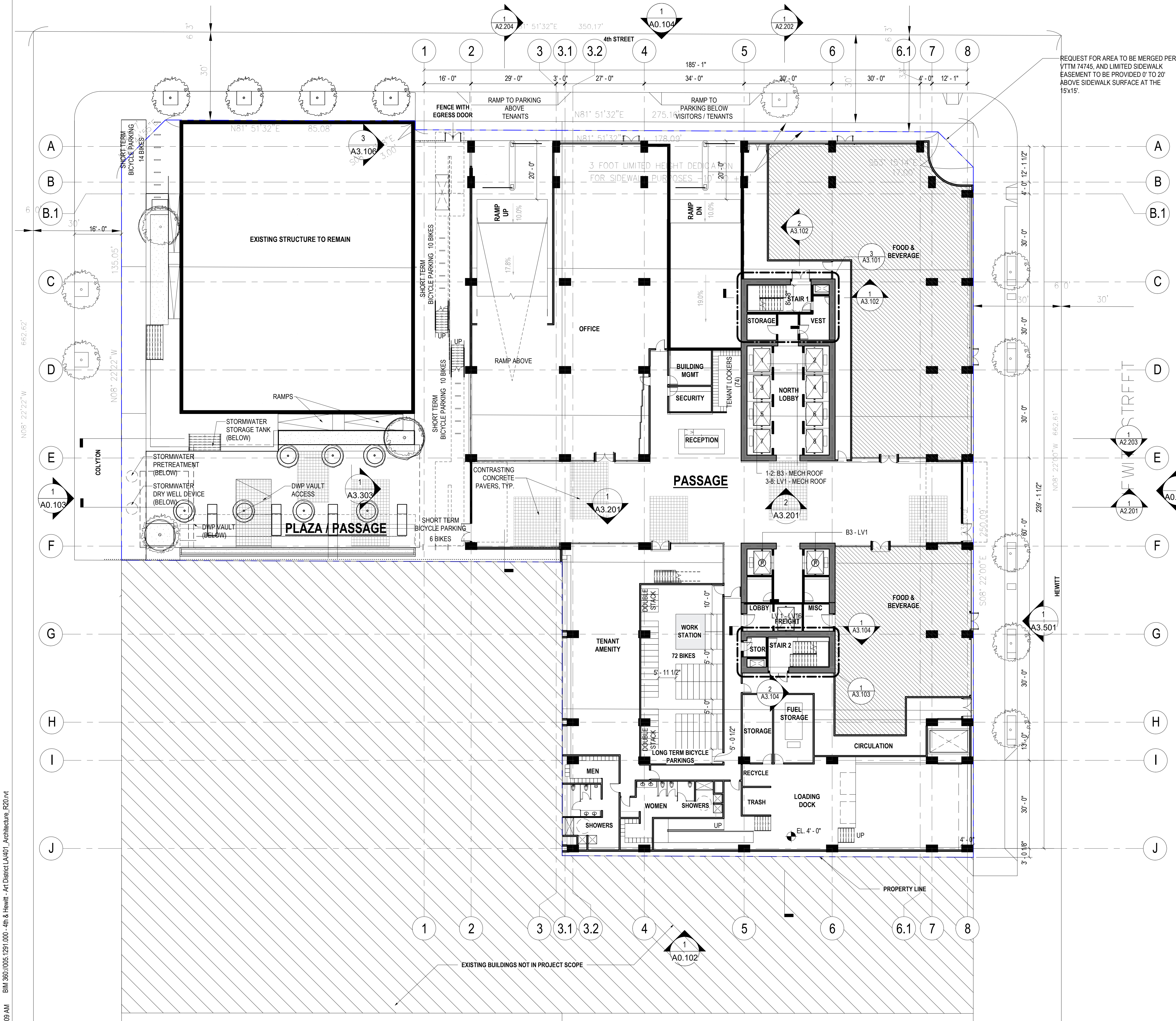
**A1.200**

**KEY PLAN**



**PLOT PLAN**  
 1/16" = 1'-0" SCALE.

9/8/2023 1:03:47 PM BIM\_360/1005.1291.000 - 4th & Hewitt - Art District LA401 - Architecture\_F20.rvt



**SHEET NOTES**

**CUB Note**  
 Possible F&B Area \*

\* PURSUANT TO LAMC SECTION 12.24.W1, APPLICANT REQUESTS A MASTER CONDITIONAL USE APPROVAL TO PERMIT THE SALE AND DISPENSING OF A FULL LINE OF ALCOHOLIC BEVERAGES FOR ON-SITE CONSUMPTION FOR UP TO 6 ESTABLISHMENTS, FOR A TOTAL OF UP TO 15,949 SQUARE FEET (INCLUDES 8,149 SQUARE FEET OF F&B SPACE PLUS 7,800 SQUARE FEET OF THE EXISTING STRUCTURE TO REMAIN).

**SECTION 3202 ENCROACHMENTS**  
 SECTION 3202.3.2  
 WHERE THE VERTICAL CLEARANCE ABOVE GRADE TO PROJECTING WINDOWS, BALCONIES, ARCHITECTURAL FEATURES OR MECHANICAL EQUIPMENT IS MORE THAN 8 FEET, 1 INCH OF CLEARANCE ABOVE 8 FEET, BUT THE MAXIMUM ENCROACHMENT SHALL BE 4 FEET.

REQUEST FOR AREA TO BE MERGED PER VTTM 74745, AND LIMITED SIDEWALK EASEMENT TO BE PROVIDED 0' TO 20' ABOVE SIDEWALK SURFACE AT THE 15x15'.

**EXHIBIT A**  
**CPC-2017-469-GPA-VZC-HD-MCUB-SPR**  
**October 12, 2023**

**GENERAL NOTES**

**LEGENDARY INVESTORS GROUP, LLC**  
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**SALT**  
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 Tel 213.234.0057

Date	Description
1 02/06/2017	ENTITLEMENT SET
2 07/14/2017	ENTITLEMENT SET
3 10/25/2017	ENTITLEMENT SET
6 05/31/2019	ENTITLEMENT SET
7 10/02/2019	ENTITLEMENT SET
8 04/07/2020	ENTITLEMENT SET
10 03/16/2022	ENTITLEMENT SET
11 06/16/2023	ENTITLEMENT SET
12 10/12/2023	ENTITLEMENT SET

Seal / Signature

**NOT FOR CONSTRUCTION**

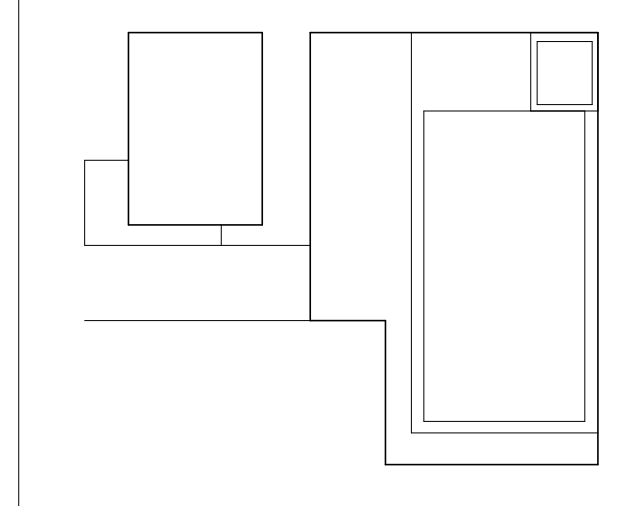
Project Name  
 401 S Hewitt

Project Number  
 05.1291.000

Description  
 PLAN - OVERALL - GROUND FLOOR

Scale  
 1/16" = 1'-0"

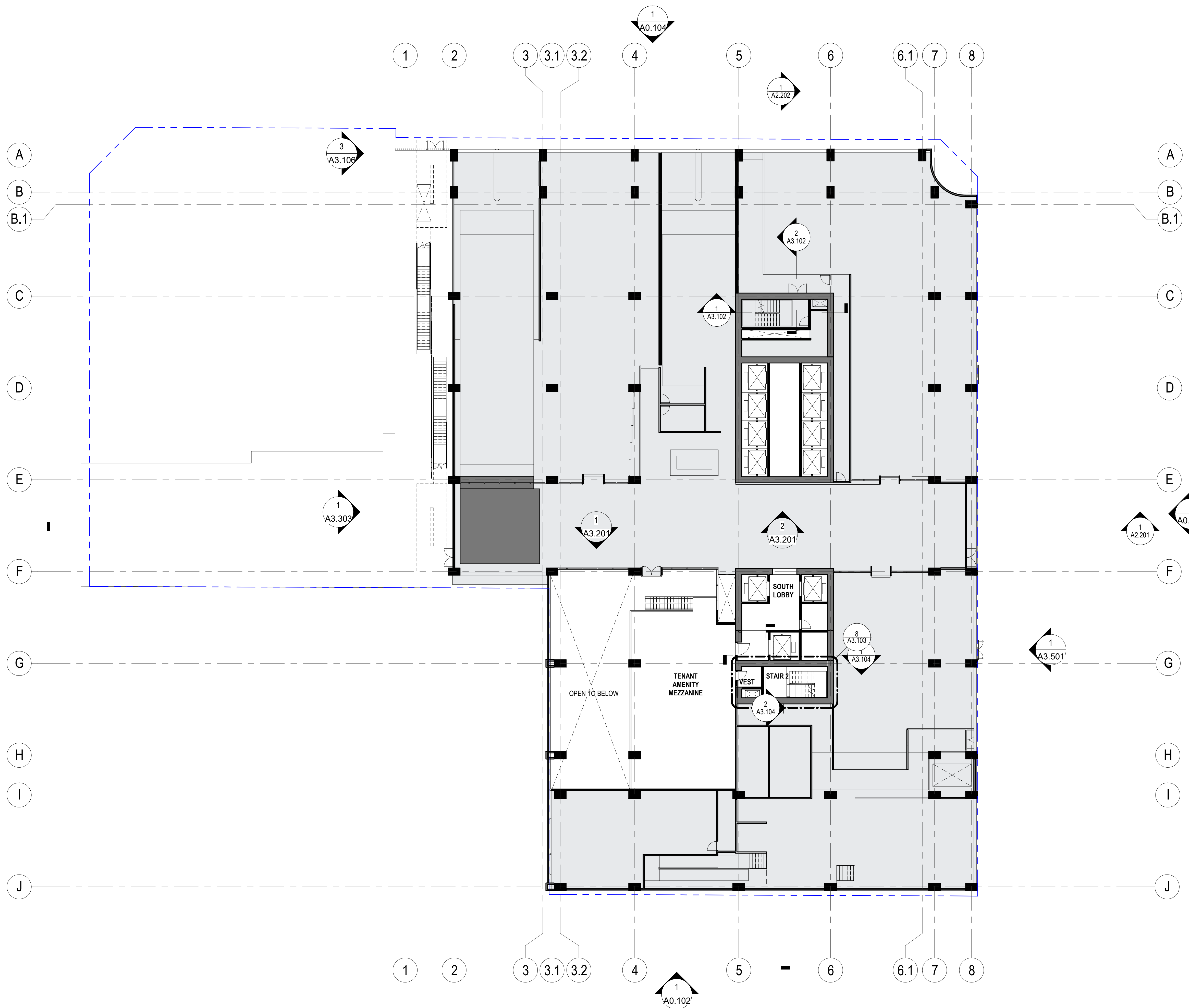
**KEY PLAN**



**A1.201**



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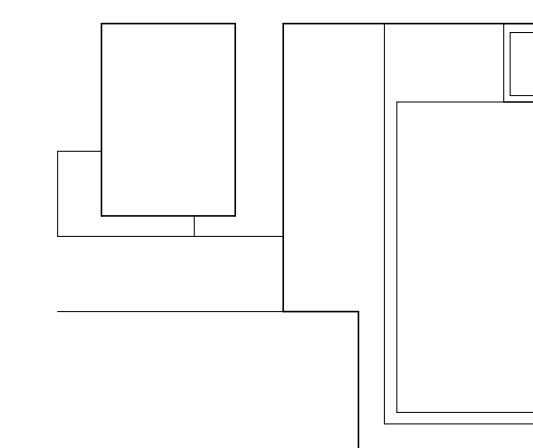
GROUND LEVEL MEZZANINE 1  
1/16" = 1'-0" SCALE

SHEET NOTES

**EXHIBIT A**  
**CPC-2017-469-GPA-VZC-HD-MCUP-SPR**  
**October 12, 2023**

GENERAL NOTES

KEY PLAN



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423 Gin Ling Way  
Los Angeles, CA 90012  
Tel 213.234.0057

Date	Description
5 01/31/2019	100% SD
7 10/02/2019	ENTITLEMENT SET
8 04/07/2020	ENTITLEMENT SET
10 03/16/2022	ENTITLEMENT SET
11 06/16/2023	ENTITLEMENT SET
12 10/12/2023	ENTITLEMENT SET

Seal / Signature

**NOT FOR CONSTRUCTION**

Project Name

401 S Hewitt

Project Number

05.1291.000

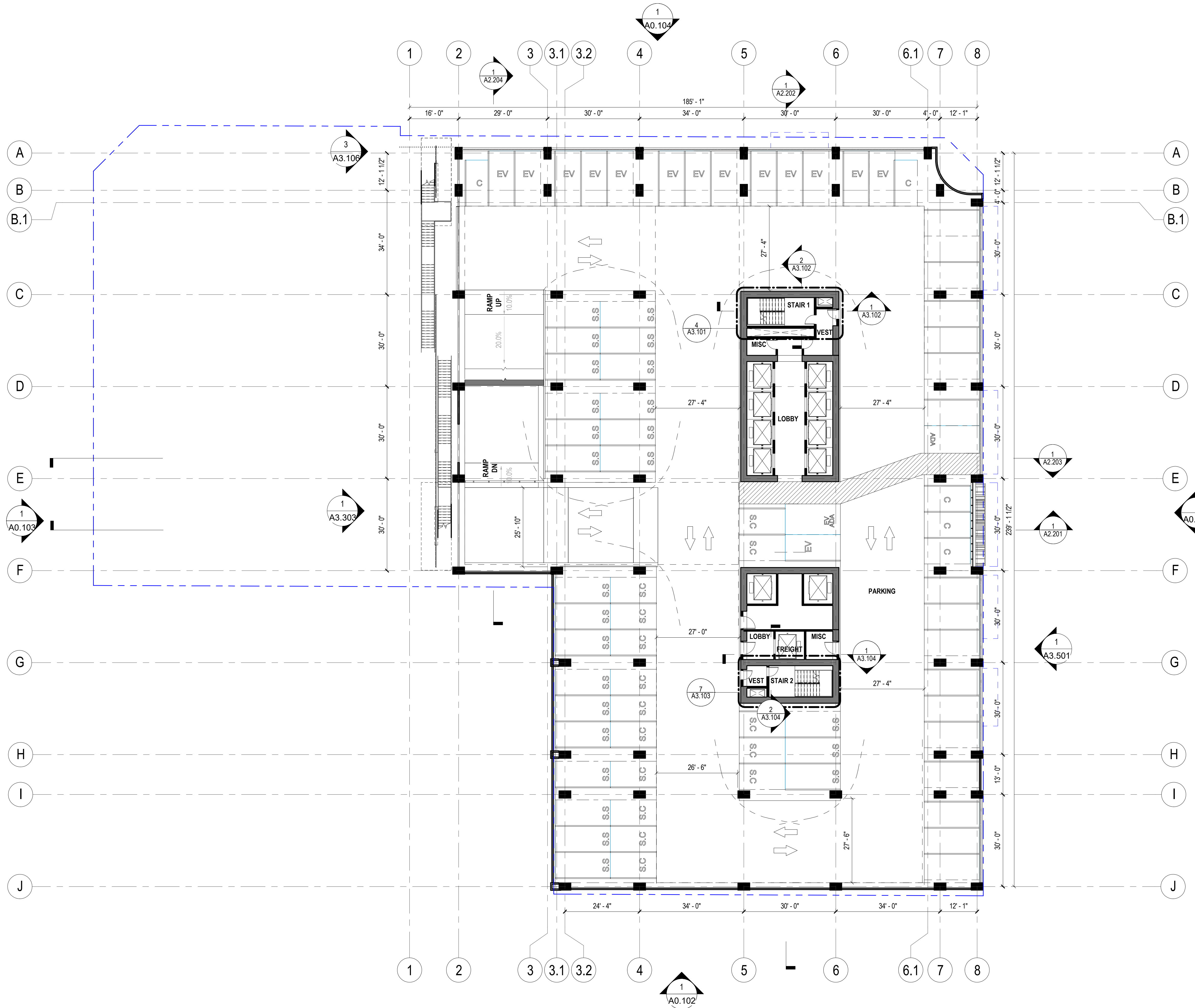
Description

PLAN - OVERALL - GROUND FLOOR  
MEZZANINE

Scale

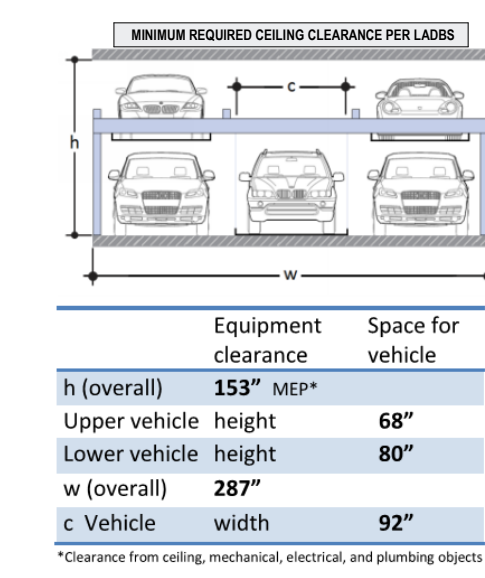
1/16" = 1'-0"

**A1.201.M**



**SHEET NOTES**

Parking Schedule - Level 2		
Level	Count	Type
LEVEL 2	1	ADA_ 9' x 18' (5' Aisle)
LEVEL 2	2	Compact_ 7' 6" x 15' - 90 deg
LEVEL 2	3	Compact_ 8' 6" x 15' - 90 deg
LEVEL 2	1	EV AMBULATORY ADA_ 10' x 18' (5' Aisle)
LEVEL 2	30	Stacking Compact_ 8' 6" x 15' - 90 deg
LEVEL 2	50	Stacking Standard_ 8' 6" x 18' - 90 deg
LEVEL 2	14	Standard EV_ 8' 6" x 18' - 90 deg
LEVEL 2	17	Standard_ 8' 6" x 18' - 90 deg
Grand total	118	

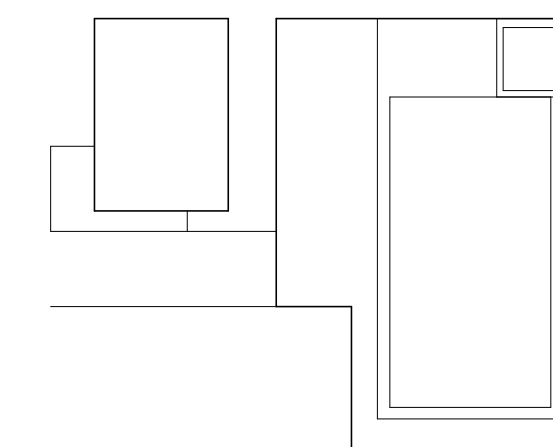


**GENERAL NOTES**

NOTE:  
 All areas shown in plan are Net areas.  
 C: Compact Parking Stall  
 EV: Electric Vehicle Parking Stall  
 S.S: Stacking Standard Parking Stall  
 S.C: Stacking Compact Parking Stall

**EXHIBIT A**  
**CPC-2017-469-GPA-VZC-HD-MCUP-SPR**  
**October 12, 2023**

**KEY PLAN**



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**Date Description**

- 1 02/06/2017 ENTITLEMENT SET
- 2 07/14/2017 ENTITLEMENT SET
- 3 10/25/2017 ENTITLEMENT SET
- 6 05/31/2019 ENTITLEMENT SET
- 7 10/02/2019 ENTITLEMENT SET
- 8 04/07/2020 ENTITLEMENT SET
- 11 06/16/2023 ENTITLEMENT SET
- 12 10/12/2023 ENTITLEMENT SET

Seal / Signature

**NOT FOR CONSTRUCTION**

Project Name

401 S Hewitt

Project Number

05.1291.000

Description

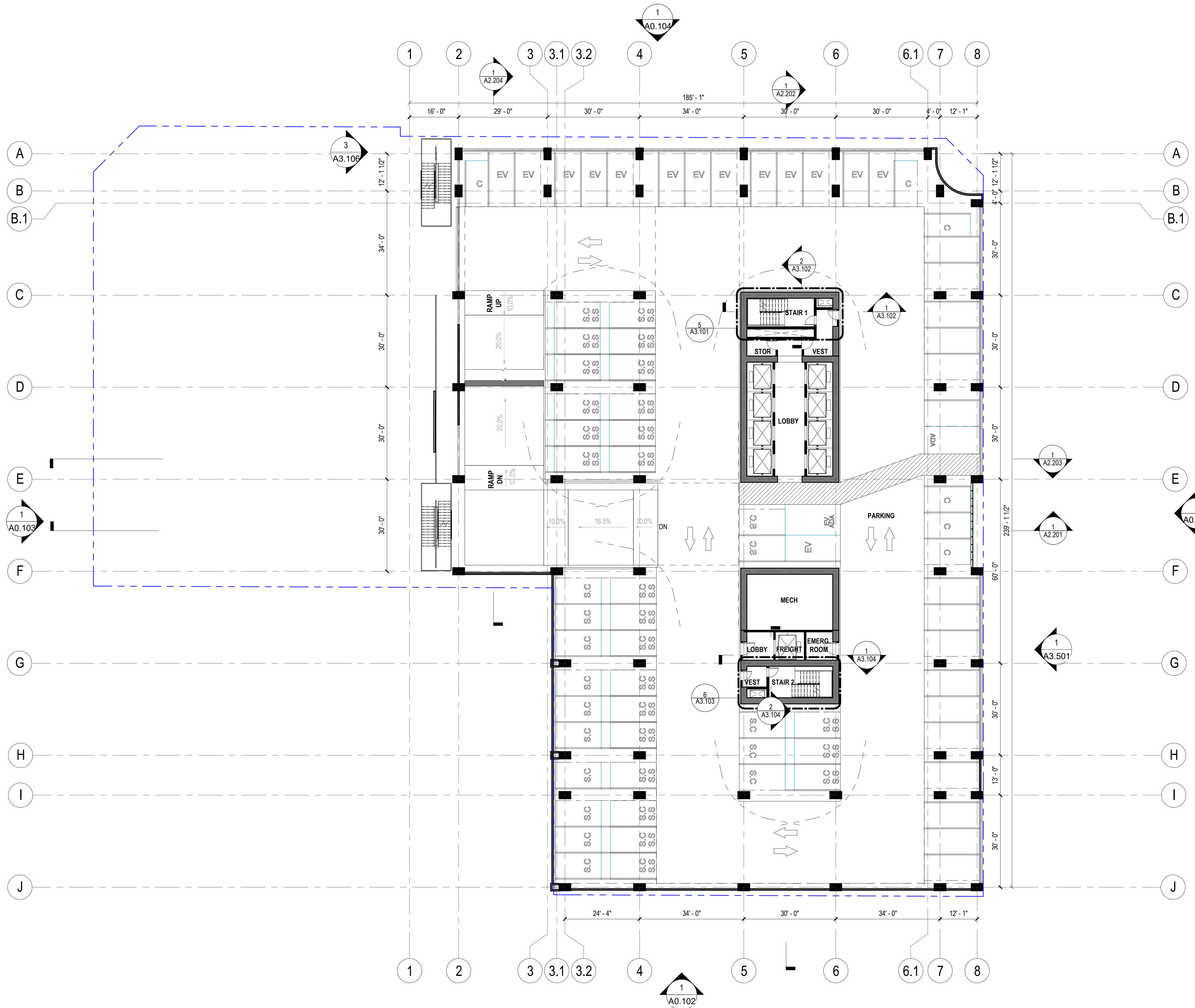
PLAN - OVERALL - LEVEL 2

Scale

1/16" = 1'-0"

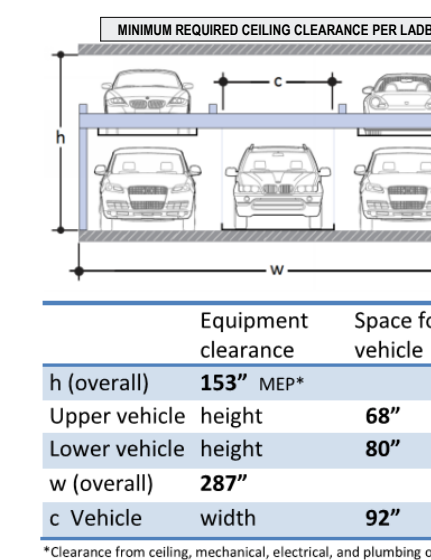
**A1.202**

**LEVEL 2**  
 1/16" = 1'-0" SCALE.



**SHEET NOTES**

Parking Schedule - Level 3		
Level	Count	Type
LEVEL 3	1	ADA_ 9' x 18' (5' Aisle)
LEVEL 3	3	Compact_ 7' 6" x 15' - 90 deg
LEVEL 3	3	Compact_ 8' 6" x 15' - 90 deg
LEVEL 3	1	EV AMBULATORY ADA_ 10' x 18' (5' Aisle)
LEVEL 3	52	Stacking Compact_ 8' 6" x 15' - 90 deg
LEVEL 3	25	Stacking Standard_ 8' 6" x 18' - 90 deg
LEVEL 3	14	Standard EV_ 8' 6" x 18' - 90 deg
LEVEL 3	16	Standard_ 8' 6" x 18' - 90 deg
<b>Grand total</b>	<b>115</b>	

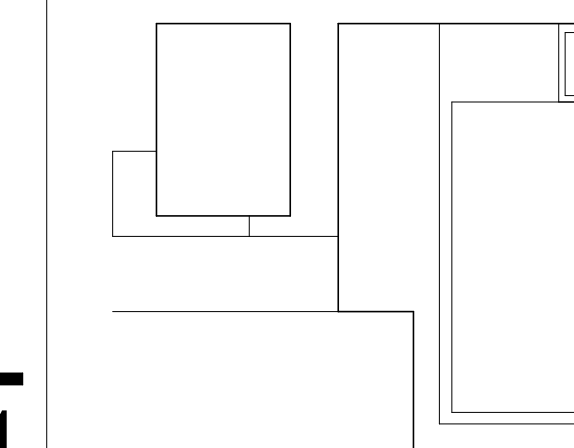


**GENERAL NOTES**

**NOTE:**  
 All areas shown in plan are Net areas.  
 C: Compact Parking Stall  
 EV: Electric Vehicle Parking Stall  
 S.S: Stacking Standard Parking Stall  
 S.C: Stacking Compact Parking Stall

**EXHIBIT A**  
**CPC-2017-469-GPA-VZC-HD-MCUP-SPR**  
**October 12, 2023**

**KEY PLAN**



**Date Description**

- 1 02/06/2017 ENTITLEMENT SET
- 2 07/14/2017 ENTITLEMENT SET
- 3 10/25/2017 ENTITLEMENT SET
- 6 05/31/2019 ENTITLEMENT SET
- 7 10/02/2019 ENTITLEMENT SET
- 8 04/07/2020 ENTITLEMENT SET
- 11 06/16/2023 ENTITLEMENT SET
- 12 10/12/2023 ENTITLEMENT SET

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**NOT FOR CONSTRUCTION**

Project Name

401 S Hewitt

Project Number

05.1291.000

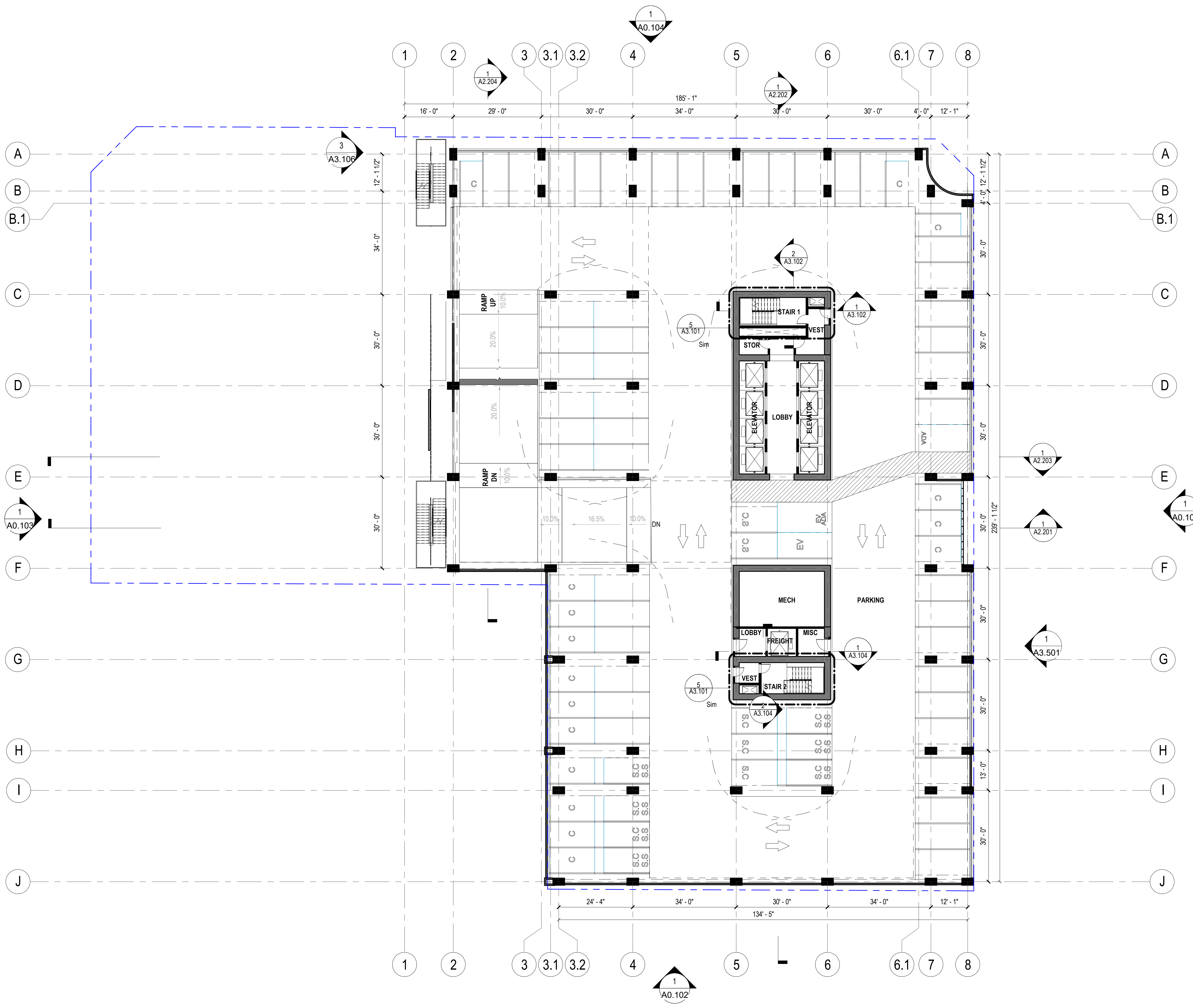
Description

PLAN - OVERALL - LEVEL 3

Scale

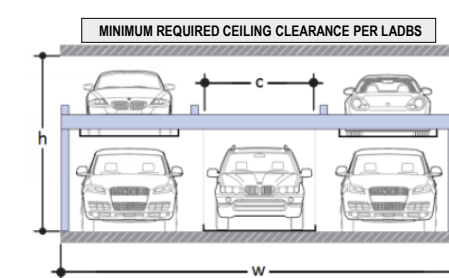
1/16" = 1'-0"

**A1.203**



**SHEET NOTES**

Parking Schedule - Level 4		
Level	Count	Type
LEVEL 4	1	ADA_ 9' x 18' (5' Aisle)
LEVEL 4	3	Compact_ 7' 6" x 15' - 90 deg
LEVEL 4	13	Compact_ 8' 6" x 15' - 90 deg
LEVEL 4	1	EV AMBULATORY ADA_ 10' x 18' (5' Aisle)
LEVEL 4	17	Stacking Compact_ 8' 6" x 15' - 90 deg
LEVEL 4	7	Stacking Standard_ 8' 6" x 18' - 90 deg
LEVEL 4	1	Standard EV_ 8' 6" x 18' - 90 deg
LEVEL 4	47	Standard_ 8' 6" x 18' - 90 deg
Grand total	90	

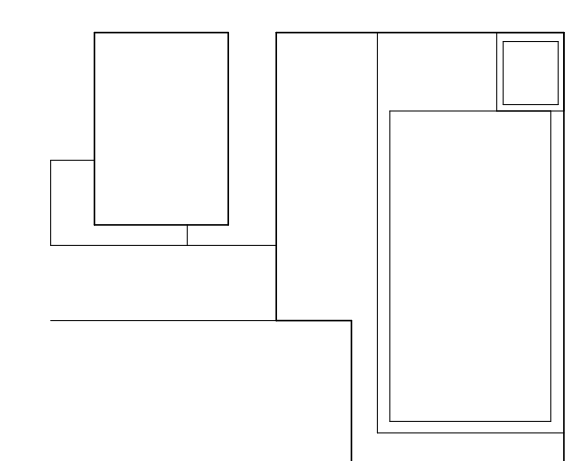


**GENERAL NOTES**

NOTE:  
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 S.C: Stacking Compact Parking Stall

**EXHIBIT A**  
**CPC-2017-469-GPA-VZC-HD-MCUP-SPR**  
**October 12, 2023**

**KEY PLAN**



401 S. Hewitt, Los Angeles, CA 90013



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 Los Angeles, California 90071  
 United States

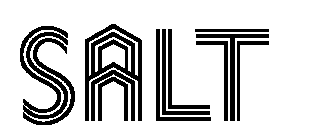
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Date	Description
1 02/06/2017	ENTITLEMENT SET
2 07/14/2017	ENTITLEMENT SET
3 10/25/2017	ENTITLEMENT SET
6 05/31/2019	ENTITLEMENT SET
7 10/02/2019	ENTITLEMENT SET
8 04/07/2020	ENTITLEMENT SET
11 06/16/2023	ENTITLEMENT SET
12 10/12/2023	ENTITLEMENT SET

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**NOT FOR CONSTRUCTION**

Project Name  
 401 S Hewitt

Project Number  
 05.1291.000

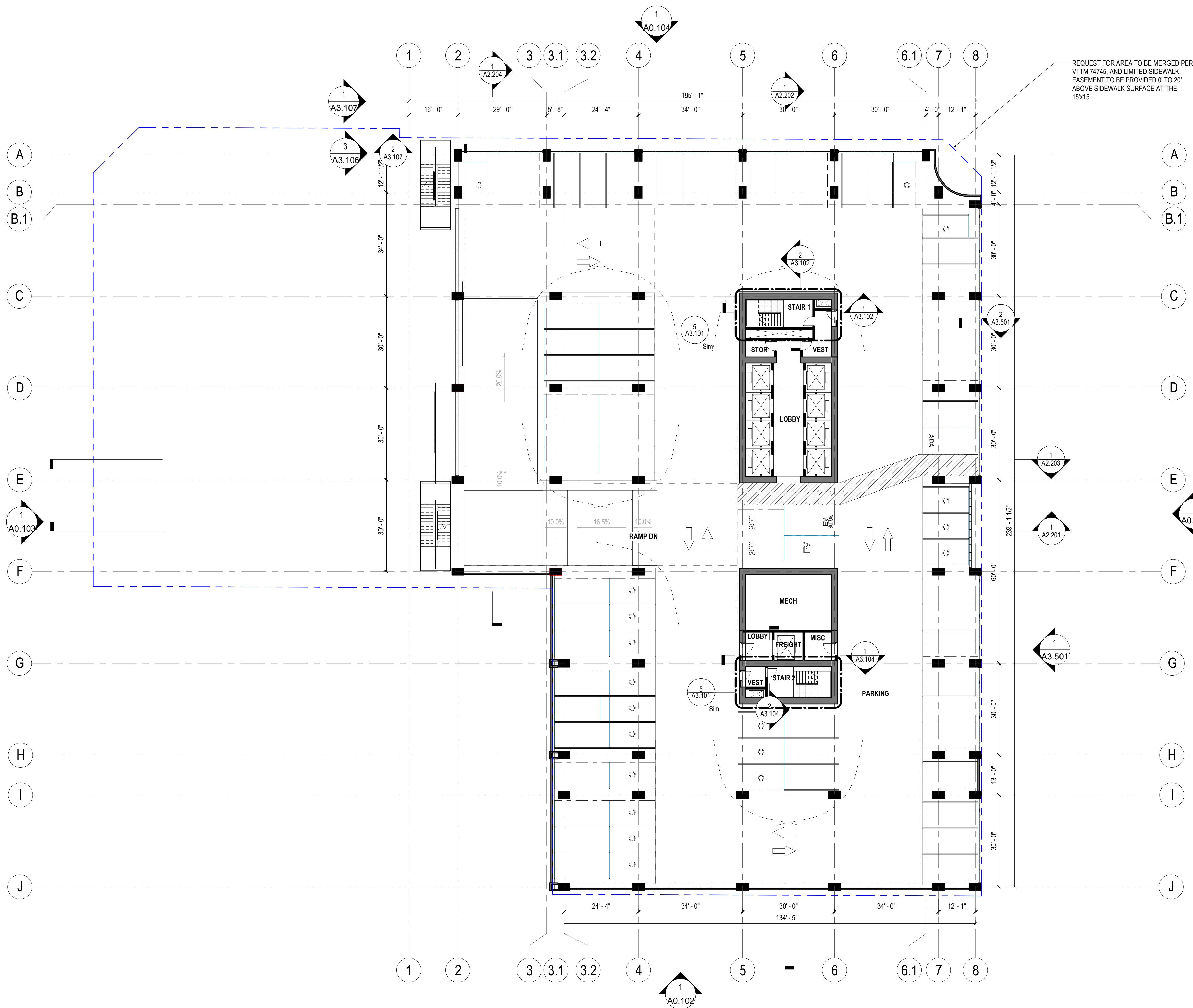
Description  
 PLAN - OVERALL - LEVEL 4

Scale  
 1/16" = 1'-0"

**A1.204**

LEVEL 4  
 1/16" = 1'-0" SCALE.

6/15/2023 2:24:54 PM BIM\_360/1005.1291.000 - 4th & Hewitt - Art District LA401 - Architecture\_F20.rvt



**SHEET NOTES**

Parking Schedule - Level 5		
Level	Count	Type
LEVEL 5	1	ADA, 9' x 18' (5' Aisle)
LEVEL 5	3	Compact, 7' 6" x 15' - 90 deg
LEVEL 5	16	Compact, 8' 6" x 15' - 90 deg
LEVEL 5	1	EV AMBULATORY ADA, 10' x 18' (5' Aisle)
LEVEL 5	4	Stacking Compact, 8' 6" x 15' - 90 deg
LEVEL 5	1	Standard EV, 8' 6" x 18' - 90 deg
LEVEL 5	55	Standard, 8' 6" x 18' - 90 deg
Grand total	81	



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Date	Description
1 02/06/2017	ENTITLEMENT SET
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7 10/02/2019	ENTITLEMENT SET
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**EXHIBIT A**  
**CPC-2017-469-GPA-VZC-HD-MCUP-SPR**  
**October 12, 2023**

**GENERAL NOTES**

NOTE:  
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S.C: Stacking Compact Parking Stall

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**NOT FOR CONSTRUCTION**

Project Name  
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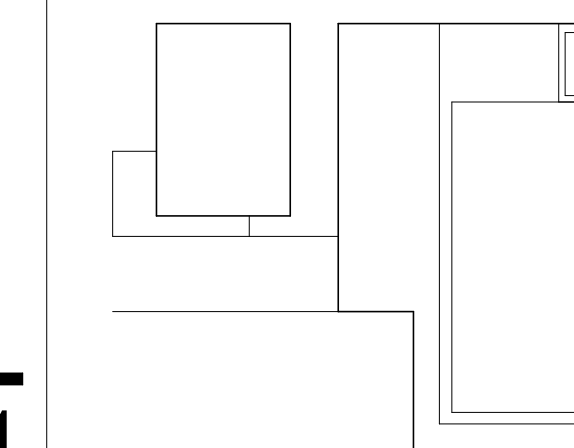
Project Number  
05.1291.000

Description  
PLAN - OVERALL - LEVEL 5

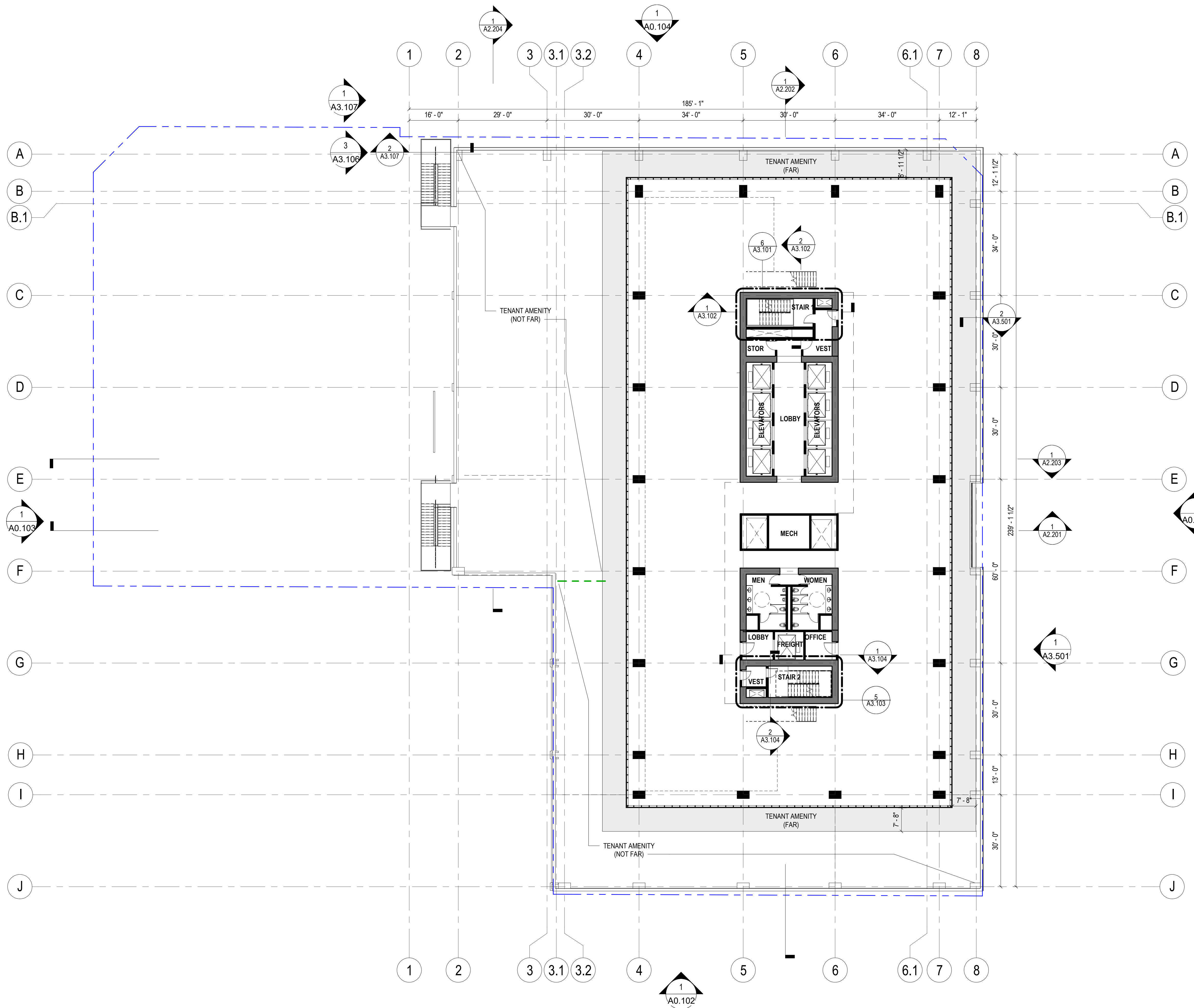
Scale  
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**A1.205**

**KEY PLAN**



**LEVEL 5**  
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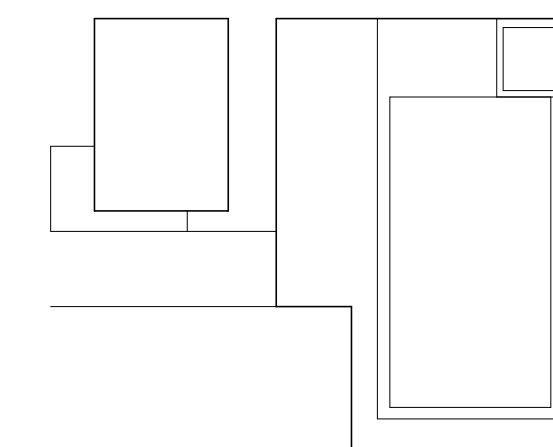


**SHEET NOTES**

**EXHIBIT A**  
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**October 12, 2023**

**GENERAL NOTES**

**KEY PLAN**



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12 10/12/2023	ENTITLEMENT SET

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Project Name  
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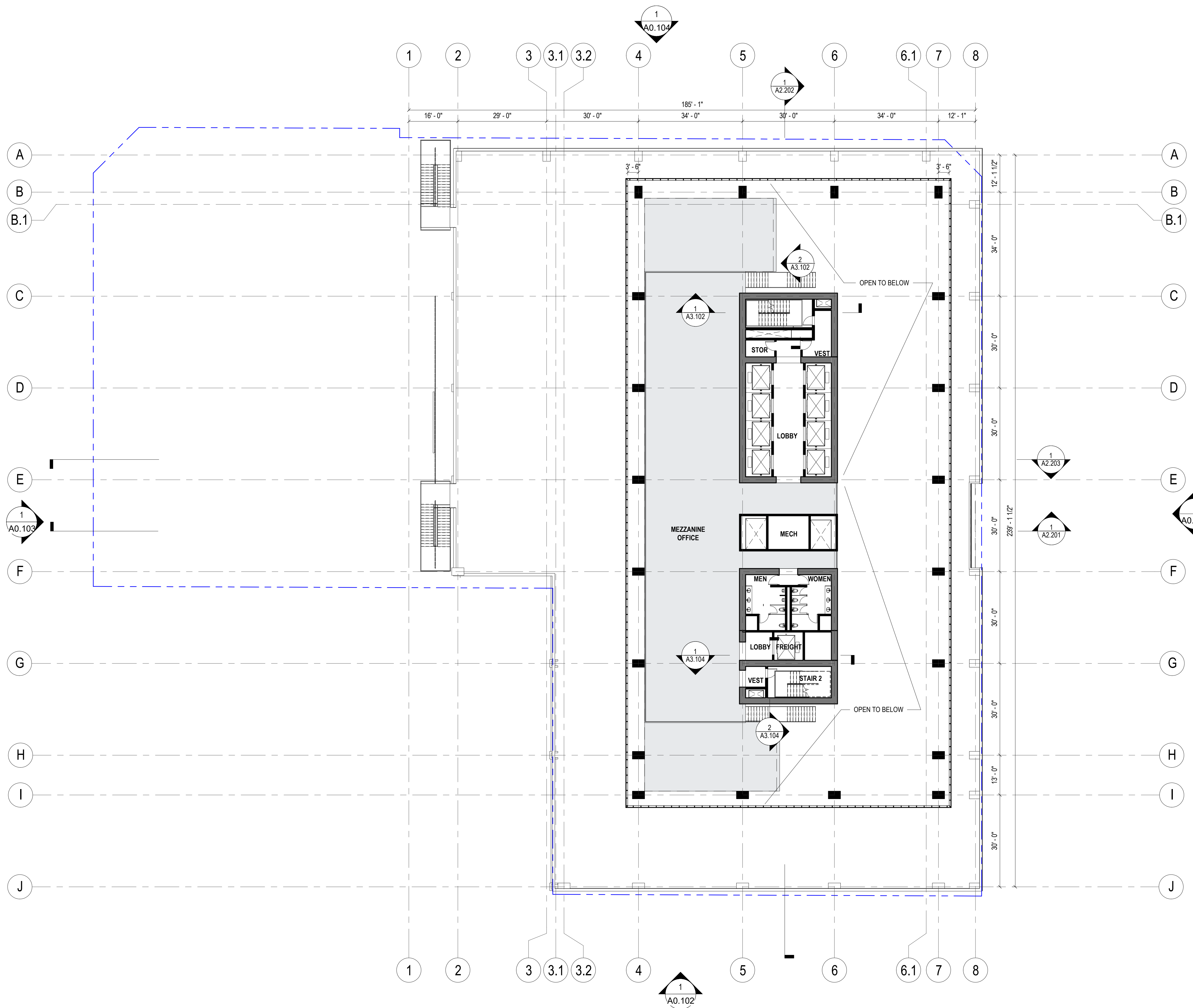
Description  
 PLAN - OVERALL - LEVEL 6

Scale  
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**A1.206**

**LEVEL 6**  
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LEVEL 6 MEZZANINE 1  
1/16" = 1'-0" SCALE

SHEET NOTES



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Date	Description
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11 06/16/2023	ENTITLEMENT SET
12 10/12/2023	ENTITLEMENT SET

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**CPC-2017-469-GPA-VZC-HD-MCUP-SPR**  
**October 12, 2023**

GENERAL NOTES

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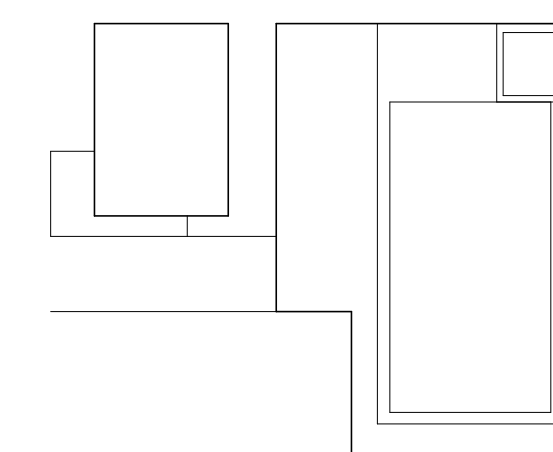
Project Name  
401 S Hewitt

Project Number  
05.1291.000

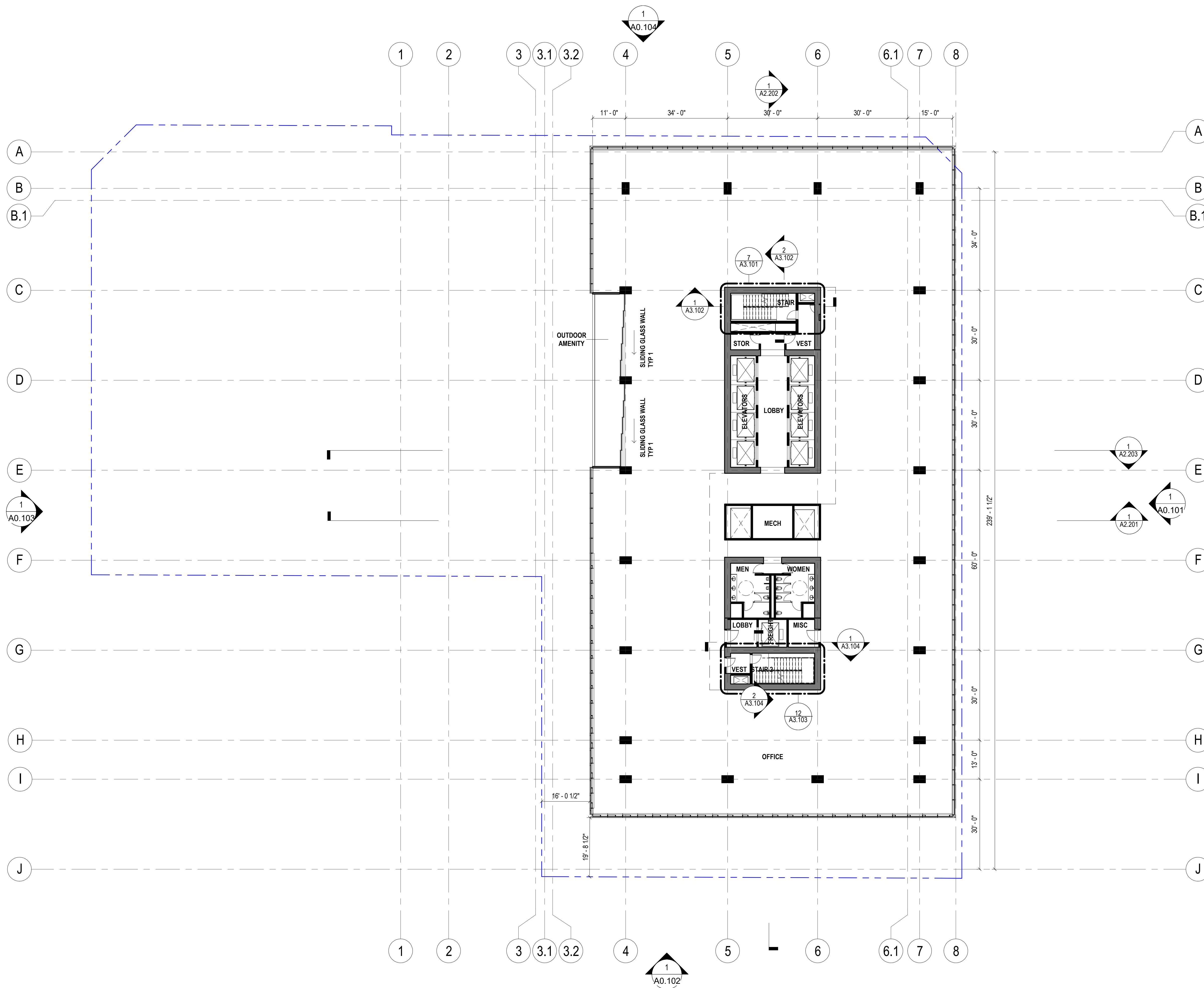
Description  
PLAN - OVERALL - LEVEL 6  
MEZZANINE

Scale  
1/16" = 1'-0"

KEY PLAN



**A1.206.M**

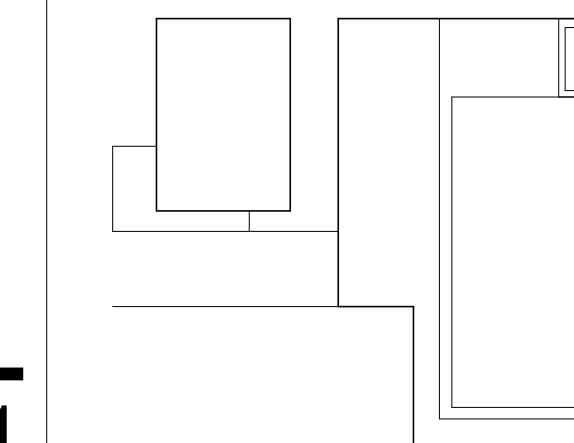


**SHEET NOTES**

**EXHIBIT A**  
**CPC-2017-469-GPA-VZC-HD-MCUP-SPR**  
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**GENERAL NOTES**

**KEY PLAN**



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Date	Description
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12 10/12/2023	ENTITLEMENT SET

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Project Name

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Project Number

05.1291.000

Description

PLAN - OVERALL - LEVEL 7

Scale

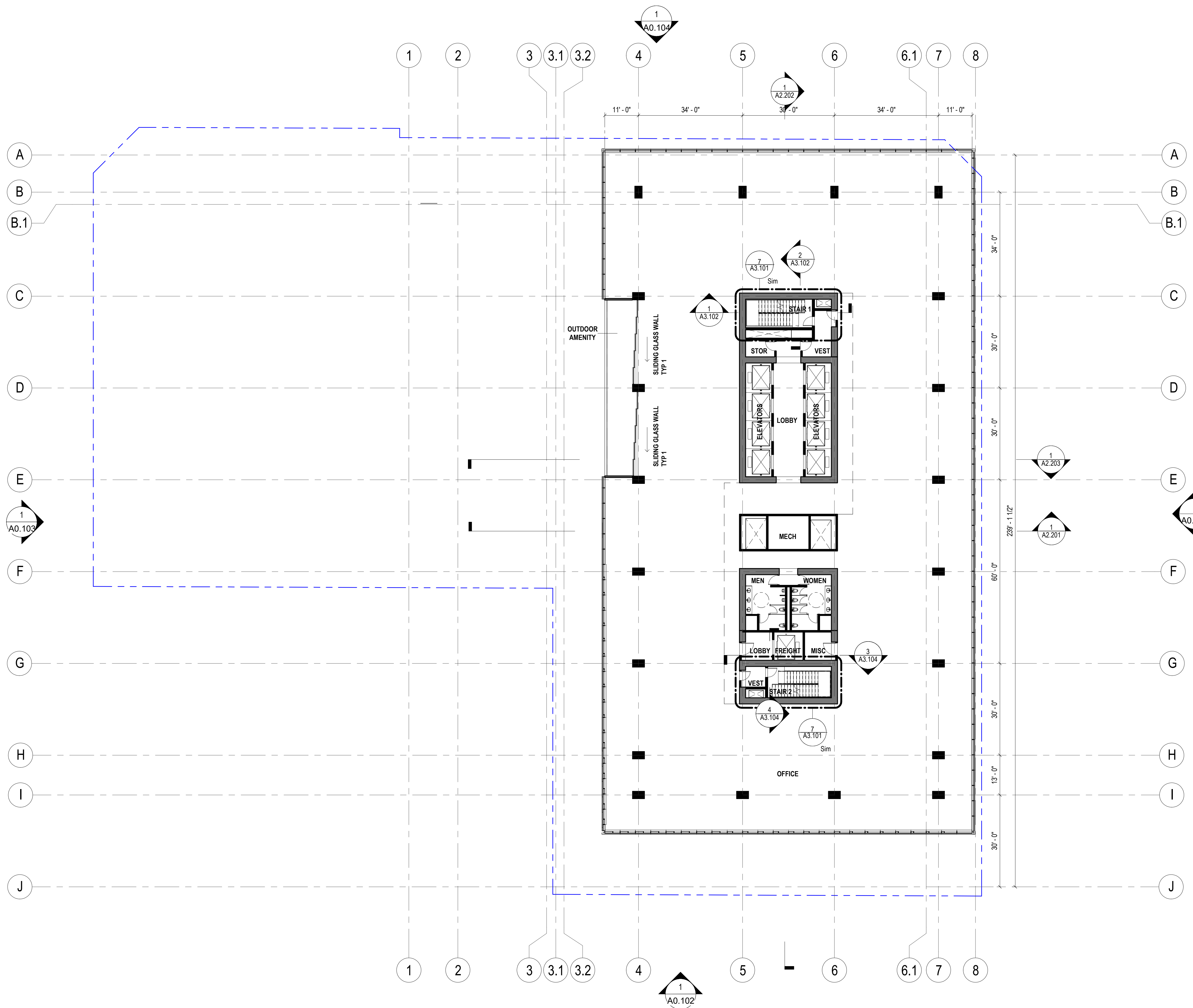
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**A1.207**

**LEVEL 7**  
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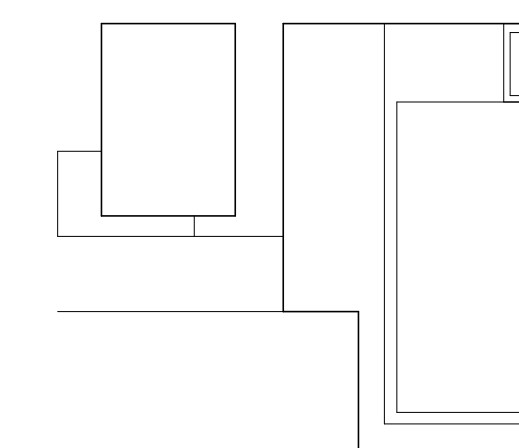


**SHEET NOTES**

**EXHIBIT A**  
**CPC-2017-469-GPA-VZC-HD-MCUP-SPR**  
**October 12, 2023**

**GENERAL NOTES**

**KEY PLAN**



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8 04/07/2020	ENTITLEMENT SET
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12 10/12/2023	ENTITLEMENT SET

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Project Number  
 05.1291.000

Description  
 PLAN - OVERALL - LEVEL 8

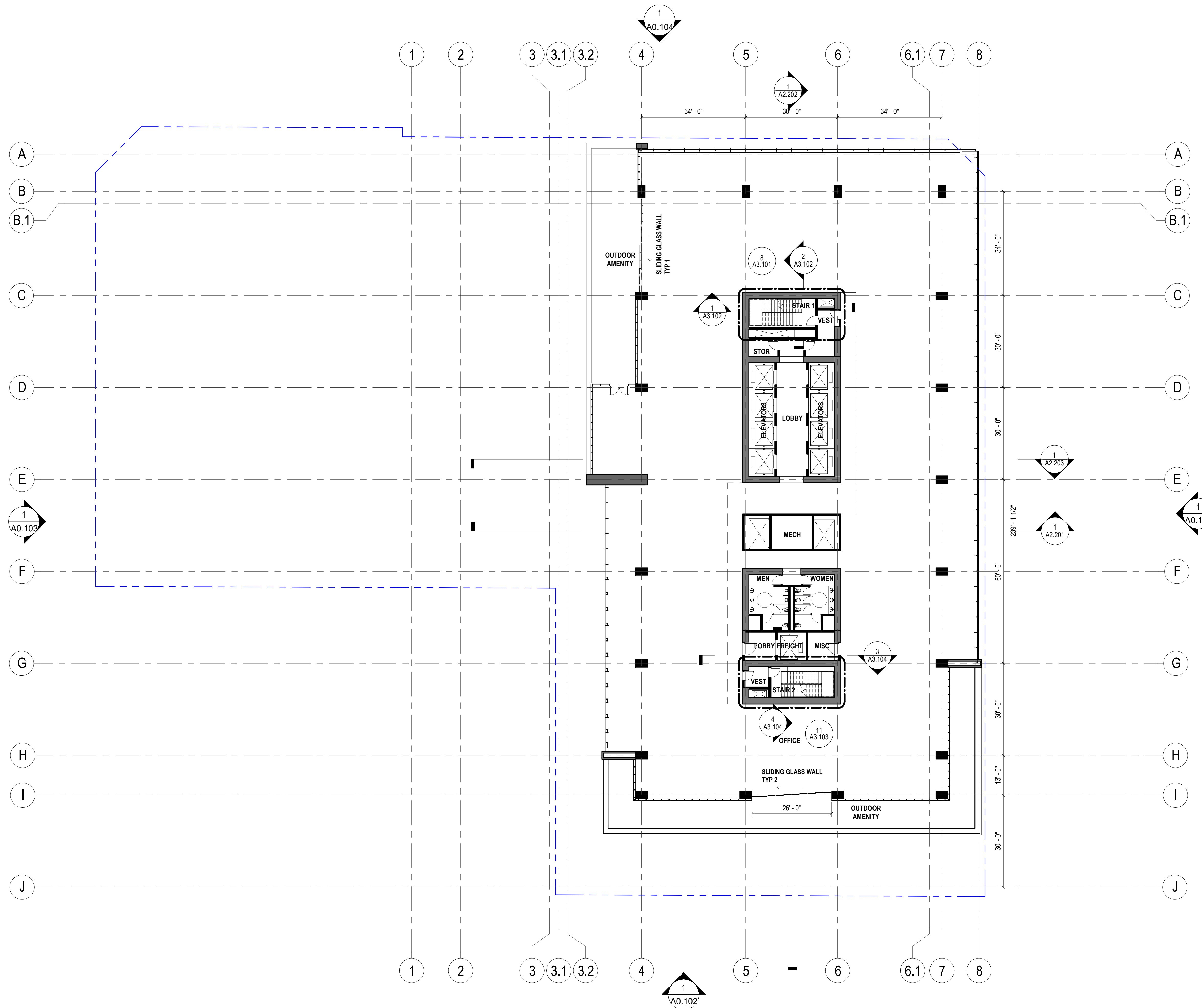
Scale  
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**A1.208**

**LEVEL 8**  
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1

6/15/2023 2:25:49 PM BIM 360/1005.1291.000 - 4th & Hewitt - Art District LA401 - Architecture\_F20.rvt



**SHEET NOTES**



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Date	Description
1 02/06/2017	ENTITLEMENT SET
2 07/14/2017	ENTITLEMENT SET
3 10/25/2017	ENTITLEMENT SET
6 05/31/2019	ENTITLEMENT SET
7 10/02/2019	ENTITLEMENT SET
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12 10/12/2023	ENTITLEMENT SET

**EXHIBIT A**  
**CPC-2017-469-GPA-VZC-HD-MCUP-SPR**  
**October 12, 2023**

**GENERAL NOTES**

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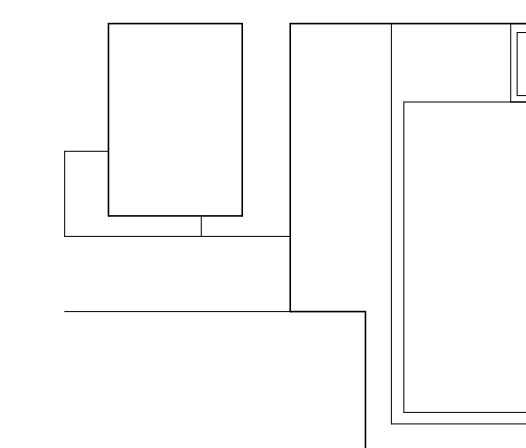
Project Name  
401 S Hewitt

Project Number  
05.1291.000

Description  
PLAN - OVERALL - LEVEL 9

Scale  
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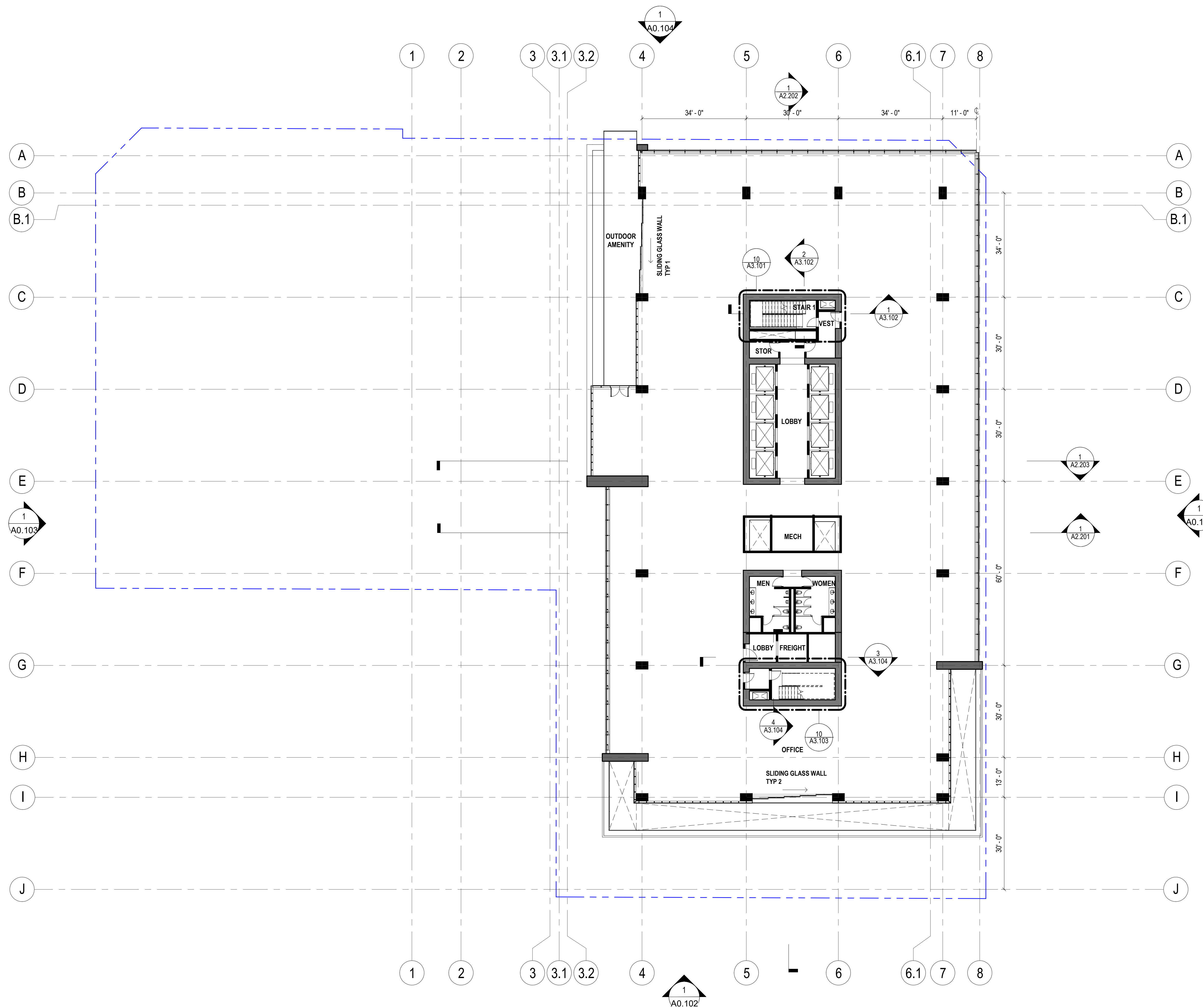
**KEY PLAN**



**A1.209**

**LEVEL 9**  
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1

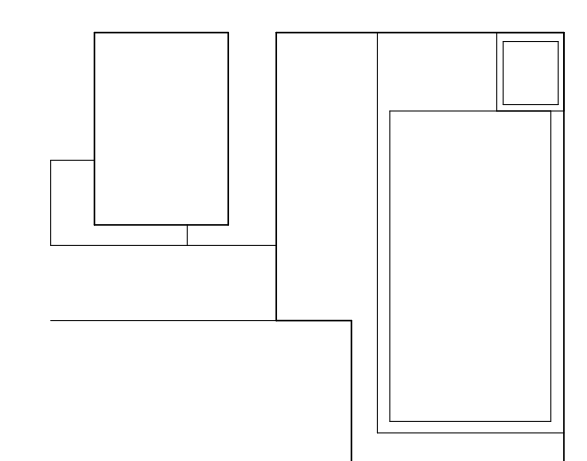


**SHEET NOTES**

**EXHIBIT A**  
**CPC-2017-469-GPA-VZC-HD-MCUP-SPR**  
**October 12, 2023**

**GENERAL NOTES**

**KEY PLAN**



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Date	Description
7 10/02/2019	ENTITLEMENT SET
8 04/07/2020	ENTITLEMENT SET
11 06/16/2023	ENTITLEMENT SET
12 10/12/2023	ENTITLEMENT SET

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Project Name  
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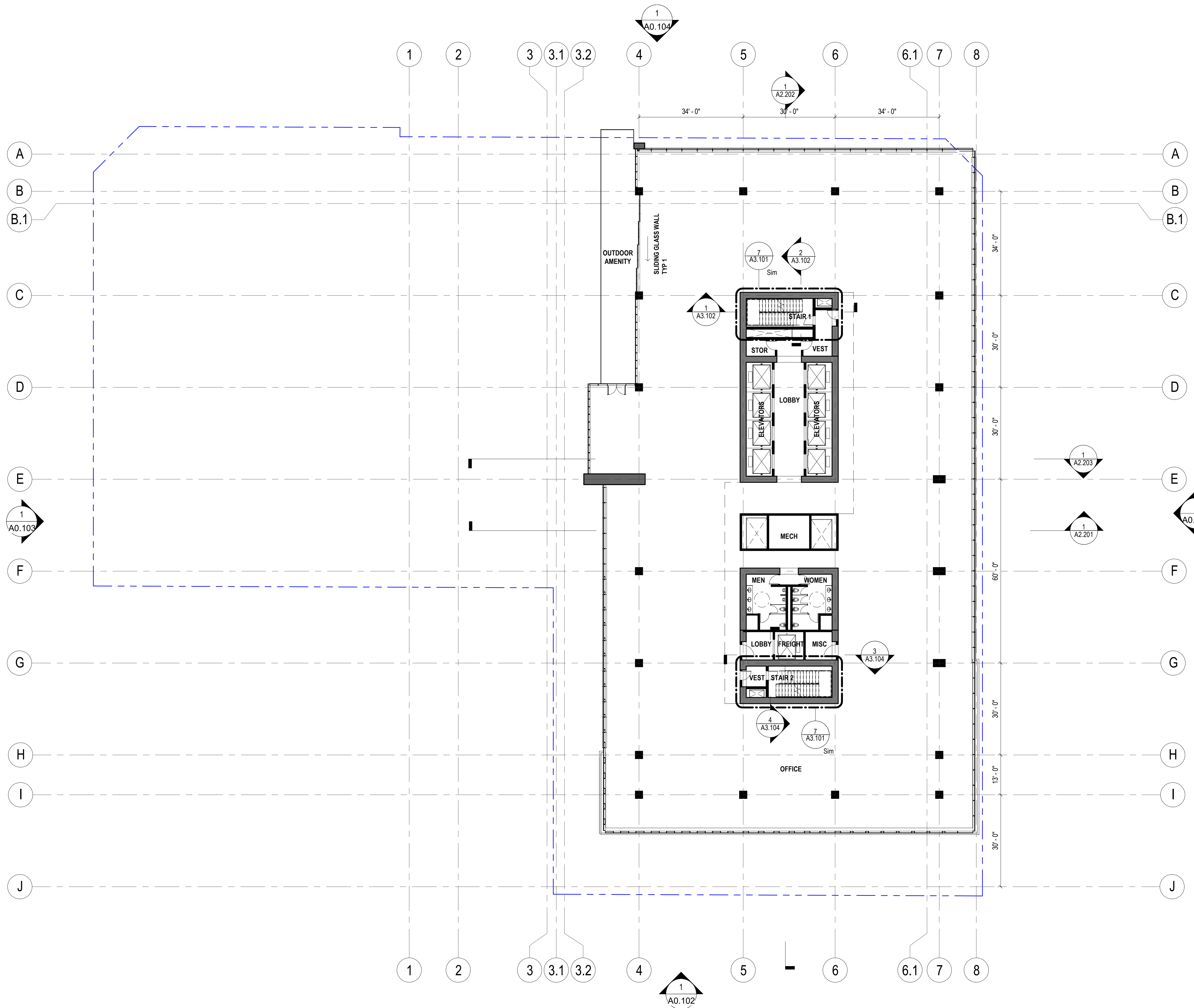
Project Number  
 05.1291.000

Description  
 PLAN - OVERALL - LEVEL 10

Scale  
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**A1.210**

**LEVEL 10**  
 1/16" = 1'-0" SCALE

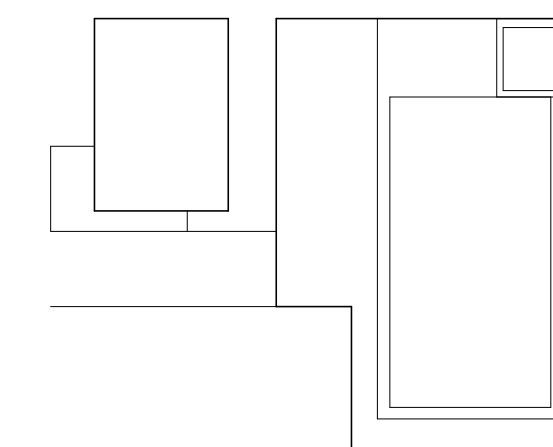


**SHEET NOTES**

**EXHIBIT A**  
**CPC-2017-469-GPA-VZC-HD-MCUP-SPR**  
**October 12, 2023**

**GENERAL NOTES**

**KEY PLAN**



Date	Description
1 02/06/2017	ENTITLEMENT SET
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8 04/07/2020	ENTITLEMENT SET
11 06/16/2023	ENTITLEMENT SET
12 10/12/2023	ENTITLEMENT SET

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 CONSTRUCTION**

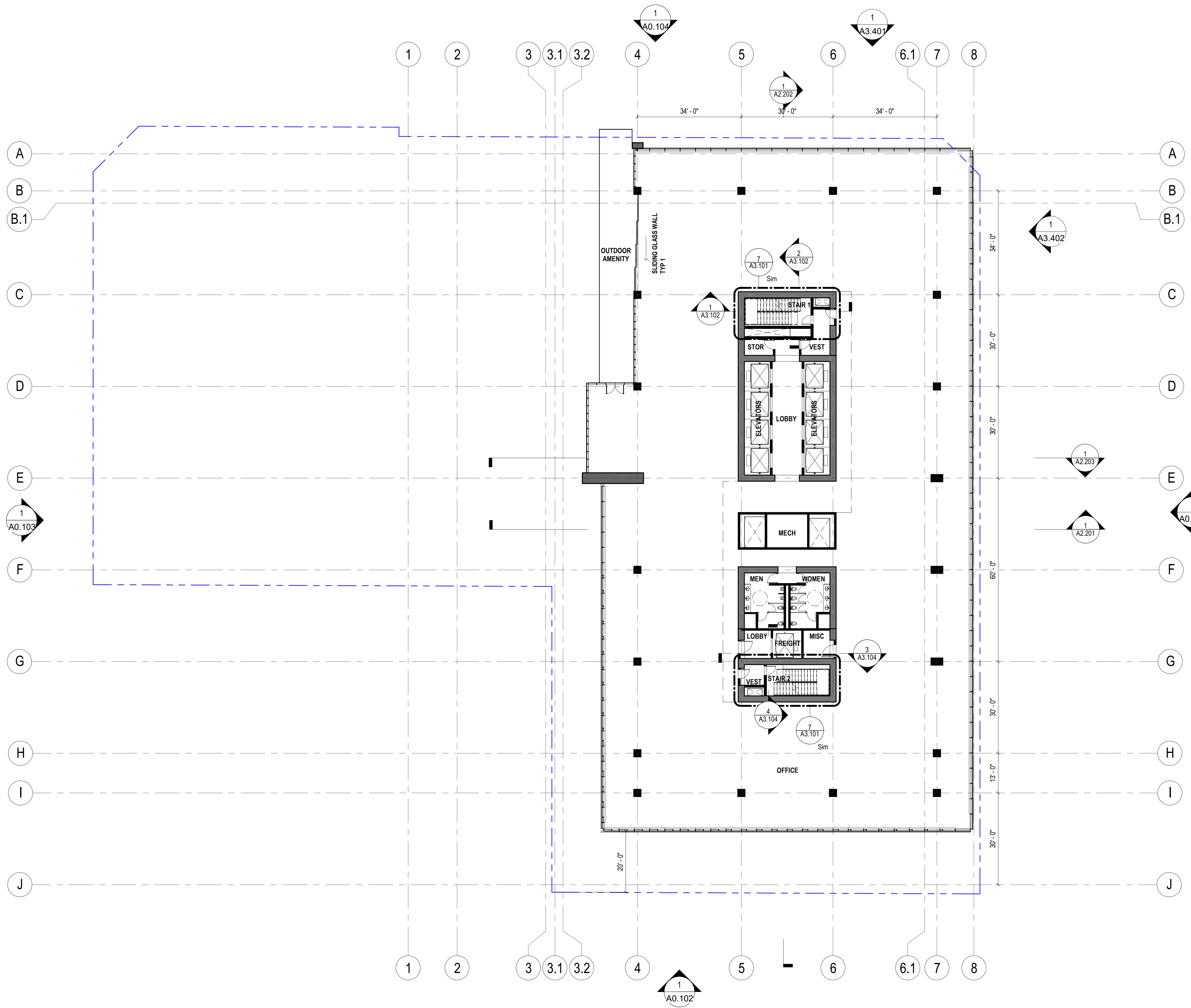
Project Name  
 401 S Hewitt

Project Number  
 05.1291.000

Description  
 PLAN - OVERALL - LEVEL 11

Scale  
 1/16" = 1'-0"

**A1.211**



**SHEET NOTES**



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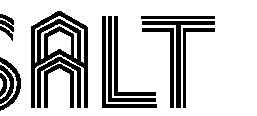
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Date	Description
02/06/2017	ENTITLEMENT SET
07/14/2017	ENTITLEMENT SET
10/25/2017	ENTITLEMENT SET
05/31/2019	ENTITLEMENT SET
10/02/2019	ENTITLEMENT SET
04/07/2020	ENTITLEMENT SET
06/16/2023	ENTITLEMENT SET
10/12/2023	ENTITLEMENT SET

**EXHIBIT A**  
**CPC-2017-469-GPA-VZC-HD-MCUP-SPR**  
**October 12, 2023**

**GENERAL NOTES**

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**NOT FOR CONSTRUCTION**

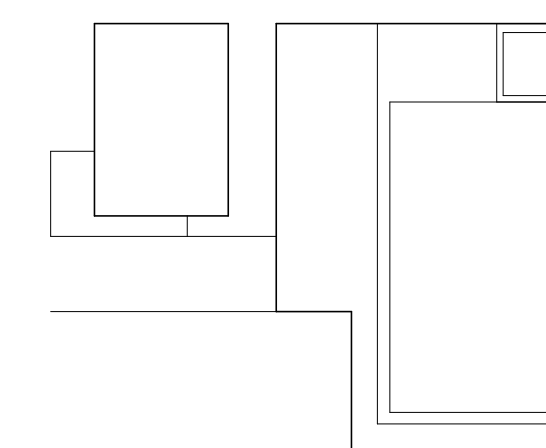
Project Name  
401 S Hewitt

Project Number  
05.1291.000

Description  
PLAN - OVERALL - LEVEL 12

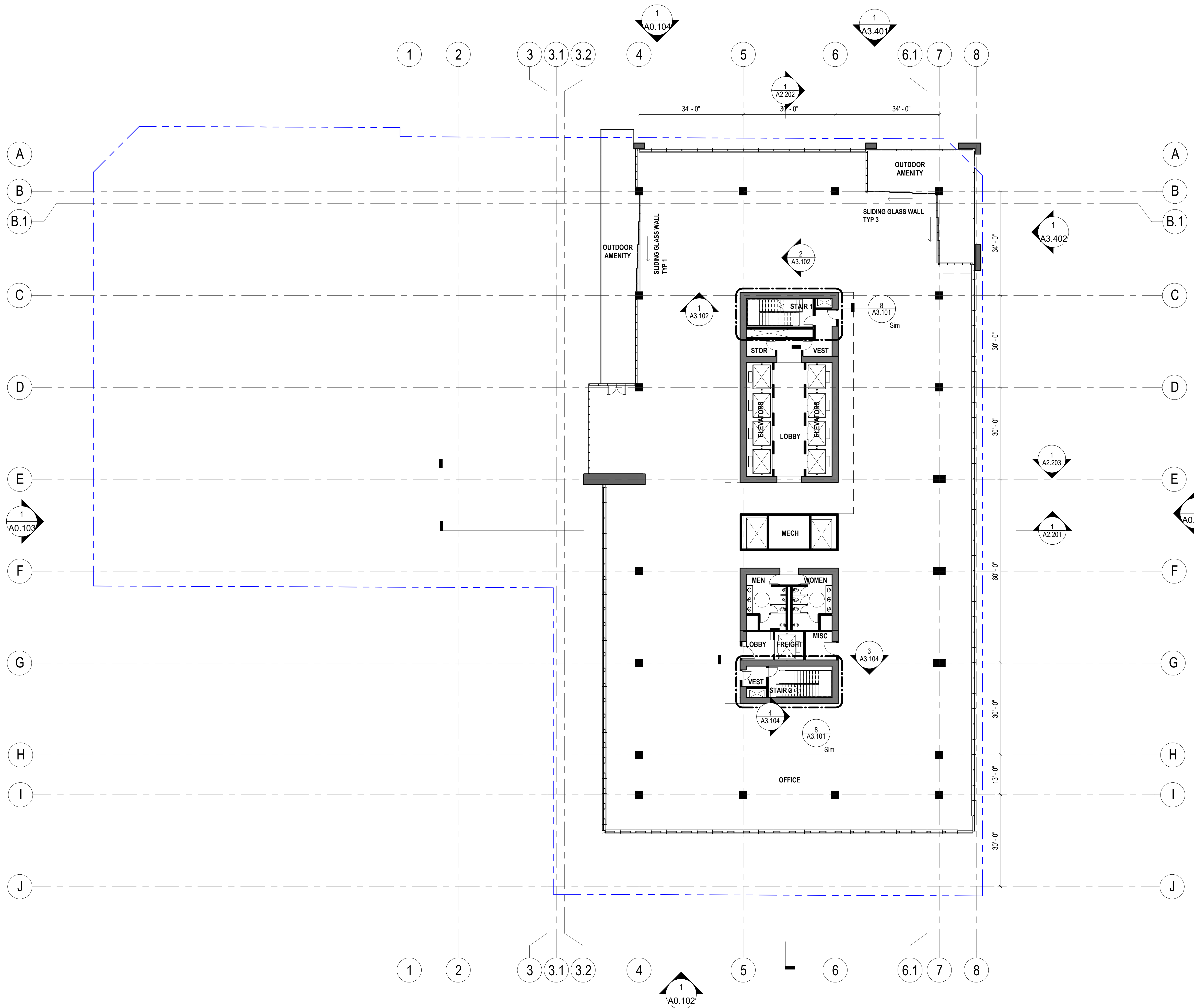
Scale  
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**KEY PLAN**



**LEVEL 12**  
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**A1.212**

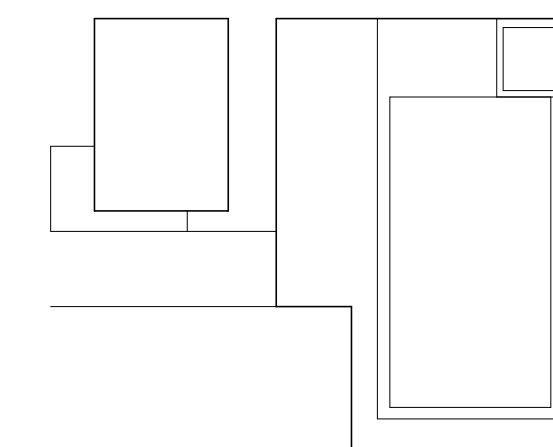


**SHEET NOTES**

**EXHIBIT A**  
**CPC-2017-469-GPA-VZC-HD-MCUP-SPR**  
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**GENERAL NOTES**

**KEY PLAN**



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Project Name  
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Project Number  
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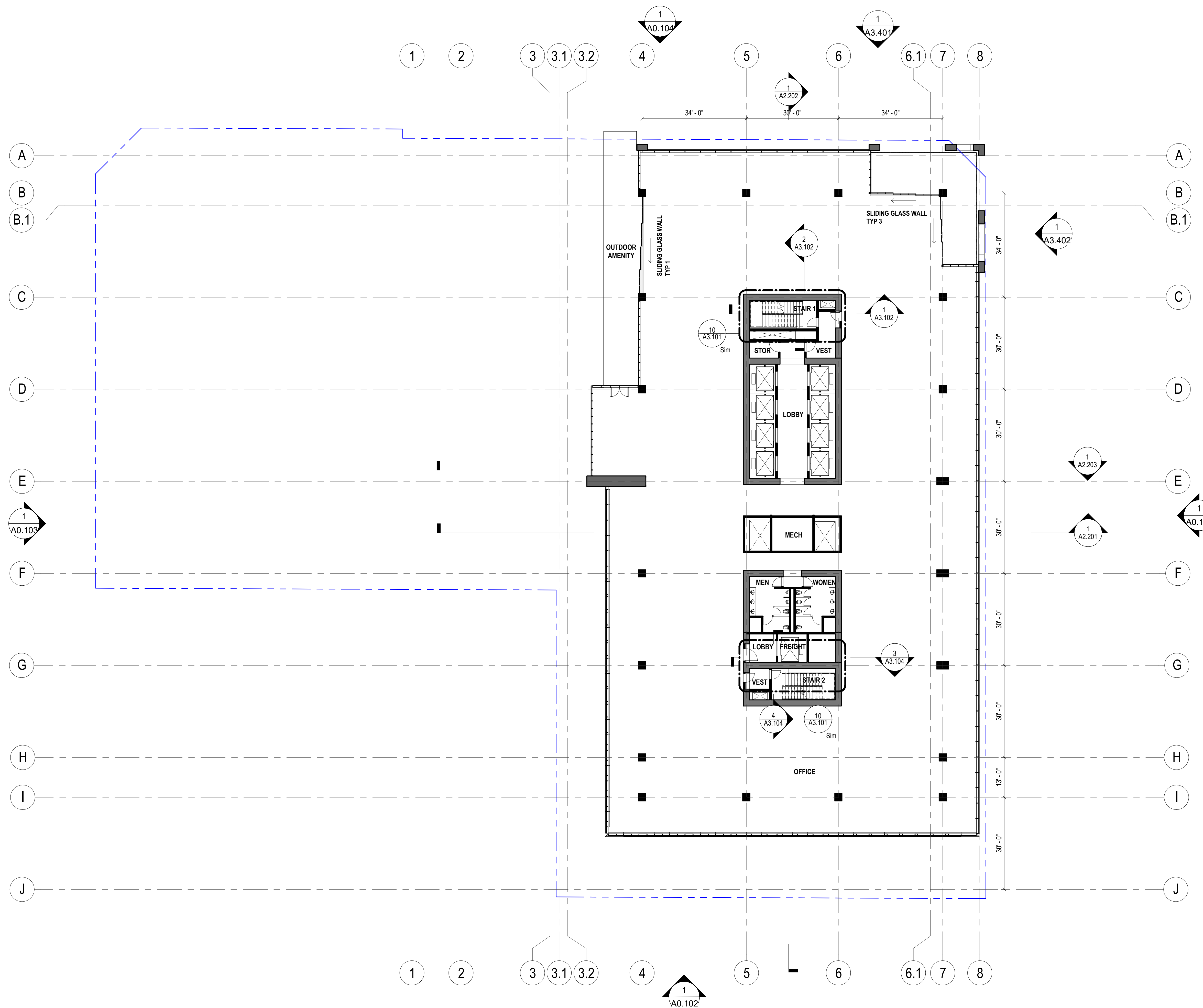
Description  
 PLAN - OVERALL - LEVEL 13

Scale  
 1/16" = 1'-0"

**A1.213**

**LEVEL 13**  
 1/16" = 1'-0" SCALE.

6/15/2023 2:26:33 PM BIM\_360/1005.1291.000 - 4th & Hewitt - Art District LA401 - Architecture\_F20.rvt

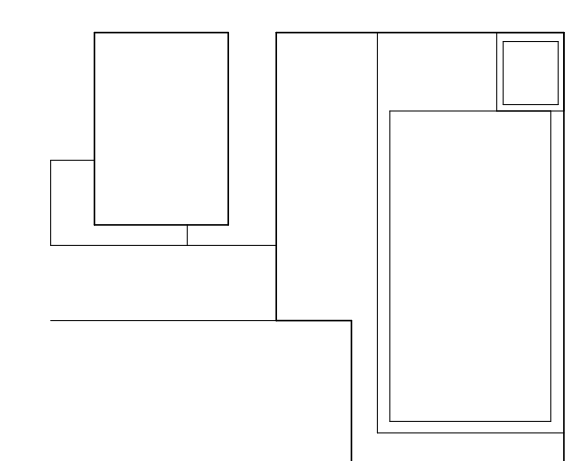


**SHEET NOTES**

**EXHIBIT A**  
**CPC-2017-469-GPA-VZC-HD-MCUP-SPR**  
**October 12, 2023**

**GENERAL NOTES**

**KEY PLAN**



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423 Gin Ling Way  
Los Angeles, CA 90012  
Tel 213.234.0057

Date	Description
7 10/02/2019	ENTITLEMENT SET
8 04/07/2020	ENTITLEMENT SET
11 06/16/2023	ENTITLEMENT SET
12 10/12/2023	ENTITLEMENT SET

Seal / Signature

**NOT FOR CONSTRUCTION**

Project Name  
401 S Hewitt

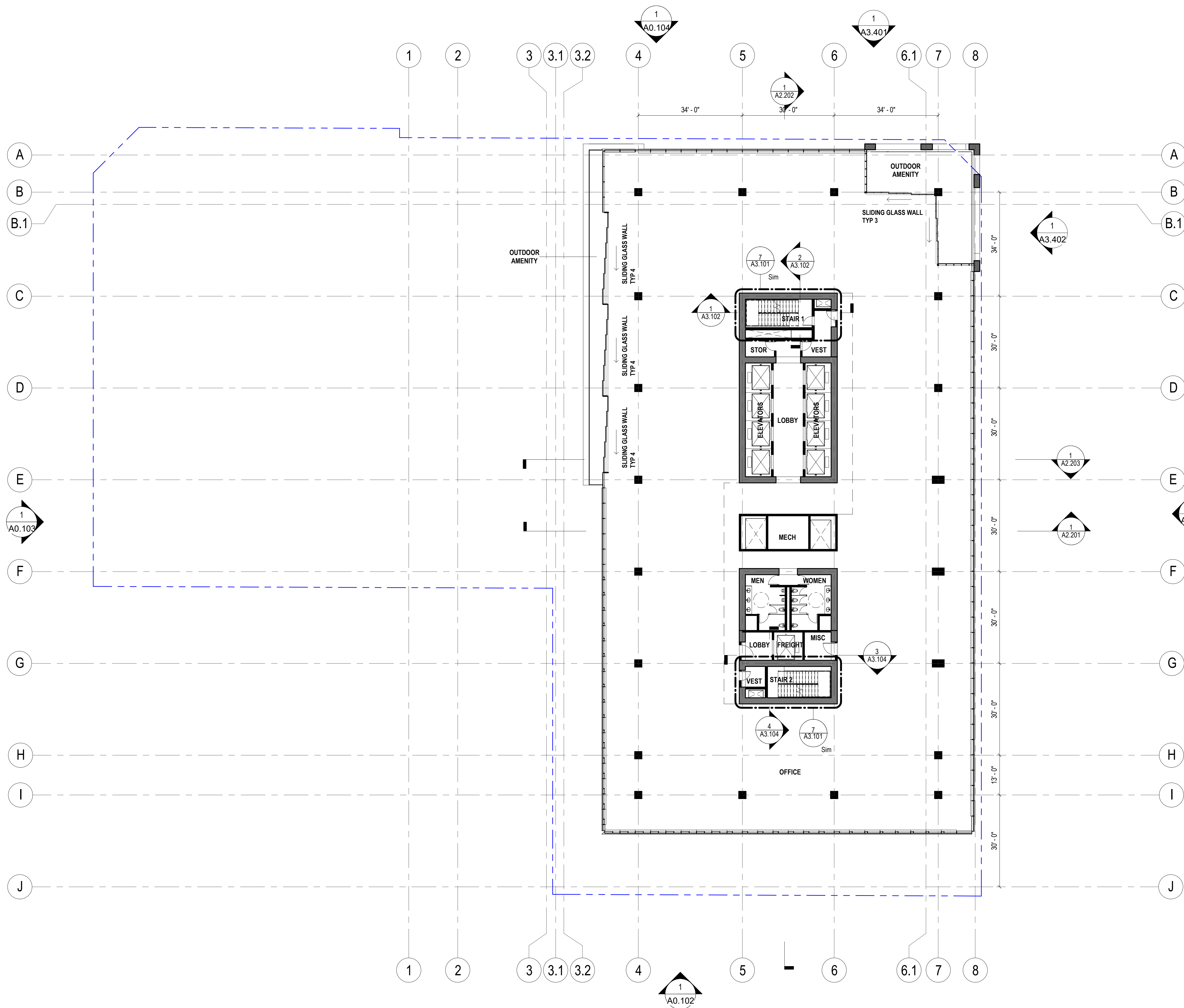
Project Number  
05.1291.000

Description  
PLAN - OVERALL - LEVEL 14

Scale  
1/16" = 1'-0"

**A1.214**

**LEVEL 14**  
1/16" = 1'-0" SCALE.



SHEET NOTES



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**EXHIBIT A**  
**CPC-2017-469-GPA-VZC-HD-MCUP-SPR**  
**October 12, 2023**

GENERAL NOTES

Seal / Signature

**NOT FOR  
CONSTRUCTION**

Project Name

401 S Hewitt

Project Number

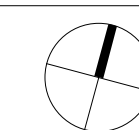
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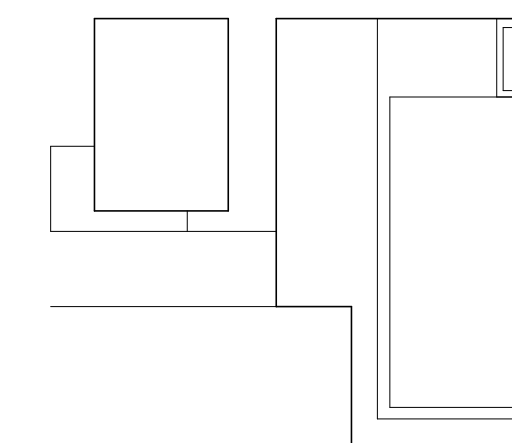
PLAN - OVERALL - LEVEL 15

Scale

1/16" = 1'-0"



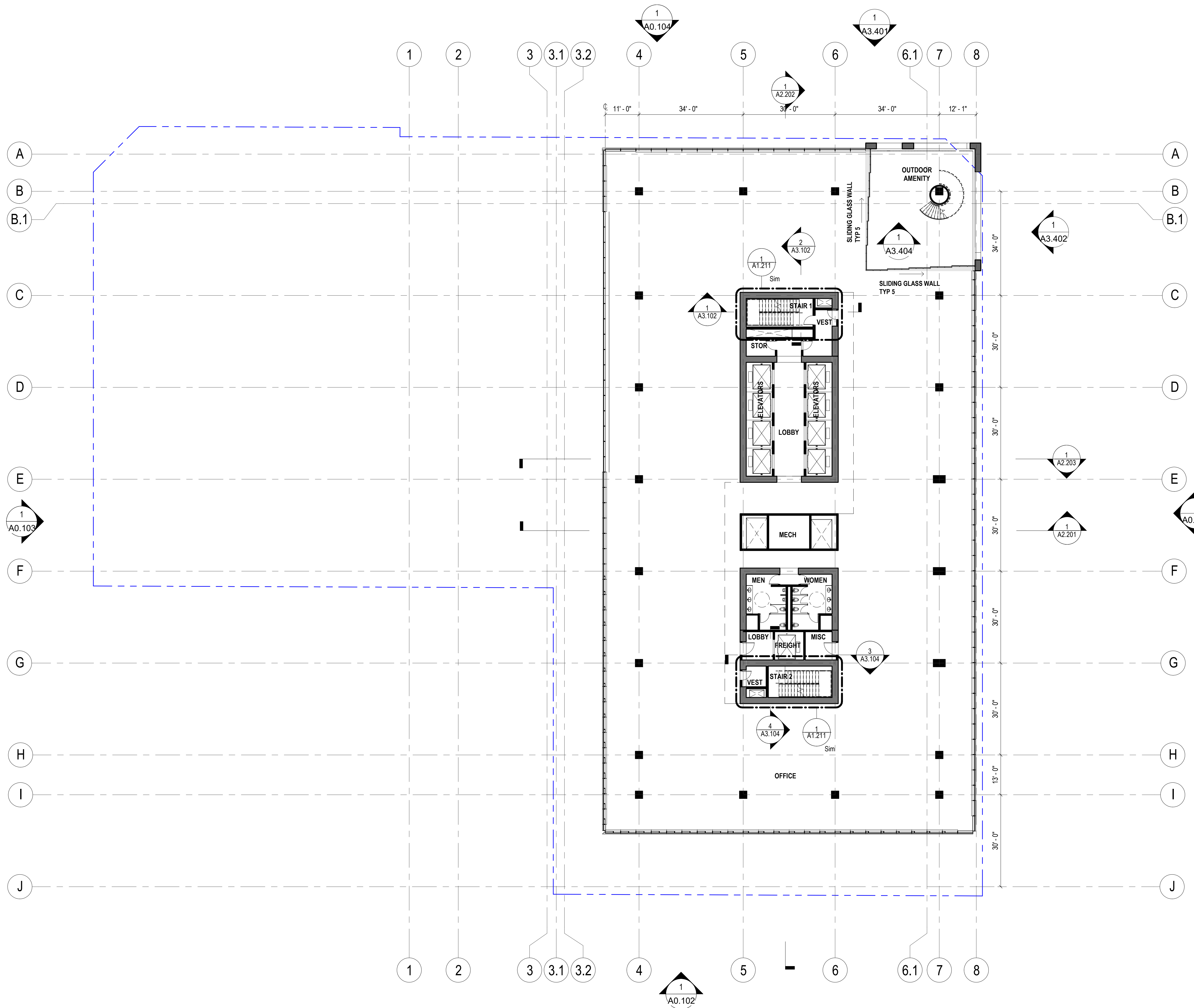
KEY PLAN



LEVEL 15  
1/16" = 1'-0" SCALE.

1





SHEET NOTES



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**October 12, 2023**

GENERAL NOTES

Seal / Signature

**NOT FOR CONSTRUCTION**

Project Name

401 S Hewitt

Project Number

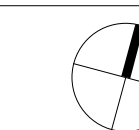
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Description

PLAN - OVERALL - LEVEL 16

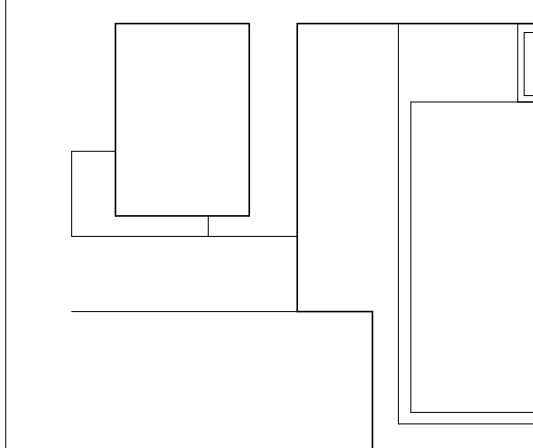
Scale

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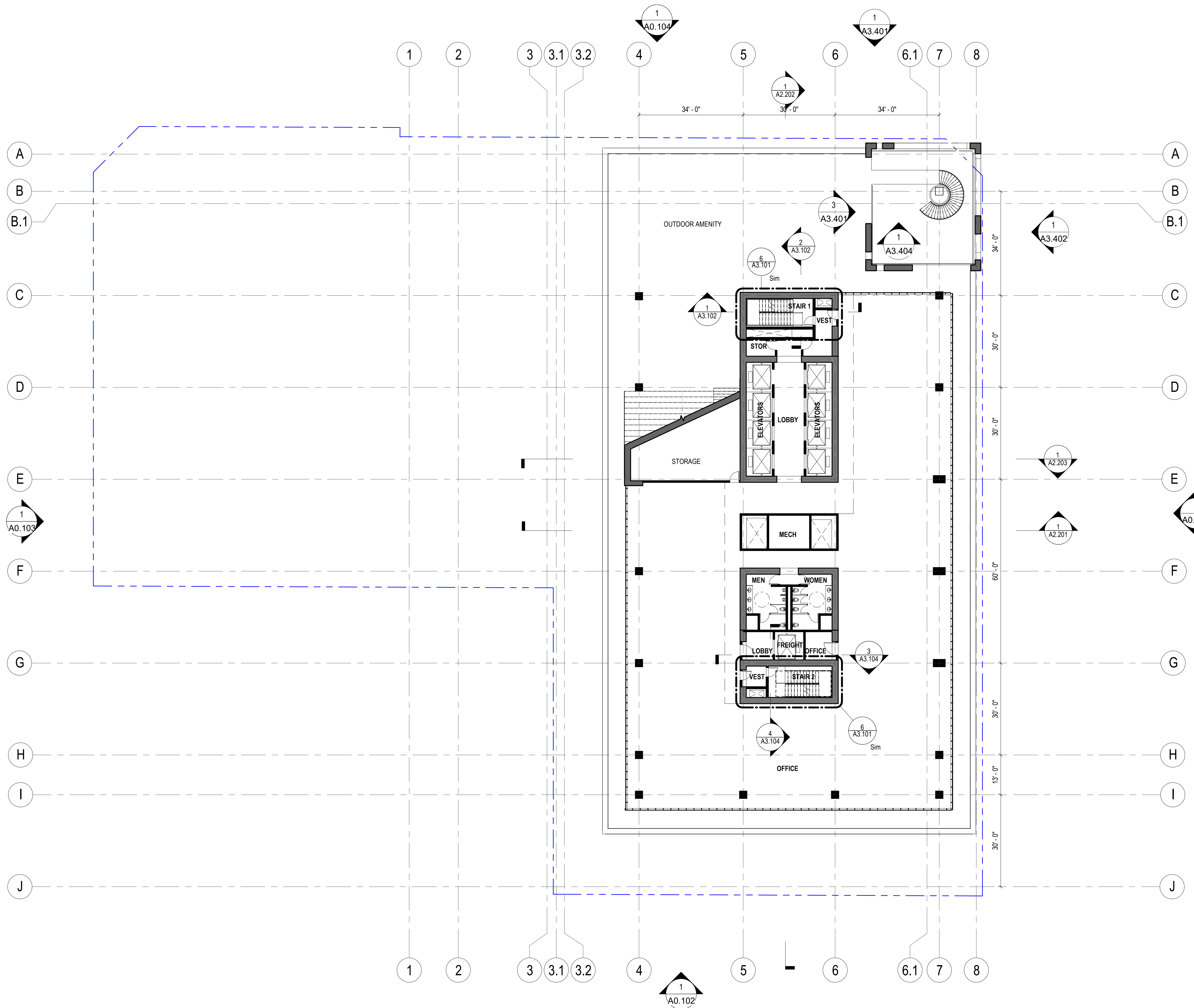


**A1.216**

KEY PLAN



LEVEL 16  
1/16" = 1'-0" SCALE.



**SHEET NOTES**



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**EXHIBIT A**  
**CPC-2017-469-GPA-VZC-HD-MCUP-SPR**  
**October 12, 2023**

**GENERAL NOTES**

Seal / Signature

**NOT FOR CONSTRUCTION**

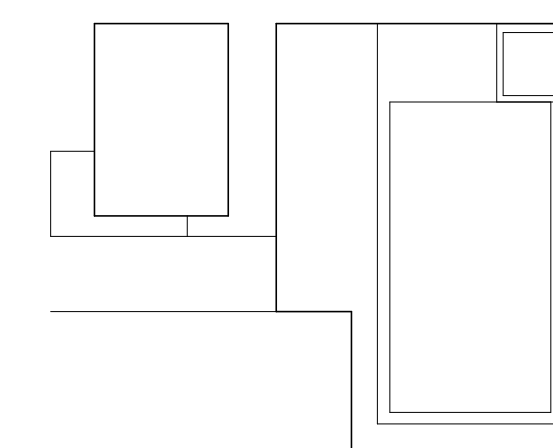
Project Name  
401 S Hewitt

Project Number  
05.1291.000

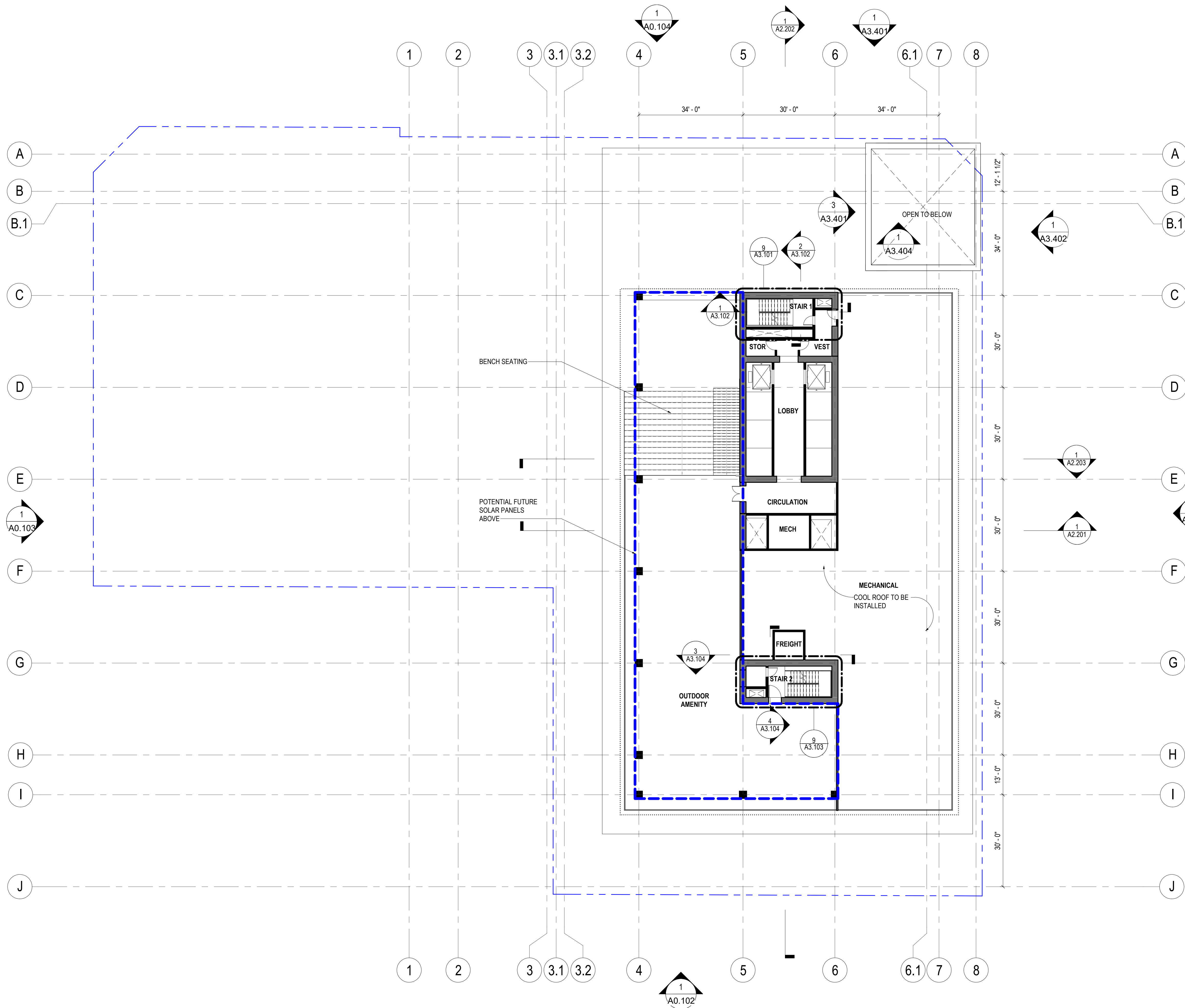
Description  
PLAN - OVERALL - LEVEL 17

Scale  
1/16" = 1'-0"

**KEY PLAN**



**LEVEL 17**  
1/16" = 1'-0" SCALE.



SHEET NOTES



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Tel 213.234.0057

Date	Description
1 02/06/2017	ENTITLEMENT SET
2 07/14/2017	ENTITLEMENT SET
3 10/25/2017	ENTITLEMENT SET
6 05/31/2019	ENTITLEMENT SET
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**EXHIBIT A**  
**CPC-2017-469-GPA-VZC-HD-MCUP-SPR**  
**October 12, 2023**

GENERAL NOTES

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**NOT FOR  
CONSTRUCTION**

Project Name

401 S Hewitt

Project Number

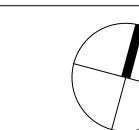
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Description

PLAN - OVERALL - LEVEL 18

Scale

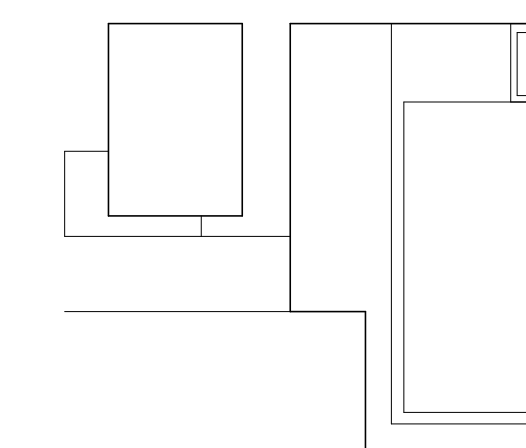
1/16" = 1'-0"



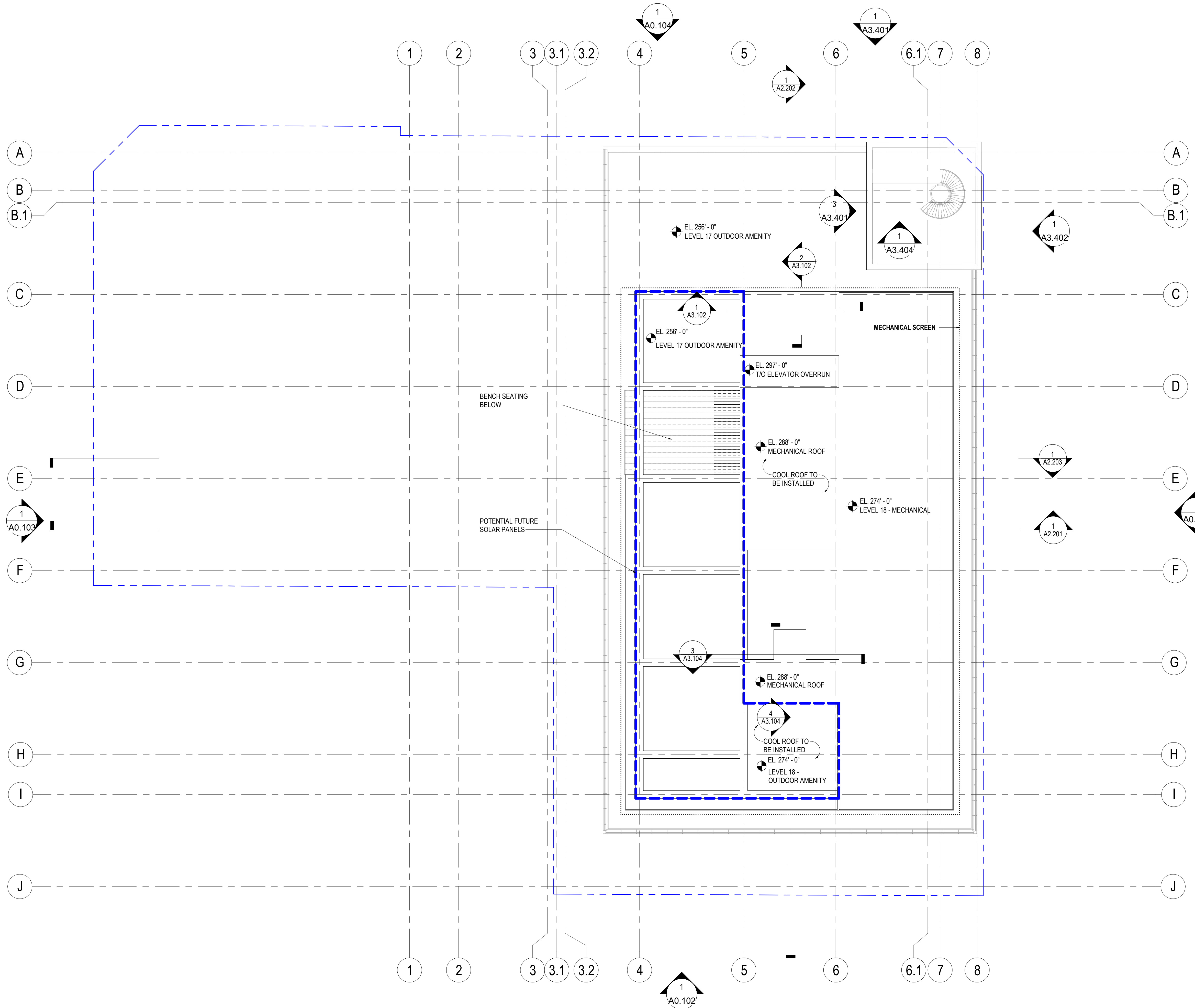
**A1.218**

LEVEL 18  
1/16" = 1'-0" SCALE.

KEY PLAN



9/8/2023 4:59:23 PM BIM 360://005.1291.000 - 4th & Hewitt - Art District LA401 - Architecture\_F20.rvt



### SHEET NOTES



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**EXHIBIT A**  
**CPC-2017-469-GPA-VZC-HD-MCUP-SPR**  
**October 12, 2023**

### GENERAL NOTES

Seal / Signature

**NOT FOR  
CONSTRUCTION**

Project Name

401 S Hewitt

Project Number

05.1291.000

Description

PLAN - OVERALL - ROOF

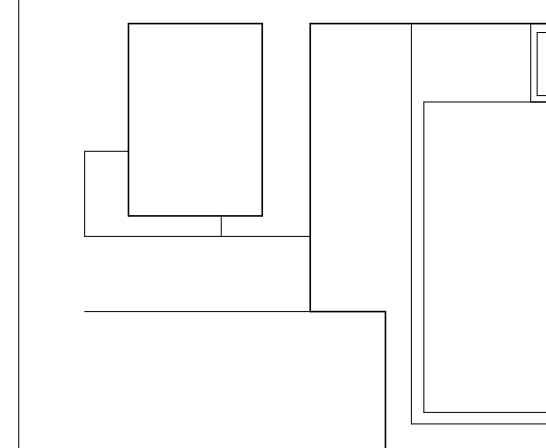
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**A1.219**

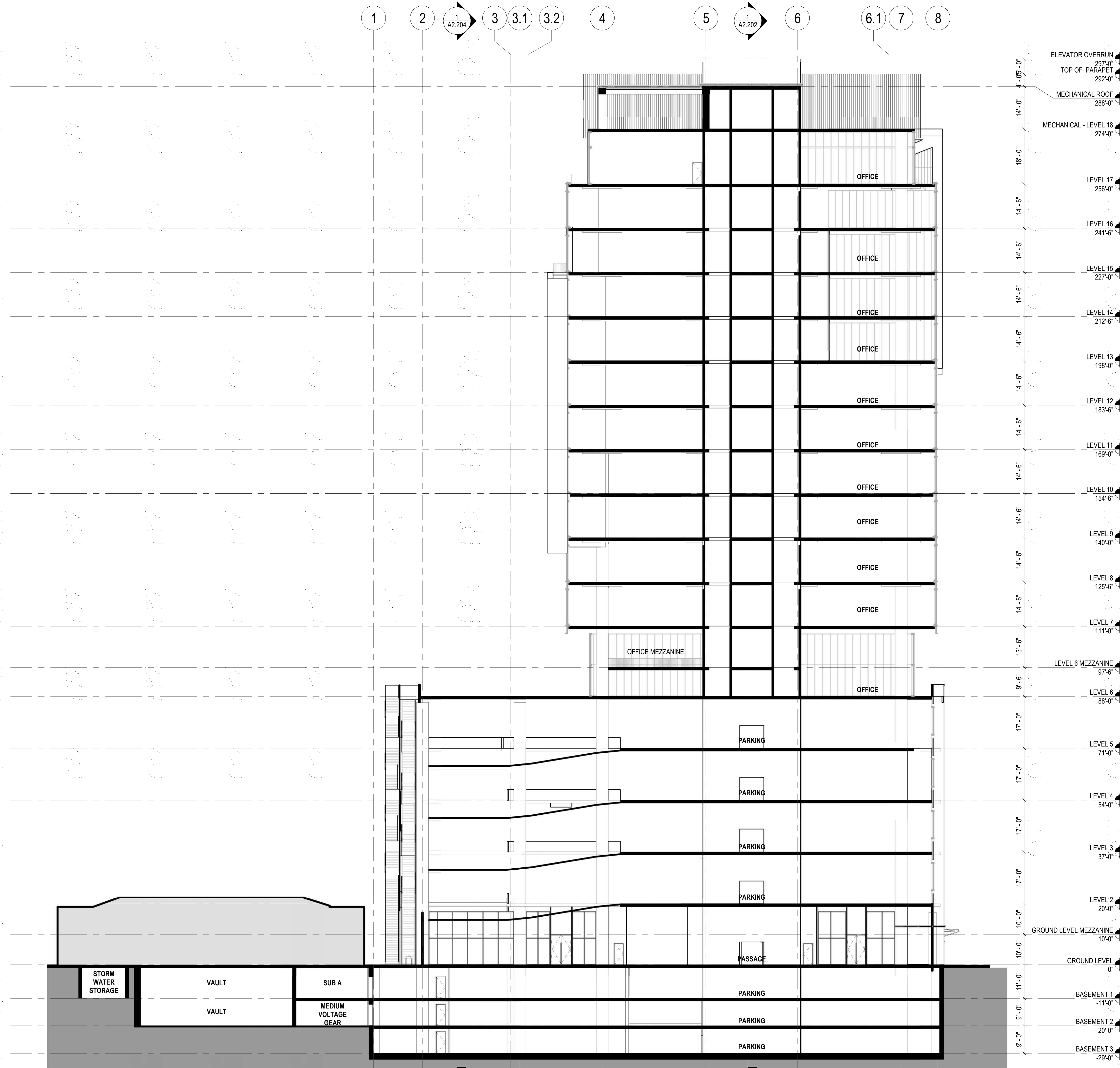
© 2017 Gensler

### KEY PLAN



**ROOF**  
1/16" = 1'-0" SCALE: **1**

3/15/2022 1:15:10 PM BIM\_360/005.1291.000 - 4th & Hewitt - Art District LA401 - Architecture\_F20.rvt



LONGITUDINAL SECTION AT PASSAGE  
1/16" = 1'-0" SCALE.

**SHEET NOTES**

**SECTION 3202 ENCROACHMENTS**  
SECTION 3202.3.2  
WHERE THE VERTICAL CLEARANCE ABOVE GRADE TO PROJECTING WINDOWS, BALCONIES, ARCHITECTURAL FEATURES OR MECHANICAL EQUIPMENT IS MORE THAN 8 FEET, 1 INCH OF CLEARANCE ABOVE 8 FEET, BUT THE MAXIMUM ENCROACHMENT SHALL BE 4 FEET.



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**EXHIBIT A**  
**CPC-2017-469-GPA-VZC-HD-MCUP-**  
**October 12, 2023**

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**NOT FOR CONSTRUCTION**

Project Name  
401 S Hewitt

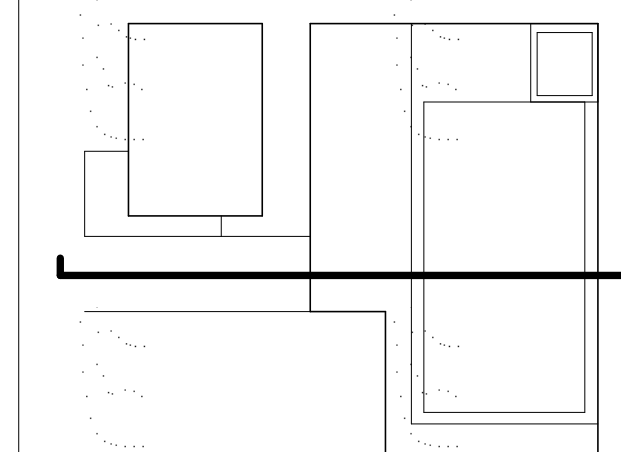
Project Number  
05.1291.000

Description  
SECTION - LONGITUDINAL AT PASSAGE

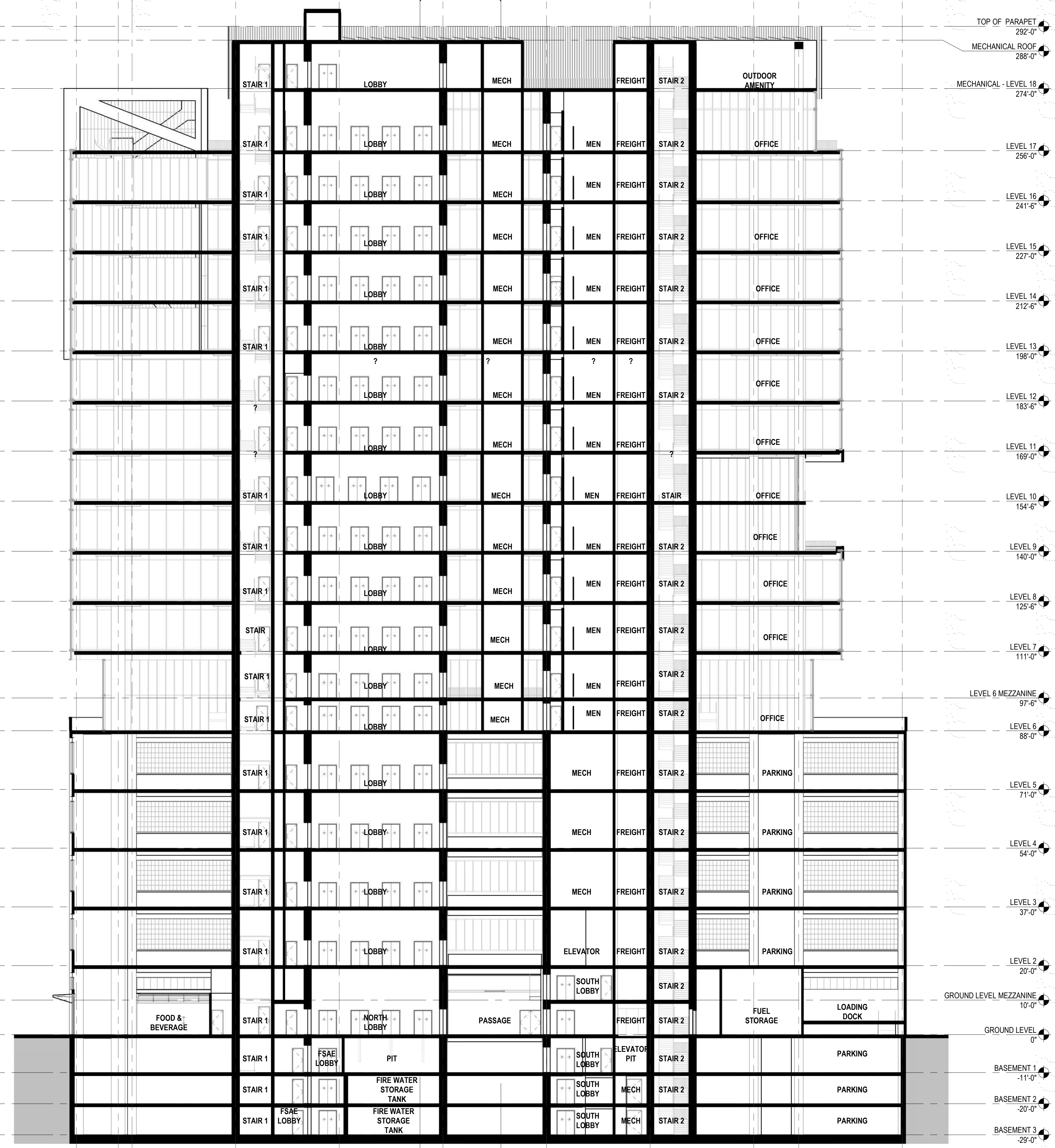
Scale  
1/16" = 1'-0"

**A2.201**

**KEY PLAN**



A B B.1 C D E F G H I J



TOP OF PARAPET 292'-0"  
 MECHANICAL ROOF 288'-0"  
 MECHANICAL - LEVEL 18 274'-0"  
 LEVEL 17 256'-0"  
 LEVEL 16 241'-6"  
 LEVEL 15 227'-0"  
 LEVEL 14 212'-6"  
 LEVEL 13 198'-0"  
 LEVEL 12 183'-6"  
 LEVEL 11 169'-0"  
 LEVEL 10 154'-6"  
 LEVEL 9 140'-0"  
 LEVEL 8 125'-6"  
 LEVEL 7 111'-0"  
 LEVEL 6 MEZZANINE 97'-6"  
 LEVEL 6 88'-0"  
 LEVEL 5 71'-0"  
 LEVEL 4 54'-0"  
 LEVEL 3 37'-0"  
 LEVEL 2 20'-0"  
 GROUND LEVEL MEZZANINE 10'-0"  
 GROUND LEVEL 0'  
 BASEMENT 1 -11'-0"  
 BASEMENT 2 -20'-0"  
 BASEMENT 3 -29'-0"

TRANSVERSAL SECTION AT ELEVATOR LOBBY 1  
 1/16" = 1'-0" SCALE

**SHEET NOTES**

**SECTION 3202 ENCROACHMENTS**  
 SECTION 3202.3.2  
 WHERE THE VERTICAL CLEARANCE ABOVE GRADE TO PROJECTING WINDOWS, BALCONIES, ARCHITECTURAL FEATURES OR MECHANICAL EQUIPMENT IS MORE THAN 8 FEET, 1 INCH OF CLEARANCE ABOVE 8 FEET, BUT THE MAXIMUM ENCROACHMENT SHALL BE 4 FEET.

**LEGENDARY**  
 INVESTORS GROUP, LLC

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**ARC**  
 engineering

**PSOMAS**

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**SALT**

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Date	Description
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**October 12, 2023**

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Project Name  
 401 S Hewitt

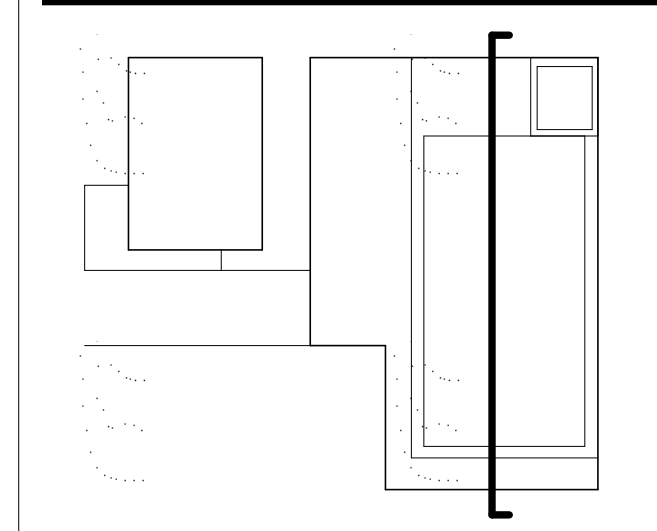
Project Number  
 05.1291.000

Description  
 SECTION - TRANSVERSAL AT ELEVATOR LOBBY

Scale  
 1/16" = 1'-0"

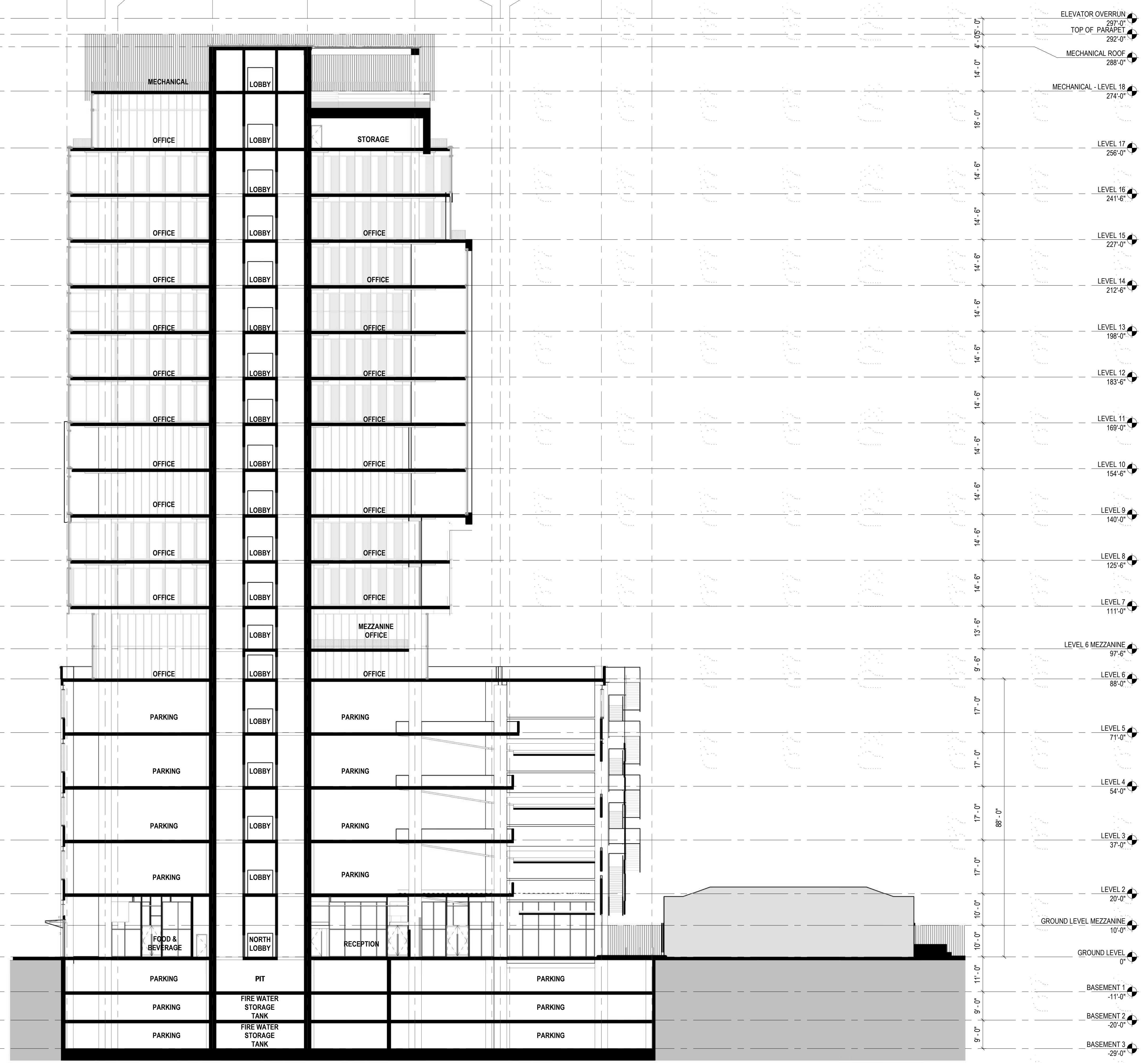
**A2.202**

**KEY PLAN**



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8 7 6.1 6 5 4 3.2 3.1 3 2 1



ELEVATOR OVERRUN 297'-0"  
 TOP OF PARAPET 292'-0"  
 MECHANICAL ROOF 288'-0"  
 MECHANICAL - LEVEL 18 274'-0"  
 LEVEL 17 256'-0"  
 LEVEL 16 241'-6"  
 LEVEL 15 227'-0"  
 LEVEL 14 212'-6"  
 LEVEL 13 198'-0"  
 LEVEL 12 183'-6"  
 LEVEL 11 169'-0"  
 LEVEL 10 154'-6"  
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 LEVEL 5 71'-0"  
 LEVEL 4 54'-0"  
 LEVEL 3 37'-0"  
 LEVEL 2 20'-0"  
 GROUND LEVEL MEZZANINE 10'-0"  
 GROUND LEVEL 0"  
 BASEMENT 1 -11'-0"  
 BASEMENT 2 -20'-0"  
 BASEMENT 3 -29'-0"

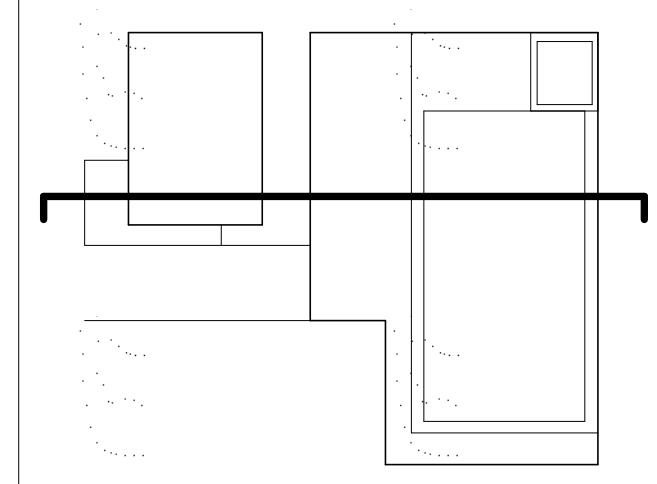
LONGITUDINAL SECTION AT CORE  
 1/16" = 1'-0" SCALE.

SHEET NOTES

**EXHIBIT A**  
**CPC-2017-469-GPA-VZC-HD-MCUP-SPR**  
**October 12, 2023**

GENERAL NOTES

KEY PLAN



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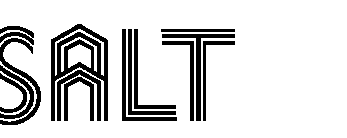


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10/12/2023	ENTITLEMENT SET

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Project Number  
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Description  
 SECTION - LONGITUDINAL AT CORE

Scale  
 1/16" = 1'-0"

**A2.203**

GENERAL LANDSCAPE NOTES

ALL EXISTING GRADING, CURB LAYOUT, EASEMENTS AND UTILITIES ARE BASED ON SURVEY INFORMATION PROVIDED BY ARCHITECT, AND ARE SHOWN FOR INFORMATION ONLY.

REFER TO CIVIL DRAWINGS FOR PROPOSED UTILITIES. NOTIFY ARCHITECT OF ANY DISCREPANCIES. CONTRACTOR TO VERIFY LOCATION OF ALL UTILITIES ABOVE AND BELOW GRADE PRIOR TO CONSTRUCTION.

ALL ARCHITECTURAL ELEMENTS ARE SHOWN ON LANDSCAPE PLANS FOR REFERENCE ONLY. REFER TO ARCHITECTURAL DRAWINGS PREPARED BY GENSLER FOR ACTUAL ARCHITECTURAL INFORMATION. NOTIFY ARCHITECT OF ANY DISCREPANCIES PRIOR TO CONSTRUCTION.

WRITTEN DIMENSIONS TAKE PRECEDENCE OVER SCALED DIMENSIONS.

ALL LAYOUT DIMENSIONS ARE FROM PLAN VIEW CALCULATIONS. FIELD DIMENSIONS MAY VARY FROM PLAN DUE TO ACTUAL LENGTHS ALONG SLOPED SURFACES.

ALL LAYOUT DIMENSIONS ARE BACK OF CURB, FACE OF WALL AND/OR FACE OF BUILDING UNLESS OTHERWISE NOTED.

DIMENSIONS MARKED "VERIFY" ARE TO BE FIELD MEASURED. ANY DISCREPANCIES FROM THE NOTED DIMENSIONS ARE TO BE BROUGHT TO THE ATTENTION OF THE ARCHITECT PRIOR TO CONTINUING WORK.

COORDINATE PROPOSED WALKS AND RAMPS WITH EXISTING CONDITIONS. LAYOUT OF ARCS TO BE SMOOTH AND CONTINUOUS. STAKE PROPOSED WALKS AND REVIEW IN FIELD WITH ARCHITECT PRIOR TO FORMING.

ALL DETACHED WALKS SHALL BE CONSTRUCTED PARALLEL TO EXISTING CURB AND GUTTER UNLESS OTHERWISE NOTED.

LIMIT OF WORK LINE FOR CONSTRUCTION IS SHOWN DIAGRAMMATICALLY AND OCCURS AT BACK OF CURB, EDGE OF ROAD, FACE OF BUILDING OR PROPERTY LINE UNLESS OTHERWISE NOTED.

DRAWING AND PLAN NOTES REPRESENT FINISHED, BUILT CONDITIONS. ALL BRACING, TEMPORARY SUPPORTS AND SHORING NECESSARY FOR CONSTRUCTION ARE THE SOLE RESPONSIBILITY OF THE CONTRACTOR.

ALL SYMBOLS ARE SHOWN DIAGRAMMATICALLY ILLUSTRATING APPROXIMATE LOCATION OF EXISTING AND PROPOSED MATERIALS. ANY DISCREPANCIES OR CONFLICTS BETWEEN EXISTING AND PROPOSED CONDITIONS SHALL BE REPORTED TO THE ARCHITECT PRIOR TO CONSTRUCTION.

REFER TO SPECIFICATIONS FOR ADDITIONAL INFORMATION PERTAINING TO THE PROJECT MATERIALS, PROCEDURE AND INSTALLATION.

GENERAL GRADING NOTES

MAXIMUM SLOPES IN LANDSCAPE AREAS SHALL NOT EXCEED 3:1 AND MINIMUM SLOPES IN LANDSCAPE AREAS SHALL BE 2% UNLESS OTHERWISE NOTED ON THE DRAWINGS.

MAXIMUM CROSS SLOPE ON ALL CONCRETE WALKS SHALL NOT EXCEED 2%; MAXIMUM SLOPE ALONG LENGTH OF WALKS SHALL NOT EXCEED 4.99%, UNLESS OTHERWISE NOTED.

PROVIDE SMOOTH, CONTINUOUS TRANSITIONS BETWEEN SLOPES UNLESS OTHERWISE NOTED OR INDICATED ON THE DRAWINGS.

FINAL GRADING TO BE FIELD REVIEWED AND APPROVED BY LANDSCAPE ARCHITECT PRIOR TO SEEDING, SODDING, OR PLANTING.

CONTRACTOR TO GRADE AND REPAIR ALL DISTURBED AREAS IN ORDER TO CREATE A SMOOTH TRANSITION FROM NEW GRADE TO EXISTING GRADE, INCLUDING HARDSCAPE AND SOFTSCAPE AREAS. NOTIFY ARCHITECT OF ANY CONFLICTS.



LANDSCAPE ABBREVIATIONS

AD	AREA DRAIN	EW	EACH WAY	NTS	NOT TO SCALE	SPECS	SPECIFICATIONS
APPROX	APPROXIMATE	EXIST	EXISTING	OC	ON CENTER(S)	SQ	SQUARE
ARCH	ARCHITECT	EXP	EXPOSED	OD	OUTSIDE DIAMETER	SS	STAINLESS STEEL
AVG	AVERAGE	EXT	EXTERIOR	OPP	OPPOSITE	STA	STATION
B&B	BALLED & BURLAPPED	FC	FACE OF CURB	PA	PLANTING AREA	STD	STANDARD
BF	BOTTOM OF FOOTING	FG	FINISH GRADE	PC	PLUMBING CONTRACTOR	STL	STEEL
BLDG	BUILDING	FL	FLOW LINE	PED	PEDESTRIAN	STR	STRUCTURAL
BM	BENCHMARK	FOS	FACE OF STEP	PERF	PERFORATE(D)	TC	TOP OF CURB
BOC	BACK OF CURB	FOW	FACE OF WALL	PI	POINT OF INTERSECTION	TF	TOP OF FOOTING
BOS	BOTTOM OF SLOPE/RAMP BEARING	FP	FIRE PROTECTION	PL	PROPERTY LINE	THK	THICK
BS	BOTTOM OF STEP/STAIR	FS	FINISHED SURFACE	POB	POINT OF BEGINNING	TLF	TOP OF LIGHT FOOTING
BW	BOTTOM OF WALL	FT	FOOT (FEET)	POC	POINT OF CONNECTION	TOPO	TOPOGRAPHY
CAL	CALIPER	FTG	FOOTING	PRV	PRESSURE REDUCING VALVE	TOS	TOP OF SLOPE
CB	CATCH BASIN	GA	GAUGE	PSF	POUNDS PER SQUARE FOOT	TS	TOP OF STEP
CF	CUBIC FEET	GALV	GALVANIZED	PSI	POUNDS PER SQUARE INCH	TR	TOP OF RAMP
CIP	CAST-IN-PLACE	GB	GRADE BREAK	PT	POINT OF TANGENCY	TRANS	TRANSFORMER
CJ	CONTROL JOINT	GC	GENERAL CONTRACTOR	PVC	POLYVINYL CHLORIDE	TW	TOP OF WALL
CL	CENTERLINE	GPM	GALLON PER MINUTE	PVMT	PAVEMENT	TYP	TYPICAL
CLR	CLEAR(ANCE)	HORIZ	HORIZONTAL	PVR	PAVER(S)	VAR	VARIES
CO	CLEAN OUT	HP	HIGH POINT	QTY	QUANTITY	VERT	VERTICAL
COL	COLUMN	HT	HEIGHT	R	RADIUS	VOL	VOLUME
CONC	CONCRETE	ID	INSIDE DIAMETER	RD	ROOF DRAIN	W/	WITH
CONT	CONTINUOUS	IN	INCHES	RE	REFERENCE	W/O	WITHOUT
CU	CUBIC	INV	INVERT ELEVATION	REINF	REINFORCE(D), (ING)	WP	WATERPROOFING
DEG	DEGREE	JT	JOINT	REQD	REQUIRED	WT	WEIGHT
DEMO	DEMOLISH, DEMOLITION DRAIN INLET	LP	LOW POINT	REV	REVISION(S), REVISED	WWF	WELDED WIRE FABRIC
DIA	DIAMETER	LS	LANDSCAPE STRUCTURAL	RIM	RIM ELEVATION	YD	YARD
DIM	DIMENSION	MAX	MAXIMUM	ROW	RIGHT-OF-WAY		
DN	DOWN	MH	MANHOLE	SAN	SANITARY		
DP	DRAIN PANEL	MIN	MINIMUM	SCH	SCHEDULE		
DTL	DETAIL	MISC	MISCELLANEOUS	SD	STORM DRAIN		
DWG	DRAWING	MTD	MOUNTED	SEC	SECTION		
EA	EACH	MTL	METAL	SF	SQUARE FOOT		
EJ	EXPANSION JOINT	NA	NOT APPLICABLE	SHT	SHEET		
ELV	ELEVATION	NIC	NOT IN CONTRACT	SIM	SIMILAR		
EQ	EQUAL	NOM	NOMINAL	SM	STORMWATER MANAGEMENT		

LANDSCAPE SYMBOL KEY

DETAIL MARKER		SPOT ELEVATION		ADA BOLLARD		IRRIGATION VALVE BOX	
EXTERIOR ELEVATION MARKER		DATUM ELEVATION		ASH RECEPTACLE		SITE ELECTRIC BOX	
SECTION MARKER		TOP OF MULCH ELEVATION		BIKE RACK		PEDESTRIAN LIGHT POLES	
REVISION NUMBER		GRADE SLOPE DOWN		BIKE REPAIR STATION		PEDESTRIAN LIGHT POLES	
DRAWING TITLE		MATCH EXISTING SURFACE WITH PROPOSED		CAMPUS BLUE PHONE			
LAYOUT POINT		ALIGN		DOOR			
LAYOUT POINT		DIAMETER		PLANTER DRAIN			
TREE GRATE		NORTH ARROW		EXISTING & PROPOSED SITE UTILITY COVERS			

**EXHIBIT A**  
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Seal / Signature

**NOT FOR CONSTRUCTION**

Project Name  
401 S Hewitt

Project Number  
05.1291.000

Description  
LANDSCAPE NOTES, SYMBOLS AND ABBREVIATIONS

Scale  
NOT TO SCALE

**L0.01**



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agence ter  
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**EXHIBIT A**  
**CPC-2017-469-GPA-VZC-HD-MCUP-SPR**  
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SUMMARY OF LANDSCAPED AREA	
LEVEL	AREA
GROUND LEVEL SHRUB & GROUND COVER	1,001
ROOF LEVEL 6 SHRUB & GROUND COVER	2860
ROOF LEVEL 17 SHRUB & GROUND COVER	2385
<b>PROJECT TOTAL</b>	<b>6,246</b>

NOTE: TOTAL SHRUB AND GROUND COVER EXCLUDINGS STREETSCAPE  
TOTAL STREET TREES REMOVED: 3  
REPLACED 2:1: 6  
TOTAL STREET TREES PLANTED: 12

Seal / Signature

**NOT FOR CONSTRUCTION**

Project Name  
401 S Hewitt

Project Number  
05.1291.000

Description  
LANDSCAPE SITE PLAN

Scale  
1/16" = 1'-0"

**L1.01**

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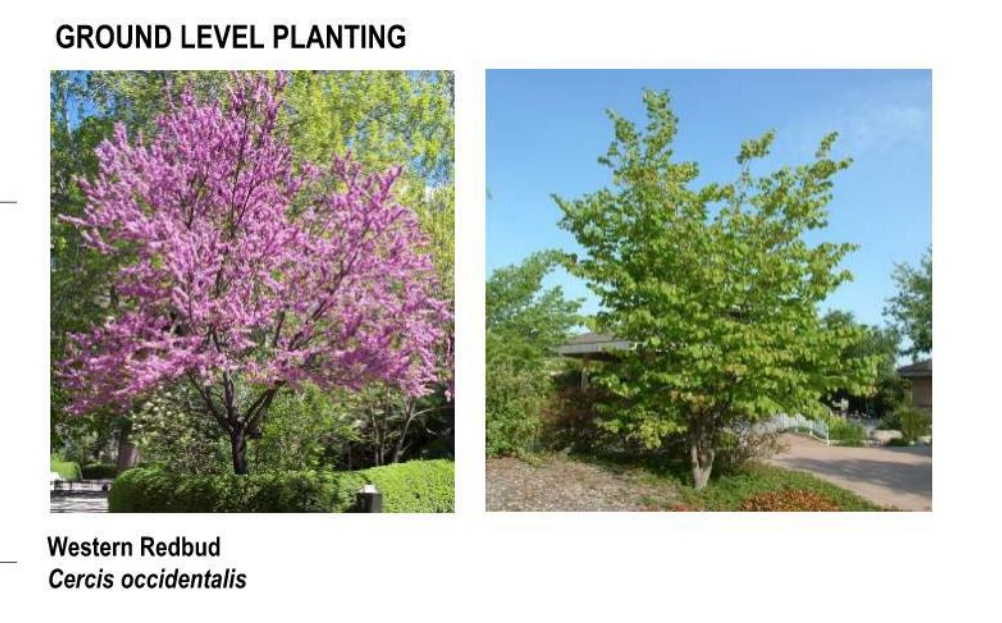
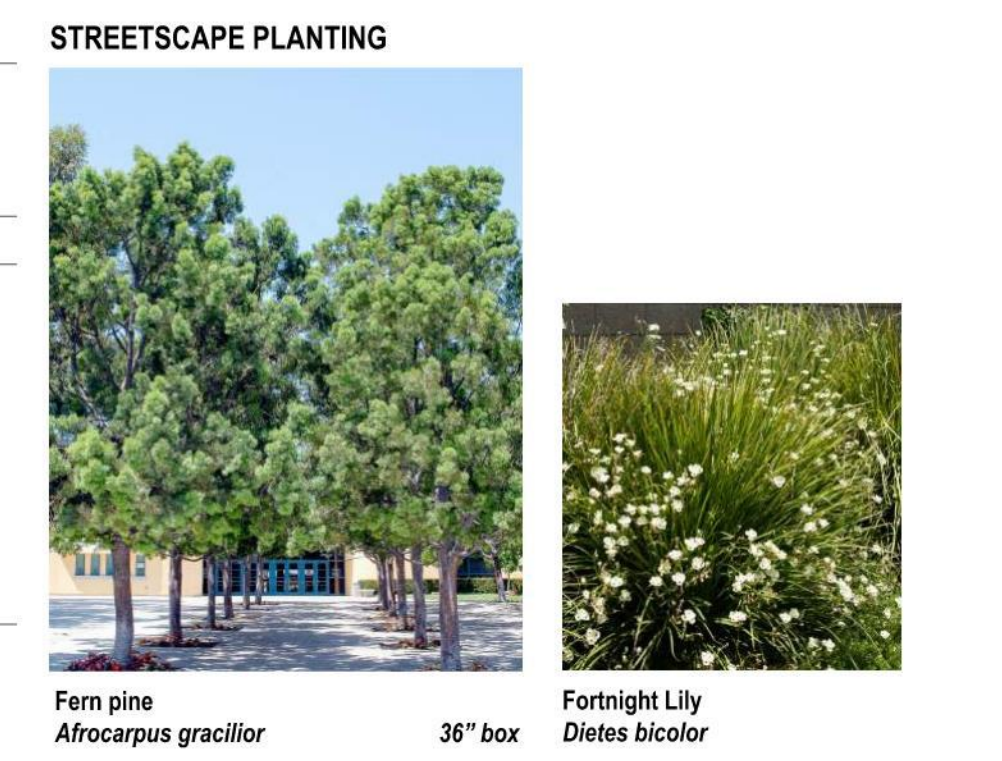
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**EXHIBIT A**  
**CPC-2017-469-GPA-VZC-HD-MCUP-SPR**  
**October 12, 2023**

- MATERIALS AND PLANTING LEGEND**
- CONCRETE PAVER
  - PLANTING AREA
  - CIP CONC. PAVING W/ SAWCUT PATTERN
  - MOVEABLE PLANTERS
  - 36" BOX STREET TREE SPECIES - *Afrocarpus gracillior*  
Fern pine  
TOTAL: 10
  - 60" BOX SPECIMEN TREE SPECIES - *Cercis occidentalis*  
Western Redbud  
TOTAL: 3

Seal / Signature

**NOT FOR CONSTRUCTION**

Project Name  
401 S Hewitt

Project Number  
05.1291.000

Description  
LANDSCAPE MATERIALS & PLANTING PLAN

Scale  
1/16" = 1'-0"

**L1.02**

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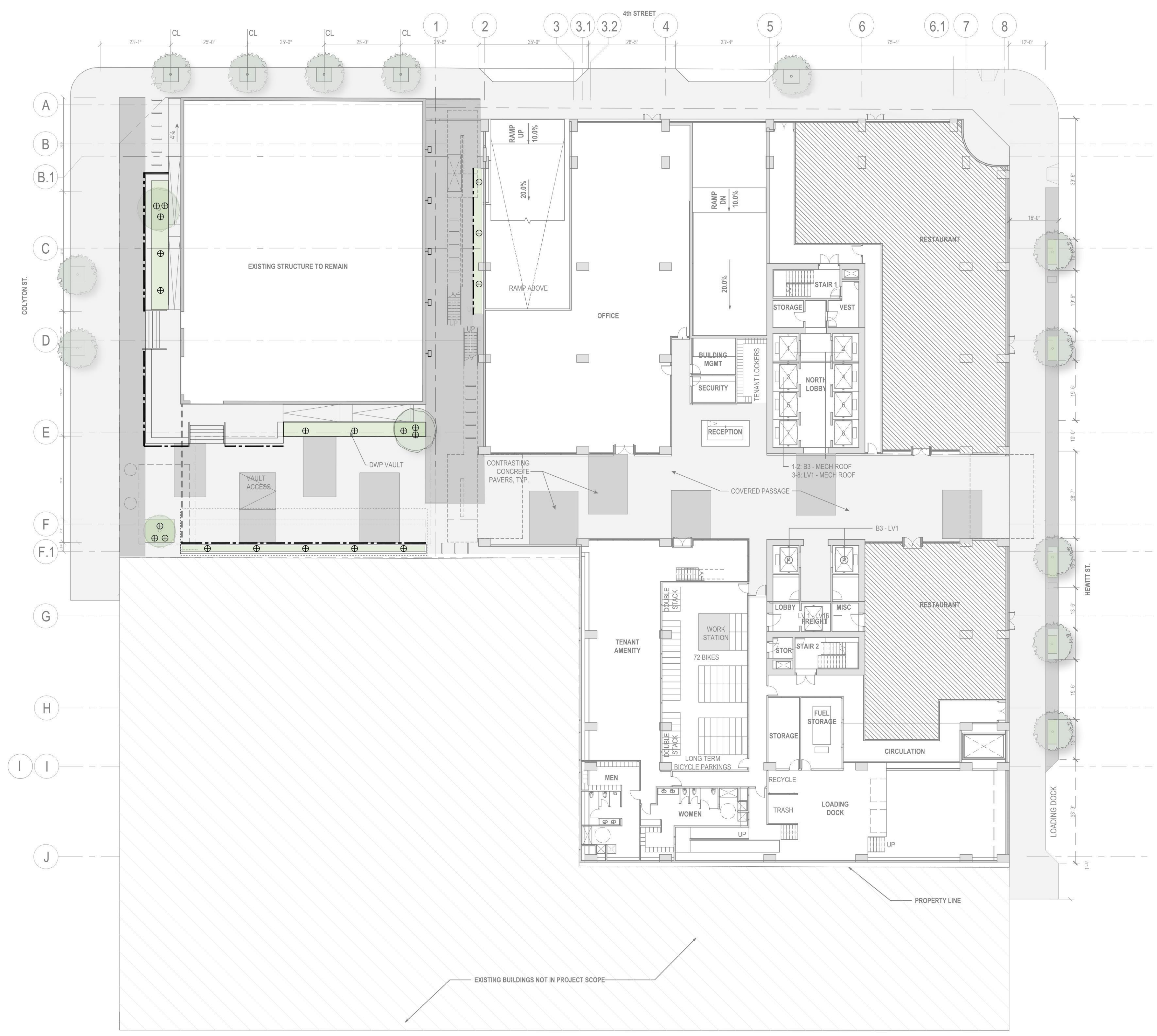
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**EXHIBIT A**  
**CPC-2017-469-GPA-VZC-HD-MCUP-SPR**  
**October 12, 2023**



Seal / Signature

**NOT FOR  
CONSTRUCTION**

Project Name  
401 S Hewitt

Project Number  
05.1291.000

Description  
LANDSCAPE LIGHTING PLAN

Scale  
1/16" = 1'-0"

**L1.03**

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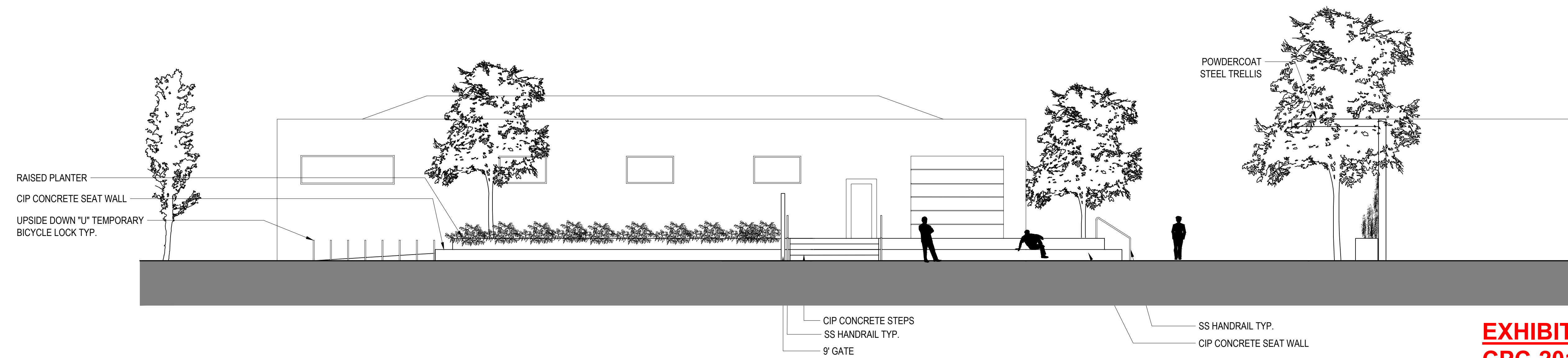
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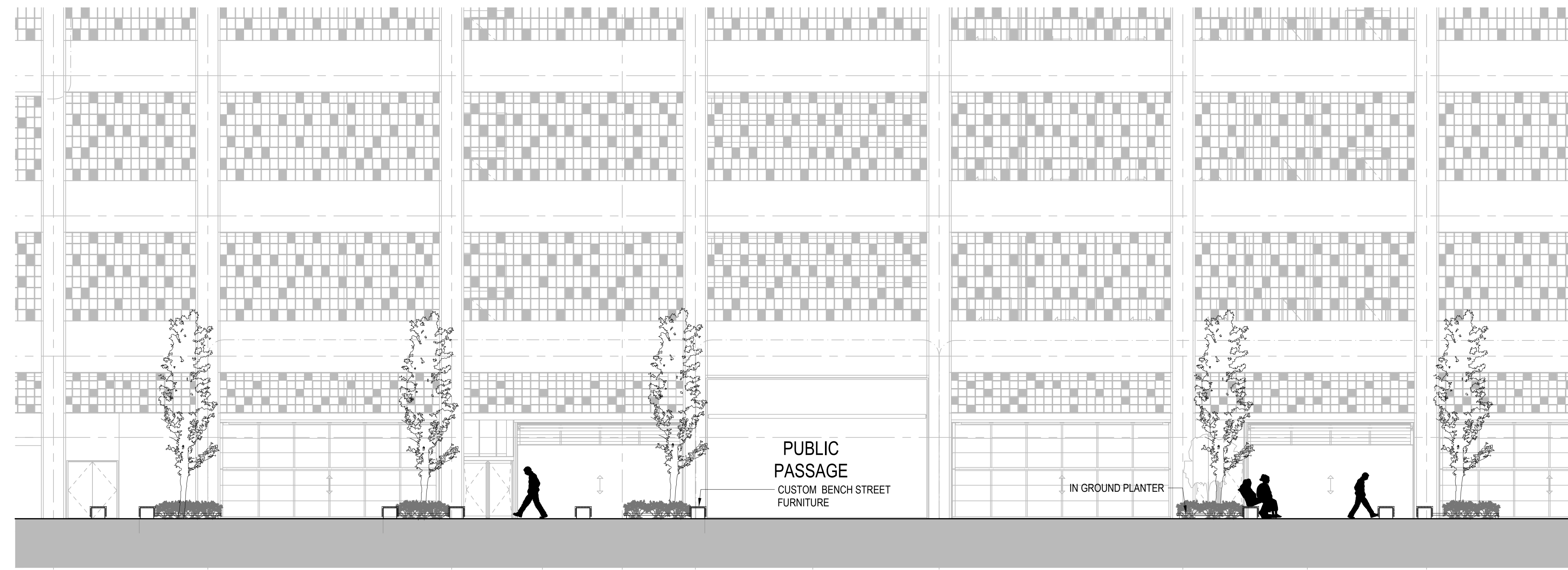
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**EXHIBIT A**  
**CPC-2017-469-GPA-VZC-HD-MCUP-SPR**  
**October 12, 2023**

1 SITE ELEVATION WEST  
L2.01



2 SITE ELEVATION EAST  
L2.01

Seal / Signature

**NOT FOR  
CONSTRUCTION**

Project Name  
401 S Hewitt

Project Number  
05.1291.000

Description  
SITE SECTIONS

Scale  
1/8" = 1'-0"

**L2.01**

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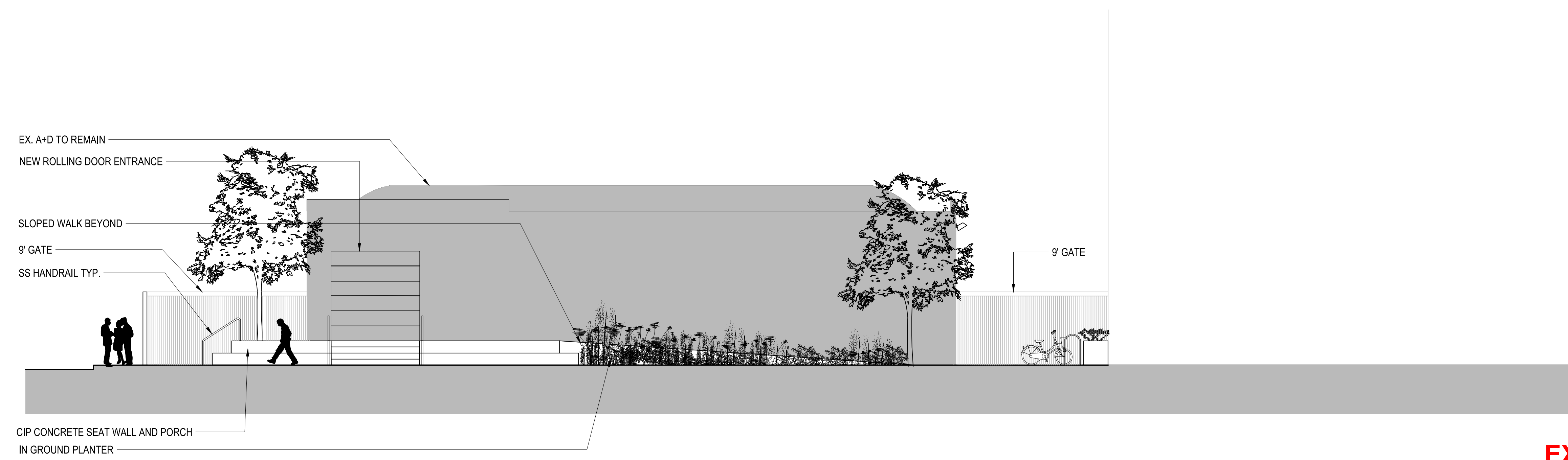
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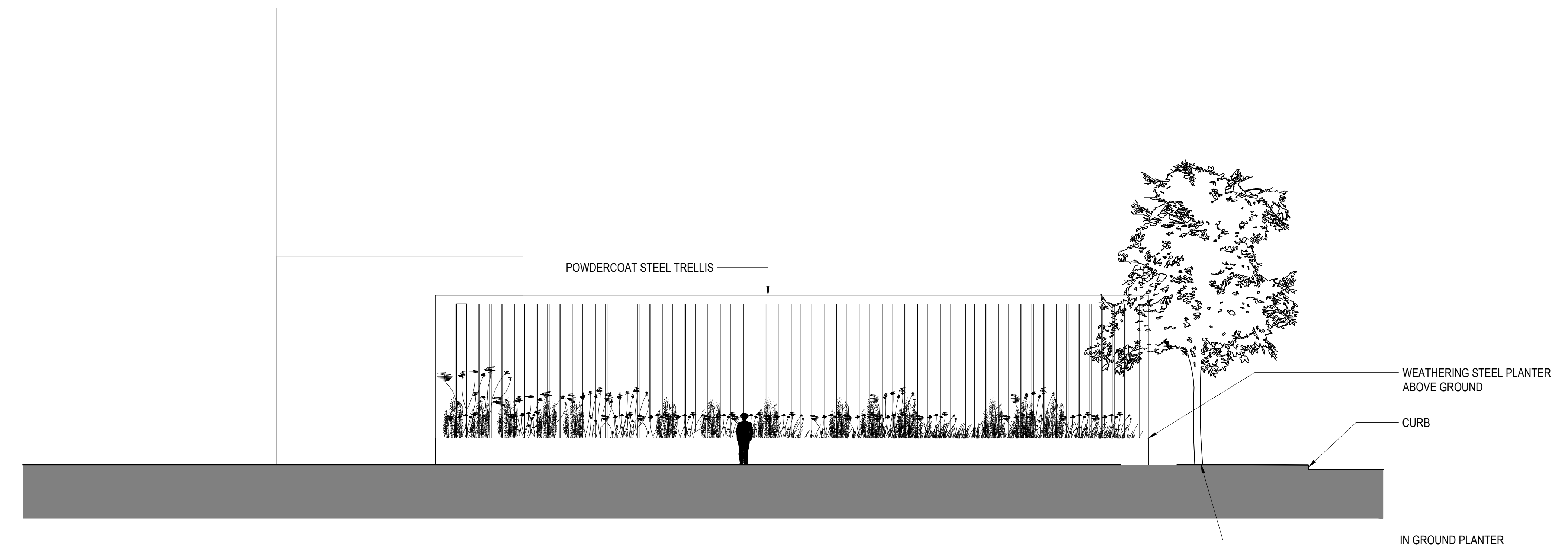
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1 SITE ELEVATION NORTH  
L2.02



2 SITE ELEVATION SOUTH  
L2.02

**EXHIBIT A**  
**CPC-2017-469-GPA-VZC-HD-MCUP-SPR**  
**October 12, 2023**

Seal / Signature

**NOT FOR  
CONSTRUCTION**

Project Name  
401 S Hewitt

Project Number  
05.1291.000

Description  
SITE ELEVATIONS

Scale  
1/8" = 1'-0"

**L2.02**

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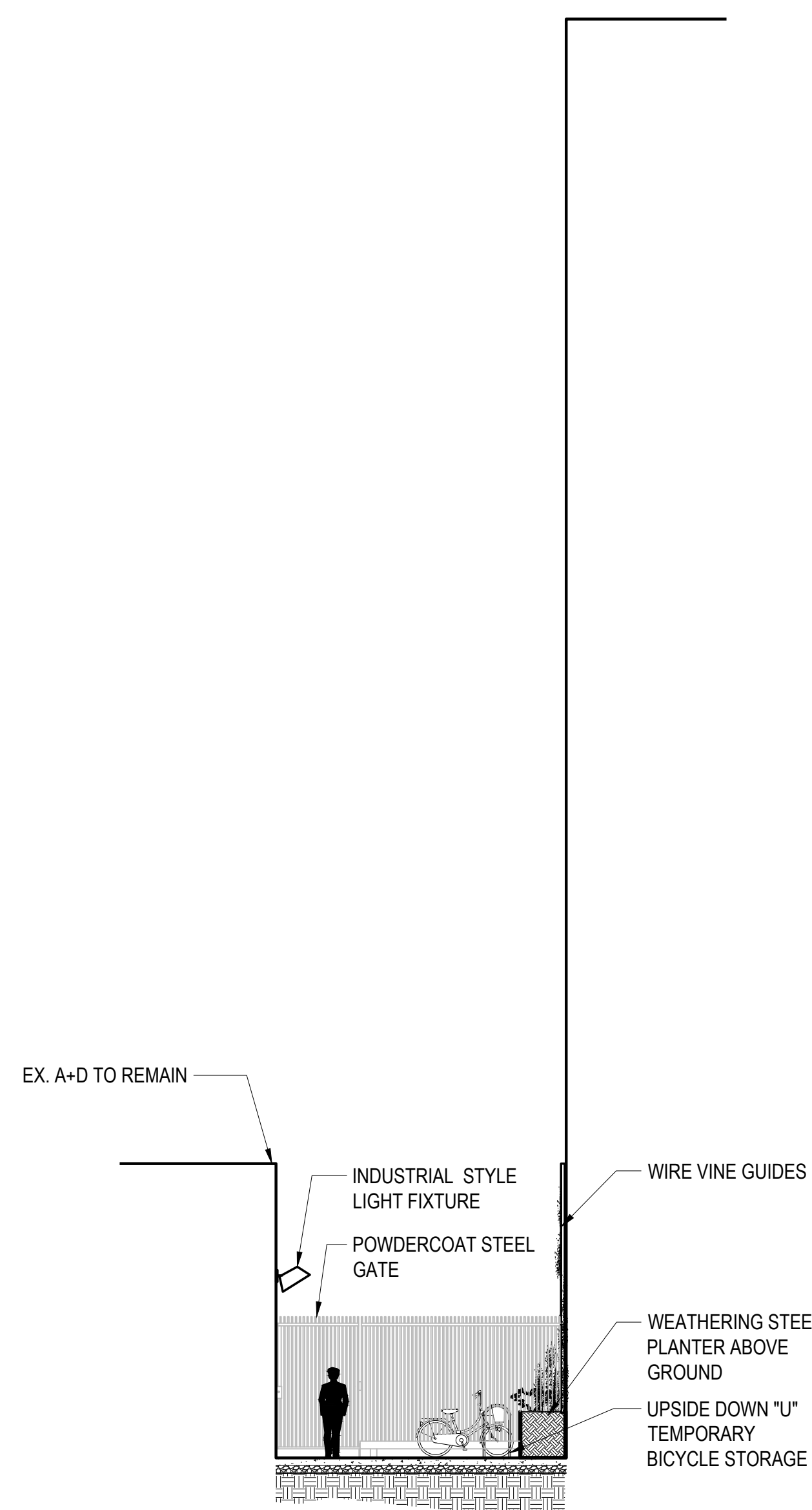
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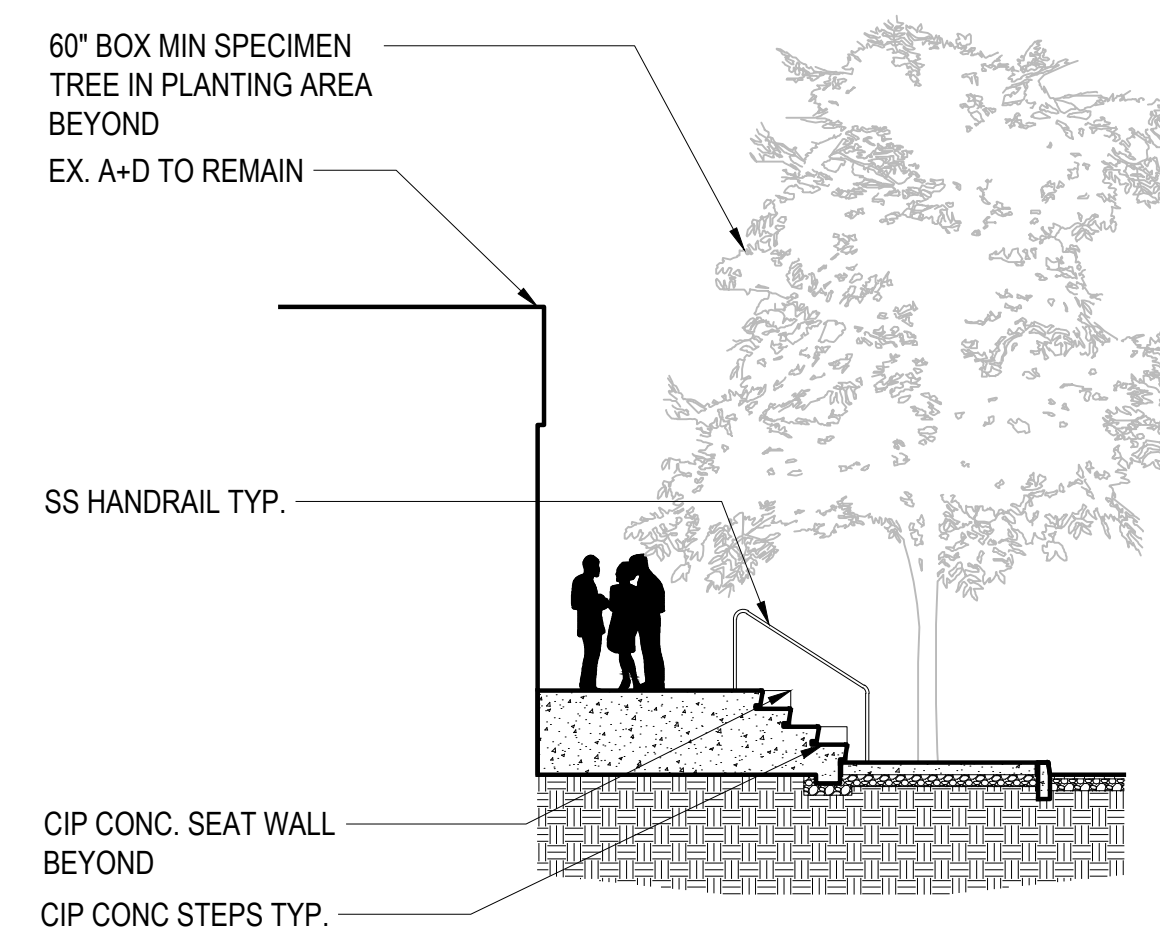
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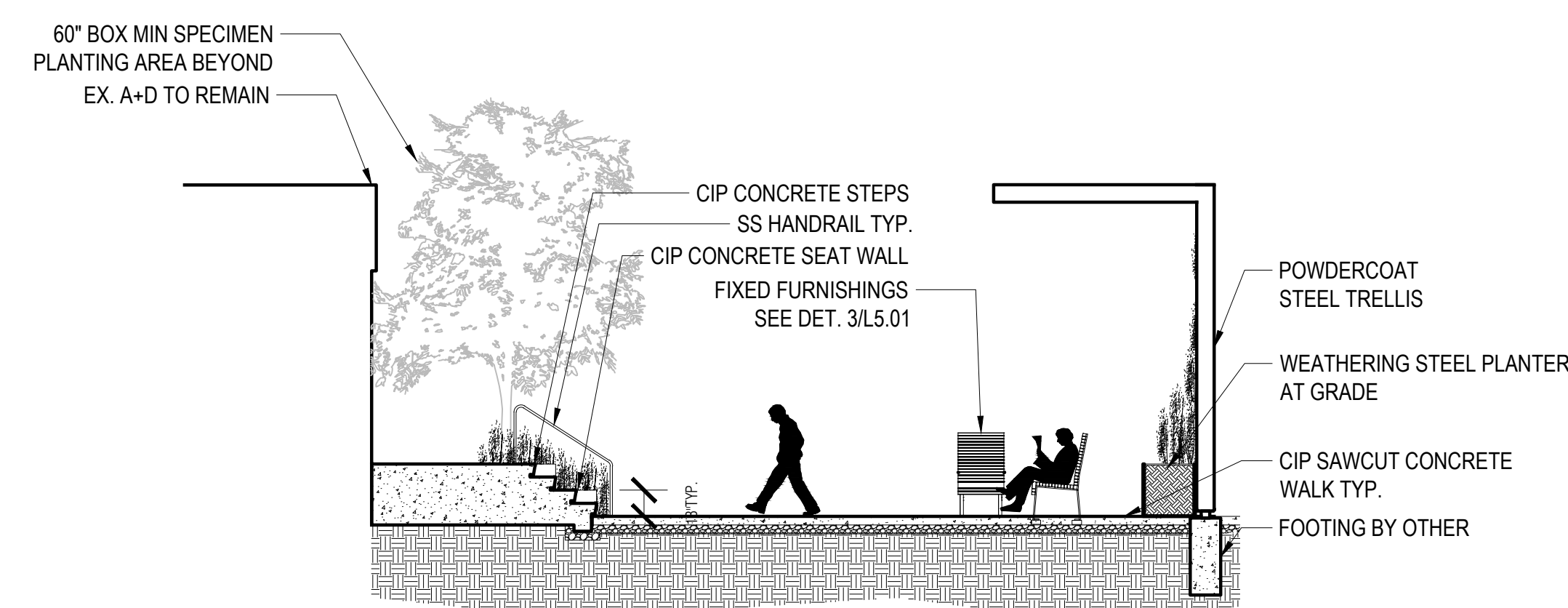
**EXHIBIT A**  
**CPC-2017-469-GPA-VZC-HD-MCUP-SPR**  
**October 12, 2023**



1 SITE SECTION  
L3.01



2 SITE SECTION  
L3.01



3 SITE SECTION  
L3.01

Seal / Signature

**NOT FOR  
CONSTRUCTION**

Project Name

401 S Hewitt

Project Number

05.1291.000

Description

SITE SECTIONS

Scale

1/8" = 1'-0"

**L3.01**

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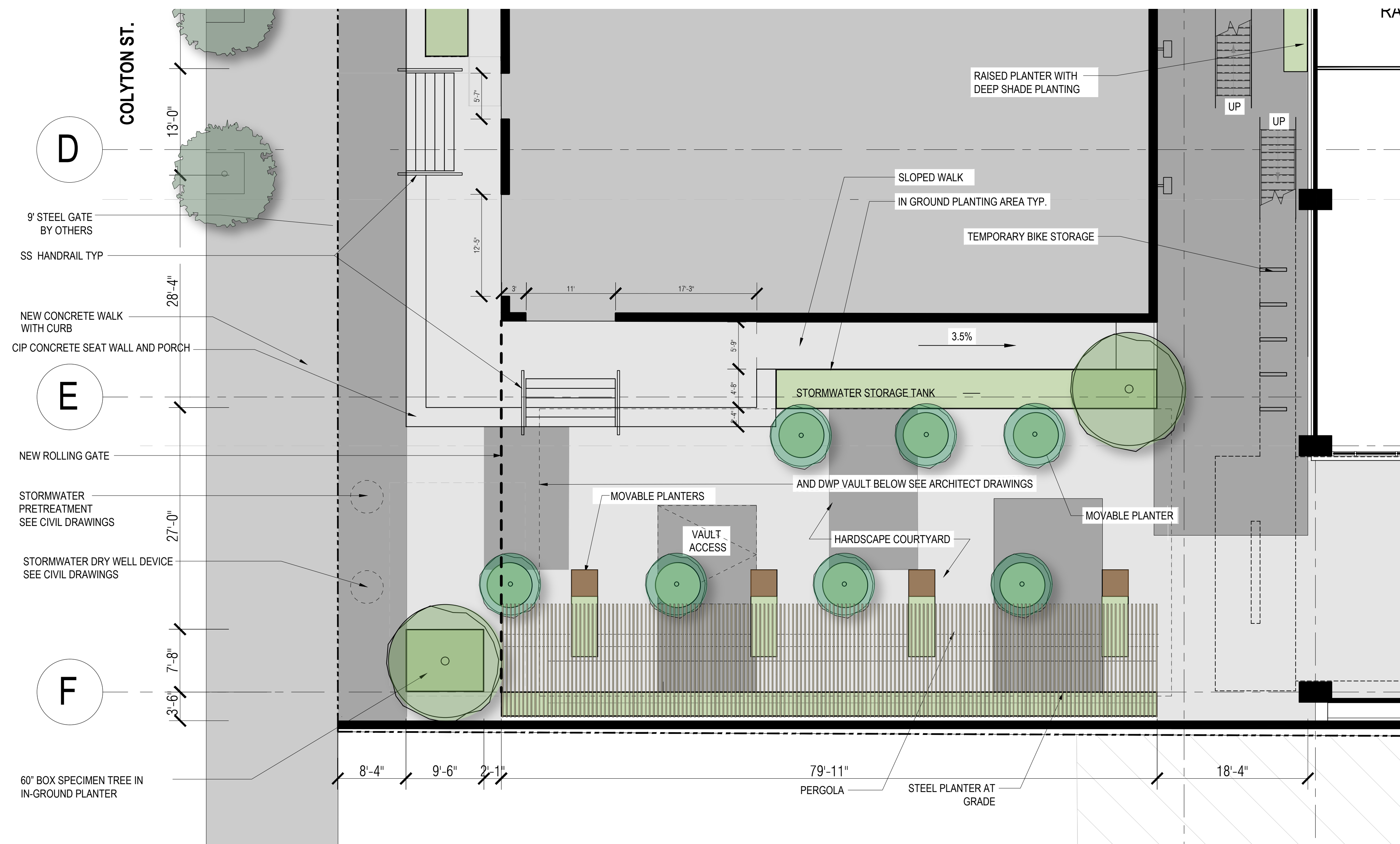
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**EXHIBIT A**  
**CPC-2017-469-GPA-VZC-HD-MCUP-SPR**  
**October 12, 2023**



**GROUND LEVEL PLANTING**



Western Redbud  
*Cercis occidentalis*



Creeping mountain lilac  
*Ceanothus 'Joyce Coulter'*  
Carmel Sur manzanita  
*Arctostaphylos 'Carmel Sur'*  
Island alum root  
*Heuchera maxima*



Agave species  
*Agave spp.*  
Platinum Beauty Lomandra  
*Lomandra longifolia 'Platinum beauty'*

**MATERIALS AND PLANTING LEGEND**

- CONCRETE PAVER
- PLANTING AREA
- CIP CONC. PAVING W/ SAWCUT PATTERN
- MOVEABLE PLANTERS
- 36" BOX STREET TREE  
SPECIES - *Afrocarpus gracilior*  
Fern pine  
TOTAL: 12
- 60" BOX SPECIMEN TREE  
SPECIES - *Cercis occidentalis*  
Western Redbud  
TOTAL: 3

Seal / Signature

**NOT FOR CONSTRUCTION**

Project Name

401 S Hewitt

Project Number

05.1291.000

Description

COURTYARD PLAN ENLARGEMENT

Scale

1/8" = 1'-0"

**L4.01**

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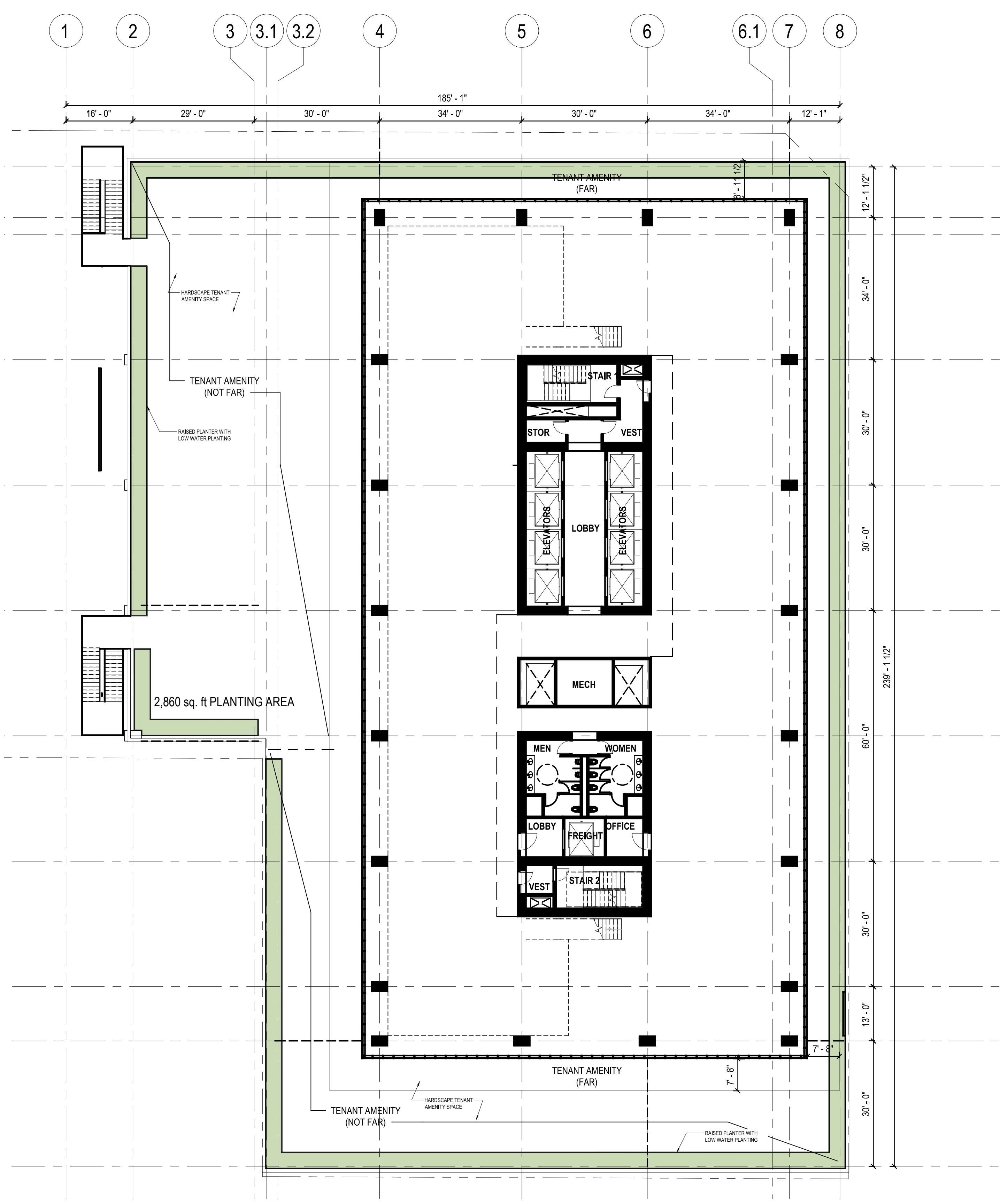


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1 LEVEL 6 EXTERIOR PLANTING  
L4.02

6th FLOOR PLANTING



Atlas fescue  
*Festuca mairei*



Seaside Buckwheat  
*Eriogonum latifolium*



purpletop vervain  
*Verbena bonariensis*



Black Sage  
*Salvia mellifera*



California fuchsia  
*Epilobium canum*

**EXHIBIT A**  
**CPC-2017-469-GPA-VZC-HD-MCUP-SPR**  
**October 12, 2023**

SUMMARY OF LANDSCAPED AREA	
LEVEL	AREA
GROUND LEVEL SHRUB & GROUND COVER	1,001
ROOF LEVEL 6 SHRUB & GROUND COVER	2860
ROOF LEVEL 17 SHRUB & GROUND COVER	2385
<b>PROJECT TOTAL</b>	<b>6,246</b>

Seal / Signature

**NOT FOR CONSTRUCTION**

Project Name  
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Project Number  
05.1291.000

Description  
AMENITY LEVEL PLANTING

Scale  
1/16" = 1'-0"

**L4.02**



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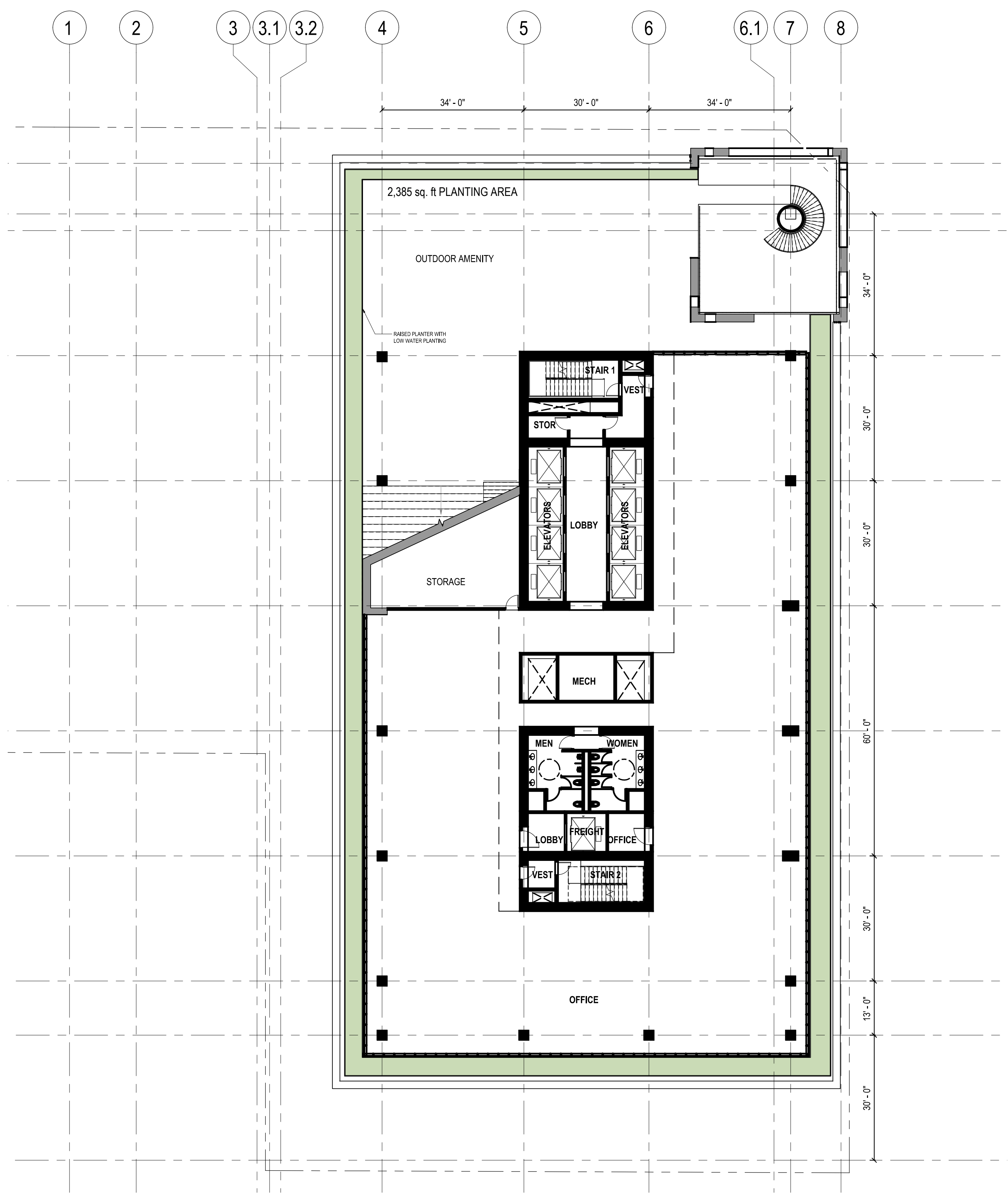


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SUMMARY OF LANDSCAPED AREA	
LEVEL	AREA
GROUND LEVEL SHRUB & GROUNDCOVER	1,001
ROOF LEVEL 6 SHRUB & GROUNDCOVER	2860
ROOF LEVEL 17 SHRUB & GROUNDCOVER	2385
<b>PROJECT TOTAL</b>	<b>6,246</b>



Coral Fountain  
*Russelia equisetiformis*



Rosemary  
*rosemary officinalis*



Black Sage  
*Salvia mellifera*



Cousin Itt Acacia  
*Acacia cognata 'Cousin Itt'*



Dwarf Coast Rosemary  
*Westringia fruticosa Grey Box*

1 LEVEL 17 EXTERIOR PLANTING  
L4.03

Seal / Signature

**NOT FOR  
CONSTRUCTION**

Project Name  
401 S Hewitt

Project Number  
05.1291.000

Description  
AMENITY LEVEL PLANTING

Scale  
1/16" = 1'-0"

**L4.03**

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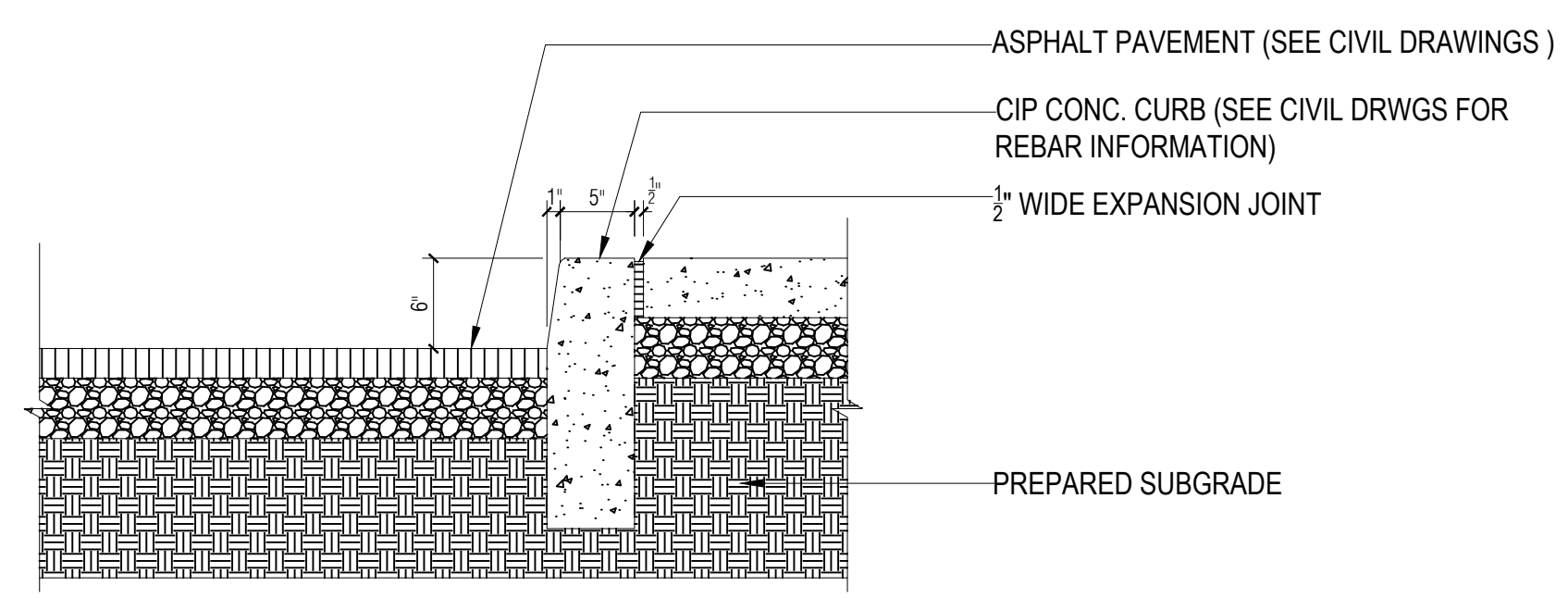
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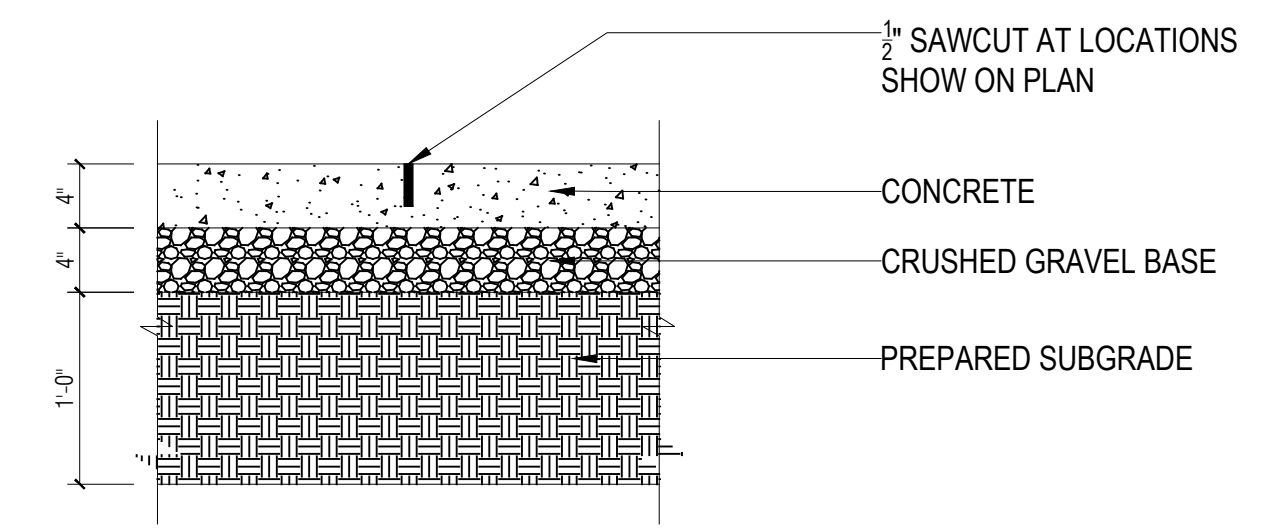
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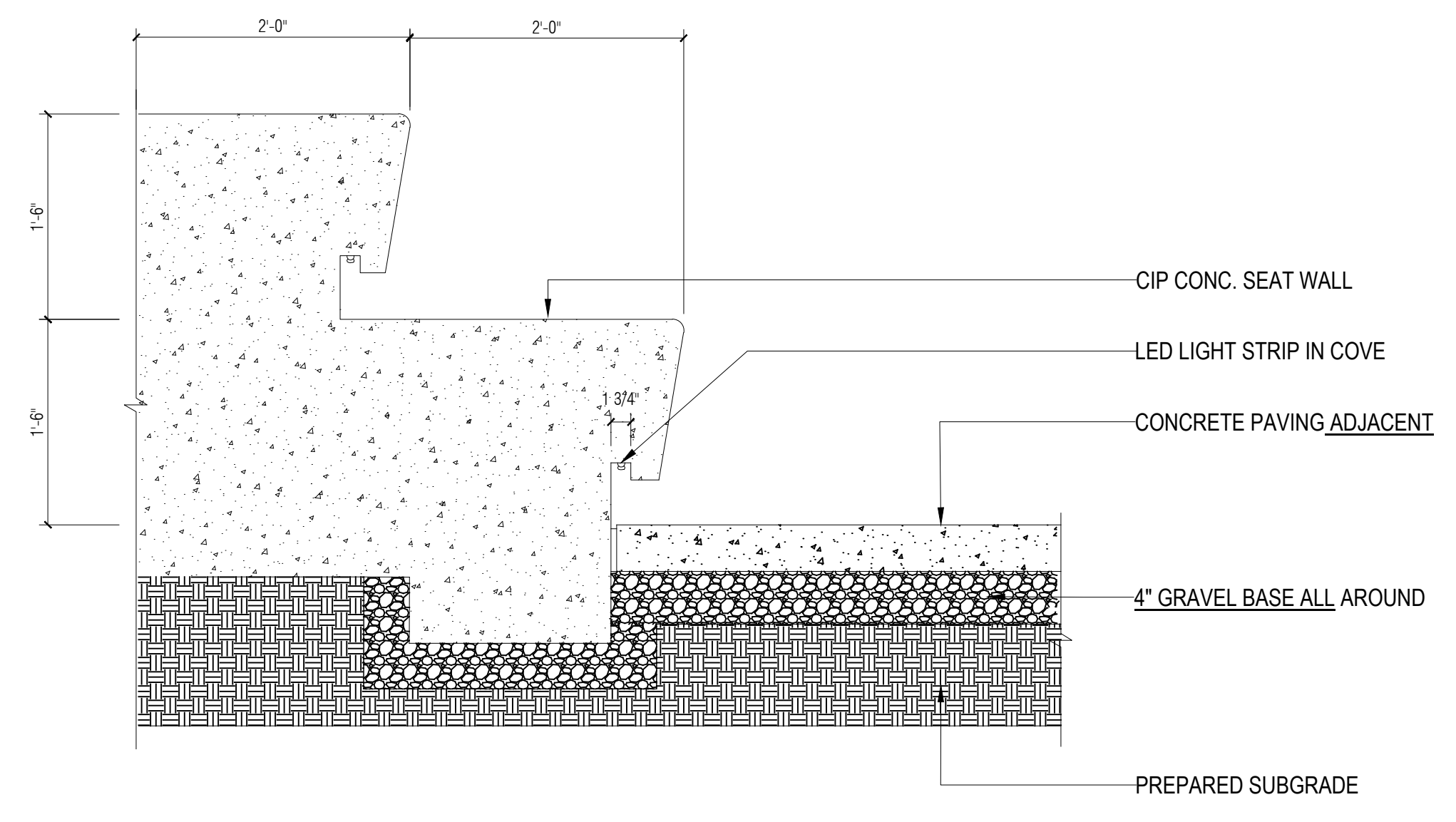
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1 CONC. CURB AT ASPHALT  
L5.01 SCALE: 1"=1'-0"

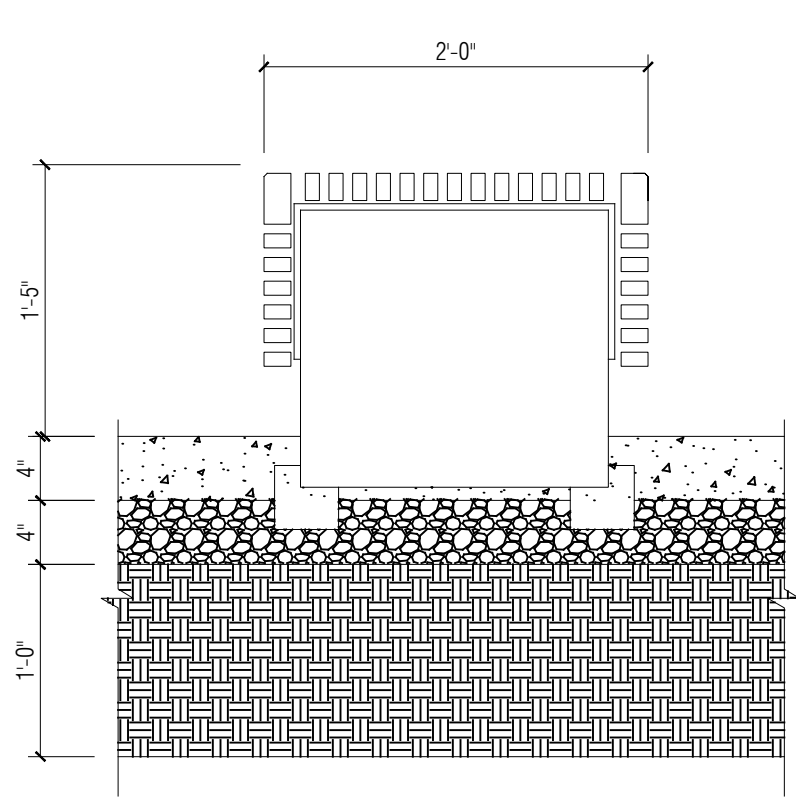


2 CONC. PAVING 4" THICK  
L5.01 SCALE: 1"=1'-0"

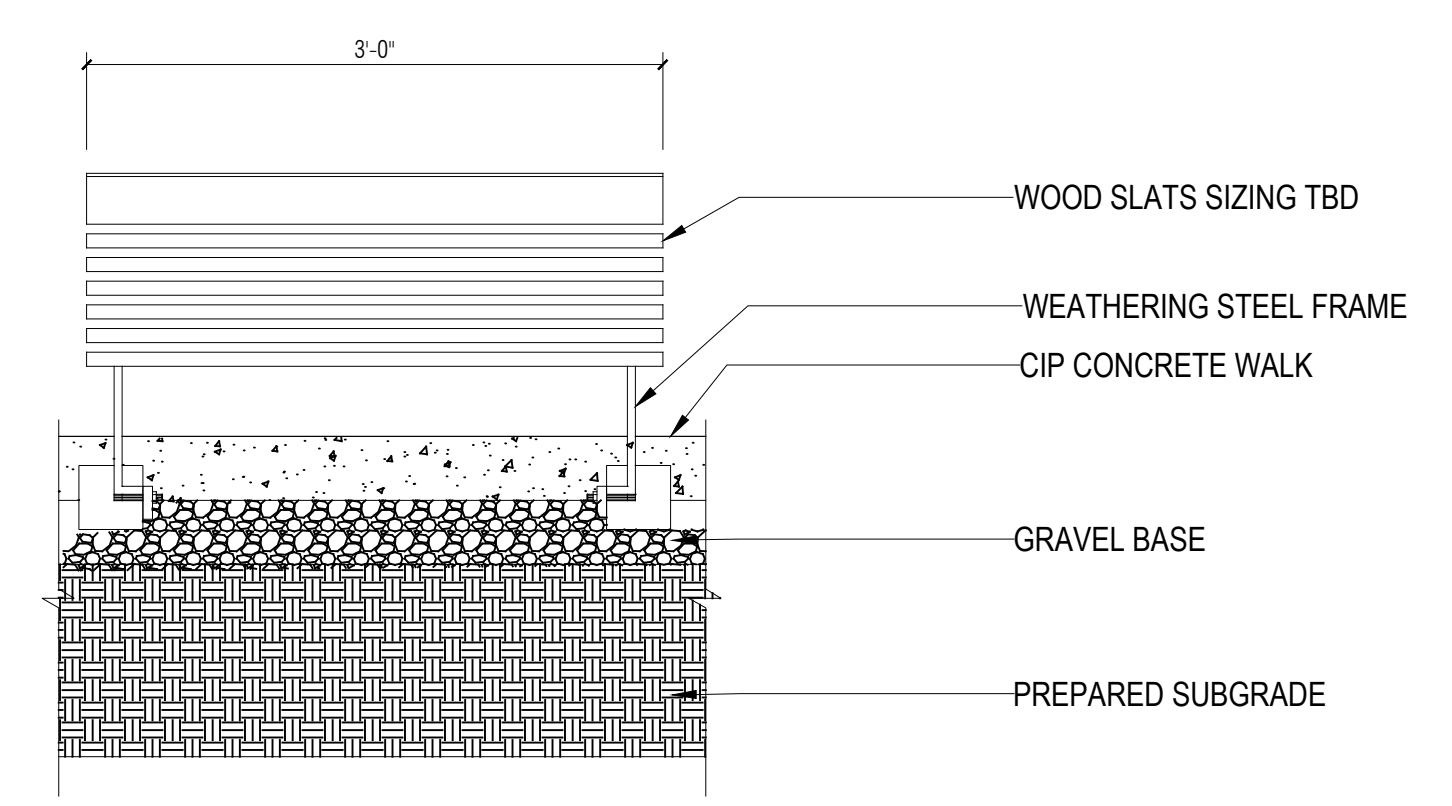


4 CIP SEATWALL AT PAVING  
L5.01 SCALE: 1"=1'-0"

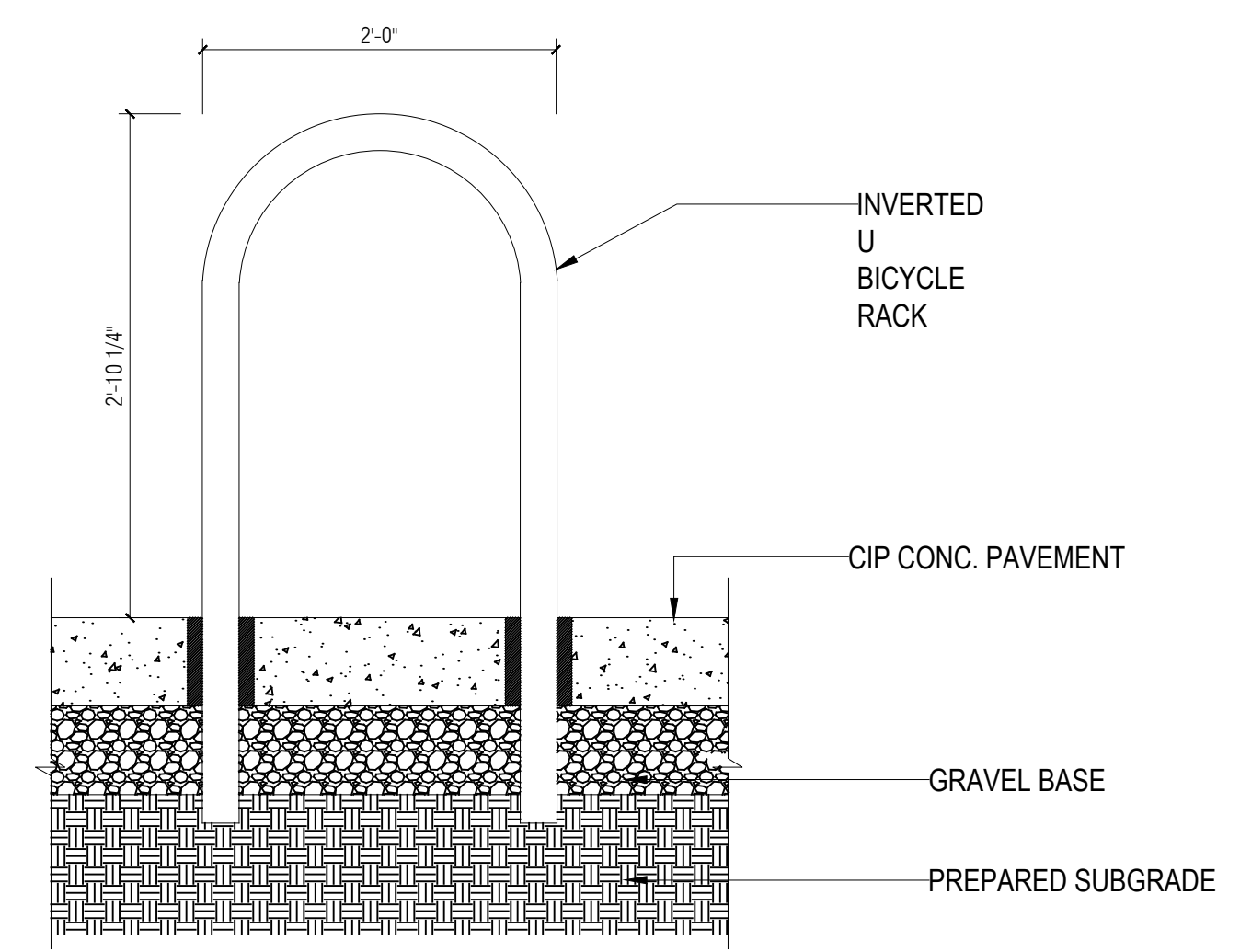
**EXHIBIT A**  
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**October 12, 2023**



5 STREET BENCH DETAIL  
L5.01 SCALE: 1"=1'-0"



6 TYP. BIKE RACK SECTIONS  
L5.01 SCALE: 1"=1'-0"



Seal / Signature

**NOT FOR CONSTRUCTION**

Project Name

401 S Hewitt

Project Number

05.1291.000

Description

DETAILS

Scale

REFER TO DETAIL FOR SCALE

**L5.01**

**Gensler**

500 South Figueroa Street  
Los Angeles, California 90071  
United States  
Tel 213.327.3600  
Fax 213.327.3601

WALTER P MOORE



707 Wilshire Blvd., Suite 2100  
Los Angeles, CA 90017  
Tel 213.292.6500  
Fax 213.292.6600

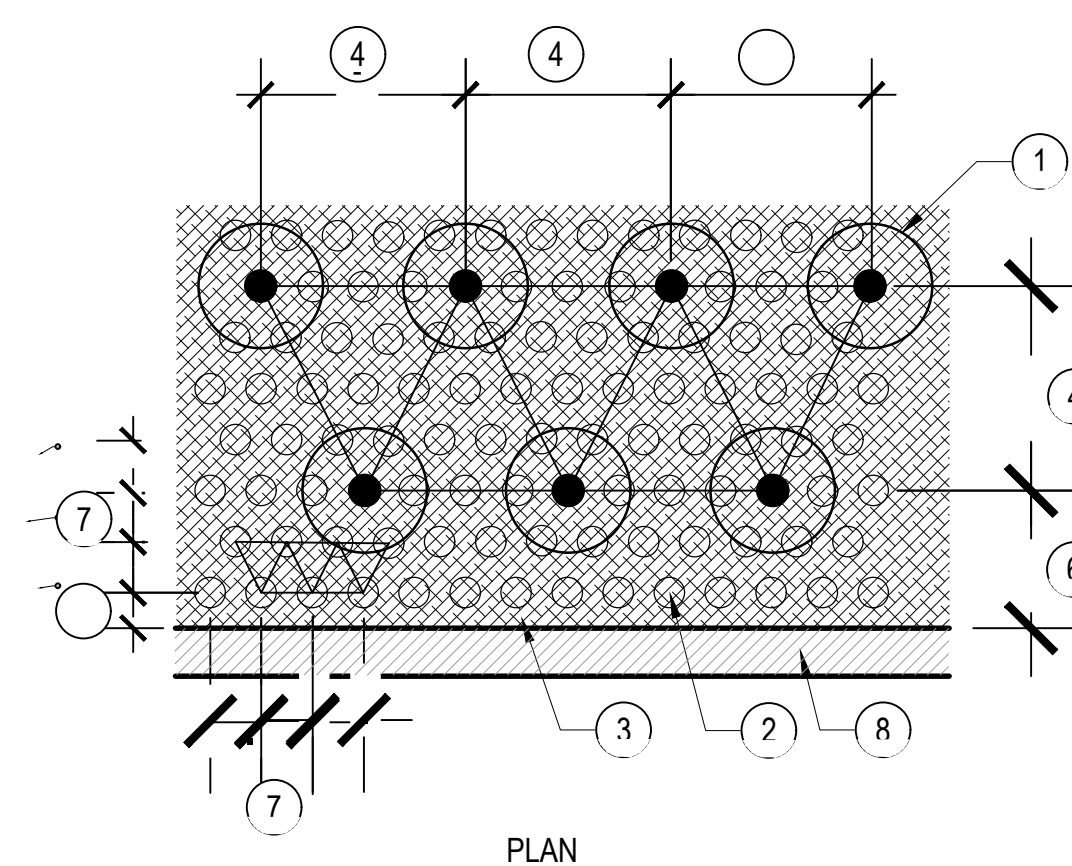
277 S. Lake Street  
Burbank, CA 91502  
Tel 818.508.6300  
Fax 818.508.7050



555 Flower St., Suite 4300  
Los Angeles, CA 90071  
Tel 213.223.1400



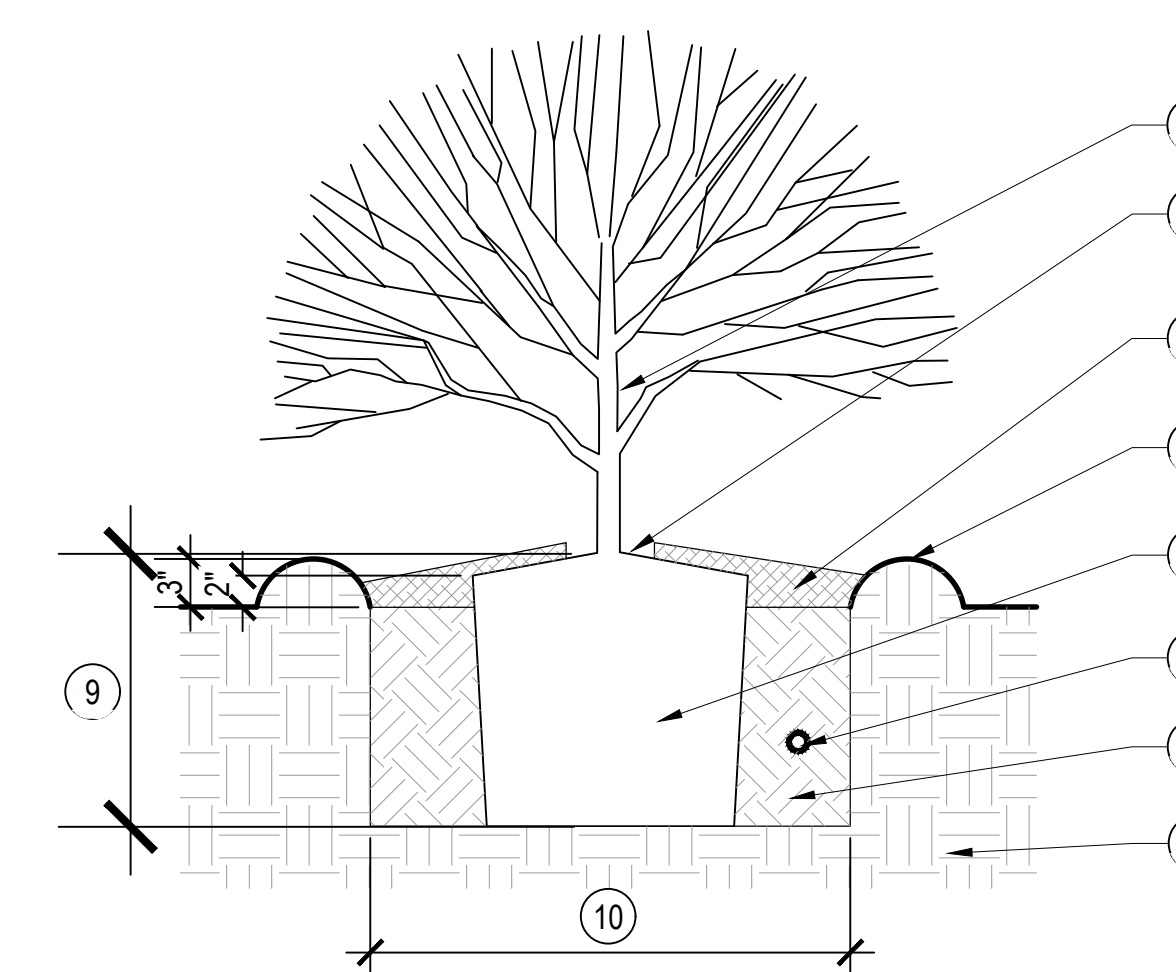
423 Gin Lin Way  
Los Angeles, CA 90012  
Tel 213.234.0057



- LEGEND**
1. SHRUB-TRIANGULATED ROWS.
  2. GROUNDCOVER-TRIANGULATED ROWS.
  3. MULCH.
  4. O.C. SPACING FROM PLANT LEG.
  5. 1/2 SPECIFIED SPACING DISTANCE FROM PLANT LEGEND.
  6. 3/4 SPECIFIED SPACING DISTANCE FROM PLANT LEGEND.
  7. SPACE G.C. PER PLANT LEGEND
  8. HARDSCAPE ELEMENT-CURB, SIDEWALK, WALL, ETC.

- NOTES**
1. SPACE SHRUB AND GROUNDCOVER PER PLANT LEGEND.

**1 SHRUB AND GROUNDCOVER SPACING**  
SCALE: 1"=1'-0"



- LEGEND**
1. SHRUB.
  2. HOLD MULCH BACK FROM CROWN OF PLANT.
  3. MULCH 3" DEPTH.
  4. WATER BASIN: 3" HT.
  5. ROOTBALL: TOP 2" ABOVE FINISHED GRADE.
  6. FERTILIZER TABLET
  7. AMENDED BACKFILL REFER TO AGRONOMY REPORT.
  8. SITE SOIL
  9. ROOTBALL DEPTH
  10. 2X ROOTBALL WIDTH.

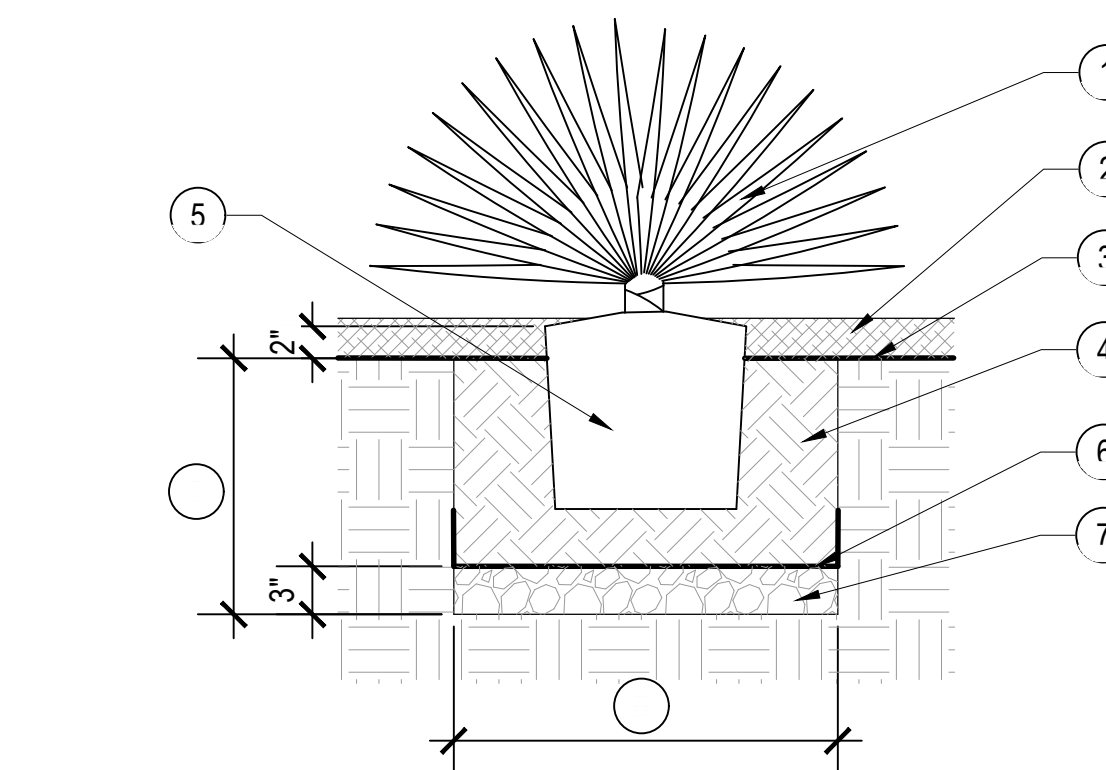
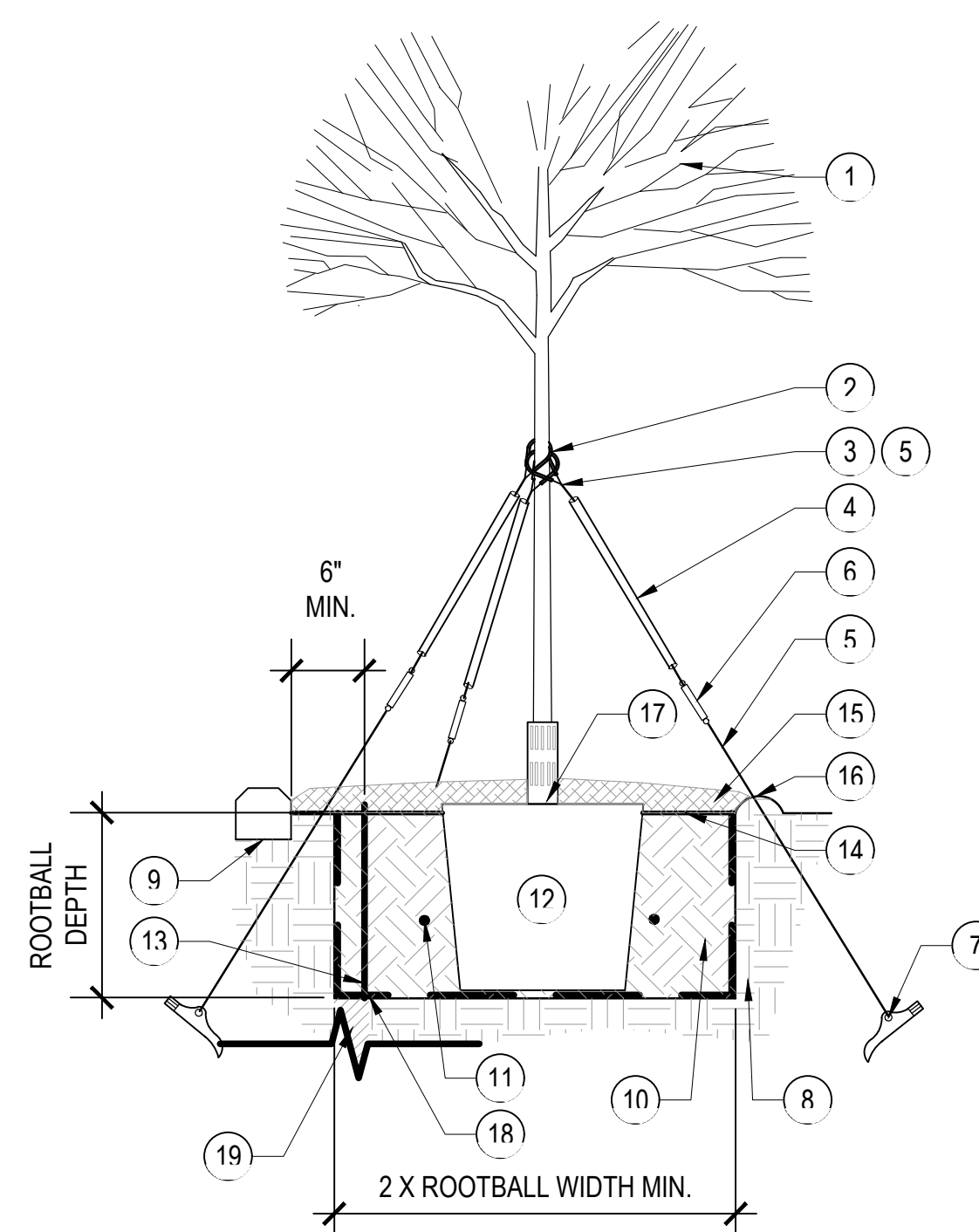
**3 SHRUB PLANTING**  
SCALE: 1"=1'-0"

**EXHIBIT A**  
**CPC-2017-469-GPA-VZC-HD-MCUP-SPR**  
**October 12, 2023**

- LEGEND**
1. TREE.
  2. 21" LONG PVC TUBING (COVERING CABLE).
  3. (2) 1/8" CABLE CLAMPS.
  4. 1/2" DIAMETER WHITE PLASTIC TUBING.
  5. 1/8" GALVANIZED AIRCRAFT CABLE.
  6. 3/8" X 3" ZINC-PLATED TURNBUCKLE.
  7. 'DUCKBILL' EARTH ANCHOR.
  8. SITE SOIL.
  9. EDGE OF LANDSCAPE AREA. CONCRETE CURB OR MOW CURB.
  10. AMENDED BACKFILL PER AGRONOMIC SOILS REPORT.
  11. FERTILIZER TABLET. SEE SPECIFICATIONS.
  12. ROOTBALL: SET TOP OF ROOTBALL 2" ABOVE FINISH GRADE.
  13. LINEAR ROOT CONTROL BARRIER. INSTALL PER MANUFACTURER'S RECOMMENDATIONS.
  14. FINISH GRADE.
  15. 3" MULCH. SEE SPECIFICATIONS.
  16. 3" HIGH WATER BASIN. REMOVE AT END OF MAINTENANCE PERIOD.
  17. TREE TRUNK PROTECTOR (ON TREES IN TURF).
  18. FILTER FABRIC ABOVE SUMP.
  19. 12" DIA. X 6" DEEP SAND FILLED SUMP.

- NOTES**
1. MINIMUM OF 3 GUYS PER TREE. AGENCY / OWNER REPRESENTATIVE MAY REQUIRE MORE DEPENDING ON SITUATION.

**10 TREE GUYING WITH ANCHORS - ON GRADE**  
SCALE: NTS



- LEGEND**
1. SUCCULENT.
  2. 3" LAYER OF MULCH.
  3. FINISH GRADE.
  4. AMENDED BACKFILL. REFER TO AGRONOMY SOILS REPORT.
  5. ROOTBALL: 2" ABOVE FINISHED GRADE
  6. FILTER FABRIC.
  7. 3" LAYER CRUSHED ROCK
  8. 2X ROOTBALL WIDTH
  9. 1.5 X ROOTBALL DEPTH.

**2 SUCCULENT SHRUB PLANTING**  
SCALE: 1"=1'-0"

Seal / Signature

**NOT FOR  
CONSTRUCTION**

Project Name  
401 S Hewitt

Project Number  
05.1291.000

Description  
DETAILS

Scale  
REFER TO DETAIL FOR SCALE

**L5.02**



VIEW OF WEST FACADE OF BUILDING



VIEW OF BUILDING FROM NORTHEAST CORNER OF HEWITT AND 4TH STREET



VIEW OF HEWITT STREET FRONTAGE

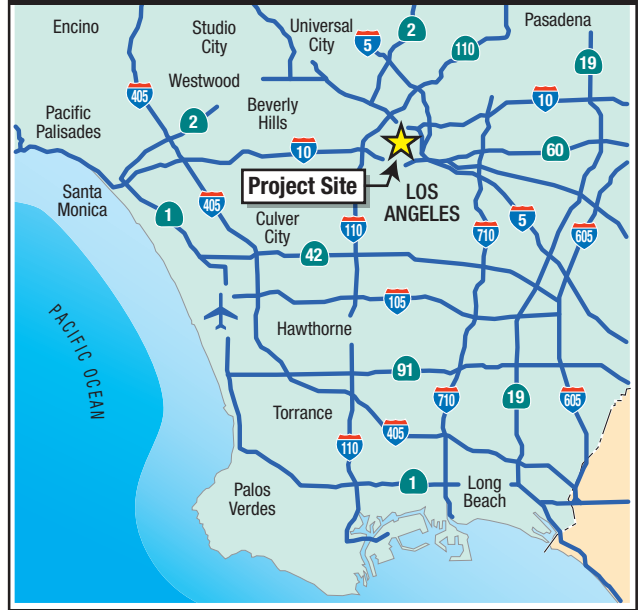


VIEW OF EAST FACADE OF BUILDING



VIEW OF COURTYARD

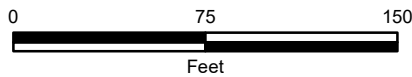
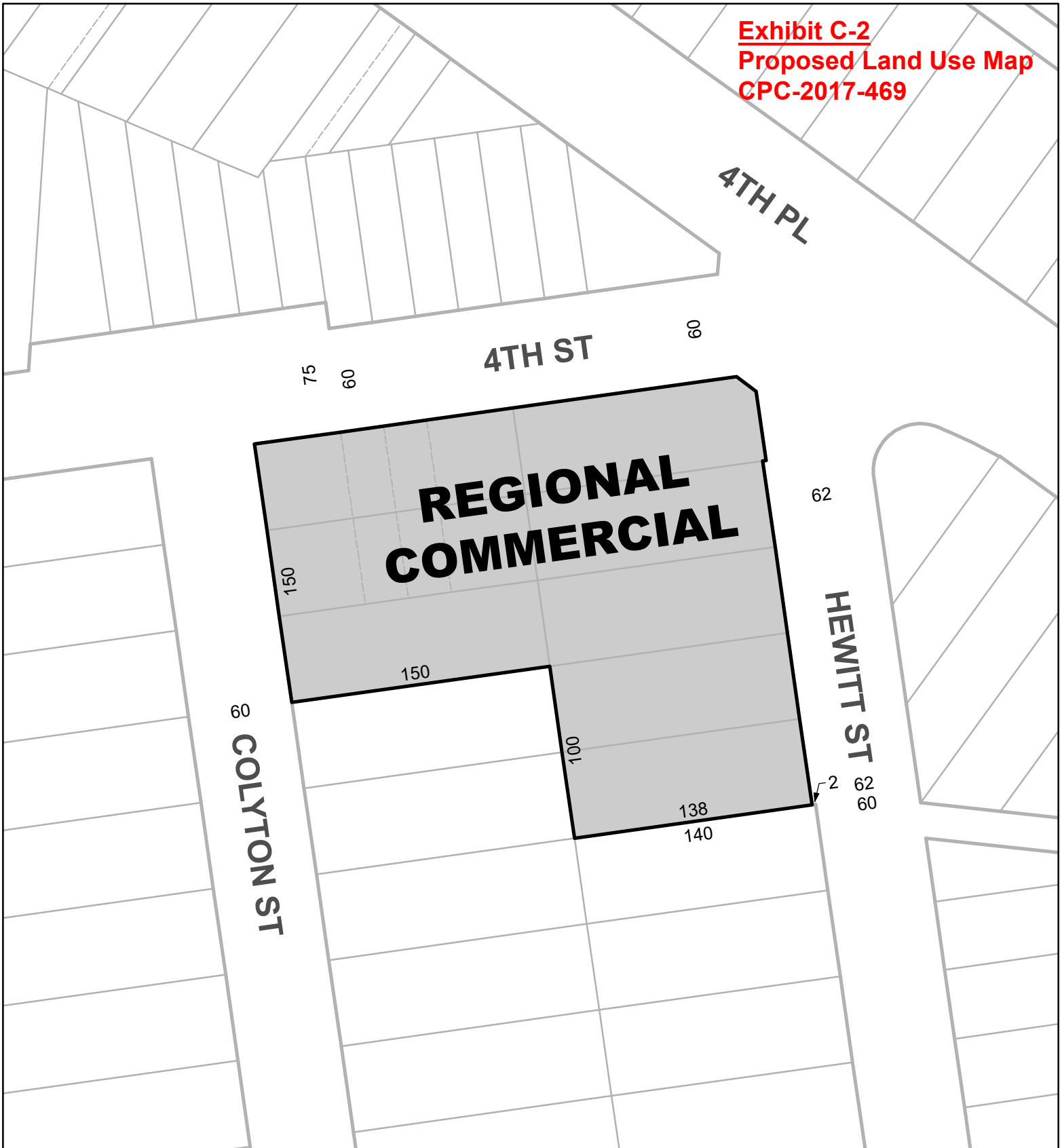
# Regional Map



# Exhibit C-1 Vicinity Map CPC-2017-469



**Exhibit C-2**  
**Proposed Land Use Map**  
**CPC-2017-469**



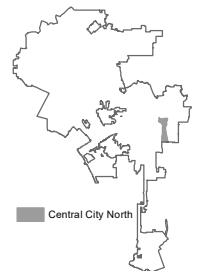
CPC-2017-469-GPA-VZC-HD-MCUP-SPR

AA/cf

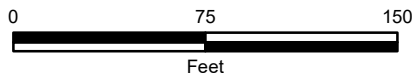
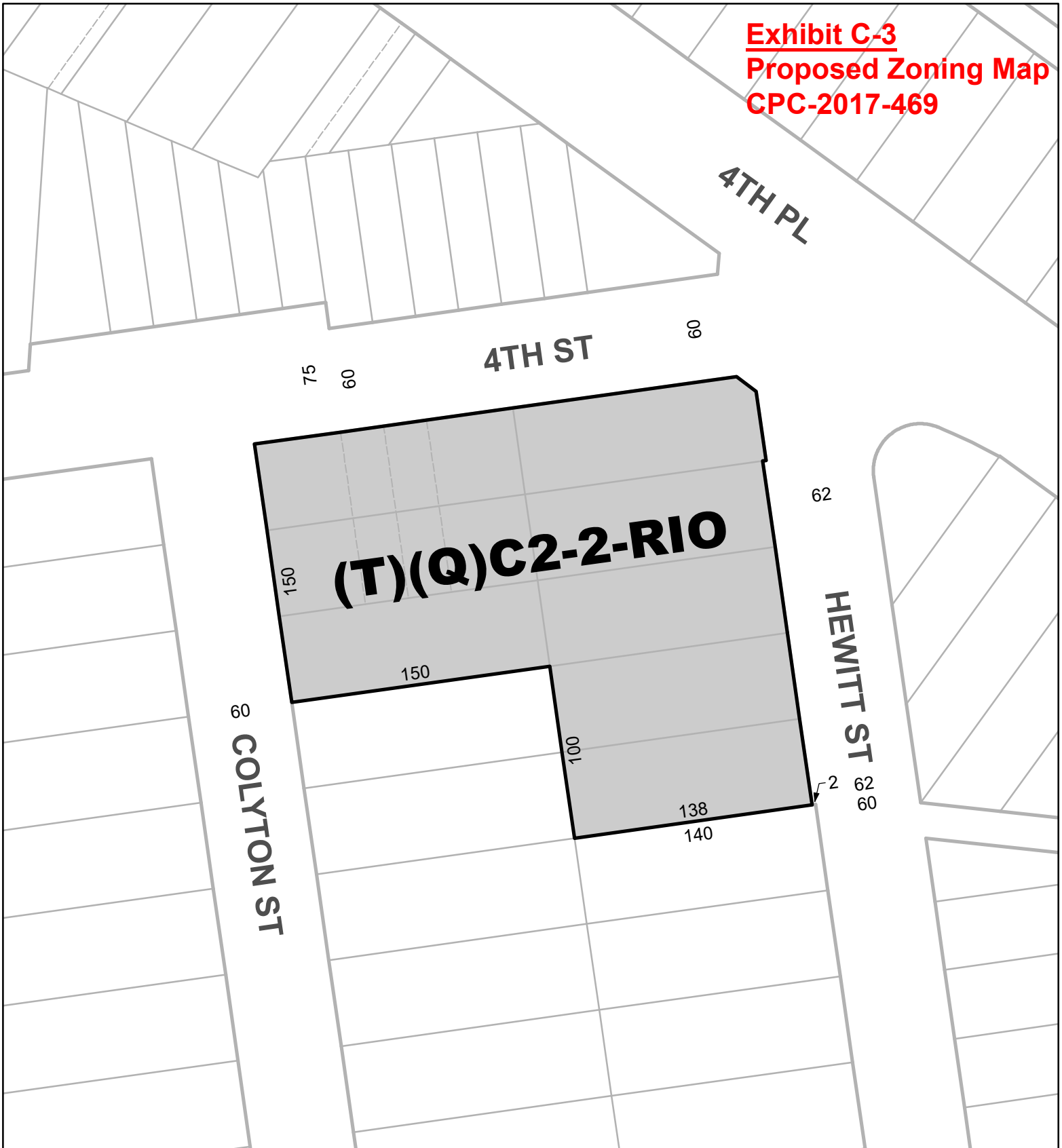
092923

CENTRAL CITY NORTH

City of Los Angeles

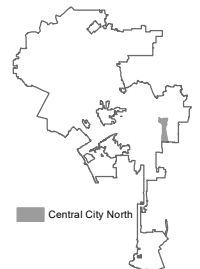


**Exhibit C-3**  
**Proposed Zoning Map**  
**CPC-2017-469**



CPC-2017-469-GPA-VZC-HD-MCUP-SPR  
AA/cf 092923

City of Los Angeles



## **IV. Mitigation Monitoring Program**

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### **1. Introduction**

This Mitigation Monitoring Program (MMP) has been prepared pursuant to Section 21081.6 of the Public Resources Code (PRC), which requires a lead agency to adopt a “reporting or monitoring program for changes to the project or conditions of project approval, adopted in order to mitigate or avoid significant effects on the environment.” In addition, Section 15097(a) of the California Environmental Quality Act (CEQA) Guidelines requires that a public agency adopt a program for monitoring or reporting mitigation measures and project revisions, which it has required to mitigate or avoid significant environmental effects. This MMP has been prepared in compliance with the requirements of CEQA, Section 21081.6 of the PRC, and Section 15097 of the CEQA Guidelines.

The City of Los Angeles (City) is the Lead Agency for the 4<sup>th</sup> and Hewitt Project (Project) and therefore is responsible for administering and implementing the MMP. A public agency may delegate reporting or monitoring responsibilities to another public agency or to a private entity that accepts the delegation; however, until mitigation measures have been completed, the Lead Agency remains responsible for ensuring that implementation of the mitigation measures occurs in accordance with the program.

An Environmental Impact Report (EIR) has been prepared to address the potential environmental impacts of the Project. The evaluation of the Project’s impacts in the EIR takes into consideration the project design features (PDF) and applies mitigation measures (MM) needed to avoid or reduce potentially significant environmental impacts. This MMP is designed to monitor implementation of the PDFs and MMs identified for the Project.

### **2. Purpose**

It is the intent of this MMP to provide a record of the project design features and mitigation measures that are required of the Project; identify the responsible enforcement and monitoring agencies; establish the phase, frequency, and duration of monitoring; and convey the manner by which the Project is required to achieve compliance and the materials that document compliance for the record.



### 3. Organization

As shown in this MMP, each identified project design feature and mitigation measure for the Project is listed and categorized by environmental impact area, with accompanying identification of the following:

- **Enforcement Agency:** the agency with the power to enforce the project design feature or mitigation measure.
- **Monitoring Agency:** the agency to which reports involving feasibility, compliance, implementation, and development are made.
- **Monitoring Phase:** the phase of the Project during which the project design feature or mitigation measure shall be monitored.
- **Monitoring Frequency:** the frequency at which the project design feature or mitigation measure shall be monitored.
- **Action(s) Indicating Compliance:** the action(s) by which the Enforcement or Monitoring Agency indicates that compliance with the identified project design feature or mitigation measure has been implemented.

### 4. Administrative Procedures and Enforcement

This MMP shall be enforced throughout all phases of the Project. The Applicant shall be responsible for implementing each project design feature and mitigation measure and shall be obligated to provide certification, as identified below, to the appropriate monitoring and enforcement agencies that each project design feature and mitigation measure has been implemented. The Applicant shall maintain records demonstrating compliance with each project design feature and mitigation measure. Such records shall be made available to the City upon request.

During the construction phase and prior to the issuance of building permits, the Applicant shall retain an independent Construction Monitor (either via the City or through a third-party consultant), approved by the Department of City Planning, who shall be responsible for monitoring implementation of project design features and mitigation measures during construction activities consistent with the monitoring phase and frequency set forth in this MMP.

The Construction Monitor shall also prepare documentation of the Applicant's compliance with the project design features and mitigation measures during construction every 90 days in a form satisfactory to the Department of City Planning. The documentation must be signed by the Applicant and Construction Monitor and be included as part of the

Applicant's Compliance Report. The Construction Monitor shall be obligated to report to the Enforcement Agency of any non-compliance with the project design features and mitigation measures within two businesses days if the Applicant does not correct the non-compliance within a reasonable time of notification to the Applicant by the monitor or if the non-compliance is repeated. Such non-compliance shall be appropriately addressed by the Enforcement Agency.

## **5. Program Modification**

After review and approval of the final MMP by the Lead Agency, minor changes and modifications to the MMP are permitted, but can only be made subject to City approval. The Lead Agency, in conjunction with any appropriate agencies or departments, will determine the adequacy of any proposed change or modification. This flexibility is necessary in light of the nature of the MMP and the need to protect the environment. No changes will be permitted unless the MMP continues to satisfy the requirements of CEQA, as determined by the Lead Agency.

The Project shall be in substantial conformance with the project design features and mitigation measures contained in this MMP. The enforcing departments or agencies may determine substantial conformance with project design features and mitigation measures in the MMP in their reasonable discretion. If the department or agency cannot find substantial conformance, a project design feature and mitigation measure may be modified or deleted as follows: the enforcing department or agency, or the decision maker for a subsequent discretionary project related approval finds that the modification or deletion complies with CEQA, including CEQA Guidelines, Sections 15162 and 15164, which could include the preparation of an addendum or subsequent environmental clearance, if necessary, to analyze the impacts from the modifications to or deletion of the project design features and mitigation measures. Any addendum or subsequent CEQA clearance shall explain why the project design feature or mitigation measure is no longer needed, not feasible, or the other basis for modifying or deleting the project design feature or mitigation measure, and that the modification will not result in a new significant impact consistent with the requirements of CEQA. Under this process, the modification or deletion of a project design feature or mitigation measure shall not, in and of itself, require a modification to any Project discretionary approval unless the Director of Planning also finds that the change to the project design feature or mitigation measure results in a substantial change to the Project or the non-environmental conditions of approval.

## 6. Mitigation Monitoring Program

### a) Air Quality

#### (1) Project Design Features

**AQ-PDF-1:** The Applicant will make a reasonable effort to attain diesel-powered equipment that will meet United States Environmental Protection Agency Tier 4 Final emission reduction technology for nonroad diesel engines to utilize during the construction period.

- **Enforcement Agency:** Los Angeles Department of Building and Safety; South Coast Air Quality Management District
- **Monitoring Agency:** Los Angeles Department of City Planning; or Los Angeles Department of Building and Safety
- **Monitoring Phase:** Construction
- **Monitoring Frequency:** Once during Project plan check (requiring proof of compliance); Periodically during field inspection
- **Action Indicating Compliance:** Project plan approval; Issuance of applicable building permit (requiring proof of compliance); Field inspection sign-off

### b) Cultural Resources

#### (1) Mitigation Measures

**CUL-MM-1 Archaeological Resource Monitoring.** Prior to the issuance of a demolition permit, the Applicant or its Successor shall retain a Qualified Archaeologist who meets the Secretary of the Interior's Professional Qualifications Standards (Qualified Archaeologist) to oversee an archaeological monitor who shall be present during construction activities on the Project Site such as demolition, clearing/grubbing, grading, trenching, or any other construction excavation activity associated with the Project. The activities to be monitored shall also include off-site improvements in the vicinity of the Project Site, such as utility, sidewalk, or road improvements. The monitor shall have the authority to direct the pace of construction equipment in areas of high sensitivity. The frequency of monitoring shall be based on the rate of excavation and grading activities, the materials being excavated (younger sediments vs. older sediments), and the depth of excavation, and if found, the abundance and type of

archaeological resources encountered. Full-time monitoring may be reduced to part-time inspections, or ceased entirely, if determined adequate by the Qualified Archaeologist. Prior to commencement of excavation activities, an archaeological Sensitivity Training shall be carried out by the Qualified Archaeologist, focusing on how to identify archaeological resources that may be encountered during earthmoving activities and the procedures to be followed in such an event.

- **Enforcement Agency:** Los Angeles Department of City Planning; Los Angeles Department of Building and Safety
- **Monitoring Agency:** Los Angeles Department of Building and Safety
- **Monitoring Phase:** Pre-Construction; Construction
- **Monitoring Frequency:** To be determined by consultation with the Qualified Archaeologist if resources are discovered
- **Action Indicating Compliance:** Field inspection sign-off

**CUL-MM-2 Archaeological Resource Discovery.** In the event that historic or prehistoric archaeological resources are unearthed, ground-disturbing activities shall be halted or diverted away from the vicinity of the find so that the find can be evaluated. A 50-foot buffer shall be established by the Qualified Archaeologist around the find where construction activities shall not be allowed to continue. Work shall be allowed to continue outside of the buffer area. All archaeological resources unearthed by Project construction activities shall be evaluated by the Qualified Archaeologist. If a resource is determined by the Qualified Archaeologist to constitute a “historical resource” pursuant to California Environmental Quality Act (CEQA) Guidelines Section 15064.5 (a) or a “unique archaeological resource” pursuant to Public Resources Code (PRC) Section 21083.2 (g), the Qualified Archaeologist shall coordinate with the Applicant and the Department of City Planning to develop a formal treatment plan that would serve to reduce impacts to the resources. If any prehistoric archaeological sites are encountered within the Project area, consultation with interested Native American parties shall be conducted to apprise them of any such findings and solicit any comments they may have regarding appropriate treatment and disposition of the resources. The treatment plan established for the resources shall be in accordance with State CEQA Guidelines Section 15064.5(f) for historical resources and PRC Section 21083.2(b) for unique archaeological resources. As noted in California Code of

Regulations Section 15126.4(b)(A), preservation in place (i.e., avoidance) is the preferred manner of treatment. If, in coordination with the City's Office of Historic Resources and with final approval by the Department of City Planning, it is determined that preservation in place is not feasible, appropriate treatment of the resources shall be developed by the Qualified Archaeologist and may include implementation of archaeological data recovery excavations to remove the resource along with subsequent laboratory processing analysis. Any archaeological material collected shall be curated at a public, non-profit institution with a research interest in the materials, if such an institution agrees to accept the material. If no institution accepts the archaeological materials, they shall be donated to a local school or historical society in the area for educational purposes.

- **Zanja Conduit System Discovery.** In the event that Zanja Conduit System-related infrastructure is unearthed, ground-disturbing activities shall be halted or diverted away from the vicinity of the find so that the find can be evaluated. An appropriate exclusion area that accounts for the linear nature of the resource shall be established by a Qualified Archaeologist, meeting the Secretary of the Interior Standards in Archaeology. Construction activities shall not be allowed to continue within the exclusion area until directed by the Qualified Archaeologist in consultation with the Department of City Planning, but work shall be allowed to continue outside of the exclusion area. The Qualified Archaeologist shall coordinate with the Applicant or its Successor, the Department of City Planning, and the City's Office of Historic Resources (OHR) to develop a formal treatment plan for the resource that would serve to mitigate impacts to the resource(s). The treatment measures listed in California Code of Regulations Section 15126.4(b) shall be considered when determining appropriate treatment for the Zanja resource. Treatment shall be designed to address the Zanja resource's eligibility under Criterion 1 (significant events) and 4 (scientific data) as well as eligibility as a unique archaeological resource of the likely form of the Zanja, to the best of current knowledge (e.g., is it assumed to be made of wood/concrete/earthen etc., based on known archival research) and may include implementation of data recovery excavations to remove the resource along with subsequent laboratory processing and analysis. At a minimum, a commemoration program that includes the development of an interpretive exhibit/display/signage or plaque at the Project Site shall be developed. In addition, other public educational and/or

interpretive treatment measures shall be developed as determined appropriate by the Qualified Archaeologist in consultation with the OHR. Any associated artifacts collected that are not made part of the interpretation/education collection shall be curated or donated as specified above (see “Archaeological Resource Discovery”).

- **Enforcement Agency:** Los Angeles Department of City Planning; Los Angeles Department of Building and Safety
- **Monitoring Agency:** Los Angeles Department of Building and Safety
- **Monitoring Phase:** Construction
- **Monitoring Frequency:** To be determined by consultation with the Qualified Archaeologist if resources are discovered
- **Action Indicating Compliance:** Field inspection sign-off

**CUL-MM-3 Archaeological Resource Documentation.** Following the conclusion of archaeological monitoring but prior to the release of the grading bond, the Qualified Archaeologist shall prepare a final report and complete the appropriate California Department of Parks and Recreation Site Forms. The report shall include a description of archaeological resources unearthed (Zanja-related or other archaeological resources), if any; treatment of the resources; results of the artifact processing, analysis, research; and an evaluation of the resources with respect to the California Register and the California Environmental Quality Act. The report and the Site Forms shall be submitted by the Project Applicant or its Successor to the Department of City Planning, the South Central Coastal Information Center, and representatives of other appropriate or concerned agencies to signify the satisfactory completion of the development and required mitigation measures.

- **Enforcement Agency:** Los Angeles Department of City Planning
- **Monitoring Agency:** Los Angeles Department of City Planning
- **Monitoring Phase:** Pre-Construction; Construction
- **Monitoring Frequency:** Once at Project plan check
- **Action Indicating Compliance:** Project plan approval; Issuance of building permit

## c) Hazardous and Hazardous Materials

### (1) Mitigation Measures

**HAZ-MM-1** Following demolition of on-site structures and prior to redevelopment of the Project Site, the Applicant shall retain a qualified environmental professional to perform a Supplemental Phase II Subsurface Site Investigation. The Supplemental Phase II Subsurface Site Investigation shall focus on soils in those areas that were identified as inaccessible during the Phase II Subsurface Site Investigation: the areas of the on-site wastewater clarifier, auto repair floor pit, and wastewater separator structures. In addition, due to the low level of petroleum hydrocarbons reported at B2 at 10 feet below ground surface (bgs), the Supplemental Phase II Subsurface Site Investigation shall also include the area of the former truck wash rack. In the event that soils contaminated by petroleum products or other hazardous chemicals are encountered during the investigation, a qualified environmental professional shall be retained to oversee the proper characterization and disposal of waste and remediation of impacted soil and/or materials, as necessary.

- **Enforcement Agency:** Los Angeles Department of City Planning; Los Angeles Department of Building and Safety
- **Monitoring Agency:** Los Angeles Department of Building and Safety
- **Monitoring Phase:** Pre-construction
- **Monitoring Frequency:** Once at Project plan check (requiring proof of compliance); Once during field investigation
- **Action Indicating Compliance:** Project plan approval; Issuance of grading permit; Field inspection sign-off

**HAZ-MM-2** Prior to the commencement of soil-disturbing activities, the Applicant shall retain a qualified environmental professional to prepare a Soil Management Plan for review and approval by the City of Los Angeles Department of Building and Safety. Soil-disturbing activities include excavation, grading, trenching, utility installation or repair, and other human activities that may potentially bring contaminated soil to the surface. The approved Soil Management Plan shall be implemented during soil-disturbing activities on the Project Site and shall establish policies and requirements for the testing, management, transport, and disposal of soils. The Soil Management Plan shall describe specific soil-handling controls required to assure compliance

with local, State and federal overseeing agencies, as well as to prevent unacceptable exposure to contaminated soil and prevent the improper disposal of contaminated soils, if encountered.

- **Enforcement Agency:** Los Angeles Department of City Planning; Los Angeles Department of Building and Safety; Department of Toxic Substances Control; or Los Angeles Regional Water Quality Control Board
- **Monitoring Agency:** Los Angeles Department of Building and Safety
- **Monitoring Phase:** Pre-construction; Construction
- **Monitoring Frequency:** Once prior to issuance of grading permit; Ongoing with periodic field inspections during construction for Soil Management Plan implementation
- **Action Indicating Compliance:** Approval of Soil Management Plan; Issuance of grading permit; Compliance report by a qualified environmental consultant

## d) Greenhouse Gas Emissions

### (1) Project Design Features

**GHG-PDF-1:** The Office Building will be designed to achieve the equivalent of the United States Green Building Council (USGBC) Leadership in Energy and Environmental Design (LEED) Silver Certification level for new buildings. Prior to the issuance of building permits, documentation that indicates the Office Building is designed to achieve the number of points that would be required for LEED Silver Certification will be provided to the City. The specific sustainability features that will be integrated into the Project design to enable the Project to meet this standard may include, but will not be limited to, the following:

- Use of Energy Star rated products and appliances.
- Use of high-efficiency wall and/or roof insulation.
- Use of light-emitting diode (LED) lighting or other energy-efficient lighting technologies, such as occupancy sensors or daylight harvesting and dimming controls, where appropriate, to reduce electricity use.
- **Enforcement Agency:** Los Angeles Department of City Planning; Los Angeles Department of Building and Safety
- **Monitoring Agency:** Los Angeles Department of Building and Safety



- **Monitoring Phase:** Pre-construction; Construction
- **Monitoring Frequency:** Once at Project plan check; Ongoing during field inspection
- **Action Indicating Compliance:** Issuance of building permit; Field inspection sign-off

## e) Noise and Vibration

### (1) Project Design Features

**NOI-PDF-1:** All capable diesel-powered construction vehicles will be equipped with exhaust mufflers, aftermarket dampening systems, or other suitable noise reduction devices.

- **Enforcement Agency:** Los Angeles Department of City Planning; Los Angeles Department of Building and Safety
- **Monitoring Agency:** Los Angeles Department of Building and Safety
- **Monitoring Phase:** Pre-construction; Construction
- **Monitoring Frequency:** Once at Project plan check; Ongoing with periodic field inspection during construction
- **Action Indicating Compliance:** Issuance of demolition permit; Field inspection sign-off

**NOI-PDF-2:** Power construction equipment (including combustion engines), fixed or mobile, will be equipped with state-of-the-art noise shielding and muffling devices (consistent with manufacturers' standards). All equipment will be properly maintained to assure that no additional noise, due to worn or improperly maintained parts, would be generated.

- **Enforcement Agency:** Los Angeles Department of City Planning; Los Angeles Department of Building and Safety
- **Monitoring Agency:** Los Angeles Department of Building and Safety
- **Monitoring Phase:** Pre-construction; Construction
- **Monitoring Frequency:** Once at Project plan check; Ongoing with periodic field inspection during construction

- **Action Indicating Compliance:** Issuance of demolition permit; Field inspection sign-off

**NOI-PDF-3:** Grading and construction contractors will use rubber-tired equipment rather than metal-tracked equipment.

- **Enforcement Agency:** Los Angeles Department of City Planning; Los Angeles Department of Building and Safety
- **Monitoring Agency:** Los Angeles Department of Building and Safety
- **Monitoring Phase:** Pre-construction; Construction
- **Monitoring Frequency:** Once at Project plan check; Ongoing with periodic field inspection during construction
- **Action Indicating Compliance:** Issuance of grading permit; Field inspection sign-off

**NOI-PDF-4:** An on-site construction manager will be responsible for responding to local complaints about construction noise. Notices will be sent to residential units within 500 feet of the construction site and signs will be posted at the construction site that list the telephone number for the on-site construction manager.

- **Enforcement Agency:** Los Angeles Department of City Planning; Los Angeles Department of Building and Safety
- **Monitoring Agency:** Los Angeles Department of Building and Safety
- **Monitoring Phase:** Pre-construction; Construction
- **Monitoring Frequency:** Once at Project plan check; Once before the onset of demolition activities to confirm signs/notices are posted
- **Action Indicating Compliance:** Issuance of demolition permit; Field inspection sign-off

**NOI-PDF-5:** Construction supervisors will be informed of Project-specific noise requirements, noise issues for sensitive land uses adjacent to the Project construction Site, and/or equipment operations to ensure compliance with the required regulations and best practices.

- **Enforcement Agency:** Los Angeles Department of City Planning; Los Angeles Department of Building and Safety

- **Monitoring Agency:** Los Angeles Department of Building and Safety
- **Monitoring Phase:** Pre-construction; Construction
- **Monitoring Frequency:** Once at the onset of demolition activities and as needed when new personnel begin work
- **Action Indicating Compliance:** Field training sign-off

**NOI-PDF-6:** Rooftop mechanical equipment, including heating, ventilation, and air conditioning (HVAC) systems, will be acoustically screened from off-site locations and will include vibration-attenuation mounts.

- **Enforcement Agency:** Los Angeles Department of City Planning; Los Angeles Department of Building and Safety
- **Monitoring Agency:** Los Angeles Department of Building and Safety
- **Monitoring Phase:** Construction
- **Monitoring Frequency:** Once at Project plan check; Once at field inspection
- **Action Indicating Compliance:** Issuance of occupancy permit; Field inspection sign-off

## (2) Mitigation Measures

**NOI-MM-1** Subject to off-site property owner agreement, a temporary construction barrier on the rooftop of 428 South Hewitt Street, near the edge of the rooftop facing the Project Site, shall be erected during the Project demolition and grading phases and when equipment is used on the ground floor during building construction and paving. The barrier shall be least four feet in height and constructed of a material with a Sound Transmission Class (STC) rating of at least STC-30 (such as acoustic panels or sound barrier products) or a transmission loss of at least 20 decibels (dB) at 500 hertz (such as 1/2-inch plywood). In addition to the rooftop barrier, a temporary construction barrier of approximately 300 feet in length and 24 feet in height, located at the eastern edge and southeastern corner of the Project Site, and constructed of a material with a rating of STC-35 or greater (such as acoustic panels or sound barrier products) or providing a transmission loss of at least 25 dB at 500 hertz (such as 3/4-inch plywood), shall be erected during the Project demolition and grading phases and when equipment is used on the ground floor during building construction and paving.

- **Enforcement Agency:** Los Angeles Department of City Planning; Los Angeles Department of Building and Safety
- **Monitoring Agency:** Los Angeles Department of Building and Safety
- **Monitoring Phase:** Pre-Construction; Construction
- **Monitoring Frequency:** Once at Project plan check; Once at field inspection
- **Action Indicating Compliance:** Issuance of demolition, grading, and building permits; Field inspection sign-off

**NOI-MM-2** Prior to demolition, the Applicant shall retain the services of a structural engineer or other qualified professional to conduct pre-construction surveys to document the current physical conditions of the following identified vibration-sensitive receptors: 418 Colyton Street, 424 Colyton Street, and 427 South Hewitt Street.

- **Enforcement Agency:** Los Angeles Department of City Planning; Los Angeles Department of Building and Safety
- **Monitoring Agency:** Los Angeles Department of Building and Safety
- **Monitoring Phase:** Pre-Construction
- **Monitoring Frequency:** Once at Project plan check
- **Action Indicating Compliance:** Issuance of demolition permit

**NOI-MM-3** Prior to the issuance of grading permits, the Applicant shall retain the services of a structural engineer or other qualified professional to prepare a demolition and shoring plan to ensure the proper protection and treatment of the properties at 418 Colyton Street, 424 Colyton Street, and 427 South Hewitt Street during construction. The plan shall include appropriate measures to protect these properties from damage due to demolition of existing structures, excavation or other ground-disturbing activities, vibration, soil settlement, and general construction activities. The plan shall be submitted to the City of Los Angeles' Office of Historic Resources for review and approval.

- **Enforcement Agency:** Los Angeles Department of City Planning; Los Angeles Office of Historic Resources; Los Angeles Department of Building and Safety
- **Monitoring Agency:** Los Angeles Department of Building and Safety

- **Monitoring Phase:** Pre-Construction; Construction
- **Monitoring Frequency:** Once at Project plan check; Ongoing with periodic field inspection during construction
- **Action Indicating Compliance:** Demolition and Shoring Plan approval; Issuance of demolition, grading, and building permits

**NOI-MM-4** Prior to the issuance of grading permits, the Applicant shall retain the services of an acoustical engineer or other qualified professional to develop and implement a structural monitoring program during construction. The performance standards of the structural monitoring program shall include the following:

- Documentation, consisting of video and/or photographic documentation of accessible and visible areas on the exterior of the receptor buildings (refer to NOI-MM-2).
- A registered civil engineer, certified engineering geologist, or vibration control engineer shall review the appropriate vibration criteria for the identified vibration receptors, taking into consideration their age, construction, condition, and other factors related to vibration sensitivity in order to develop additional recommendations for the structural monitoring program.
- Vibration sensors shall be installed on and/or around the identified vibration receptors to monitor for horizontal and vertical movement. These sensors shall remain in place for the duration of excavation, shoring, and grading phases.
- The vibration sensors shall be equipped with real-time warning system capabilities that can immediately alert construction supervisors when monitored vibration levels approach or exceed threshold limits. The registered civil engineer, certified engineering geologist, or vibration control engineer shall determine the appropriate limits.
- Should an exceedance of vibration thresholds occur, work in the vicinity of the affected area shall be halted and the respective vibration receptor shall be inspected for any damage. Results of the inspection shall be logged. In the event that damage occurs, the damage shall be repaired in consultation with a qualified preservation consultant. In the event of an exceedance, feasible

steps to reduce vibratory levels shall be undertaken, such as halting/staggering concurrent activities and utilizing lower-vibratory techniques.

- **Enforcement Agency:** Los Angeles Department of City Planning; Los Angeles Office of Historic Resources; Los Angeles Department of Building and Safety
- **Monitoring Agency:** Los Angeles Department of Building and Safety
- **Monitoring Phase:** Pre-Construction; Construction
- **Monitoring Frequency:** Once at Project plan check; Ongoing with periodic field inspection during construction
- **Action Indicating Compliance:** Approval of Structural Monitoring Program; Issuance of demolition, grading, and building permits

## f) Public Services – Police Protection Services

### (1) Project Design Features

**POL-PDF-1:** Prior to issuance of a demolition permit, the Project will:

- Provide security fencing around the perimeter of the Project Site during the construction phase; and
- Provide on-site security personnel whose duties will include construction site entrance and exit monitoring.

Prior to issuance of a certificate of occupancy, the Project will:

- Provide on-site security personnel whose duties will include Office Building (including parking levels) video surveillance monitoring and fire/life/safety system monitoring; and
- Provide adequate security lighting of parking areas, elevators, lobbies, and pathways for pedestrian orientation and to reduce areas of concealment.

The Applicant will consult with the Los Angeles Police Department (LAPD) to ensure that available and feasible crime prevention features have been incorporated during the construction period and into the Project design and receive LAPD's approval.

- **Enforcement Agency:** Los Angeles Department of City Planning; Los Angeles Police Department; or Los Angeles Department of Building and Safety

- **Monitoring Agency:** Los Angeles Department of City Planning; Los Angeles Police Department; or Los Angeles Department of Building and Safety
- **Monitoring Phase:** Pre-Construction (security fencing around the Project site during construction); Construction (security personnel during construction); Post-Construction (security personnel during operations, video surveillance, and lighting)
- **Monitoring Frequency:** Once at Project plan check; Ongoing with periodic field inspection during construction; Once following occupancy during field inspection
- **Action Indicating Compliance:** Project plan approval; LAPD approval of compliance documentation; Issuance of Certificate of Occupancy; Field inspection sign-off

**POL-PDF-2: Emergency Procedures Plan.** Prior to the issuance of a certificate of occupancy, the Applicant or its successor will develop an Emergency Procedures Plan that addresses emergency concerns and practices and provides a diagram that illustrates each portion of the property, including access routes. The plan will be submitted to the Los Angeles Police Department (LAPD) Central Area Commanding Officer for review and approval.

- **Enforcement Agency:** Los Angeles Department of City Planning; Los Angeles Police Department; or Los Angeles Department of Building and Safety
- **Monitoring Agency:** Los Angeles Police Department
- **Monitoring Phase:** Post-Construction
- **Monitoring Frequency:** Once at Project plan check (requiring Emergency Procedures Plan)
- **Action Indicating Compliance:** Project plan approval; LAPD approval of Emergency Procedures Plan; Issuance of Certificate of Occupancy

## g) Transportation

### (1) Project Design Features

**TRANS-PDF-1: Construction Traffic Management Plan.** The Applicant will prepare and submit a detailed Construction Traffic Management Plan to the City for review and approval. The Construction Traffic Management Plan will include temporary street closure information, a detour plan, haul routes, and an equipment staging plan. The Construction Traffic Management Plan will formalize how construction will be carried out and identify

specific actions that will be required to reduce effects on the surrounding community. The Construction Traffic Management Plan will be based on the nature and timing of the specific construction activities and other projects in the vicinity of the Project Site, and will include, but not be limited to, the following elements, as appropriate:

- Advanced notification of adjacent property owners and occupants, as well as nearby schools, of upcoming construction activities, including durations and daily hours of construction.
- Prohibition of construction worker parking on adjacent residential streets.
- Prohibition of construction-related vehicle parking on surrounding public streets.
- Temporary pedestrian and vehicular traffic controls during all construction activities adjacent to East 4<sup>th</sup> Street, Colyton Street, and South Hewitt Street to ensure traffic safety on public rights-of-way. These controls shall include, but are not limited to, flag people trained in pedestrian and student safety.
- Temporary traffic control during all construction activities adjacent to public rights-of-way to improve traffic flow on public roadways (e.g., flag men).
- Scheduling of construction activities to reduce the effect on traffic flow on surrounding arterial streets.
- Safety precautions for pedestrians and bicyclists through such measures as alternate routing and protection barriers as appropriate, including along all identified Los Angeles Unified School District (LAUSD) pedestrian routes to nearby schools.
- Scheduling of construction-related deliveries, haul trips, etc., so as to occur outside the commuter peak hours to the extent feasible, and so as to not impede school drop-off and pick-up activities and students using LAUSD's identified pedestrian routes to nearby schools.
- Coordination with public transit agencies to provide advanced notifications of stop relocations and durations.
- Advanced notification of temporary parking removals and duration of removals.
- Provision of detour plans to address temporary road closures during construction.
- **Enforcement Agency:** Los Angeles Department of Transportation; Los Angeles Department of City Planning



- **Monitoring Agency:** Los Angeles Department of Transportation; Los Angeles Department of City Planning
- **Monitoring Phase:** Pre-Construction; Construction
- **Monitoring Frequency:** Once, prior to issuance of the demolition, grading or building permit; Periodic field inspections during construction
- **Action Indicating Compliance:** Approval of Construction Traffic Management Plan by Los Angeles Department of Transportation prior to issuance of demolition, grading, or building permit (Pre-Construction); Compliance certification report submitted by Project contractor (Construction)

**TRANS-PDF-2: Transportation Management Organization.** The Applicant will provide its fair share of seed funding for the Arts District portion of a Downtown/Arts District Transportation Management Organization (TMO), following approval of the Project, by providing funding for TMO operations and marketing efforts. The Applicant will commit its fair share required in the first year to cover the cost of launching the Arts District portion of a Downtown/Arts District TMO and will continue to commit to nine additional years (10 years in total), as a charter member with annual dues.

- **Enforcement Agency:** Los Angeles Department of Transportation; Los Angeles Department of City Planning
- **Monitoring Agency:** Los Angeles Department of Transportation; Los Angeles Department of City Planning
- **Monitoring Phase:** Pre-occupancy
- **Monitoring Frequency:** Once annually for ten years
- **Action Indicating Compliance:** Issuance of Certificate of Occupancy

**TRANS-PDF-3: Transportation Demand Management (TDM) Program.** The Project will develop and implement a Transportation Demand Management (TDM) program to promote non-auto travel and reduce the use of single-occupant vehicle trips. The TDM program will be subject to review and approval by the City of Los Angeles Department of City Planning and Los Angeles Department of Transportation (LADOT). The TDM Program must be approved by LADOT prior to the issuance of the first Certificate of Occupancy. The strategies in the TDM program may include, but would not be limited to, the following:

- Educational Programs/On-Site TDM Coordinator – A TDM coordinator on the building management staff would reach out to employers and employees directly

to make them aware of the various programs offered and promote the benefits of the TDM.

- **Transportation Information Center/Kiosks** – A Transportation Information Center is a centrally-located commuter information center where Project employees and visitors can obtain information regarding commute programs, and individuals can obtain real-time information for planning travel without using an automobile. A Transportation Information Center will support orientation for new employees as well as providing information about transit schedules, commute planning, rideshare, telecommuting, and bicycle and pedestrian plans.
- **Bicycle and Pedestrian Amenities** – The Project would incorporate features for bicyclists and pedestrians, such as exclusive access points, secured bicycle parking facilities and showers. Additionally, the Project Site would be designed to be a friendly and convenient environment for pedestrians.
- **City Bicycle Plan Trust Fund** – The Applicant would contribute to the City Bicycle Plan Trust Fund for implementation of bicycle improvements in the Project area under the 2010 Bicycle Plan and Mobility Plan.
- **Ridesharing Services Programs** – The TDM program would provide services to match employees together to establish carpools and vanpools.
- **Incentives for Using Alternative Travel Modes** – The TDM program may include, but would not be limited to, various incentives for use of its programs. For example, carpool and vanpool users could be offered preferential load/unload areas or convenient designated parking spaces. Unbundled parking is a program wherein parking spaces are rented separately from the building space, which allows for a separate charge for parking and the flexibility to vary the number of spaces rented.
- **Mobility Hub Support** – The Project would support existing and/or future efforts by LADOT to provide first-mile and last-mile service for transit users through the mobility hub program. Mobility hubs, typically located at or near public transit centers, would provide amenities such as, but not limited to, bicycle parking, and transit information. In cooperation with the proposed Downtown/Arts District Transportation Management Organization (TMO), the Project could provide space for similar amenities at the Project Site to complement future mobility hubs in the Study Area.
- **Enforcement Agency:** Los Angeles Department of Transportation; Los Angeles Department of City Planning

- **Monitoring Agency:** Los Angeles Department of Transportation; Los Angeles Department of City Planning
- **Monitoring Phase:** Post-Construction
- **Monitoring Frequency:** Once at Project plan check
- **Action Indicating Compliance:** TDM approval; Issuance of occupancy permit

## **h) Utilities and Service Systems – Water Supply and Infrastructure**

### **(1) Project Design Features**

**WS-PDF-1: Water Conservation Features.** The Project will provide the following water efficiency features:

- High Efficiency Toilets with a flush volume of 1.1 gallons per flush, or less.
- Showerheads with a flow rate of 1.5 gallons per minute, or less.
- Domestic Water Heating System located in close proximity to point(s) of use.
- Drip/Subsurface Irrigation (Micro-Irrigation)/Bubblers for trees.
- Proper Hydro-zoning/Zoned Irrigation.
- Drought Tolerant Plants.
- **Enforcement Agency:** Los Angeles Department of Water and Power; Los Angeles Department of Building and Safety; Los Angeles Department of City Planning
- **Monitoring Agency:** Los Angeles Department of Building and Safety
- **Monitoring Phase:** Pre-Construction; Construction
- **Monitoring Frequency:** Once at Project plan check; Once prior to issuance of Certificate of Occupancy
- **Action Indicating Compliance:** Project plan approval; Issuance of building permit; Issuance of Certificate of Occupancy

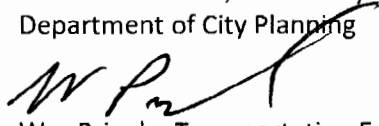
CITY OF LOS ANGELES  
INTER-DEPARTMENTAL CORRESPONDENCE

**Exhibit E**  
**LADOT Inter-Departmental**  
**Memo dated April 14, 2020**  
**CPC-2017-469**

405 S. Hewitt St  
DOT Case No. CEN 20-49411

Date: April 14, 2020

To: Milena Zasadzien, Senior City Planner  
Department of City Planning



From: Wes Pringle, Transportation Engineer  
Department of Transportation

Subject: **UPDATED TRANSPORTATION IMPACT ANALYSIS FOR THE 4<sup>TH</sup> AND HEWITT  
COMMERCIAL DEVELOPMENT LOCATED AT 405 SOUTH HEWITT STREET**

*On July 11, 2018, the Department of Transportation (DOT) issued a traffic assessment report to the Department of City Planning on the proposed commercial project located at 405 South Hewitt Street. However, subsequent to the release of this report, pursuant to Senate Bill (SB) 743 and the recent changes to Section 15064.3 of the State's California Environmental Quality Act (CEQA) Guidelines, the City of Los Angeles adopted vehicle miles traveled (VMT) as the criteria by which to determine transportation impacts under CEQA. Therefore, in response to this action the applicant submitted a VMT analysis for the proposed project in addition to the previous analysis submitted on March 2018. Therefore, please replace the previous July 11, 2018 DOT assessment, in its entirety, with this report which addresses the totality of the transportation analysis.*

-----

The Department of Transportation (DOT) has reviewed the supplemental traffic analysis, dated February 2020, prepared by Gibson Transportation Consulting, for the commercial project located at 405 South Hewitt Street. In compliance with Senate Bill 743 and the California Environmental Quality Act (CEQA), a vehicle miles traveled (VMT) analysis is required to identify the project's ability to promote the reduction of green-house gas emissions, access to diverse land-uses, and the development of multi-modal networks. The significance of a project's impact in this regard is measured against the VMT thresholds established in DOT's Transportation Assessment Guidelines (TAG), as described below.

**DISCUSSION AND FINDINGS**

A. Project Description

The project site is currently occupied by a museum, office use, and storage use located in the Arts District on the south side of 4th Street between Colyton Street and Hewitt Street. The museum will remain and the office and storage uses will be removed in order to construct 311,682 square feet of office space and 8,149 square feet of commercial space. The study did not include the number of parking spaces proposed for the project. Access to the parking

garage would be provided via two driveways on 4th Street. Access to the loading dock would be provided via a driveway on Hewitt Street. The project is expected to be completed by 2023.

B. CEQA Screening Threshold

Prior to accounting for trip reductions resulting from the application of Transportation Demand Management (TDM) Strategies, a trip generation analysis was conducted to determine if the project would exceed the net 250 daily vehicle trips screening threshold. Using the City of Los Angeles VMT Calculator tool, which draws upon trip rate estimates published in the Institute of Transportation Engineers' (ITE's) Trip Generation, 9<sup>th</sup> Edition manual as well as applying trip generation adjustments when applicable, based on sociodemographic data and the built environment factors of the project's surroundings, it was determined that the project **does** exceed the net 250 daily vehicle trips threshold.

C. Transportation Impacts

On July 30, 2019, pursuant to Senate Bill (SB) 743 and the recent changes to Section 15064.3 of the State's California Environmental Quality Act (CEQA) Guidelines, the City of Los Angeles adopted vehicle miles traveled (VMT) as a criteria in determining transportation impacts under CEQA. The new DOT Transportation Assessment Guidelines (TAG) provide instructions on preparing transportation assessments for land use proposals and defines the significant impact thresholds.

The DOT VMT Calculator tool measures project impact in terms of Household VMT per Capita, and Work VMT per Employee. DOT identified distinct thresholds for significant VMT impacts for each of the seven Area Planning Commission (APC) areas in the City. For the Central APC area, in which the project is located, the following thresholds have been established:

- Household VMT per Capita: 6.0
- Work VMT per Employee: 7.6

As cited in the VMT Analysis report, the VMT projection for the proposed project is 7.2 for the Work VMT. There is no household VMT for this project. Therefore, it is concluded that implementation of the Project would not result in a significant Work VMT impact. A copy of the VMT Calculator summary reports is provided as **Attachment 1** to this report.

Additionally, the analysis included further discussion of the transportation impact thresholds:

- T-1 Conflicting with plans, programs, ordinances, or policies
- T-2.1 Causing substantial vehicle miles traveled
- T-2.2 Substantially inducing additional automobile travel
- T-3 Substantially increasing hazards due to a geometric design feature or incompatible use.

A Project's impacts per Thresholds T-2.1 and 2.2 are determined by using the VMT calculator and are discussed above. The assessment determined that the project would not have a significant transportation impact under any of the above thresholds.

D. Safety, Access and Circulation

During the preparation of the new CEQA guidelines, the State's Office of Planning and Research stressed that lead agencies can continue to apply traditional operational analysis requirements to inform land use decisions provided that such analyses were outside of the CEQA process. The authority for requiring non-CEQA transportation analysis and requiring improvements to address potential circulation deficiencies, lies in the City of Los Angeles' Site Plan Review authority as established in Section 16.05 of the Los Angeles Municipal Code (LAMC), Section 16.05. Therefore, DOT continues to require and review a project's site access, circulation, and operational plan to determine if any safety and access enhancements, transit amenities, intersection improvements, traffic signal upgrades, neighborhood traffic calming, or other improvements are needed. In accordance with this authority, the project has completed a circulation analysis using a "level of service" screening methodology that indicates that the trips generated by the proposed development will likely result in adverse circulation conditions at several locations. DOT has reviewed this analysis and determined that it adequately discloses operational concerns. A copy of the circulation analysis table that summarizes these potential deficiencies is provided as **Attachment 2** to this report.

## PROJECT REQUIREMENTS

### A. Corrective Measures (Non-CEQA Analysis)

In the Traffic Study report, the analysis included a review of current and potential future deficiencies that may result from the project. To address these deficiencies, the applicant should be required to implement the following corrective measures.

#### 1. **Transportation Demand Management (TDM) Program**

A TDM program, which includes design elements and trip reduction strategies, would reduce the project's overall trip generation by discouraging single occupancy vehicle use and by promoting the use of alternative travel modes. Through strategic building design and orientation, this project can facilitate access to existing transit services, provide a pedestrian-friendly environment, promote non-automobile travel and support the goals of a trip-reduction program.

A preliminary TDM program shall be prepared and provided for DOT review prior to the issuance of the first building permit for this project and a final TDM program approved by DOT is required prior to the issuance of the first certificate of occupancy for the project. The preliminary plan will include, at a minimum, measures consistent with the City's Trip Reduction Ordinance. As recommended by the transportation study, the TDM program could include, but is not be limited to the following:

- Educational Programs/On-Site TDM Coordinator who reaches out to employers and employees promoting the benefits of TDM;
- Centrally located Transportation Information Center/Kiosk where employees and visitors can obtain information regarding commute programs and real-time commuter information;
- Bicycle and pedestrian-friendly environment with exclusive access points, secured bicycle facilities, and showers;
- A one-time fixed-fee contribution of \$50,000 to be deposited into the City's Bicycle Plan Trust Fund prior to the issuance of any certificates of occupancy to be used to

implement bicycle improvements within the Project area;

- Ridesharing Services Program which would match employees together to establish carpools and vanpools;
- Guaranteed ride home (GRH) program;
- Parking incentives and administrative support for the formation of carpools and vanpools;
- Unbundled parking;
- Mobility Hub support of existing and/or future efforts by LADOT for Mobility Hubs by providing amenities such as bicycle parking, transit information, etc. at the project site (subject to design feasibility);
- Record a Covenant and Agreement to ensure that the TDM program will be maintained.

2. **Downtown/Arts District Transportation Management Organization (TMO)**

The project proposes to contribute to the formation and marketing of and participation in the Downtown/Arts District TMO. The project would provide its fair share of seed funding for the TMO in the first year to cover the cost of launching the TMO and continue to commit to nine additional years (10 years in total) as a charter member with annual dues. The TMO would offer similar services to those described above but would have a much wider reach than the project's local TDM plan and can result in much greater trip reduction benefits. The TMO could be instrumental in promoting the use of transit and the City's bike share and car share programs that will be installed in the coming years within the Downtown area. The TMO's activities would help augment or implement some of the strategies described above for the project specific TDM plan.

C. Highway Dedication and Street Widening Requirements

Per the new Mobility Element of the General Plan, **4<sup>th</sup> Street**, an Avenue III, would require a 23-foot half-width roadway within a 36-foot half-width right-of-way; **Colyton Street** and **Hewitt Street**, both Collector Streets, would require a 20-foot half-width roadway within a 33-foot half-width right-of-way. The applicant should check with BOE's Land Development Group to determine the specific highway dedication, street widening and/or sidewalk requirements for this project.

D. Construction Impacts

DOT recommends that a construction work site traffic control plan be submitted to DOT's Citywide Temporary Traffic Control Section or Permit Plan Review Section for review and approval prior to the start of any construction work. Refer to <http://ladot.lacity.org/what-we-do/plan-review> to determine which section to coordinate review of the work site traffic control plan. The plan should show the location of any roadway or sidewalk closures, traffic detours, haul routes, hours of operation, protective devices, warning signs and access to abutting properties. DOT also recommends that all construction related traffic be restricted to off-peak hours to the extent feasible.

E. Parking Requirements

The study did not indicate the number of parking spaces to be provided. The developer should check with the Department of Building and Safety on the number of parking spaces needed.

F. Driveway Access and Circulation

Access to the parking garage would be provided via two right-in/right-out driveways on 4th Street, with one driveway accessing the subterranean parking levels and one driveway accessing the above-grade parking levels. Access to the at-grade loading dock would be provided via a full-access driveway on Hewitt Street. Conceptually, the proposed site plan is acceptable to DOT. The review of this study does not constitute approval of the driveway dimensions, access and circulation scheme, and loading/unloading area for the project. Any changes to the project's site access, circulation scheme, or loading/unloading area after issuance of this report would require separate review and approval and should be coordinated with DOT's Citywide Planning Coordination Section at 201 N. Figueroa Street, 5th Floor, Room 550, at (213) 482-7024. The applicant should contact DOT for driveway width and internal circulation requirements prior to the commencement of building or parking layout design efforts so that such traffic flow considerations are designed and incorporated early into the building and parking layout plans. All driveways should be Case 2 driveways and 30 feet for two-way operations or 18 feet for one-way operations. All delivery truck loading and unloading should take place on site with no vehicles having to back into the project via any of the project driveways. A copy of the project's site plan is provided as **Attachment 3** to this report.

G. Development Review Fees

Section 19.15 of the Los Angeles Municipal Code identifies specific fees for traffic study review, condition clearance, and permit issuance. The applicant shall comply with any applicable fees per this ordinance.

If you have any questions, please contact me at (213) 972-8482.

Attachments

*J:\Letters\2020\CEN20-49411\_405 Hewitt St MU\_vmt\_ltr.docx*

- c: Shaylee Papadakis, Council District No. 14  
Edward Yu, Central District, DOT  
Taimour Tanavoli, Case Management Office, DOT  
Matthew Masuda, BOE Development Services  
Sarah Drobis & Emily Wong, Gibson Transportation Consulting



## CITY OF LOS ANGELES VMT CALCULATOR Version 1.2



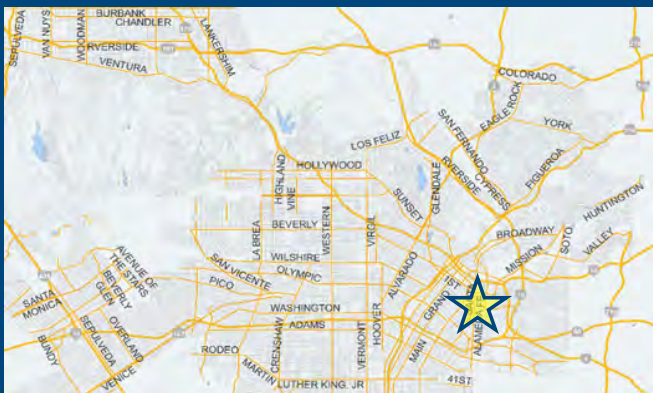
*Project Screening Criteria: Is this project required to conduct a vehicle miles traveled analysis?*

### Project Information

Project:

Scenario:

Address:



If the project is replacing an existing number of residential units with a smaller number of residential units, is the proposed project located within one-half mile of a fixed-rail or fixed-guideway transit station?

Yes  No

### Existing Land Use

Land Use Type	Value	Unit
Office   General Office	3.515	ksf
Office   General Office	3.515	ksf

[Click here to add a single custom land use type \(will be included in the above list\)](#)

### Proposed Project Land Use

Land Use Type	Value	Unit
Retail   High-Turnover Sit-Down Restaurant	8.149	ksf
Retail   High-Turnover Sit-Down Restaurant	8.149	ksf
Office   General Office	311.682	ksf

[Click here to add a single custom land use type \(will be included in the above list\)](#)

### Project Screening Summary

Existing Land Use	Proposed Project
27 Daily Vehicle Trips	2,830 Daily Vehicle Trips
201 Daily VMT	20,381 Daily VMT
<b>Tier 1 Screening Criteria</b>	
Project will have less residential units compared to existing residential units & is within one-half mile of a fixed-rail station. <input type="checkbox"/>	
<b>Tier 2 Screening Criteria</b>	
The net increase in daily trips < 250 trips	2,803 Net Daily Trips
The net increase in daily VMT ≤ 0	20,180 Net Daily VMT
The proposed project consists of only retail land uses ≤ 50,000 square feet total.	8,149 ksf
<b>The proposed project is required to perform VMT analysis.</b>	



# CITY OF LOS ANGELES VMT CALCULATOR Version 1.2

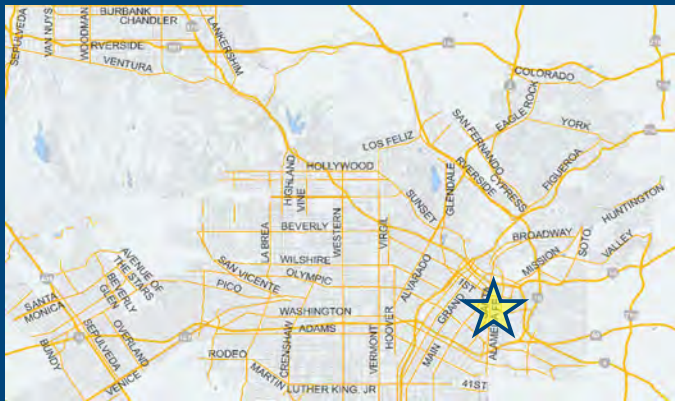


## Project Information

Project:

Scenario:

Address:



Proposed Project Land Use Type	Value	Unit
Retail   High-Turnover Sit-Down Restaurant	8.14	
Office   General Office	311	

## TDM Strategies

Select each section to show individual strategies  
Use  to denote if the TDM strategy is part of the proposed project or is a mitigation strategy

	Proposed Project	With Mitigation
Max Home Based TDM Achieved?	No	No
Max Work Based TDM Achieved?	No	No

- A** Parking
- B** Transit
- C** Education & Encouragement
- D** Commute Trip Reductions
- E** Shared Mobility
- F** Bicycle Infrastructure
  - Implement/Improve On-street Bicycle Facility Select Proposed Prj or Mitigation to include this strategy  
 Proposed Prj  Mitigation
  - Include Bike Parking Per LAMC Select Proposed Prj or Mitigation to include this strategy  
 Proposed Prj  Mitigation
  - Include Secure Bike Parking and Showers Select Proposed Prj or Mitigation to include this strategy  
 Proposed Prj  Mitigation
- G** Neighborhood Enhancement

## Analysis Results

Proposed Project	With Mitigation
<b>2,756</b> Daily Vehicle Trips	<b>2,756</b> Daily Vehicle Trips
<b>19,848</b> Daily VMT	<b>19,848</b> Daily VMT
<b>0.0</b> Household VMT per Capita	<b>0.0</b> Household VMT per Capita
<b>7.2</b> Work VMT per Employee	<b>7.2</b> Work VMT per Employee

Significant VMT Impact?	
<b>Household: No</b> Threshold = 6.0 15% Below APC	<b>Household: No</b> Threshold = 6.0 15% Below APC
<b>Work: No</b> Threshold = 7.6 15% Below APC	<b>Work: No</b> Threshold = 7.6 15% Below APC



# CITY OF LOS ANGELES VMT CALCULATOR

## Report 1: Project & Analysis Overview

Date: January 15, 2020

Project Name: 4th & Hewitt

Project Scenario:

Project Address: 401 S HEWITT ST, 90013



Version 1.2

Project Information			
Land Use Type		Value	Units
Housing	Single Family	0	DU
	Multi Family	0	DU
	Townhouse	0	DU
	Hotel	0	Rooms
	Motel	0	Rooms
Affordable Housing	Family	0	DU
	Senior	0	DU
	Special Needs	0	DU
	Permanent Supportive	0	DU
Retail	General Retail	0.000	ksf
	Furniture Store	0.000	ksf
	Pharmacy/Drugstore	0.000	ksf
	Supermarket	0.000	ksf
	Bank	0.000	ksf
	Health Club	0.000	ksf
	High-Turnover Sit-Down Restaurant	8.149	ksf
	Fast-Food Restaurant	0.000	ksf
	Quality Restaurant	0.000	ksf
	Auto Repair	0.000	ksf
	Home Improvement	0.000	ksf
	Free-Standing Discount	0.000	ksf
	Movie Theater	0	Seats
Office	General Office	311.682	ksf
	Medical Office	0.000	ksf
Industrial	Light Industrial	0.000	ksf
	Manufacturing	0.000	ksf
	Warehousing/Self-Storage	0.000	ksf
School	University	0	Students
	High School	0	Students
	Middle School	0	Students
	Elementary	0	Students
	Private School (K-12)	0	Students
Other		0	Trips

# CITY OF LOS ANGELES VMT CALCULATOR

## Report 1: Project & Analysis Overview

Date: January 15, 2020

Project Name: 4th & Hewitt

Project Scenario:

Project Address: 401 S HEWITT ST, 90013



Version 1.2

<b>Analysis Results</b>			
Total Employees: 1,279			
Total Population: 0			
<b>Proposed Project</b>		<b>With Mitigation</b>	
2,756	Daily Vehicle Trips	2,756	Daily Vehicle Trips
19,848	Daily VMT	19,848	Daily VMT
0	Household VMT per Capita	0	Household VMT per Capita
7.2	Work VMT per Employee	7.2	Work VMT per Employee
<b>Significant VMT Impact?</b>			
<b>APC: Central</b>			
Impact Threshold: 15% Below APC Average			
Household = 6.0			
Work = 7.6			
<b>Proposed Project</b>		<b>With Mitigation</b>	
VMT Threshold	Impact	VMT Threshold	Impact
Household > 6.0	No	Household > 6.0	No
Work > 7.6	No	Work > 7.6	No

# CITY OF LOS ANGELES VMT CALCULATOR

## Report 2: TDM Inputs

Date: January 15, 2020

Project Name: 4th & Hewitt

Project Scenario:

Project Address: 401 S HEWITT ST, 90013



Version 1.2

TDM Strategy Inputs				
Strategy Type	Description	Proposed Project	Mitigations	
<b>Parking</b>	<i>Reduce parking supply</i>	<i>City code parking provision (spaces)</i>	0	0
		<i>Actual parking provision (spaces)</i>	0	0
	<i>Unbundle parking</i>	<i>Monthly cost for parking (\$)</i>	\$0	\$0
	<i>Parking cash-out</i>	<i>Employees eligible (%)</i>	0%	0%
	<i>Price workplace parking</i>	<i>Daily parking charge (\$)</i>	\$0.00	\$0.00
		<i>Employees subject to priced parking (%)</i>	0%	0%
	<i>Residential area parking permits</i>	<i>Cost of annual permit (\$)</i>	\$0	\$0
(cont. on following page)				

# CITY OF LOS ANGELES VMT CALCULATOR

## Report 2: TDM Inputs

Date: January 15, 2020

Project Name: 4th & Hewitt

Project Scenario:

Project Address: 401 S HEWITT ST, 90013



Version 1.2

TDM Strategy Inputs, Cont.				
Strategy Type	Description	Proposed Project	Mitigations	
<b>Transit</b>	<i>Reduce transit headways</i>	<i>Reduction in headways (increase in frequency) (%)</i>	0%	
		<i>Existing transit mode share (as a percent of total daily trips) (%)</i>	0%	
		<i>Lines within project site improved (&lt;50%, &gt;=50%)</i>	0	
	<i>Implement neighborhood shuttle</i>	<i>Degree of implementation (low, medium, high)</i>	0	0
		<i>Employees and residents eligible (%)</i>	0%	0%
	<i>Transit subsidies</i>	<i>Employees and residents eligible (%)</i>	0%	0%
<i>Amount of transit subsidy per passenger (daily equivalent) (\$)</i>		\$0.00	\$0.00	
<b>Education &amp; Encouragement</b>	<i>Voluntary travel behavior change program</i>	<i>Employees and residents participating (%)</i>	0%	
	<i>Promotions and marketing</i>	<i>Employees and residents participating (%)</i>	0%	
(cont. on following page)				

# CITY OF LOS ANGELES VMT CALCULATOR

## Report 2: TDM Inputs

Date: January 15, 2020

Project Name: 4th & Hewitt

Project Scenario:

Project Address: 401 S HEWITT ST, 90013



Version 1.2

TDM Strategy Inputs, Cont.				
Strategy Type	Description	Proposed Project	Mitigations	
<b>Commuter Trip Reductions</b>	<i>Required commute trip reduction program</i>	<i>Employees participating (%)</i>	0%	0%
	<i>Alternative Work Schedules and Telecommute</i>	<i>Employees participating (%)</i>	0%	0%
		<i>Type of program</i>	0	0
		<i>Degree of implementation (low, medium, high)</i>	0	0
	<i>Employer sponsored vanpool or shuttle</i>	<i>Employees eligible (%)</i>	0%	0%
		<i>Employer size (small, medium, large)</i>	0	0
	<i>Ride-share program</i>	<i>Employees eligible (%)</i>	0%	0%
<b>Shared Mobility</b>	<i>Car share</i>	<i>Car share project setting (Urban, Suburban, All Other)</i>	0	0
	<i>Bike share</i>	<i>Within 600 feet of existing bike share station - OR- implementing new bike share station (Yes/No)</i>	0	0
	<i>School carpool program</i>	<i>Level of implementation (Low, Medium, High)</i>	0	0
(cont. on following page)				

# CITY OF LOS ANGELES VMT CALCULATOR

## Report 2: TDM Inputs

Date: January 15, 2020

Project Name: 4th & Hewitt

Project Scenario:

Project Address: 401 S HEWITT ST, 90013



Version 1.2

TDM Strategy Inputs, Cont.				
Strategy Type	Description	Proposed Project	Mitigations	
<b>Bicycle Infrastructure</b>	<i>Implement/Improve on-street bicycle facility</i>	<i>Provide bicycle facility along site (Yes/No)</i>	0	0
	<i>Include Bike parking per LAMC</i>	<i>Meets City Bike Parking Code (Yes/No)</i>	0	0
	Include secure bike parking and showers	Includes indoor bike parking/lockers, showers, & repair station (Yes/No)	Yes	Yes
<b>Neighborhood Enhancement</b>	<i>Traffic calming improvements</i>	<i>Streets with traffic calming improvements (%)</i>	0%	0%
		<i>Intersections with traffic calming improvements (%)</i>	0%	0%
	Pedestrian network improvements	Included (within project and connecting off-site/within project only)	within project and connecting off-site	within project and connecting off-site



# CITY OF LOS ANGELES VMT CALCULATOR

## Report 3: TDM Outputs

Date: January 15, 2020

Project Name: 4th & Hewitt

Project Scenario:

Project Address: 401 S HEWITT ST, 90013



Version 1.2

### TDM Adjustments by Trip Purpose & Strategy

Place type: Suburban Center

		Home Based Work Production		Home Based Work Attraction		Home Based Other Production		Home Based Other Attraction		Non-Home Based Other Production		Non-Home Based Other Attraction		Source
		Proposed	Mitigated	Proposed	Mitigated	Proposed	Mitigated	Proposed	Mitigated	Proposed	Mitigated	Proposed	Mitigated	
<b>Parking</b>	Reduce parking supply	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	
	Unbundle parking	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	
	Parking cash-out	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	
	Price workplace parking	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	
	Residential area parking permits	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	
<b>Transit</b>	Reduce transit headways	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	TDM Strategy Appendix, Transit sections 1 - 3
	Implement neighborhood shuttle	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	
	Transit subsidies	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	
<b>Education &amp; Encouragement</b>	Voluntary travel behavior change program	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	TDM Strategy Appendix, Education & Encouragement sections 1 - 2
	Promotions and marketing	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	
<b>Commute Trip Reductions</b>	Required commute trip reduction program	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	TDM Strategy Appendix, Commute Trip Reductions sections 1 - 4
	Alternative Work Schedules and Telecommute Program	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	
	Employer sponsored vanpool or shuttle	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	
	Ride-share program	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	
<b>Shared Mobility</b>	Car-share	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	TDM Strategy Appendix, Shared Mobility sections 1 - 3
	Bike share	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	
	School carpool program	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	

# CITY OF LOS ANGELES VMT CALCULATOR

## Report 3: TDM Outputs

Date: January 15, 2020

Project Name: 4th & Hewitt

Project Scenario:

Project Address: 401 S HEWITT ST, 90013



Version 1.2

### TDM Adjustments by Trip Purpose & Strategy, Cont.

#### Place type: Suburban Center

		Home Based Work Production		Home Based Work Attraction		Home Based Other Production		Home Based Other Attraction		Non-Home Based Other Production		Non-Home Based Other Attraction		Source
		Proposed	Mitigated	Proposed	Mitigated	Proposed	Mitigated	Proposed	Mitigated	Proposed	Mitigated	Proposed	Mitigated	
		<b>Bicycle Infrastructure</b>	Implement/ Improve on-street bicycle facility	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
	Include Bike parking per LAMC	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
	Include secure bike parking and showers	0.6%	0.6%	0.6%	0.6%	0.6%	0.6%	0.6%	0.6%	0.6%	0.6%	0.6%	0.6%	
<b>Neighborhood Enhancement</b>	Traffic calming improvements	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	TDM Strategy Appendix, Neighborhood Enhancement sections 1 - 2
	Pedestrian network improvements	2.0%	2.0%	2.0%	2.0%	2.0%	2.0%	2.0%	2.0%	2.0%	2.0%	2.0%	2.0%	

### Final Combined & Maximum TDM Effect

	Home Based Work Production		Home Based Work Attraction		Home Based Other Production		Home Based Other Attraction		Non-Home Based Other Production		Non-Home Based Other Attraction	
	Proposed	Mitigated	Proposed	Mitigated	Proposed	Mitigated	Proposed	Mitigated	Proposed	Mitigated	Proposed	Mitigated
	<b>COMBINED TOTAL</b>	3%	3%	3%	3%	3%	3%	3%	3%	3%	3%	3%
<b>MAX. TDM EFFECT</b>	3%	3%	3%	3%	3%	3%	3%	3%	3%	3%	3%	3%

$$= \text{Minimum}(X\%, 1 - [(1-A) * (1-B) \dots])$$

where X%=

<b>PLACE</b>	urban	75%
<b>TYPE</b>	compact infill	40%
<b>MAX:</b>	suburban center	20%
	suburban	15%

Note:  $(1 - [(1-A) * (1-B) \dots])$  reflects the dampened combined effectiveness of TDM Strategies (e.g., A, B, ...). See the TDM Strategy Appendix (*Transportation Assessment Guidelines Attachment G*) for further discussion of dampening.

# CITY OF LOS ANGELES VMT CALCULATOR

## Report 4: MXD Methodology

Date: January 15, 2020

Project Name: 4th & Hewitt

Project Scenario:

Project Address: 401 S HEWITT ST, 90013



Version 1.2

### MXD Methodology - Project Without TDM

	Unadjusted Trips	MXD Adjustment	MXD Trips	Average Trip Length	Unadjusted VMT	MXD VMT
Home Based Work Production	0	0.0%	0	6.2	0	0
Home Based Other Production	0	0.0%	0	4.6	0	0
Non-Home Based Other Production	599	-14.0%	515	7.4	4,433	3,811
Home-Based Work Attraction	1,685	-31.5%	1,154	8.2	13,817	9,463
Home-Based Other Attraction	1,267	-49.0%	646	5.9	7,475	3,811
Non-Home Based Other Attraction	599	-14.0%	515	6.4	3,834	3,296

### MXD Methodology with TDM Measures

	<i>Proposed Project</i>			<i>Project with Mitigation Measures</i>		
	TDM Adjustment	Project Trips	Project VMT	TDM Adjustment	Mitigated Trips	Mitigated VMT
Home Based Work Production	-2.6%	0	0	-2.6%	0	0
Home Based Other Production	-2.6%	0	0	-2.6%	0	0
Non-Home Based Other Production	-2.6%	501	3,711	-2.6%	501	3,711
Home-Based Work Attraction	-2.6%	1,124	9,216	-2.6%	1,124	9,216
Home-Based Other Attraction	-2.6%	629	3,711	-2.6%	629	3,711
Non-Home Based Other Attraction	-2.6%	502	3,210	-2.6%	502	3,210

### MXD VMT Methodology Per Capita & Per Employee

Total Population: 0

Total Employees: 1,279

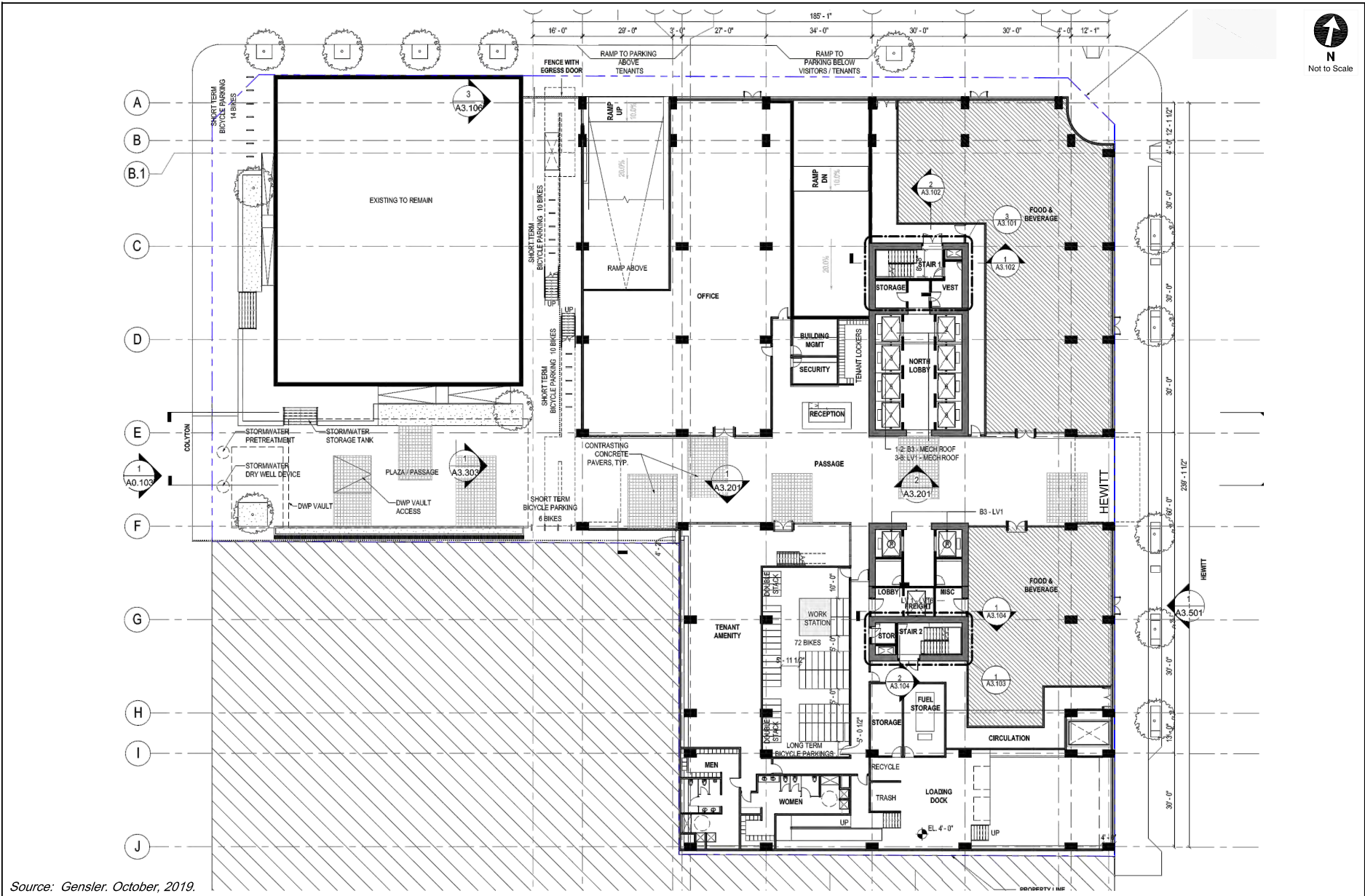
APC: Central

	<i>Proposed Project</i>	<i>Project with Mitigation Measures</i>
<i>Total Home Based Production VMT</i>	<b>0</b>	<b>0</b>
<i>Total Home Based Work Attraction VMT</i>	<b>9,216</b>	<b>9,216</b>
<i>Total Home Based VMT Per Capita</i>	<b>0.0</b>	<b>0.0</b>
<i>Total Work Based VMT Per Employee</i>	<b>7.2</b>	<b>7.2</b>

Attachment 2

**TABLE 13  
FUTURE WITH PROJECT CONDITIONS WITH TRANSPORTATION IMPROVEMENTS (YEAR 2023)  
INTERSECTION LEVEL OF SERVICE**

No.	Intersection	Peak Hour	Future without Project Conditions		Future with Project Conditions		Future with Project Conditions with Transportation Improvements	
			V/C	LOS	V/C	LOS	V/C	LOS
1.	Central Avenue & 3rd Street	A.M.	0.760	C	0.761	C	0.751	C
		P.M.	0.594	A	0.599	A	0.588	A
2.	Central Avenue & 4th Street	A.M.	0.377	A	0.388	A	0.376	A
		P.M.	0.733	C	0.734	C	0.724	C
3.	Central Avenue & 6th Street	A.M.	0.636	B	0.642	B	0.631	B
		P.M.	0.949	E	0.951	E	0.941	E
4.	Central Avenue & 7th Street	A.M.	0.893	D	0.894	D	0.884	D
		P.M.	0.937	E	0.937	E	0.927	E
5.	Alameda Street & 2nd Street	A.M.	0.588	A	0.611	B	0.597	A
		P.M.	0.673	B	0.690	B	0.677	B
6.	Alameda Street & 3rd Street/4th Place	A.M.	1.012	F	1.059	F	1.042	F
		P.M.	0.809	D	0.852	D	0.835	D
7.	Alameda Street & 4th Street	A.M.	0.612	B	0.761	C	0.729	C
		P.M.	1.004	F	1.045	F	1.028	F
8.	Alameda Street & 6th Street	A.M.	0.871	D	0.924	E	0.906	E
		P.M.	1.265	F	1.285	F	1.272	F
9.	Alameda Street & 7th Street	A.M.	0.961	E	0.973	E	0.961	E
		P.M.	1.071	F	1.079	F	1.068	F
10.	Alameda Street & Olympic Boulevard	A.M.	0.905	E	0.931	E	0.917	E
		P.M.	0.955	E	0.961	E	0.950	E
11.	Alameda Street & I-10 Eastbound Ramps	A.M.	0.757	C	0.782	C	0.769	C
		P.M.	0.804	D	0.821	D	0.809	D
12.	Vignes Street & 1st Street	A.M.	0.471	A	0.471	A	0.461	A
		P.M.	0.682	B	0.682	B	0.672	B
13.	Merrick Street/Molino Street & 4th Street	A.M.	0.892	D	0.913	E	0.900	D
		P.M.	0.754	C	0.763	C	0.751	C
14.	Mateo Street & 6th Street	A.M.	0.563	A	0.571	A	0.559	A
		P.M.	0.517	A	0.533	A	0.521	A
15.	Mateo Street & 7th Street	A.M.	1.007	F	1.012	F	1.001	F
		P.M.	1.185	F	1.185	F	1.175	F
16.	Santa Fe Avenue & 7th Street	A.M.	0.981	E	0.987	E	0.977	E
		P.M.	1.203	F	1.203	F	1.193	F
17.	Santa Fe Avenue & 8th Street	A.M.	0.671	B	0.671	B	0.661	B
		P.M.	0.689	B	0.689	B	0.679	B
18.	US 101 Northbound Off-Ramp & 4th Street	A.M.	0.842	D	0.869	D	0.854	D
		P.M.	0.513	A	0.521	A	0.510	A
19.	Boyle Avenue & 4th Street	A.M.	0.823	D	0.834	D	0.822	D
		P.M.	0.937	E	0.941	E	0.931	E
20.	Boyle Avenue & Whittier Boulevard	A.M.	0.781	C	0.787	C	0.776	C
		P.M.	0.786	C	0.788	C	0.777	C
21.	I-5 Northbound Ramps & 4th Street	A.M.	0.908	E	0.918	E	0.906	E
		P.M.	0.956	E	0.974	E	0.961	E
22.	Soto Street & 4th Street	A.M.	0.720	C	0.726	C	0.715	C
		P.M.	0.883	D	0.890	D	0.879	D
23.	I-5 Southbound Ramps & 4th Street	A.M.	0.880	D	0.908	E	0.894	D
		P.M.	0.792	C	0.812	D	0.799	C



Source: Gensler. October, 2019.

PROJECT SITE PLAN

FIGURE 1